

Appendix

A	Traffic Counts - CTDOT
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A Traffic Counts – CTDOT

Status: OK

West Combined East

CNTN-011 - Combined - e/w

Route 565 - 1.53 mi (Dowd Ave)-West of Dunham Road

Town.....Canton
 Station.....11
 Location..... 41.824414,-72.897254
 Posted Speed Limit.....35 MPH
 2015-Major Collector 5.....2015-Urban
 Start Report.....21-Nov-2022 11:00AM
 All Vehicle Peak Hour.....22-Nov-2022 04:00PM
 End Report.....23-Nov-2022 01:00PM
 Annualized ADT.....8600
 24-Hour Count..... 8700 * G4(0.97) = 8439.0
 Day 1.....+ 9000 * G4(0.97) = 17169.0
 UnRounded AADT.....17169.0 / 2 = 8584.5
 OK 2022 Mon 21-Nov -this report-.....8600
 OK 2019 Mon 04-Nov8000
 OK 2016 Mon 05-Dec7700
 OK 2013 Mon 30-Dec7400
 OK 2007 Tue 27-Nov8300
 Dataset Details.....1

	Count	Percent	Veh. Feet
Motorcycles.....	44	0.23%	>0-8
Passenger Cars.....	18692	97.16%	>8-25
Single-Unit Trucks...	491	2.55%	>25-50
Combination Trucks...	11	0.06%	>50
Total Vehicles.....	19238		

	Single	Combo
Peak Hour Truck Volume.....	21	0
% Total Peak Hour Volume.....	2.6%	0.0%
24 Hour Truck Volume.....	235	10
All-Vehicle Annualized ADT.....	8600	8600
24Hour T-Vol % of A-V AADT.....	2.7%	0.1%
PeakHr T-Vol % of A-V AADT.....	0.2%	0.0%
K-Factor (Peak/AADT).....	9.4%	9.4%

(AADT & Legacy AADT match)

Hour	Motor Cycle	Pass Cars	Single Unit	Combo Unit	Day Total
21-Nov					
Mon					
12:00am					0
01:00am					0
02:00am					0
03:00am					0
04:00am					0
05:00am					0
06:00am					0
07:00am					0
08:00am					0
09:00am					0
10:00am	x	x	x	x	0
11:00am	.	523	13	.	536
12:00pm	1	549	15	.	565
01:00pm	2	581	17	.	600
02:00pm	3	706	24	.	733
03:00pm	5	748	18	.	771
04:00pm	1	809	16	.	826
05:00pm	1	683	6	1	691
06:00pm	.	510	2	.	512
07:00pm	.	330	1	.	331
08:00pm	1	226	.	.	227
09:00pm	.	118	.	.	118
10:00pm	.	60	1	.	61
11:00pm	.	38	.	.	38
Totals	14	5881	113	1	6009
Percent	0.23	97.87	1.88	0.02	
22-Nov					
Tue					
12:00am	.	7	.	.	7
01:00am	.	8	.	.	8
02:00am	.	9	.	.	9
03:00am	.	7	.	.	7
04:00am	1	11	3	.	15
05:00am	.	82	2	.	84
06:00am	1	235	22	2	260
07:00am	2	579	29	.	610
08:00am	1	545	15	2	563
09:00am	2	559	10	.	571
10:00am	4	527	25	1	557
11:00am	.	644	14	2	660
12:00pm	2	598	13	2	615
01:00pm	4	627	18	1	650
02:00pm	5	757	19	.	781
03:00pm	3	782	19	.	804
04:00pm	2	784	21	.	807
05:00pm	2	687	11	.	700
06:00pm	1	460	4	.	465
07:00pm	.	293	5	.	298
08:00pm	.	264	1	.	265
09:00pm	.	121	1	.	122
10:00pm	.	93	1	.	94
11:00pm	.	46	2	.	48
Totals	30	8725	235	10	9000
Percent	0.33	96.94	2.61	0.11	
23-Nov					
Wed					
12:00am	.	15	.	.	15
01:00am	.	13	.	.	13

2022 CNTN-011 - Class

02:00am	.	8	.	.	8
03:00am	.	11	1	.	12
04:00am	.	14	2	.	16
05:00am	.	75	2	.	77
06:00am	.	228	10	.	238
07:00am	.	533	25	.	558
08:00am	.	523	14	.	537
09:00am	.	592	15	.	607
10:00am	.	605	21	.	626
11:00am	.	690	24	.	714
12:00pm	.	779	29	.	808
01:00pm	x	x	x	x	0
02:00pm					0
03:00pm					0
04:00pm					0
05:00pm					0
06:00pm					0
07:00pm					0
08:00pm					0
09:00pm					0
10:00pm					0
11:00pm					0
Totals	0	4086	143	0	4229
Percent	0.00	96.62	3.38	0.00	

Status: OK West Combined East

CNTN-011 - Combined - e/w

Route 565 - 1.53 mi (Dowd Ave)-West of Dunham Road

Town.....Canton	Hour	MPH 0-15	MPH 16-20	MPH 21-25	MPH 26-30	MPH 31-35	MPH 36-40	MPH 41-45	MPH 46-50	MPH 51-55	MPH 56-60	MPH 61-65	MPH 66-70	MPH 71-75	MPH 76+	Total Vol
Station.....11																
Location..... 41.824414,-72.897254																
Posted Speed Limit.....35 MPH																
2015-Major Collector 5.....2015-Urban																
Start Report.....21-Nov-2022 11:00AM																
End Report.....23-Nov-2022 01:00PM																
Dataset Details.....1																
All Vehicles Average Speed..... 38 MPH																
Total Vehicles.....19238																
Percentile Sampling Period... 9AM to 4PM																
Vehicle Too Close.....5 secs																
Vehicle Too Slow..... < 20 MPH																
Speed Lower Limit....15 MPH below Posted																
<u>All Report Days</u>		x	x	x	x	x	x	x	x	x	x	x	x	x	x	
85th Percentile Speed.....43.1 MPH	11:00am	.	.	4	22	109	214	158	27	2	.	.	.	.	.	536
50th Percentile Speed.....38.1 MPH	12:00pm	2	2	1	18	118	249	141	28	6	.	.	.	.	.	569
10 MPH Pace (70%).....34-43 MPH	01:00pm	1	3	9	15	147	232	154	34	5	.	.	.	.	.	600
All Hours Total Vehicles.....19238	02:00pm	1	2	8	50	167	337	150	16	2	.	.	.	.	.	731
In-Period Total Vehicles.....10598	03:00pm	4	3	10	43	224	303	158	23	2	.	1	.	.	.	771
Omitted Vehicles Too Close (48%)...5060	04:00pm	1	.	1	37	270	348	154	14	1	.	.	.	.	.	826
Omitted Vehicles Too Slow (0%).....33	05:00pm	.	.	1	29	190	304	149	17	1	.	.	.	.	.	691
Sampled Vehicles (52%).....5505	06:00pm	.	.	.	6	98	229	149	24	6	.	.	.	.	.	511
	07:00pm	.	1	.	14	44	150	100	20	1	1	.	.	.	.	331
<u>Monday 21-Nov-2022</u>	08:00pm	.	1	.	6	26	100	71	18	4	1	.	.	.	.	221
85th Percentile Speed.....43.2 MPH	09:00pm	.	2	1	3	13	52	38	6	3	.	.	.	.	.	118
50th Percentile Speed.....38.0 MPH	10:00pm	.	.	1	3	8	17	21	8	2	1	.	.	.	.	61
10 MPH Pace (68%).....33-42 MPH	11:00pm	.	.	.	.	8	12	10	7	1	.	.	.	.	.	38
All Hours Total Vehicles.....6009	Totals	9	14	36	246	1422	2547	1453	242	36	3	1	0	0	0	6009
In-Period Total Vehicles.....3205	Percent	0.15	0.23	0.60	4.09	23.66	42.39	24.18	4.03	0.60	0.05	0.02	0.00	0.00	0.00	
Omitted Vehicles Too Close (48%)...1531	<u>Tuesday</u>															
Omitted Vehicles Too Slow (0%).....9	22-Nov															
Sampled Vehicles (52%).....1665	12:00am	.	.	.	.	4	1	1	1	.	.	.	.	.	.
	01:00am	.	.	.	1	2	4	.	1	.	.	.	.	.	.	8
<u>Tuesday 22-Nov-2022</u>	02:00am	.	.	.	1	.	1	3	2	1	1	.	.	.	.	9
85th Percentile Speed.....43.0 MPH	03:00am	.	.	.	.	2	2	2	2	.	1	.	.	.	.	7
50th Percentile Speed.....38.1 MPH	04:00am	.	.	.	2	3	4	4	1	1	.	.	.	.	.	11
10 MPH Pace (70%).....34-43 MPH	05:00am	.	1	.	3	7	28	29	13	2	.	1	.	.	.	81
All Hours Total Vehicles.....9000	06:00am	2	1	3	8	57	105	65	19	.	.	.	.	.	.	260
In-Period Total Vehicles.....4638	07:00am	1	2	9	24	106	260	184	22	2	.	.	.	.	.	610
Omitted Vehicles Too Close (48%)...2204	08:00am	.	.	3	9	104	228	182	33	4	.	.	.	.	.	560
Omitted Vehicles Too Slow (0%).....6	09:00am	.	.	1	19	110	257	160	22	2	.	.	.	.	.	571
Sampled Vehicles (52%).....2428	10:00am	.	1	2	16	122	258	139	17	2	.	.	.	.	.	551
	11:00am	.	2	2	30	136	294	165	28	2	1	.	.	.	.	660
	12:00pm	.	1	4	22	128	253	177	27	3	.	.	.	.	.	611
<u>Wednesday 23-Nov-2022</u>	01:00pm	.	1	.	22	166	267	166	23	5	.	.	.	.	.	650
85th Percentile Speed.....43.3 MPH	02:00pm	3	5	11	45	197	314	174	28	4	.	.	.	.	.	781
50th Percentile Speed.....38.3 MPH	03:00pm	.	.	.	4	61	219	305	176	34	3	2	.	.	.	801
10 MPH Pace (71%).....33-42 MPH	04:00pm	1	1	4	40	232	377	137	12	2	1	.	.	.	.	801
All Hours Total Vehicles.....4229	05:00pm	.	1	1	30	203	347	100	12	5	1	.	.	.	.	700
In-Period Total Vehicles.....2755	06:00pm	.	1	1	7	75	220	134	23	3	1	.	.	.	.	461
Omitted Vehicles Too Close (48%)...1325	07:00pm	.	1	.	5	39	133	101	17	2	.	.	.	.	.	298
Omitted Vehicles Too Slow (1%).....18	08:00pm	.	.	.	1	56	130	65	10	2	.	1	.	.	.	261
Sampled Vehicles (51%).....1412	09:00pm	.	.	1	2	14	42	46	14	2	.	.	.	.	1	121
	10:00pm	.	.	1	.	7	30	32	21	3	.	.	.	.	.	91
	11:00pm	.	.	.	.	8	20	13	5	1	.	.	1	.	.	48
	Totals	7	18	47	348	1991	3883	2255	387	52	8	2	1	0	1	9000
	Percent	0.08	0.20	0.52	3.87	22.12	43.14	25.06	4.30	0.58	0.09	0.02	0.01	0.00	0.01	
<u>Wednesday</u>																
23-Nov	12:00am	.	.	.	.	1	8	4	1	1	.	.	.	.	.	11
	01:00am	.	.	.	1	2	3	4	2	1	.	.	.	.	.	11
	02:00am	.	.	.	.	3	1	1	2	1	.	.	.	.	.	8
	03:00am	.	.	.	.	7	3	1	1	.	1	.	.	.	.	11
	04:00am	.	.	1	1	1	2	10	1	.	.	.	.	.	.	11
	05:00am	.	.	.	3	11	25	19	16	2	.	1	.	.	.	71
	06:00am	.	.	3	9	36	78	88	20	3	1	.	.	.	.	238
	07:00am	4	3	4	18	82	259	154	30	4	.	.	.	.	.	558
	08:00am	.	.	19	84	213	174	43	4	.	.	.	.	.	.	531
	09:00am	1	.	5	24	123	268	150	33	1	1	.	.	.	1	601
	10:00am	.	.	2	18	106	283	194	21	2	.	.	.	.	.	621
	11:00am	.	.	.	16	125	347	191	28	6	1	.	.	.	.	711
	12:00pm	44	6	18	61	191	298	160	26	4	.	.	.	.	.	801
	01:00pm	x	x	x	x	x	x	x	x	x	x	x	x	x	x	
	02:00pm															
	03:00pm															
	04:00pm															
	05:00pm															
	06:00pm															
	07:00pm															
	08:00pm															
	09:00pm															
	10:00pm															
	11:00pm															
	Totals	49	9	33	170	765	1792	1152	224	29	4	1	0	0	1	4229
	Percent	1.16	0.21	0.78	4.02	18.09	42.37	27.24	5.30	0.69	0.09	0.02	0.00	0.00	0.02	

Status: OK

CNTN-011 - Combined - e/w

Route 565 - 1.53 mi (Dowd Ave)-West of Dunham Road

	21-Nov	22-Nov	23-Nov
	Mon	Tue	Wed
Town.....Canton			
Station.....11			
Location..... 41.824414,-72.897254	12:00am	7	15
Posted Speed Limit.....35 MPH	01:00am	8	13
2015-Major Collector 5.....2015-Urban	02:00am	9	8
Start Report.....21-Nov-2022 11:00AM	03:00am	7	12
End Report.....23-Nov-2022 01:00PM	04:00am	15	16
Annualized ADT.....8600	05:00am	84	77
24-Hour Count... 8700 * G4(0.97) = 8439.0	06:00am	260	238
Day 1.....+ 9000 * G4(0.97) = 17169.0	07:00am	610	558
UnRounded AADT.....17169.0 / 2 = 8584.5	08:00am	563	537
OK 2022 Mon 21-Nov -this report-...8600	09:00am	571	607
OK 2019 Mon 04-Nov .....8000	10:00am	x	626
OK 2016 Mon 05-Dec .....7700	11:00am	536	660
OK 2013 Mon 30-Dec .....7400	12:00pm	565	615
OK 2007 Tue 27-Nov .....8300	01:00pm	600	650
Dataset Details.....1	02:00pm	733	781
	03:00pm	771	804
	04:00pm	826	807
	05:00pm	691	700
	06:00pm	512	465
	07:00pm	331	298
	08:00pm	227	265
	09:00pm	118	122
	10:00pm	61	94
	11:00pm	38	48
Totals	6009	9000	4229

Status: OK [East] [Combined] [West]

CNTN-013 - Combined - e/w

Route 44 - 41.05 mi West of Route 177

Internal: Probable occlusion

Hour	MPH 0-15	MPH 16-20	MPH 21-25	MPH 26-30	MPH 31-35	MPH 36-40	MPH 41-45	MPH 46-50	MPH 51-55	MPH 56-60	MPH 61-65	MPH 66-70	MPH 71-75	MPH 76+
Town.....Canton														
Station.....13														
Location.....41.823783,-72.888449														
Posted Speed Limit.....40 MPH														
2015-Principal Arterial - Other 3...2015-Urban														
Start Report.....21-Nov-2022 07:00PM														
End Report.....23-Nov-2022 12:00PM														
Dataset Details.....2														
All Vehicles Average Speed..... 23 MPH														
Total Vehicles.....38822														
Percentile Sampling Period..... 9AM to 4PM														
Vehicle Too Close.....5 secs														
Vehicle Too Slow..... < 25 MPH														
Speed Lower Limit.....15 MPH below Posted														
Monday 21-Nov														
12:00am														
01:00am														
02:00am														
03:00am														
04:00am														
05:00am														
06:00am														
07:00am														
08:00am														
09:00am														
10:00am	x	x	x	x	x	x	x	x	x	x	x	x	x	x
11:00am	x	x	x	x	x	x	x	x	x	x	x	x	x	x
12:00pm	x	x	x	x	x	x	x	x	x	x	x	x	x	x
01:00pm	x	x	x	x	x	x	x	x	x	x	x	x	x	x
02:00pm	x	x	x	x	x	x	x	x	x	x	x	x	x	x
03:00pm	x	x	x	x	x	x	x	x	x	x	x	x	x	x
04:00pm	x	x	x	x	x	x	x	x	x	x	x	x	x	x
05:00pm	x	x	x	x	x	x	x	x	x	x	x	x	x	x
06:00pm	x	x	x	x	x	x	x	x	x	x	x	x	x	x
07:00pm	19	252	285	181	115	80	20	2	2	1	1	1	1	1
08:00pm	9	95	172	160	114	33	13	4	1	1	1	1	1	1
09:00pm	6	74	103	96	69	49	18	1	2	1	1	1	1	1
10:00pm	3	27	41	57	57	28	10	2	2	2	1	1	1	1
11:00pm	1	9	29	24	20	12	10	4	1	1	1	1	1	1
Totals	37	457	630	518	375	202	71	13	6	3	2	0	0	0
Percent	1.60	19.74	27.21	22.38	16.20	8.73	3.07	0.56	0.26	0.13	0.09	0.00	0.00	0.00
Tuesday 22-Nov														
12:00am	2	3	10	12	7	8	5	2	1	1	1	1	1	1
01:00am	5	1	10	6	2	6	4	1	1	1	1	1	1	1
02:00am	2	3	6	7	5	6	3	3	1	1	1	1	1	1
03:00am	1	1	4	4	12	10	6	5	3	1	1	1	1	1
04:00am	4	8	10	14	18	15	12	8	1	1	1	1	1	1
05:00am	9	39	37	76	104	86	38	22	6	1	1	1	1	1
06:00am	60	150	196	207	175	106	70	13	3	1	1	1	1	1
07:00am	206	409	393	305	162	60	16	3	1	1	1	1	1	1
08:00am	192	502	445	317	196	63	20	2	1	1	1	1	1	1
09:00am	209	583	388	299	162	58	13	3	1	1	1	1	1	1
10:00am	178	563	471	254	156	59	16	1	1	1	1	1	1	1
11:00am	243	625	498	249	124	31	9	1	1	1	1	1	1	1
12:00pm	214	685	472	278	158	46	13	1	1	1	1	1	1	1
01:00pm	235	698	538	265	124	59	20	1	1	1	1	1	1	1
02:00pm	254	813	469	233	147	40	6	1	1	1	1	1	1	1
03:00pm	310	877	534	242	117	29	7	2	1	1	1	1	1	1
04:00pm	289	1056	472	193	149	42	6	1	1	1	1	1	1	1
05:00pm	210	1046	450	203	106	37	8	1	1	1	1	1	1	1
06:00pm	119	493	430	252	154	73	20	4	1	1	1	1	1	1
07:00pm	27	218	294	221	133	66	21	6	1	1	1	1	1	1
08:00pm	13	123	241	190	120	52	21	2	1	2	1	1	1	1
09:00pm	7	88	124	122	81	46	19	4	1	1	1	1	1	1
10:00pm	6	51	77	83	40	40	17	5	2	1	1	1	1	1
11:00pm	2	23	32	43	31	16	7	2	1	1	1	1	1	1
Totals	2797	9058	6601	4075	2483	1054	377	90	19	4	1	1	0	0
Percent	10.53	34.10	24.85	15.34	9.35	3.97	1.42	0.34	0.07	0.02	0.00	0.00	0.00	0.00
Wednesday 23-Nov														
12:00am	1	7	15	19	21	8	3	2	1	1	1	1	1	1
01:00am	1	4	9	8	8	4	4	3	1	1	1	1	1	1
02:00am	1	2	3	8	1	9	3	1	1	1	1	1	1	1
03:00am	1	4	5	5	8	10	5	5	2	1	1	1	1	1
04:00am	1	5	4	17	16	24	16	7	3	2	1	1	1	1
05:00am	9	30	42	64	92	74	61	21	3	1	1	1	1	1
06:00am	24	121	151	146	166	144	73	24	1	3	1	1	1	1
07:00am	177	423	362	272	187	60	24	5	1	1	1	1	1	1
08:00am	161	432	421	333	169	69	17	6	1	1	1	1	1	1
09:00am	187	507	467	265	158	37	14	3	1	1	1	1	1	1
10:00am	249	666	449	241	140	48	8	1	1	1	1	1	1	1
11:00am	292	724	445	231	105	39	13	1	1	1	1	1	1	1
12:00pm														
01:00pm														
02:00pm														
03:00pm														
04:00pm														
05:00pm														
06:00pm														
07:00pm														
08:00pm														
09:00pm														
10:00pm														
11:00pm														
Totals	1104	2925	2373	1609	1071	526	241	77	12	7	1	1	0	0
Percent	11.10	29.41	23.86	16.18	10.77	5.29	2.42	0.77	0.12	0.07	0.01	0.01	0.00	0.00

Status: OK East Combined West Speed

CNTN-013 - Combined - e/w

Route 44 - 41.05 mi West of Route 177

Internal: Probable occlusion

	21-Nov Mon	22-Nov Tue	23-Nov Wed
Town.....Canton	12:00am	49	77
Station.....13	01:00am	34	42
Location..... 41.823783,-72.888449	02:00am	35	28
Posted Speed Limit.....40 MPH	03:00am	46	46
2015-Principal Arterial - Other 3...2015-Urban	04:00am	92	95
Start Report.....21-Nov-2022 07:00PM	05:00am	417	398
End Report.....23-Nov-2022 12:00PM	06:00am	980	853
Annualized ADT.....25600	07:00am	1554	1510
24-Hour Count.....26154 * G4(0.97) = 25369.4	08:00am	1738	1609
Day 1.....+26560 * G4(0.97) = 51132.6	09:00am	1715	1638
UnRounded AADT.....51132.6 / 2 = 25566.3	10:00am	x	1697
OK 2022 Mon 21-Nov -this report-.....25600	11:00am	x	1780
OK 2020 Mon 19-Oct .....23000	12:00pm	x	1867
OK 2019 Mon 04-Nov .....28100	01:00pm	x	1940
OK 2016 Mon 19-Dec .....31800	02:00pm	x	1964
OK 2013 Wed 16-Jan .....31400	03:00pm	x	2118
OK 2007 Thu 06-Dec .....32600	04:00pm	x	2207
Dataset Details.....2	05:00pm	x	2060
	06:00pm	x	1546
	07:00pm	957	987
	08:00pm	600	764
	09:00pm	419	492
	10:00pm	230	321
	11:00pm	109	157
Totals	2315	26560	9947

Status: OK

North

Combined

South

CNTN-014 - Combined - n/s

Route 177 - 12.55 mi SW of US 44

Town.....Canton
 Station.....14
 Location..... 41.82342,-72.887935
 Posted Speed Limit.....40 MPH
 2015-Minor Arterial 4.....2015-Urban
 Start Report.....21-Nov-2022 09:00AM
 All Vehicle Peak Hour.....22-Nov-2022 04:00PM
 End Report.....23-Nov-2022 10:00AM
 Annualized ADT.....8400
 24-Hour Count..... 8480 * G4(0.97) = 8225.6
 Day 1.....+ 8896 * G4(0.97) = 16854.7
 Unrounded AADT.....16854.7 / 2 = 8427.4
 OK 2022 Mon 21-Nov -this report-.....8400
 OK 2019 Mon 04-Nov8100
 OK 2016 Wed 14-Dec8200
 OK 2013 Wed 16-Jan8700
 OK 2007 Tue 04-Dec8900
 Dataset Details.....1

	Count	Percent	Veh. Feet
Motorcycles.....	71	0.40%	>0-8
Passenger Cars.....	17263	96.96%	>8-25
Single-Unit Trucks...	431	2.42%	>25-50
Combination Trucks...	40	0.22%	>50
Total Vehicles.....	17805		

	Single	Combo
Peak Hour Truck Volume.....	17	0
% Total Peak Hour Volume.....	2.2%	0.0%
24 Hour Truck Volume.....	192	16
All-Vehicle Annualized ADT.....	8400	8400
24Hour T-Vol % of A-V AADT.....	2.3%	0.2%
PeakHr T-Vol % of A-V AADT.....	0.2%	0.0%
K-Factor (Peak/AADT).....	9.3%	9.3%
(AADT & Legacy AADT match)		

Hour	Motor Cycle	Pass Cars	Single Unit	Combo Unit	Day Total
21-Nov					
Mon					
12:00am					0
01:00am					0
02:00am					0
03:00am					0
04:00am					0
05:00am					0
06:00am					0
07:00am					0
08:00am	x	x	x	x	0
09:00am	3	464	24	2	493
10:00am	1	519	20	.	540
11:00am	3	522	26	.	551
12:00pm	2	579	15	3	599
01:00pm	1	571	19	1	592
02:00pm	3	646	20	1	670
03:00pm	2	684	20	1	707
04:00pm	6	777	14	.	797
05:00pm	3	698	5	1	707
06:00pm	1	457	3	.	461
07:00pm	1	332	2	1	336
08:00pm	1	230	.	.	231
09:00pm	.	131	.	1	132
10:00pm	.	61	3	.	64
11:00pm	.	32	.	.	32
Totals	27	6703	171	11	6912
Percent	0.39	96.98	2.47	0.16	
22-Nov					
Tue					
12:00am	.	17	.	.	17
01:00am	.	7	.	1	8
02:00am	.	6	.	.	6
03:00am	.	11	.	1	12
04:00am	.	21	1	.	22
05:00am	.	90	4	.	94
06:00am	.	256	8	1	265
07:00am	1	470	14	2	487
08:00am	3	640	12	2	657
09:00am	3	550	14	1	568
10:00am	1	559	22	.	582
11:00am	4	595	15	3	617
12:00pm	3	597	21	1	622
01:00pm	3	648	20	1	672
02:00pm	4	673	12	1	690
03:00pm	4	750	20	1	775
04:00pm	4	758	17	.	779
05:00pm	3	691	6	.	700
06:00pm	.	502	3	.	505
07:00pm	.	330	1	.	331
08:00pm	3	216	.	.	219
09:00pm	.	136	1	.	137
10:00pm	.	89	1	1	91
11:00pm	.	40	.	.	40
Totals	36	8652	192	16	8896
Percent	0.40	97.26	2.16	0.18	

2022 CNTN-014 - Class

23-Nov					
Wed					
12:00am	.	20	.	.	20
01:00am	.	9	.	1	10
02:00am	.	5	.	1	6
03:00am	.	8	3	.	11
04:00am	.	17	1	.	18
05:00am	.	76	4	.	80
06:00am	.	224	9	3	236
07:00am	5	453	18	2	478
08:00am	.	517	17	5	539
09:00am	3	579	16	1	599
10:00am	x	x	x	x	0
11:00am					0
12:00pm					0
01:00pm					0
02:00pm					0
03:00pm					0
04:00pm					0
05:00pm					0
06:00pm					0
07:00pm					0
08:00pm					0
09:00pm					0
10:00pm					0
11:00pm					0
Totals	8	1908	68	13	1997
Percent	0.40	95.54	3.41	0.65	

Status: OK

North

Combined

South

CNTN-014 - Combined - n/s

Route 177 - 12.55 mi SW of US 44

Town.....Canton	Hour	MPH 0-15	MPH 16-20	MPH 21-25	MPH 26-30	MPH 31-35	MPH 36-40	MPH 41-45	MPH 46-50	MPH 51-55	MPH 56-60	MPH 61-65	MPH 66-70	MPH 71-75	MPH 76+	Total Vol	
Station.....14																	
Location..... 41.82342,-72.887935																	
Posted Speed Limit.....40 MPH																	
2015-Minor Arterial 4.....2015-Urban																	
Start Report.....21-Nov-2022 09:00AM																	
End Report.....23-Nov-2022 10:00AM																	
Dataset Details.....1																	
Monday																	
21-Nov																	
12:00am																	
01:00am																	
02:00am																	
03:00am																	
04:00am																	
All Vehicles Average Speed..... 34 MPH																	
Total Vehicles.....17805																	
Percentile Sampling Period... 9AM to 4PM																	
05:00am																	
06:00am																	
Vehicle Too Close.....5 secs																	
07:00am																	
Vehicle Too Slow..... < 25 MPH																	
08:00am		x	x	x	x	x	x	x	x	x	x	x	x	x	x		
Speed Lower Limit....15 MPH below Posted																	
09:00am		1	1	13	55	184	174	57	8							490	
10:00am		4	3	12	68	205	189	53	6							540	
11:00am		.	.	5	52	220	231	40	3							550	
85th Percentile Speed.....39.8 MPH		4	1	11	71	257	207	47	1							590	
50th Percentile Speed.....35.3 MPH		1	4	9	80	209	215	67	6	1						590	
10 MPH Pace (75%).....31-40 MPH		6	6	30	115	275	196	38	3	1						670	
All Hours Total Vehicles.....17805		6	4	9	101	318	211	53	5							700	
In-Period Total Vehicles.....9277		1	4	17	141	385	205	39	5							790	
Omitted Vehicles Too Close (53%)...4872		3	7	26	138	346	153	32	1	1						700	
Omitted Vehicles Too Slow (1%).....128		1	2	14	59	204	147	31	2	1						460	
Sampled Vehicles (46%).....4277		.	2	11	39	150	102	21	8	3						330	
Monday 21-Nov-2022																	
12:00pm		.	.	2	15	91	96	26	1							230	
09:00pm		.	.	.	9	40	54	26	2	1						130	
85th Percentile Speed.....39.7 MPH		.	.	1	6	19	28	8	1	1						60	
50th Percentile Speed.....35.3 MPH		.	.	.	2	11	7	10	1	1						30	
10 MPH Pace (74%).....31-40 MPH		.	.	.	2	11	7	10	1	1						30	
All Hours Total Vehicles.....6912	Totals	27	34	160	951	2914	2215	548	53	10	0	0	0	0	0	6912	
In-Period Total Vehicles.....4152	Percent	0.39	0.49	2.31	13.76	42.16	32.05	7.93	0.77	0.14	0.00	0.00	0.00	0.00	0.00		
Omitted Vehicles Too Close (51%)...2120	Tuesday																
04:00pm		.	.	.	2	2	9	2	2	.	.	.	.	.	.	10
05:00pm		.	.	.	1	2	2	2	1	.	.	.	.	.	.	8
06:00pm		.	.	.	.	2	2	1	1	.	.	.	.	.	.	6
07:00pm		.	.	.	.	3	5	3	1	.	.	.	.	.	.	12
08:00pm		.	.	.	1	7	8	4	2	.	.	.	.	.	.	22
09:00pm		.	.	.	7	28	25	29	5	.	.	.	.	.	.	90
10:00pm		.	.	.	24	62	100	64	8	2	.	.	.	.	.	260
All Hours Total Vehicles.....8896		4	1	5	44	131	208	84	10							480	
In-Period Total Vehicles.....4526		.	.	.	8	75	245	244	80	5						650	
Omitted Vehicles Too Close (54%)...2458		5	1	9	54	219	217	56	7	.	.	.	.	.	.	560
Omitted Vehicles Too Slow (1%).....62		2	4	11	82	236	187	54	5	1	.	.	.	.	.	580
Sampled Vehicles (44%).....2006		3	2	20	73	253	213	52	1	.	.	.	.	.	.	610
Tuesday 22-Nov-2022																	
12:00am		2	1	19	86	243	201	64	4	1	1	.	.	.	.	620
01:00am		.	2	21	105	237	226	78	3	.	.	.	.	.	.	670
02:00am		3	4	11	106	328	199	35	4	.	.	.	.	.	.	690
85th Percentile Speed.....39.7 MPH		9	23	34	136	296	221	48	5	2	.	.	1	.	.	770	
50th Percentile Speed.....35.2 MPH		5	7	25	127	354	225	34	2	.	.	.	.	.	.	770
10 MPH Pace (75%).....31-40 MPH		.	.	.	7	28	25	29	5	.	.	.	.	.	.	90
All Hours Total Vehicles.....1997		.	4	23	139	356	152	24	1	1	.	.	.	.	.	700
In-Period Total Vehicles.....599		.	2	5	90	213	168	23	4	.	.	.	.	.	.	500
Omitted Vehicles Too Close (49%)...294		.	1	3	51	134	108	32	.	2	.	.	.	.	.	330
Omitted Vehicles Too Slow (2%).....9		.	.	1	9	92	94	19	3	1	.	.	.	.	.	210
Sampled Vehicles (49%).....296		.	.	3	12	56	40	18	5	3	.	.	.	.	.	130
10:00pm		.	.	.	8	33	33	15	2	.	.	.	.	.	.	90
11:00pm		.	.	.	3	10	17	9	1	.	.	.	.	.	.	40
Totals		33	54	201	1235	3542	2904	830	82	13	1	1	0	0	0	8896	
Percent		0.37	0.61	2.26	13.88	39.82	32.64	9.33	0.92	0.15	0.01	0.01	0.00	0.00	0.00		
Wednesday																	
23-Nov																	
12:00am		.	.	.	2	5	6	7	.	.	.	.	.	.	.	20
01:00am		.	.	.	.	4	2	4	.	.	.	.	.	.	.	10
02:00am		.	.	.	1	1	4	.	.	.	.	.	.	.	.	6
03:00am		.	.	.	.	3	3	4	1	.	.	.	.	.	.	11
04:00am		.	.	.	4	1	9	3	.	1	.	.	.	.	.	18
05:00am		.	.	1	6	18	28	17	7	2	1	.	.	.	.	80
06:00am		.	.	8	13	67	95	46	4	3	.	.	.	.	.	230
07:00am		3	5	17	37	131	193	81	10	1	.	.	.	.	.	470
08:00am		.	2	15	61	190	188	77	6	.	.	.	.	.	.	530
09:00am		.	3	20	68	209	225	66	7	.	1	.	.	.	.	590
10:00am		x	x	x	x	x	x	x	x	x	x	x	x	x	x		
11:00am																	
12:00pm																	
01:00pm																	
02:00pm																	
03:00pm																	
04:00pm																	
05:00pm																	
06:00pm																	
07:00pm																	
08:00pm																	
09:00pm																	
10:00pm																	
11:00pm																	
Totals		3	10	61	192	629	753	305	35	7	2	0	0	0	0	1990	
Percent		0.15	0.50	3.05	9.61	31.50	37.71	15.27	1.75	0.35	0.10	0.00	0.00	0.00	0.00		

Status: OK North Combined South Class Speed

CNTN-014 - Combined - n/s

Route 177 - 12.55 mi SW of US 44

Town.....Canton	21-Nov	22-Nov	23-Nov
Station.....14	Mon	Tue	Wed
Location..... 41.82342,-72.887935	12:00am	17	20
Posted Speed Limit.....40 MPH	01:00am	8	10
2015-Minor Arterial 4.....2015-Urban	02:00am	6	6
Start Report.....21-Nov-2022 09:00AM	03:00am	12	11
End Report.....23-Nov-2022 10:00AM	04:00am	22	18
Annualized ADT.....8400	05:00am	94	80
24-Hour Count... 8480 * G4(0.97) = 8225.6	06:00am	265	236
Day 1.....+ 8896 * G4(0.97) = 16854.7	07:00am	487	478
UnRounded AADT.....16854.7 / 2 = 8427.4	08:00am	x	657
OK 2022 Mon 21-Nov -this report-...8400	09:00am	493	568
OK 2019 Mon 04-Nov .....8100	10:00am	540	582
OK 2016 Wed 14-Dec .....8200	11:00am	551	617
OK 2013 Wed 16-Jan .....8700	12:00pm	599	622
OK 2007 Tue 04-Dec .....8900	01:00pm	592	672
Dataset Details.....1	02:00pm	670	690
	03:00pm	707	775
	04:00pm	797	779
	05:00pm	707	700
	06:00pm	461	505
	07:00pm	336	331
	08:00pm	231	219
	09:00pm	132	137
	10:00pm	64	91
	11:00pm	32	40
Totals	6912	8896	1997

Status: OK [East] [Combined] [West]

CNTN-015 - Combined - e/w

Route 44 - 41.09 mi East of Route 177

Internal: Probable occlusion

Hour	MPH 0-15	MPH 16-20	MPH 21-25	MPH 26-30	MPH 31-35	MPH 36-40	MPH 41-45	MPH 46-50	MPH 51-55	MPH 56-60	MPH 61-65	MPH 66-70	MPH 71-75	MPH 76+
Town.....Canton														
Station.....15														
Location.....41.824137,-72.886603														
Posted Speed Limit.....40 MPH														
2015-Principal Arterial - Other 3...2015-Urban														
Start Report.....22-Nov-2022 12:00AM														
End Report.....23-Nov-2022 11:00AM														
Dataset Details.....2														
All Vehicles Average Speed.....33 MPH														
Total Vehicles.....31271														
Percentile Sampling Period.....9AM to 4PM														
Vehicle Too Close.....5 secs														
Vehicle Too Slow.....< 25 MPH														
Speed Lower Limit.....15 MPH below Posted														
<u>All Report Days</u>														
85th Percentile Speed.....42.0 MPH														
50th Percentile Speed.....35.2 MPH														
10 MPH Pace (55%).....31-40 MPH														
All Hours Total Vehicles.....31271														
In-Period Total Vehicles.....15096														
Omitted Vehicles Too Close (82%).....12444														
Omitted Vehicles Too Slow (4%).....561														
Sampled Vehicles (14%).....2091														
<u>Monday 21-Nov-2022</u>														
85th Percentile Speed.....N/A														
50th Percentile Speed.....N/A														
10 MPH Pace ().....N/A														
All Hours Total Vehicles.....0														
In-Period Total Vehicles.....0														
Omitted Vehicles Too Close (0%).....0														
Omitted Vehicles Too Slow (0%).....0														
Sampled Vehicles ().....0														
<u>Tuesday 22-Nov-2022</u>														
85th Percentile Speed.....41.9 MPH														
50th Percentile Speed.....35.2 MPH														
10 MPH Pace (55%).....31-40 MPH														
All Hours Total Vehicles.....23806														
In-Period Total Vehicles.....11767														
Omitted Vehicles Too Close (83%).....9722														
Omitted Vehicles Too Slow (4%).....462														
Sampled Vehicles (13%).....1583														
<u>Wednesday 23-Nov-2022</u>														
85th Percentile Speed.....42.1 MPH														
50th Percentile Speed.....34.9 MPH														
10 MPH Pace (54%).....30-39 MPH														
All Hours Total Vehicles.....7465														
In-Period Total Vehicles.....3329														
Omitted Vehicles Too Close (82%).....2722														
Omitted Vehicles Too Slow (3%).....99														
Sampled Vehicles (15%).....508														
Totals	1106	1440	2721	4165	4871	4857	2984	1071	411	133	35	10	1	
Percent	4.65	6.05	11.43	17.50	20.46	20.40	12.53	4.50	1.73	0.56	0.15	0.04	0.00	0.00
<u>Wednesday 23-Nov</u>														
12:00am	.	.	.	3	8	19	28	7	6	2	2	.	.	.
01:00am	.	.	.	1	7	8	7	5	6	5	1	.	1	.
02:00am	.	.	.	1	2	3	8	8	3	1	1	.	.	.
03:00am	.	.	.	2	3	7	8	11	6	3	3	.	.	.
04:00am	.	.	.	3	3	13	18	23	16	9	3	4	1	.
05:00am	.	.	3	3	16	49	88	83	74	29	8	5	1	.
06:00am	.	.	8	26	75	127	211	161	91	33	8	5	.	.
07:00am	4	3	17	53	227	427	394	139	48	6	2	.	.	.
08:00am	11	14	55	166	308	453	326	70	23	4	1	.	.	.
09:00am	27	47	121	258	441	411	206	46	11	.	.	.	.	.
10:00am	71	92	242	370	405	397	149	28	4	1	2	.	.	.
11:00am	x	x	x	x	x	x	x	x	x	x	x	x	x	.
12:00pm														
01:00pm														
02:00pm														
03:00pm														
04:00pm														
05:00pm														
06:00pm														
07:00pm														
08:00pm														
09:00pm														
10:00pm														
11:00pm														
Totals	113	156	446	886	1495	1914	1443	581	288	93	31	14	3	.
Percent	1.51	2.09	5.97	11.87	20.03	25.64	19.33	7.78	3.86	1.25	0.42	0.19	0.04	0.00

Status: OK East Combined West Speed

CNTN-015 - Combined - e/w

Route 44 - 41.09 mi East of Route 177

		21-Nov	22-Nov	23-Nov
		Mon	Tue	Wed
Internal: Probable occlusion				
Town.....	Canton		47	76
Station.....	15		29	41
Location.....	41.824137,-72.886603		33	27
Posted Speed Limit.....	40 MPH		45	44
2015-Principal Arterial - Other	3...2015-Urban		88	93
Start Report.....	22-Nov-2022 12:00AM		385	359
End Report.....	23-Nov-2022 11:00AM		872	745
Annualized ADT.....	23100		1349	1320
24-Hour Count.....	23806 * G4(0.97) = 23091.8		1580	1431
UnRounded AADT.....	23091.8 / 1 = 23091.8		1546	1568
OK 2022 Mon 21-Nov -this report-	23100	x	1643	1761
OK 2020 Mon 19-Oct	20300	x	1756	x
OK 2019 Mon 04-Nov	26800	x	1815	
OK 2016 Mon 19-Dec	31300	x	1643	
OK 2007 Thu 06-Dec	31800	x	1746	
Dataset Details.....	2	x	1618	
		x	1586	
		x	1646	
		x	1530	
		x	1059	
		x	805	
		x	509	
		x	335	
		x	141	
	Totals	0	23806	7465

Status: OK [East] [Combined] [West]

CNTN-028 - Combined - e/w

Route 44 - 40.54 mi NW of Dunham Road

Internal: Probable occlusion

	MPH	MPH	MPH	MPH	MPH	MPH	MPH	MPH	MPH	MPH	MPH	MPH	MPH	MPH	MPH
Hour	0-15	16-20	21-25	26-30	31-35	36-40	41-45	46-50	51-55	56-60	61-65	66-70	71-75	76-	
Town.....Canton															
Station.....28															
Location.....41.825975,-72.897004															
Posted Speed Limit.....40 MPH															
A.K.A.....2028															
2015-Principal Arterial - Other 3...2015-Urban															
Start Report.....21-Nov-2022 10:00AM															
End Report.....23-Nov-2022 12:00PM															
Dataset Details.....2															
All Vehicles Average Speed.....38 MPH															
Total Vehicles.....40859															
Percentile Sampling Period.....9AM to 4PM															
Vehicle Too Close.....5 secs															
Vehicle Too Slow.....< 25 MPH															
Speed Lower Limit.....15 MPH below Posted															
<u>All Report Days</u>															
85th Percentile Speed.....51.4 MPH															
50th Percentile Speed.....36.2 MPH															
10 MPH Pace (46%).....28-37 MPH															
All Hours Total Vehicles.....40859															
In-Period Total Vehicles.....21974															
Omitted Vehicles Too Close (73%).....16087															
Omitted Vehicles Too Slow (3%).....646															
Sampled Vehicles (24%).....5241															
<u>Monday 21-Nov-2022</u>															
85th Percentile Speed.....51.7 MPH															
50th Percentile Speed.....36.0 MPH															
10 MPH Pace (47%).....28-37 MPH															
All Hours Total Vehicles.....14052															
In-Period Total Vehicles.....8257															
Omitted Vehicles Too Close (73%).....6013															
Omitted Vehicles Too Slow (3%).....237															
Sampled Vehicles (24%).....2007															
<u>Tuesday 22-Nov-2022</u>															
85th Percentile Speed.....51.2 MPH															
50th Percentile Speed.....36.2 MPH															
10 MPH Pace (46%).....28-37 MPH															
All Hours Total Vehicles.....19453															
In-Period Total Vehicles.....10003															
Omitted Vehicles Too Close (74%).....7401															
Omitted Vehicles Too Slow (3%).....302															
Sampled Vehicles (23%).....2300															
<u>Wednesday 23-Nov-2022</u>															
85th Percentile Speed.....51.6 MPH															
50th Percentile Speed.....36.6 MPH															
10 MPH Pace (47%).....28-37 MPH															
All Hours Total Vehicles.....7354															
In-Period Total Vehicles.....3714															
Omitted Vehicles Too Close (72%).....2673															
Omitted Vehicles Too Slow (3%).....107															
Sampled Vehicles (25%).....934															
Totals	434	470	1131	3172	4149	3234	2154	1644	1598	894	370	149	40	14	
Percent	2.23	2.42	5.81	16.31	21.33	16.62	11.07	8.45	8.21	4.60	1.90	0.77	0.21	0.07	
<u>Wednesday 23-Nov</u>															
12:00am	.	.	1	2	7	9	4	7	11	12	3	3	.	.	
01:00am	1	.	1	1	3	5	1	6	6	2	1	1	1	1	
02:00am	.	.	.	.	2	6	4	2	4	4	2	.	.	.	
03:00am	.	2	1	.	4	7	6	6	1	4	.	3	.	.	
04:00am	.	.	.	1	11	17	23	5	9	4	2	3	1	.	
05:00am	7	3	1	11	32	79	99	33	29	20	4	4	2	.	
06:00am	11	10	5	32	83	191	169	58	36	34	19	15	4	.	
07:00am	28	23	50	163	286	254	145	47	63	62	28	12	2	.	
08:00am	24	26	53	169	319	292	119	73	71	73	31	7	2	.	
09:00am	19	16	68	234	287	235	134	99	80	46	17	6	.	.	
10:00am	11	27	70	140	207	163	118	130	88	43	10	2	1	.	
11:00am	32	33	80	254	347	255	164	119	107	44	19	8	.	.	
12:00pm	x	x	x	x	x	x	x	x	x	x	x	x	x	x	
01:00pm															
02:00pm															
03:00pm															
04:00pm															
05:00pm															
06:00pm															
07:00pm															
08:00pm															
09:00pm															
10:00pm															
11:00pm															
Totals	133	140	330	1007	1588	1513	986	585	505	348	136	64	13	6	
Percent	1.81	1.90	4.49	13.69	21.59	20.57	13.41	7.95	6.87	4.73	1.85	0.87	0.18	0.08	

Status: OK East Combined West Speed

CNTN-028 - Combined - e/w

Route 44 - 40.54 mi NW of Dunham Road

Internal: Probable occlusion		21-Nov Mon	22-Nov Tue	23-Nov Wed
Town.....Canton	12:00am		38	59
Station.....28	01:00am		25	31
Location..... 41.825975,-72.897004	02:00am		26	24
Posted Speed Limit.....40 MPH	03:00am		40	35
A.K.A.....2028	04:00am		73	76
2015-Principal Arterial - Other 3...2015-Urban	05:00am		353	325
Start Report.....21-Nov-2022 10:00AM	06:00am		796	667
End Report.....23-Nov-2022 12:00PM	07:00am		1304	1164
Annualized ADT.....18800	08:00am		1328	1259
24-Hour Count.....19313 * G4(0.97) = 18733.6	09:00am	x	1278	1242
Day 1.....+19453 * G4(0.97) = 37603.0	10:00am	1215	1320	1010
UnRounded AADT.....37603.0 / 2 = 18801.5	11:00am	1249	1427	1462
OK 2022 Mon 21-Nov -this report-.....18800	12:00pm	1390	1482	x
OK 2020 Mon 19-Oct .....17000	01:00pm	1419	1290	
OK 2019 Mon 04-Nov .....20200	02:00pm	1428	1535	
OK 2016 Mon 19-Dec .....24000	03:00pm	1556	1671	
OK 2013 Wed 16-Jan .....23900	04:00pm	1701	1465	
OK 2007 Tue 04-Dec .....23900	05:00pm	1476	1138	
Dataset Details.....2	06:00pm	997	930	
	07:00pm	655	710	
	08:00pm	409	507	
	09:00pm	321	378	
	10:00pm	166	228	
	11:00pm	70	111	
	Totals	14052	19453	7354

Status: OK

North

Combined

South

CNTN-087 - Combined - n/s

[53]-Lawton Road - .01 mi NE of US 44

Town.....Canton
 Station.....87
 Location..... 41.824528,-72.887147
 Posted Speed Limit.....30 MPH
 2015-Major Collector 5.....2019-Urban
 Start Report.....21-Nov-2022 11:00AM
 All Vehicle Peak Hour.....22-Nov-2022 04:00PM
 End Report.....23-Nov-2022 11:00AM
 Annualized ADT.....6700
 24-Hour Count..... 6668 * G4(0.97) = 6468.0
 Day 1.....+ 7138 * G4(0.97) = 13391.8
 UnRounded AADT.....13391.8 / 2 = 6695.9
 OK 2022 Mon 21-Nov -this report-.....6700
 OK 2019 Mon 04-Nov6900
 OK 2016 Wed 14-Dec7200
 OK 2007 Tue 04-Dec6600
 Dataset Details.....1

	<u>Count</u>	<u>Percent</u>	<u>Veh.Feet</u>
Motorcycles.....	34	0.25%	>0-8
Passenger Cars.....	13279	97.15%	>8-25
Single-Unit Trucks...	349	2.55%	>25-50
Combination Trucks...	7	0.05%	>50
Total Vehicles.....	13669		

	<u>Single</u>	<u>Combo</u>
Peak Hour Truck Volume.....	11	0
% Total Peak Hour Volume.....	1.5%	0.0%
24 Hour Truck Volume.....	183	4
All-Vehicle Annualized ADT.....	6700	6700
24Hour T-Vol % of A-V AADT.....	2.7%	0.1%
PeakHr T-Vol % of A-V AADT.....	0.2%	0.0%
K-Factor (Peak/AADT).....	10.9%	10.9%
(AADT & Legacy AADT match)		

Hour	Motor Cycle	Pass Cars	Single Unit	Combo Unit	Day Total
21-Nov					
Mon					
12:00am					0
01:00am					0
02:00am					0
03:00am					0
04:00am					0
05:00am					0
06:00am					0
07:00am					0
08:00am					0
09:00am					0
10:00am	x	x	x	x	0
11:00am	1	341	21	.	363
12:00pm	.	383	14	.	397
01:00pm	.	431	11	1	443
02:00pm	1	469	16	.	486
03:00pm	1	619	15	.	635
04:00pm	3	664	19	.	686
05:00pm	2	627	8	.	637
06:00pm	5	336	5	.	346
07:00pm	.	209	.	.	209
08:00pm	.	134	.	.	134
09:00pm	.	99	.	.	99
10:00pm	.	46	.	.	46
11:00pm	.	14	.	.	14
Totals	13	4372	109	1	4495
Percent	0.29	97.26	2.42	0.02	
22-Nov					
Tue					
12:00am	.	9	.	.	9
01:00am	.	4	.	.	4
02:00am	.	7	.	.	7
03:00am	.	8	.	.	8
04:00am	.	13	.	.	13
05:00am	.	63	2	.	65
06:00am	.	230	8	.	238
07:00am	1	493	18	.	512
08:00am	3	493	6	.	502
09:00am	1	399	21	1	422
10:00am	.	379	12	2	393
11:00am	1	427	13	.	441
12:00pm	1	463	23	1	488
01:00pm	1	469	15	.	485
02:00pm	2	507	22	.	531
03:00pm	2	678	17	.	697
04:00pm	1	716	11	.	728
05:00pm	1	646	10	.	657
06:00pm	.	382	2	.	384
07:00pm	1	198	.	.	199
08:00pm	.	180	.	.	180
09:00pm	.	80	1	.	81
10:00pm	.	61	.	.	61
11:00pm	.	31	2	.	33
Totals	15	6936	183	4	7138
Percent	0.21	97.17	2.56	0.06	

2022 CNTN-087 - Class

23-Nov					
Wed					
12:00am	.	18	.	.	18
01:00am	.	6	.	.	6
02:00am	.	5	.	.	5
03:00am	.	7	.	.	7
04:00am	.	14	.	.	14
05:00am	.	62	.	.	62
06:00am	.	187	9	.	196
07:00am	1	433	15	.	449
08:00am	1	431	12	.	444
09:00am	.	379	12	1	392
10:00am	4	429	9	1	443
11:00am	x	x	x	x	0
12:00pm					0
01:00pm					0
02:00pm					0
03:00pm					0
04:00pm					0
05:00pm					0
06:00pm					0
07:00pm					0
08:00pm					0
09:00pm					0
10:00pm					0
11:00pm					0
Totals	6	1971	57	2	2036
Percent	0.29	96.81	2.80	0.10	

Status: OK [North] [Combined] [South]

CNTN-087 - Combined - n/s

[53]-Lawton Road - .01 mi NE of US 44

Town.....Canton	Hour	MPH 0-15	MPH 16-20	MPH 21-25	MPH 26-30	MPH 31-35	MPH 36-40	MPH 41-45	MPH 46-50	MPH 51-55	MPH 56-60	MPH 61-65	MPH 66-70	MPH 71-75	MPH 76+	Total Vol
Station.....87																
Location.....41.824528,-72.887147																
Posted Speed Limit.....30 MPH																
2015-Major Collector 5.....2019-Urban																
Start Report.....21-Nov-2022 11:00AM																
End Report.....23-Nov-2022 11:00AM																
Dataset Details.....1																
	Monday															
	21-Nov															
	12:00am															
	01:00am															
	02:00am															
	03:00am															
All Vehicles Average Speed.....36 MPH	04:00am															
Total Vehicles.....13669	05:00am															
Percentile Sampling Period...9AM to 4PM	06:00am															
Vehicle Too Close.....5 secs	07:00am															
Vehicle Too Slow.....< 15 MPH	08:00am															
Speed Lower Limit....15 MPH below Posted	09:00am															
	10:00am	x	x	x	x	x	x	x	x	x	x	x	x	x	x	
All Report Days	11:00am	.	2	9	54	109	150	33	6	.	.	.	.	.	.	361
85th Percentile Speed.....40.7 MPH	12:00pm	.	3	9	59	131	136	54	5	.	.	.	.	.	.	397
50th Percentile Speed.....35.6 MPH	01:00pm	.	1	9	49	129	174	70	10	1	.	.	.	.	.	441
10 MPH Pace (67%).....32-41 MPH	02:00pm	.	.	8	42	161	183	86	5	1	.	.	.	.	.	486
All Hours Total Vehicles.....13669	03:00pm	.	.	8	99	230	218	69	10	1	.	.	.	.	.	631
In-Period Total Vehicles.....6616	04:00pm	.	3	20	119	276	218	48	2	.	.	.	.	.	.	686
Omitted Vehicles Too Close (44%)...2883	05:00pm	.	9	10	104	265	197	50	.	1	1	.	.	.	.	637
Omitted Vehicles Too Slow (0%).....0	06:00pm	.	1	8	54	121	112	46	4	.	.	.	.	.	.	346
Sampled Vehicles (56%).....3733	07:00pm	.	.	5	21	48	91	37	7	.	.	.	.	.	.	205
	08:00pm	.	.	1	6	37	60	28	2	.	.	.	.	.	.	134
Monday 21-Nov-2022	09:00pm	1	1	.	7	17	45	22	3	3	.	.	.	.	.	95
85th Percentile Speed.....40.6 MPH	10:00pm	.	.	.	2	7	17	15	2	1	2	.	.	.	.	46
50th Percentile Speed.....35.5 MPH	11:00pm	.	.	.	.	2	5	5	2	.	.	.	.	.	.	14
10 MPH Pace (67%).....32-41 MPH	Totals	1	20	87	616	1533	1606	563	58	8	3	0	0	0	0	4495
All Hours Total Vehicles.....4495	Percent	0.02	0.44	1.94	13.70	34.10	35.73	12.53	1.29	0.18	0.07	0.00	0.00	0.00	0.00	
In-Period Total Vehicles.....2324	Tuesday															
Omitted Vehicles Too Close (43%)...998	22-Nov															
Omitted Vehicles Too Slow (0%).....0	12:00am	.	.	.	.	2	4	2	.	1	.	.	.	.	.	5
Sampled Vehicles (57%).....1326	01:00am	.	.	.	.	1	2	1	.	.	.	.	.	.	.	4
	02:00am	.	.	.	1	1	3	1	1	.	.	.	.	.	.	4
Tuesday 22-Nov-2022	03:00am	.	.	.	.	.	4	2	1	1	.	.	.	.	.	8
85th Percentile Speed.....40.7 MPH	04:00am	.	.	.	.	4	7	2	.	.	.	.	.	.	.	11
50th Percentile Speed.....35.6 MPH	05:00am	.	.	2	5	16	19	16	5	.	2	.	.	.	.	65
10 MPH Pace (67%).....32-41 MPH	06:00am	.	.	.	8	49	109	55	11	5	1	.	.	.	.	238
All Hours Total Vehicles.....7138	07:00am	.	.	2	28	156	226	89	9	2	.	.	.	.	.	512
In-Period Total Vehicles.....3457	08:00am	2	2	4	42	171	189	84	6	1	1	.	.	.	.	502
Omitted Vehicles Too Close (44%)...1533	09:00am	.	.	2	50	115	179	69	7	.	.	.	.	.	.	422
Omitted Vehicles Too Slow (0%).....0	10:00am	1	1	6	52	129	155	45	3	1	.	.	.	.	.	391
Sampled Vehicles (56%).....1924	11:00am	.	1	15	56	134	155	65	14	1	.	.	.	.	.	441
	12:00pm	.	1	5	48	149	201	74	8	1	1	.	.	.	.	488
Wednesday 23-Nov-2022	01:00pm	.	.	6	50	142	193	83	10	.	1	.	.	.	.	485
85th Percentile Speed.....41.1 MPH	02:00pm	.	.	8	65	200	175	74	6	2	.	.	1	.	.	531
50th Percentile Speed.....36.2 MPH	03:00pm	1	6	26	87	258	226	89	3	1	.	.	.	.	.	697
10 MPH Pace (67%).....33-42 MPH	04:00pm	.	1	16	99	318	248	44	2	.	.	.	.	.	.	728
All Hours Total Vehicles.....2036	05:00pm	.	.	25	94	276	215	45	2	.	.	.	.	.	.	657
In-Period Total Vehicles.....835	06:00pm	.	1	12	44	135	145	43	4	.	.	.	.	.	.	384
Omitted Vehicles Too Close (42%)...352	07:00pm	1	1	5	15	59	87	26	5	.	.	.	.	.	.	195
Omitted Vehicles Too Slow (0%).....0	08:00pm	.	.	.	14	47	72	38	9	.	.	.	.	.	.	180
Sampled Vehicles (58%).....483	09:00pm	.	.	.	4	14	36	23	4	.	.	.	.	.	.	81
	10:00pm	.	.	.	4	16	24	12	1	4	.	.	.	.	.	61
	11:00pm	.	.	.	3	7	16	5	2	.	.	.	.	.	.	31
	Totals	5	14	134	769	2399	2690	987	113	20	6	0	1	0	0	7138
	Percent	0.07	0.20	1.88	10.77	33.61	37.69	13.83	1.58	0.28	0.08	0.00	0.01	0.00	0.00	
	Wednesday															
	23-Nov															
	12:00am	.	.	.	.	3	10	5	.	.	.	.	.	.	.	18
	01:00am	.	.	.	.	.	5	.	.	.	.	.	.	1	.	6
	02:00am	.	.	.	1	1	2	1	.	.	.	.	.	.	.	5
	03:00am	.	.	.	.	1	3	3	.	.	.	.	.	.	.	7
	04:00am	.	.	.	2	3	4	3	2	.	.	.	.	.	.	14
	05:00am	.	.	.	2	16	17	22	4	1	.	.	.	.	.	62
	06:00am	.	.	4	8	43	82	47	10	1	1	.	.	.	.	196
	07:00am	.	.	5	24	114	201	89	14	1	.	1	.	.	.	445
	08:00am	.	.	3	33	105	185	104	13	.	.	.	1	.	.	444
	09:00am	.	1	4	47	121	137	72	10	.	.	.	.	.	.	392
	10:00am	.	.	4	55	140	175	61	7	1	.	.	.	.	.	441
	11:00am	x	x	x	x	x	x	x	x	x	x	x	x	x	x	
	12:00pm															
	01:00pm															
	02:00pm															
	03:00pm															
	04:00pm															
	05:00pm															
	06:00pm															
	07:00pm															
	08:00pm															
	09:00pm															
	10:00pm															
	11:00pm															
	Totals	0	1	20	172	547	819	408	61	4	1	1	1	1	0	2036
	Percent	0.00	0.05	0.98	8.45	26.87	40.23	20.04	3.00	0.20	0.05	0.05	0.05	0.05	0.00	

Status: OK North Combined South Class Speed

CNTN-087 - Combined - n/s

[53]-Lawton Road - .01 mi NE of US 44

	21-Nov	22-Nov	23-Nov
	Mon	Tue	Wed
Town.....Canton			
Station.....87			
Location..... 41.824528,-72.887147	12:00am	9	18
Posted Speed Limit.....30 MPH	01:00am	4	6
2015-Major Collector 5.....2019-Urban	02:00am	7	5
Start Report.....21-Nov-2022 11:00AM	03:00am	8	7
End Report.....23-Nov-2022 11:00AM	04:00am	13	14
Annualized ADT.....6700	05:00am	65	62
24-Hour Count... 6668 * G4(0.97) = 6468.0	06:00am	238	196
Day 1.....+ 7138 * G4(0.97) = 13391.8	07:00am	512	449
UnRounded AADT.....13391.8 / 2 = 6695.9	08:00am	502	444
OK 2022 Mon 21-Nov -this report-...6700	09:00am	422	392
OK 2019 Mon 04-Nov .....6900	10:00am	x	443
OK 2016 Wed 14-Dec .....7200	11:00am	363	441
OK 2007 Tue 04-Dec .....6600	12:00pm	397	488
Dataset Details.....1	01:00pm	443	485
	02:00pm	486	531
	03:00pm	635	697
	04:00pm	686	728
	05:00pm	637	657
	06:00pm	346	384
	07:00pm	209	199
	08:00pm	134	180
	09:00pm	99	81
	10:00pm	46	61
	11:00pm	14	33
Totals	4495	7138	2036

B Traffic Counts – Project Traffic Counts

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Route 44 at Plaza Drive
 Canton, Connecticut

File Name : 24631
 Site Code : 24631
 Start Date : 6/8/2023
 Page No : 1

Groups Printed- Lights - Trucks - Buses

Start Time	From North					Route 44 From East					PLaza Drive From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
07:00 AM	0	0	0	0	0	0	84	2	0	86	7	0	4	0	11	10	220	0	0	230	327
07:15 AM	0	0	0	0	0	0	93	4	0	97	7	0	3	1	11	9	172	0	0	181	289
07:30 AM	0	0	0	0	0	0	106	9	1	116	6	0	3	1	10	10	222	0	0	232	358
07:45 AM	0	0	0	0	0	0	116	4	0	120	12	0	2	0	14	6	216	0	0	222	356
Total	0	0	0	0	0	0	399	19	1	419	32	0	12	2	46	35	830	0	0	865	1330
08:00 AM	0	0	0	0	0	0	100	12	0	112	6	0	7	0	13	5	188	0	0	193	318
08:15 AM	0	0	0	0	0	0	105	10	0	115	11	0	5	1	17	9	208	0	0	217	349
08:30 AM	0	0	0	0	0	0	91	10	0	101	14	0	12	1	27	12	198	0	0	210	338
08:45 AM	0	0	0	0	0	0	98	15	0	113	13	0	6	0	19	15	188	0	0	203	335
Total	0	0	0	0	0	0	394	47	0	441	44	0	30	2	76	41	782	0	0	823	1340
Grand Total	0	0	0	0	0	0	793	66	1	860	76	0	42	4	122	76	1612	0	0	1688	2670
Apprch %	0	0	0	0		0	92.2	7.7	0.1		62.3	0	34.4	3.3		4.5	95.5	0	0		
Total %	0	0	0	0	0	0	29.7	2.5	0	32.2	2.8	0	1.6	0.1	4.6	2.8	60.4	0	0	63.2	
Lights	0	0	0	0	0	0	743	66	1	810	72	0	40	3	115	73	1571				
% Lights	0	0	0	0	0	0	93.7	100	100	94.2	94.7	0	95.2	75	94.3	96.1	97.5	0	0	97.4	96.2
Trucks	0	0	0	0	0	0	42	0	0	42	4	0	2	1	7	3	28	0	0	31	80
% Trucks	0	0	0	0	0	0	5.3	0	0	4.9	5.3	0	4.8	25	5.7	3.9	1.7	0	0	1.8	3
Buses	0	0	0	0	0	0	8	0	0	8	0	0	0	0	0	0	13	0	0	13	21
% Buses	0	0	0	0	0	0	1	0	0	0.9	0	0	0	0	0	0	0.8	0	0	0.8	0.8

Connecticut Counts LLC

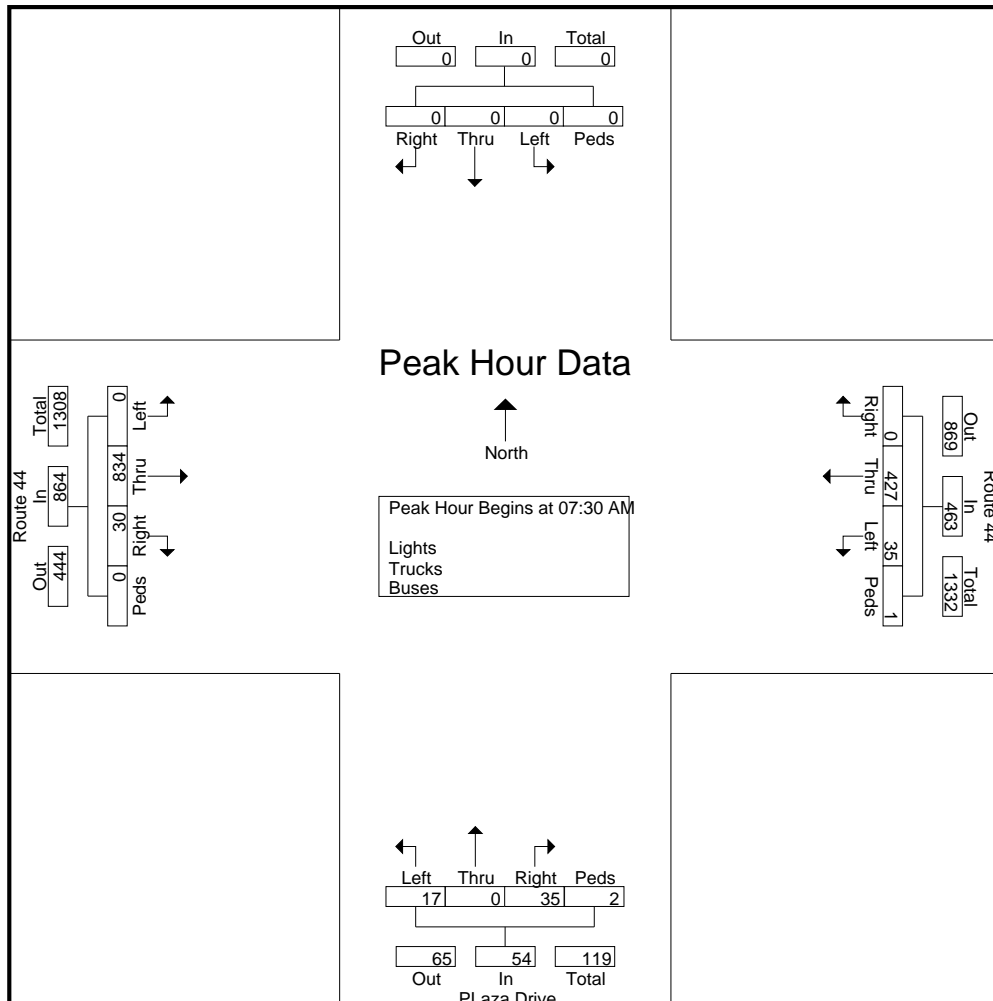
Kensington, Connecticut 06037
(860) 828-1693

File Name : 24631
Site Code : 24631
Start Date : 6/8/2023
Page No : 2

Start Time	From North					Route 44 From East					PLaza Drive From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1
Peak Hour for Entire Intersection Begins at 07:30 AM

07:30 AM	0	0	0	0	0	0	106	9	1	116	6	0	3	1	10	10	222	0	0	232	358
07:45 AM	0	0	0	0	0	0	116	4	0	120	12	0	2	0	14	6	216	0	0	222	356
08:00 AM	0	0	0	0	0	0	100	12	0	112	6	0	7	0	13	5	188	0	0	193	318
08:15 AM	0	0	0	0	0	0	105	10	0	115	11	0	5	1	17	9	208	0	0	217	349
Total Volume	0	0	0	0	0	0	427	35	1	463	35	0	17	2	54	30	834	0	0	864	1381
% App. Total	0	0	0	0	0	0	92.2	7.6	0.2		64.8	0	31.5	3.7		3.5	96.5	0	0		
PHF	.000	.000	.000	.000	.000	.000	.920	.729	.250	.965	.729	.000	.607	.500	.794	.750	.939	.000	.000	.931	.964



Connecticut Counts LLC

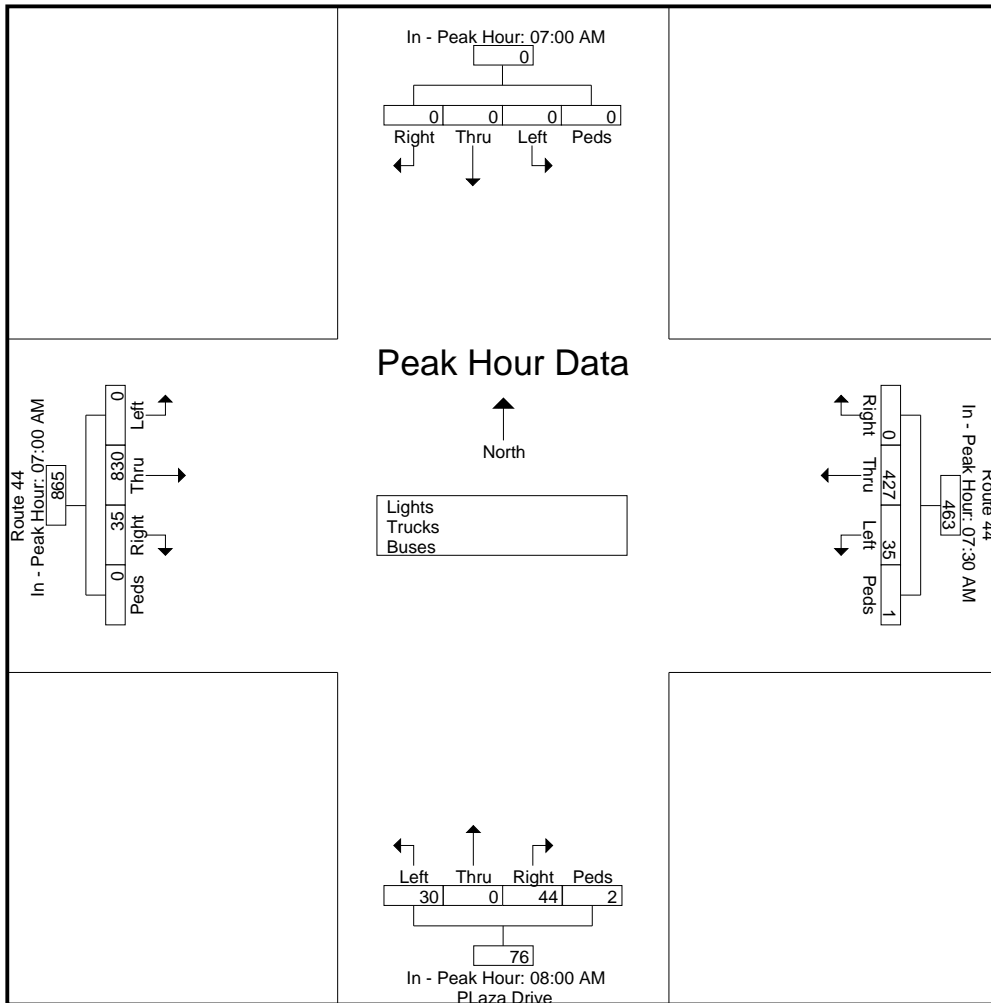
Kensington, Connecticut 06037
(860) 828-1693

File Name : 24631
Site Code : 24631
Start Date : 6/8/2023
Page No : 3

Start Time	From North					Route 44 From East					PLaza Drive From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1
Peak Hour for Each Approach Begins at:

	07:00 AM					07:30 AM					08:00 AM					07:00 AM				
+0 mins.	0	0	0	0	0	0	106	9	1	116	6	0	7	0	13	10	220	0	0	230
+15 mins.	0	0	0	0	0	0	116	4	0	120	11	0	5	1	17	9	172	0	0	181
+30 mins.	0	0	0	0	0	0	100	12	0	112	14	0	12	1	27	10	222	0	0	232
+45 mins.	0	0	0	0	0	0	105	10	0	115	13	0	6	0	19	6	216	0	0	222
Total Volume	0	0	0	0	0	0	427	35	1	463	44	0	30	2	76	35	830	0	0	865
% App. Total	0	0	0	0	0	0	92.2	7.6	0.2		57.9	0	39.5	2.6		4	96	0	0	
PHF	.000	.000	.000	.000	.000	.000	.920	.729	.250	.965	.786	.000	.625	.500	.704	.875	.935	.000	.000	.932



Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Route 44 at Plaza Drive
 Canton, Connecticut

File Name : 24633
 Site Code : 24633
 Start Date : 6/10/2023
 Page No : 1

Groups Printed- Lights - Trucks - Buses

Start Time	From North					Route 44 From East					Plaza Drive From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
11:00 AM	0	0	0	0	0	0	144	24	0	168	23	0	14	2	39	23	181	0	0	204	411
11:15 AM	0	0	0	0	0	0	136	14	2	152	22	0	15	0	37	14	170	0	0	184	373
11:30 AM	0	0	0	0	0	0	152	19	0	171	23	0	21	0	44	18	162	0	0	180	395
11:45 AM	0	0	0	0	0	0	149	23	0	172	20	0	11	1	32	16	168	0	0	184	388
Total	0	0	0	0	0	0	581	80	2	663	88	0	61	3	152	71	681	0	0	752	1567
12:00 PM	0	0	0	0	0	0	186	21	0	207	21	0	21	0	42	13	159	0	0	172	421
12:15 PM	0	0	0	0	0	0	155	37	0	192	31	0	15	0	46	11	148	0	0	159	397
12:30 PM	0	0	0	0	0	0	152	20	1	173	27	0	11	0	38	17	183	0	0	200	411
12:45 PM	0	0	0	0	0	0	138	17	0	155	13	0	13	2	28	10	157	0	0	167	350
Total	0	0	0	0	0	0	631	95	1	727	92	0	60	2	154	51	647	0	0	698	1579
Grand Total	0	0	0	0	0	0	1212	175	3	1390	180	0	121	5	306	122	1328	0	0	1450	3146
Apprch %	0	0	0	0	0	0	87.2	12.6	0.2		58.8	0	39.5	1.6		8.4	91.6	0	0		
Total %	0	0	0	0	0	0	38.5	5.6	0.1	44.2	5.7	0	3.8	0.2	9.7	3.9	42.2	0	0	46.1	
Lights	0	0	0	0	0	0	1194										1316				
% Lights	0	0	0	0	0	0	98.5	100	66.7	98.6	100	0	100	40	99	100	99.1	0	0	99.2	98.9
Trucks	0	0	0	0	0	0	16	0	1	17	0	0	0	3	3	0	11	0	0	11	31
% Trucks	0	0	0	0	0	0	1.3	0	33.3	1.2	0	0	0	60	1	0	0.8	0	0	0.8	1
Buses	0	0	0	0	0	0	2	0	0	2	0	0	0	0	0	0	1	0	0	1	3
% Buses	0	0	0	0	0	0	0.2	0	0	0.1	0	0	0	0	0	0	0.1	0	0	0.1	0.1

Connecticut Counts LLC

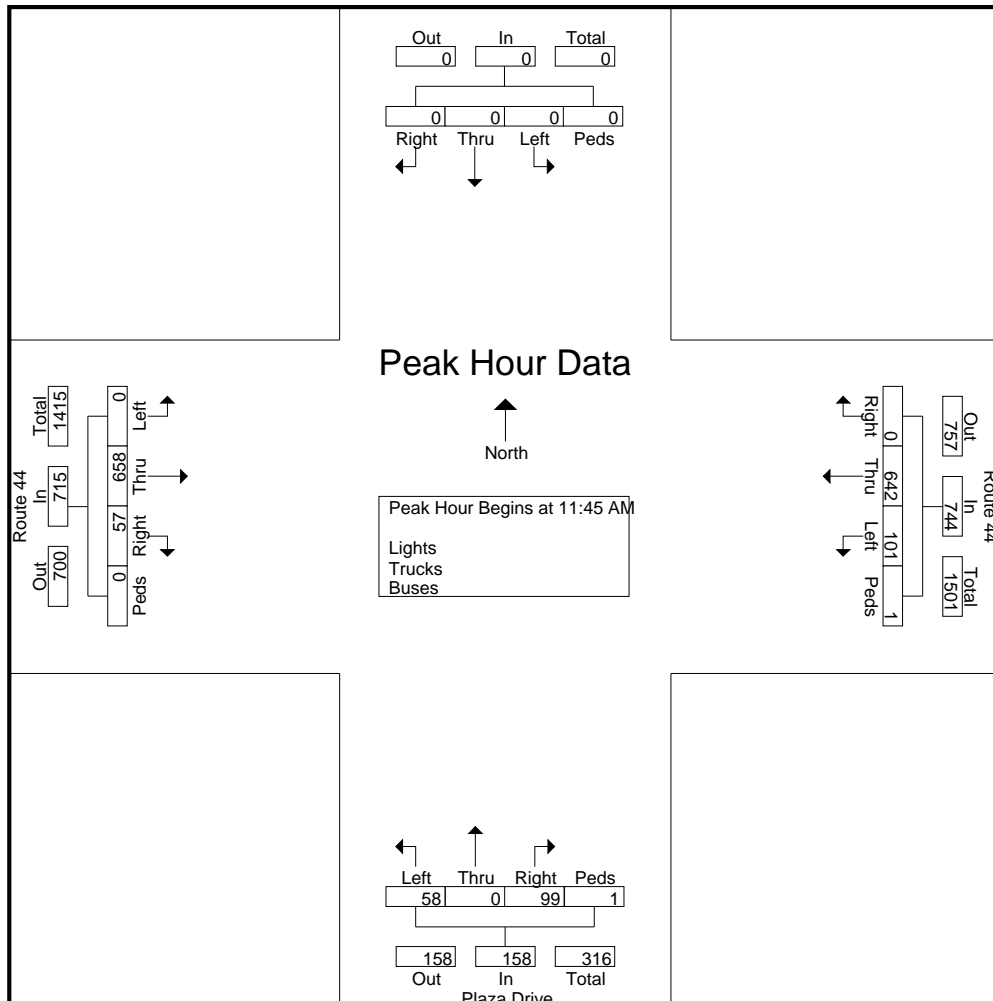
Kensington, Connecticut 06037
(860) 828-1693

File Name : 24633
 Site Code : 24633
 Start Date : 6/10/2023
 Page No : 2

Start Time	From North					Route 44 From East					Plaza Drive From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1
 Peak Hour for Entire Intersection Begins at 11:45 AM

11:45 AM	0	0	0	0	0	0	149	23	0	172	20	0	11	1	32	16	168	0	0	184	388
12:00 PM	0	0	0	0	0	0	186	21	0	207	21	0	21	0	42	13	159	0	0	172	421
12:15 PM	0	0	0	0	0	0	155	37	0	192	31	0	15	0	46	11	148	0	0	159	397
12:30 PM	0	0	0	0	0	0	152	20	1	173	27	0	11	0	38	17	183	0	0	200	411
Total Volume	0	0	0	0	0	0	642	101	1	744	99	0	58	1	158	57	658	0	0	715	1617
% App. Total	0	0	0	0	0	0	86.3	13.6	0.1		62.7	0	36.7	0.6		8	92	0	0		
PHF	.000	.000	.000	.000	.000	.000	.863	.682	.250	.899	.798	.000	.690	.250	.859	.838	.899	.000	.000	.894	.960



Connecticut Counts LLC

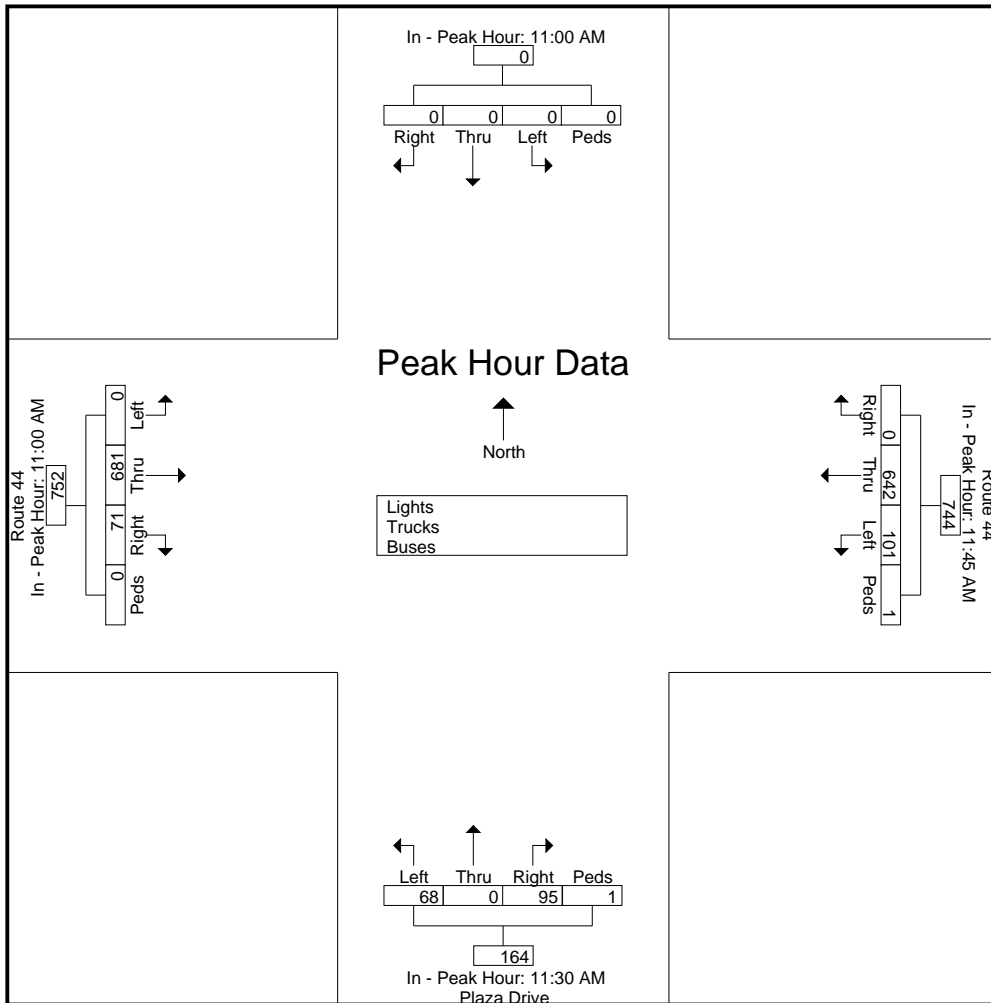
Kensington, Connecticut 06037
(860) 828-1693

File Name : 24633
 Site Code : 24633
 Start Date : 6/10/2023
 Page No : 3

Start Time	From North					Route 44 From East					Plaza Drive From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	11:00 AM					11:45 AM					11:30 AM					11:00 AM				
+0 mins.	0	0	0	0	0	0	149	23	0	172	23	0	21	0	44	23	181	0	0	204
+15 mins.	0	0	0	0	0	0	186	21	0	207	20	0	11	1	32	14	170	0	0	184
+30 mins.	0	0	0	0	0	0	155	37	0	192	21	0	21	0	42	18	162	0	0	180
+45 mins.	0	0	0	0	0	0	152	20	1	173	31	0	15	0	46	16	168	0	0	184
Total Volume	0	0	0	0	0	0	642	101	1	744	95	0	68	1	164	71	681	0	0	752
% App. Total	0	0	0	0	0	0	86.3	13.6	0.1		57.9	0	41.5	0.6		9.4	90.6	0	0	
PHF	.000	.000	.000	.000	.000	.000	.863	.682	.250	.899	.766	.000	.810	.250	.891	.772	.941	.000	.000	.922



Lights
Trucks
Buses

Connecticut Counts LLC
Kensington, Connecticut 06037
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Route 44 at Plaza Drive
 Canton, Connecticut

File Name : 24632
 Site Code : 24632
 Start Date : 6/8/2023
 Page No : 1

Groups Printed- Lights - Trucks - Buses

Start Time	From North					Route 44 From East					Plaza Drive From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
04:00 PM	0	0	0	0	0	0	203	22	0	225	26	0	17	0	43	11	148	0	0	159	427
04:15 PM	0	0	0	0	0	0	191	14	0	205	10	0	11	3	24	12	160	0	0	172	401
04:30 PM	0	0	0	0	0	0	198	31	0	229	12	0	21	0	33	16	170	0	0	186	448
04:45 PM	0	0	0	0	0	0	176	26	0	202	17	0	20	0	37	12	148	0	0	160	399
Total	0	0	0	0	0	0	768	93	0	861	65	0	69	3	137	51	626	0	0	677	1675
05:00 PM	0	0	0	0	0	0	189	22	0	211	26	0	22	0	48	8	145	0	0	153	412
05:15 PM	0	0	0	0	0	0	187	18	0	205	15	0	15	1	31	12	143	0	0	155	391
05:30 PM	0	0	0	0	0	0	199	8	1	208	16	0	9	0	25	9	149	0	0	158	391
05:45 PM	0	0	0	0	0	0	179	7	0	186	7	0	11	0	18	10	152	0	0	162	366
Total	0	0	0	0	0	0	754	55	1	810	64	0	57	1	122	39	589	0	0	628	1560
Grand Total	0	0	0	0	0	0	1522	148	1	1671	129	0	126	4	259	90	1215	0	0	1305	3235
Apprch %	0	0	0	0	0	0	91.1	8.9	0.1		49.8	0	48.6	1.5		6.9	93.1	0	0		
Total %	0	0	0	0	0	0	47	4.6	0	51.7	4	0	3.9	0.1	8	2.8	37.6	0	0	40.3	
Lights	0	0	0	0	0	0	1501										1194				
% Lights	0	0	0	0	0	0	98.6	100	100	98.7	100	0	100	50	99.2	98.9	98.3	0	0	98.3	98.6
Trucks	0	0	0	0	0	0	12	0	0	12	0	0	0	2	2	1	17	0	0	18	32
% Trucks	0	0	0	0	0	0	0.8	0	0	0.7	0	0	0	50	0.8	1.1	1.4	0	0	1.4	1
Buses	0	0	0	0	0	0	9	0	0	9	0	0	0	0	0	0	4	0	0	4	13
% Buses	0	0	0	0	0	0	0.6	0	0	0.5	0	0	0	0	0	0	0.3	0	0	0.3	0.4

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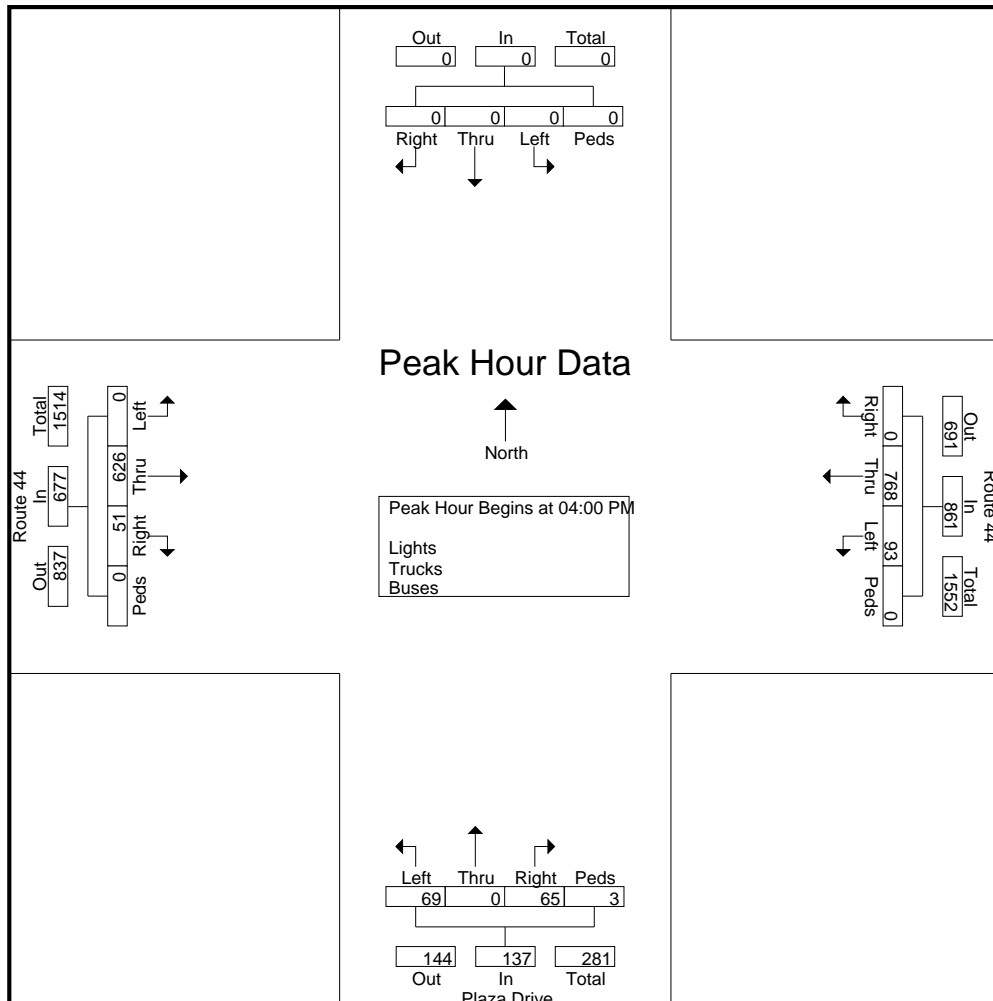
File Name : 24632
Site Code : 24632
Start Date : 6/8/2023
Page No : 2

Start Time	From North					Route 44 From East					Plaza Drive From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1

Peak Hour for Entire Intersection Begins at 04:00 PM

04:00 PM	0	0	0	0	0	0	203	22	0	225	26	0	17	0	43	11	148	0	0	159	427
04:15 PM	0	0	0	0	0	0	191	14	0	205	10	0	11	3	24	12	160	0	0	172	401
04:30 PM	0	0	0	0	0	0	198	31	0	229	12	0	21	0	33	16	170	0	0	186	448
04:45 PM	0	0	0	0	0	0	176	26	0	202	17	0	20	0	37	12	148	0	0	160	399
Total Volume	0	0	0	0	0	0	768	93	0	861	65	0	69	3	137	51	626	0	0	677	1675
% App. Total	0	0	0	0	0	0	89.2	10.8	0		47.4	0	50.4	2.2		7.5	92.5	0	0		
PHF	.000	.000	.000	.000	.000	.000	.946	.750	.000	.940	.625	.000	.821	.250	.797	.797	.921	.000	.000	.910	.935



Connecticut Counts LLC

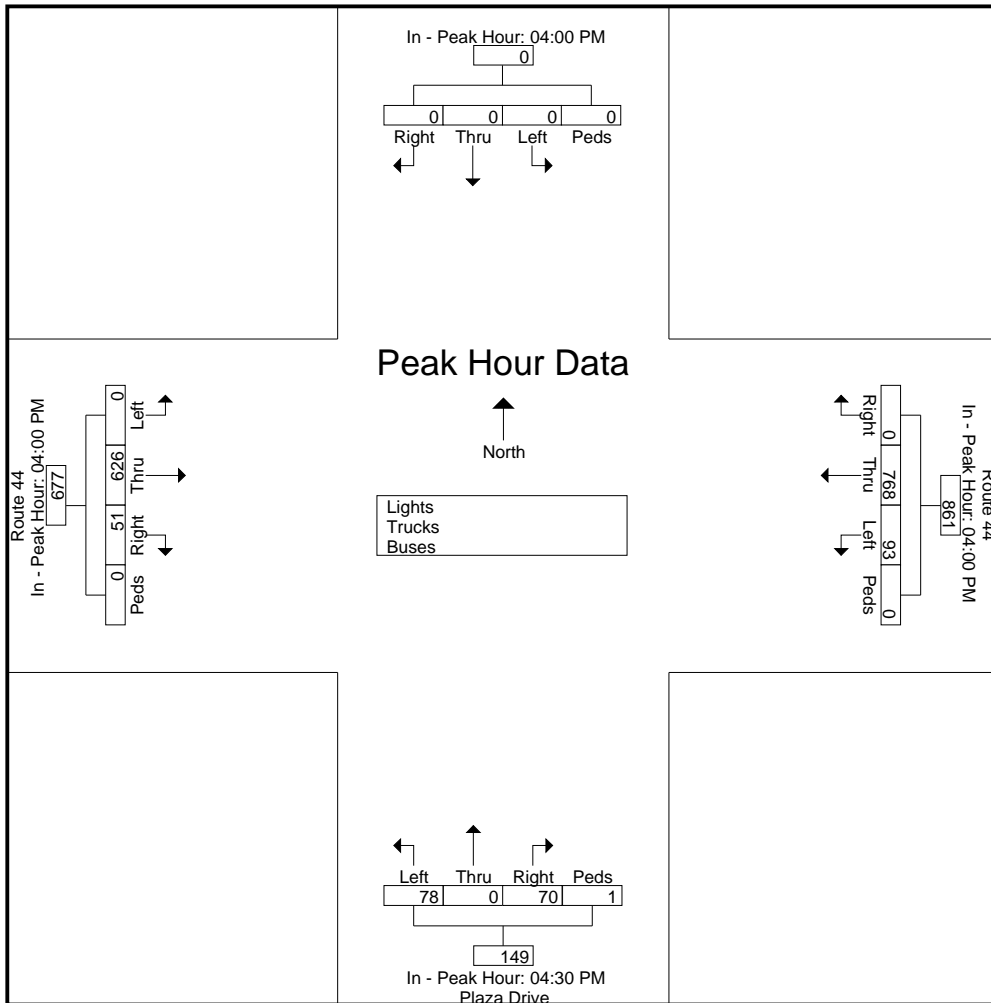
Kensington, Connecticut 06037
(860) 828-1693

File Name : 24632
Site Code : 24632
Start Date : 6/8/2023
Page No : 3

Start Time	From North					Route 44 From East					Plaza Drive From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1
Peak Hour for Each Approach Begins at:

	04:00 PM					04:00 PM					04:30 PM					04:00 PM				
+0 mins.	0	0	0	0	0	0	203	22	0	225	12	0	21	0	33	11	148	0	0	159
+15 mins.	0	0	0	0	0	0	191	14	0	205	17	0	20	0	37	12	160	0	0	172
+30 mins.	0	0	0	0	0	0	198	31	0	229	26	0	22	0	48	16	170	0	0	186
+45 mins.	0	0	0	0	0	0	176	26	0	202	15	0	15	1	31	12	148	0	0	160
Total Volume	0	0	0	0	0	0	768	93	0	861	70	0	78	1	149	51	626	0	0	677
% App. Total	0	0	0	0	0	0	89.2	10.8	0		47	0	52.3	0.7		7.5	92.5	0	0	
PHF	.000	.000	.000	.000	.000	.000	.946	.750	.000	.940	.673	.000	.886	.250	.776	.797	.921	.000	.000	.910



Connecticut Counts LLC
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Route 44 at Dowd Avenue
 Canton, Connecticut

File Name : 24634
 Site Code : 24634
 Start Date : 6/8/2023
 Page No : 1

Groups Printed- Lights - Trucks - Buses

Start Time	Private Dr From North					Route 44 From East					Dowd Avenue From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
07:00 AM	0	0	0	0	0	1	87	58	0	146	79	0	0	0	79	1	184	0	0	185	410
07:15 AM	0	0	1	0	1	0	114	79	0	193	74	0	0	0	74	0	220	0	0	220	488
07:30 AM	1	0	0	0	1	1	126	43	0	170	110	0	0	0	110	0	210	0	0	210	491
07:45 AM	3	0	0	0	3	1	124	52	0	177	89	0	0	0	89	0	240	0	0	240	509
Total	4	0	1	0	5	3	451	232	0	686	352	0	0	0	352	1	854	0	0	855	1898
08:00 AM	1	0	0	0	1	0	132	43	0	175	87	0	0	0	87	0	205	0	0	205	468
08:15 AM	0	0	0	0	0	0	127	53	0	180	72	0	0	1	73	0	220	1	0	221	474
08:30 AM	0	0	2	0	2	1	109	53	0	163	92	0	0	0	92	0	213	0	0	213	470
08:45 AM	3	0	0	0	3	1	130	83	0	214	81	0	0	0	81	0	181	0	0	181	479
Total	4	0	2	0	6	2	498	232	0	732	332	0	0	1	333	0	819	1	0	820	1891
Grand Total	8	0	3	0	11	5	949	464	0	1418	684	0	0	1	685	1	1673	1	0	1675	3789
Apprch %	72.7	0	27.3	0		0.4	66.9	32.7	0		99.9	0	0	0.1		0.1	99.9	0.1	0		
Total %	0.2	0	0.1	0	0.3	0.1	25	12.2	0	37.4	18.1	0	0	0	18.1	0	44.2	0	0	44.2	
Lights	7	0	3	0	10	5	895	444	0	1344	665	0	0	0	665	1	1624				
% Lights	87.5	0	100	0	90.9	100	94.3	95.7	0	94.8	97.2	0	0	0	97.1	100	97.1	100	0	97.1	96.2
Trucks	1	0	0	0	1	0	47	10	0	57	7	0	0	1	8	0	35	0	0	35	101
% Trucks	12.5	0	0	0	9.1	0	5	2.2	0	4	1	0	0	100	1.2	0	2.1	0	0	2.1	2.7
Buses	0	0	0	0	0	0	7	10	0	17	12	0	0	0	12	0	14	0	0	14	43
% Buses	0	0	0	0	0	0	0.7	2.2	0	1.2	1.8	0	0	0	1.8	0	0.8	0	0	0.8	1.1

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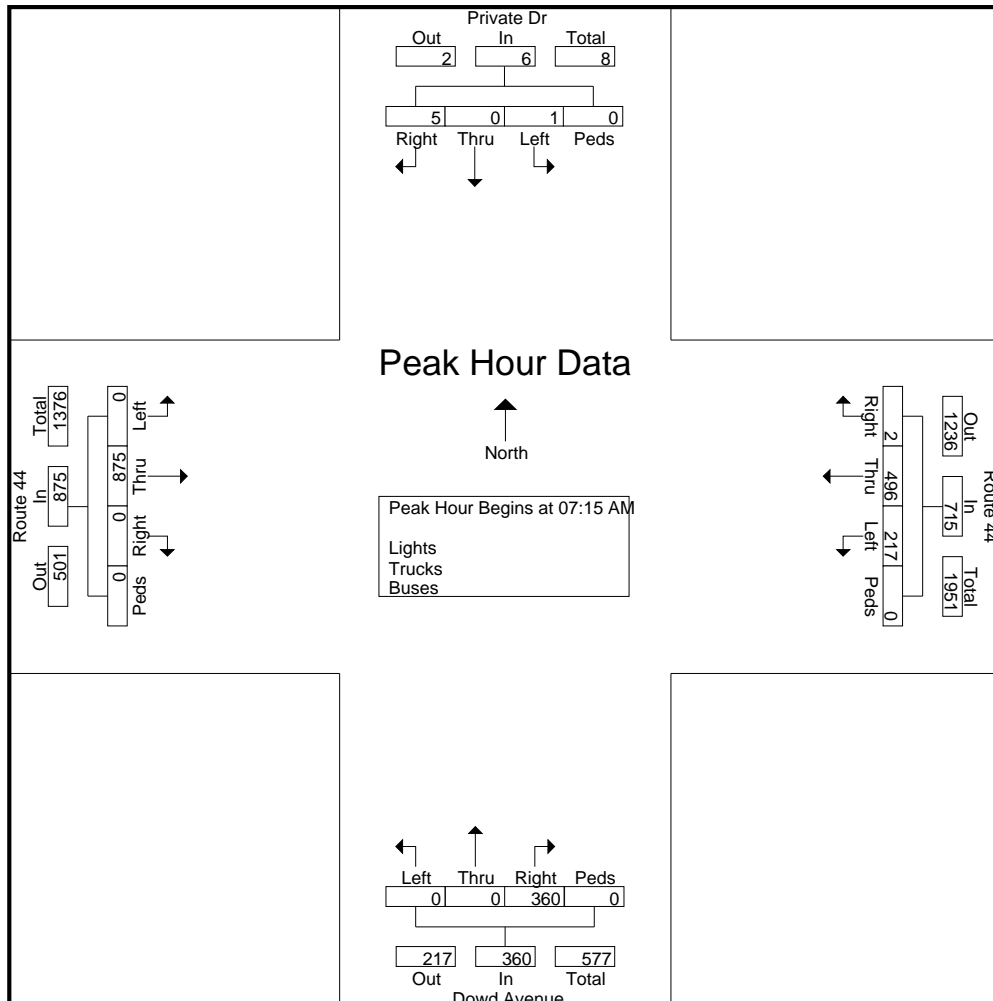
Kensington, Connecticut 06037
(860) 828-1693

File Name : 24634
Site Code : 24634
Start Date : 6/8/2023
Page No : 2

Start Time	Private Dr From North					Route 44 From East					Dowd Avenue From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1
Peak Hour for Entire Intersection Begins at 07:15 AM

07:15 AM	0	0	1	0	1	0	114	79	0	193	74	0	0	0	74	0	220	0	0	220	488
07:30 AM	1	0	0	0	1	1	126	43	0	170	110	0	0	0	110	0	210	0	0	210	491
07:45 AM	3	0	0	0	3	1	124	52	0	177	89	0	0	0	89	0	240	0	0	240	509
08:00 AM	1	0	0	0	1	0	132	43	0	175	87	0	0	0	87	0	205	0	0	205	468
Total Volume	5	0	1	0	6	2	496	217	0	715	360	0	0	0	360	0	875	0	0	875	1956
% App. Total	83.3	0	16.7	0		0.3	69.4	30.3	0		100	0	0	0		0	100	0	0		
PHF	.417	.000	.250	.000	.500	.500	.939	.687	.000	.926	.818	.000	.000	.000	.818	.000	.911	.000	.000	.911	.961



Connecticut Counts LLC

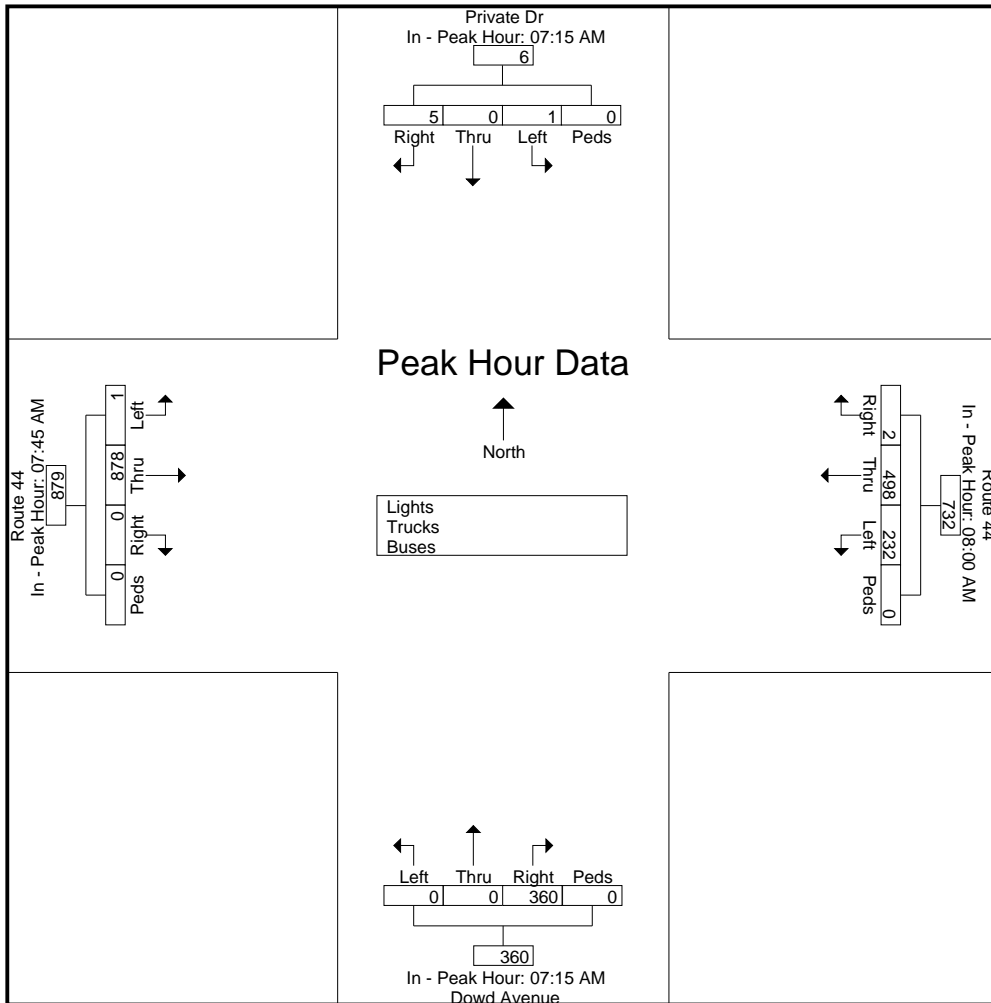
Kensington, Connecticut 06037
(860) 828-1693

File Name : 24634
 Site Code : 24634
 Start Date : 6/8/2023
 Page No : 3

Start Time	Private Dr From North					Route 44 From East					Dowd Avenue From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

Peak Hour Analysis From 07:00 AM to 08:45 AM - Peak 1 of 1
 Peak Hour for Each Approach Begins at:

	07:15 AM					08:00 AM					07:15 AM					07:45 AM				
+0 mins.	0	0	1	0	1	0	132	43	0	175	74	0	0	0	74	0	240	0	0	240
+15 mins.	1	0	0	0	1	0	127	53	0	180	110	0	0	0	110	0	205	0	0	205
+30 mins.	3	0	0	0	3	1	109	53	0	163	89	0	0	0	89	0	220	1	0	221
+45 mins.	1	0	0	0	1	1	130	83	0	214	87	0	0	0	87	0	213	0	0	213
Total Volume	5	0	1	0	6	2	498	232	0	732	360	0	0	0	360	0	878	1	0	879
% App. Total	83.3	0	16.7	0		0.3	68	31.7	0		100	0	0	0		0	99.9	0.1	0	
PHF	.417	.000	.250	.000	.500	.500	.943	.699	.000	.855	.818	.000	.000	.000	.818	.000	.915	.250	.000	.916



Connecticut Counts LLC
Kensington, Connecticut 06037
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Route 44 at Dowd Avenue
 Canton, Connecticut

File Name : 24636
 Site Code : 24636
 Start Date : 6/10/2023
 Page No : 1

Groups Printed- Lights - Trucks - Buses

Start Time	Private Drive From North					Route 44 From East					Dowd Avenue From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
11:00 AM	1	0	0	0	1	1	169	81	0	251	97	0	0	1	98	1	205	2	0	208	558
11:15 AM	5	0	0	0	5	8	179	85	0	272	65	1	0	0	66	1	189	2	3	195	538
11:30 AM	6	0	0	0	6	6	187	96	0	289	96	0	0	0	96	0	187	1	0	188	579
11:45 AM	5	0	0	0	5	0	200	80	0	280	92	0	0	1	93	0	167	2	4	173	551
Total	17	0	0	0	17	15	735	342	0	1092	350	1	0	2	353	2	748	7	7	764	2226
12:00 PM	4	0	0	0	4	5	208	96	0	309	86	0	0	0	86	0	177	3	0	180	579
12:15 PM	3	0	2	0	5	1	185	72	0	258	68	0	0	0	68	0	185	5	0	190	521
12:30 PM	5	0	0	0	5	3	235	111	0	349	110	0	0	1	111	1	210	1	0	212	677
12:45 PM	3	0	0	0	3	3	195	94	0	292	94	1	0	0	95	0	180	3	0	183	573
Total	15	0	2	0	17	12	823	373	0	1208	358	1	0	1	360	1	752	12	0	765	2350
Grand Total	32	0	2	0	34	27	1558	715	0	2300	708	2	0	3	713	3	1500	19	7	1529	4576
Apprch %	94.1	0	5.9	0		1.2	67.7	31.1	0		99.3	0.3	0	0.4		0.2	98.1	1.2	0.5		
Total %	0.7	0	0	0	0.7	0.6	34	15.6	0	50.3	15.5	0	0	0.1	15.6	0.1	32.8	0.4	0.2	33.4	
Lights	32	0	2	0	34	27	1541								1490						
% Lights	100	0	100	0	100	100	98.9	99.3	0	99	99.4	100	0	66.7	99.3	100	99.3	100	71.4	99.2	99.1
Trucks	0	0	0	0	0	0	16	4	0	20	4	0	0	1	5	0	9	0	2	11	36
% Trucks	0	0	0	0	0	0	1	0.6	0	0.9	0.6	0	0	33.3	0.7	0	0.6	0	28.6	0.7	0.8
Buses	0	0	0	0	0	0	1	1	0	2	0	0	0	0	0	0	1	0	0	1	3
% Buses	0	0	0	0	0	0	0.1	0.1	0	0.1	0	0	0	0	0	0	0.1	0	0	0.1	0.1

Connecticut Counts LLC

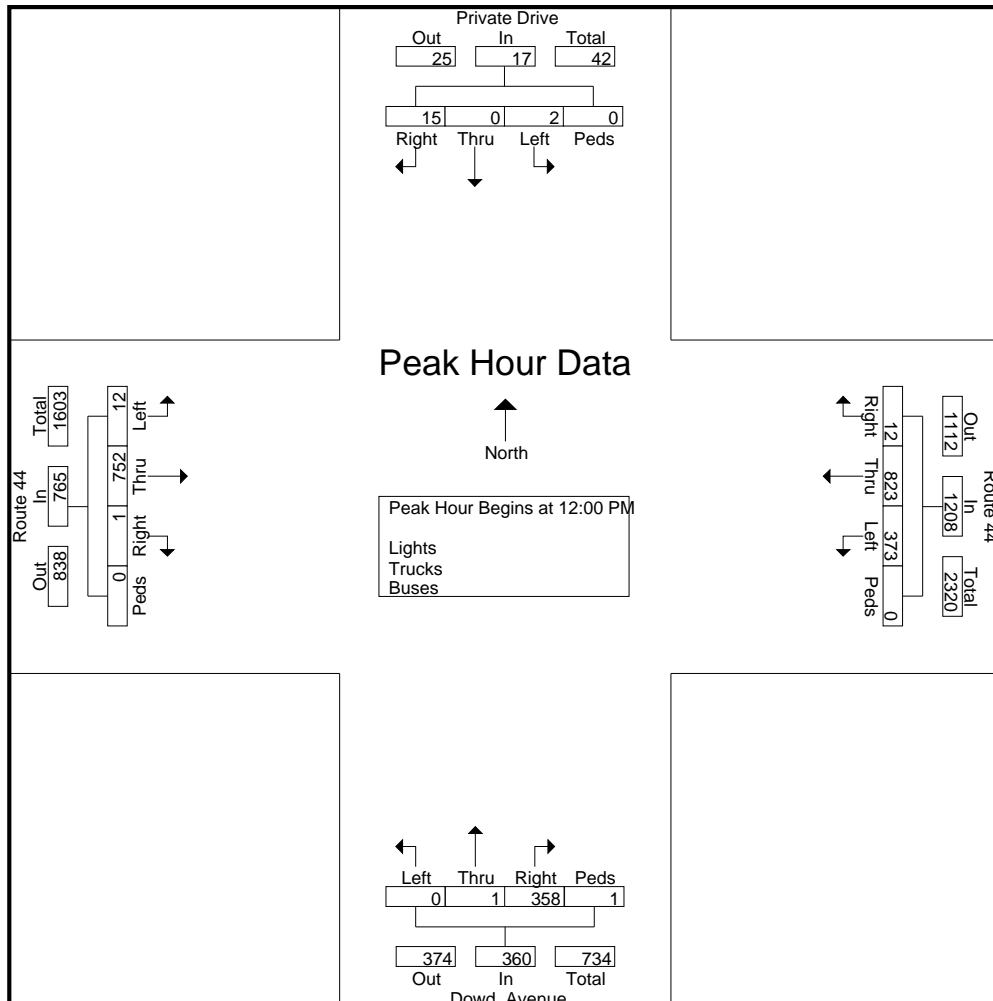
Kensington, Connecticut 06037
(860) 828-1693

File Name : 24636
Site Code : 24636
Start Date : 6/10/2023
Page No : 2

Start Time	Private Drive From North					Route 44 From East					Dowd Avenue From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1
Peak Hour for Entire Intersection Begins at 12:00 PM

12:00 PM	4	0	0	0	4	5	208	96	0	309	86	0	0	0	86	0	177	3	0	180	579
12:15 PM	3	0	2	0	5	1	185	72	0	258	68	0	0	0	68	0	185	5	0	190	521
12:30 PM	5	0	0	0	5	3	235	111	0	349	110	0	0	1	111	1	210	1	0	212	677
12:45 PM	3	0	0	0	3	3	195	94	0	292	94	1	0	0	95	0	180	3	0	183	573
Total Volume	15	0	2	0	17	12	823	373	0	1208	358	1	0	1	360	1	752	12	0	765	2350
% App. Total	88.2	0	11.8	0		1	68.1	30.9	0		99.4	0.3	0	0.3		0.1	98.3	1.6	0		
PHF	.750	.000	.250	.000	.850	.600	.876	.840	.000	.865	.814	.250	.000	.250	.811	.250	.895	.600	.000	.902	.868



Connecticut Counts LLC

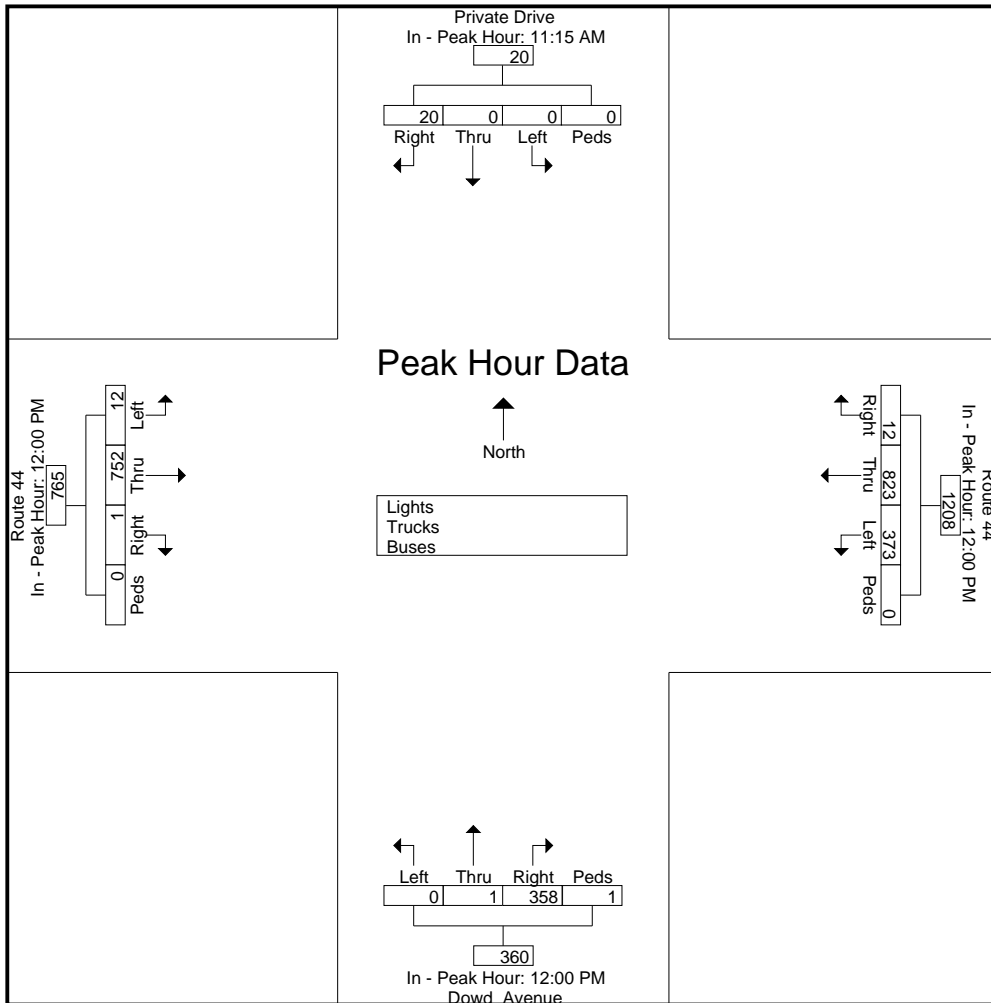
Kensington, Connecticut 06037
(860) 828-1693

File Name : 24636
Site Code : 24636
Start Date : 6/10/2023
Page No : 3

Start Time	Private Drive From North					Route 44 From East					Dowd Avenue From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

Peak Hour Analysis From 11:00 AM to 12:45 PM - Peak 1 of 1
Peak Hour for Each Approach Begins at:

	11:15 AM					12:00 PM					12:00 PM					12:00 PM				
+0 mins.	5	0	0	0	5	5	208	96	0	309	86	0	0	0	86	0	177	3	0	180
+15 mins.	6	0	0	0	6	1	185	72	0	258	68	0	0	0	68	0	185	5	0	190
+30 mins.	5	0	0	0	5	3	235	111	0	349	110	0	0	1	111	1	210	1	0	212
+45 mins.	4	0	0	0	4	3	195	94	0	292	94	1	0	0	95	0	180	3	0	183
Total Volume	20	0	0	0	20	12	823	373	0	1208	358	1	0	1	360	1	752	12	0	765
% App. Total	100	0	0	0		1	68.1	30.9	0		99.4	0.3	0	0.3		0.1	98.3	1.6	0	
PHF	.833	.000	.000	.000	.833	.600	.876	.840	.000	.865	.814	.250	.000	.250	.811	.250	.895	.600	.000	.902



Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Route 44 at Dowd Avenue
 Canton, Connecticut

File Name : 24635
 Site Code : 24635
 Start Date : 6/8/2023
 Page No : 1

Groups Printed- Lights - Trucks - Buses

Start Time	Private Drive From North					Route 44 From East					Dowd Avenue From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	
04:00 PM	1	0	0	0	1	2	215	102	0	319	86	0	0	0	86	1	176	0	0	177	583
04:15 PM	2	0	0	0	2	1	231	116	0	348	88	0	0	0	88	0	181	2	0	183	621
04:30 PM	7	0	0	0	7	1	197	123	0	321	96	0	0	0	96	0	196	1	0	197	621
04:45 PM	1	0	0	0	1	2	213	121	0	336	86	1	0	0	87	1	159	0	0	160	584
Total	11	0	0	0	11	6	856	462	0	1324	356	1	0	0	357	2	712	3	0	717	2409
05:00 PM	6	0	1	0	7	0	239	142	0	381	93	0	0	0	93	1	169	1	0	171	652
05:15 PM	0	0	0	0	0	0	233	114	0	347	91	0	0	0	91	1	170	0	0	171	609
05:30 PM	2	0	0	0	2	0	223	160	0	383	73	0	0	0	73	1	153	0	0	154	612
05:45 PM	1	0	0	0	1	1	204	114	0	319	60	0	0	0	60	0	157	1	0	158	538
Total	9	0	1	0	10	1	899	530	0	1430	317	0	0	0	317	3	649	2	0	654	2411
Grand Total	20	0	1	0	21	7	1755	992	0	2754	673	1	0	0	674	5	1361	5	0	1371	4820
Apprch %	95.2	0	4.8	0		0.3	63.7	36	0		99.9	0.1	0	0		0.4	99.3	0.4	0		
Total %	0.4	0	0	0	0.4	0.1	36.4	20.6	0	57.1	14	0	0	0	14	0.1	28.2	0.1	0	28.4	
Lights	18	0	1	0	19	7	1739										1343				
% Lights	90	0	100	0	90.5	100	99.1	98.8	0	99	98.2	100	0	0	98.2	80	98.7	100	0	98.6	98.7
Trucks	2	0	0	0	2	0	9	5	0	14	10	0	0	0	10	1	14	0	0	15	41
% Trucks	10	0	0	0	9.5	0	0.5	0.5	0	0.5	1.5	0	0	0	1.5	20	1	0	0	1.1	0.9
Buses	0	0	0	0	0	0	7	7	0	14	2	0	0	0	2	0	4	0	0	4	20
% Buses	0	0	0	0	0	0	0.4	0.7	0	0.5	0.3	0	0	0	0.3	0	0.3	0	0	0.3	0.4

Connecticut Counts LLC

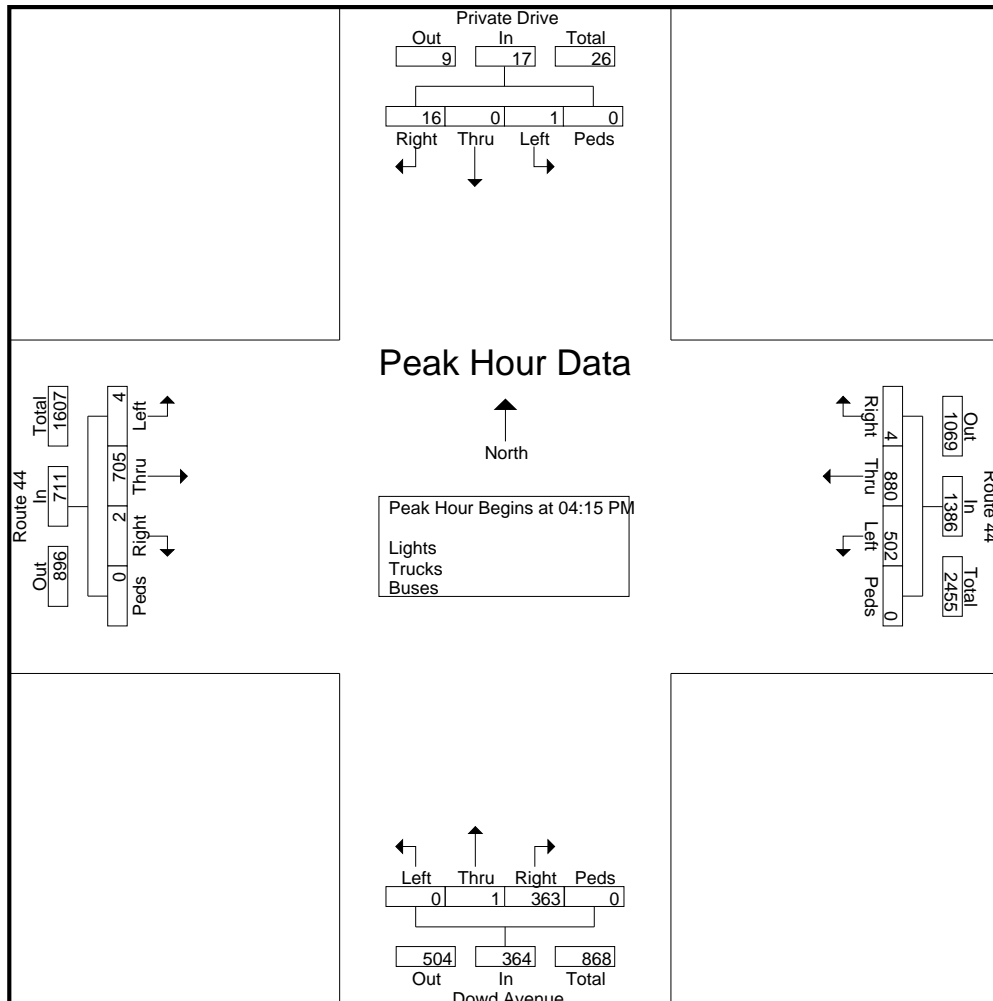
Kensington, Connecticut 06037
(860) 828-1693

File Name : 24635
Site Code : 24635
Start Date : 6/8/2023
Page No : 2

Start Time	Private Drive From North					Route 44 From East					Dowd Avenue From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1
Peak Hour for Entire Intersection Begins at 04:15 PM

04:15 PM	2	0	0	0	2	1	231	116	0	348	88	0	0	0	88	0	181	2	0	183	621
04:30 PM	7	0	0	0	7	1	197	123	0	321	96	0	0	0	96	0	196	1	0	197	621
04:45 PM	1	0	0	0	1	2	213	121	0	336	86	1	0	0	87	1	159	0	0	160	584
05:00 PM	6	0	1	0	7	0	239	142	0	381	93	0	0	0	93	1	169	1	0	171	652
Total Volume	16	0	1	0	17	4	880	502	0	1386	363	1	0	0	364	2	705	4	0	711	2478
% App. Total	94.1	0	5.9	0		0.3	63.5	36.2	0		99.7	0.3	0	0		0.3	99.2	0.6	0		
PHF	.571	.000	.250	.000	.607	.500	.921	.884	.000	.909	.945	.250	.000	.000	.948	.500	.899	.500	.000	.902	.950



Connecticut Counts LLC

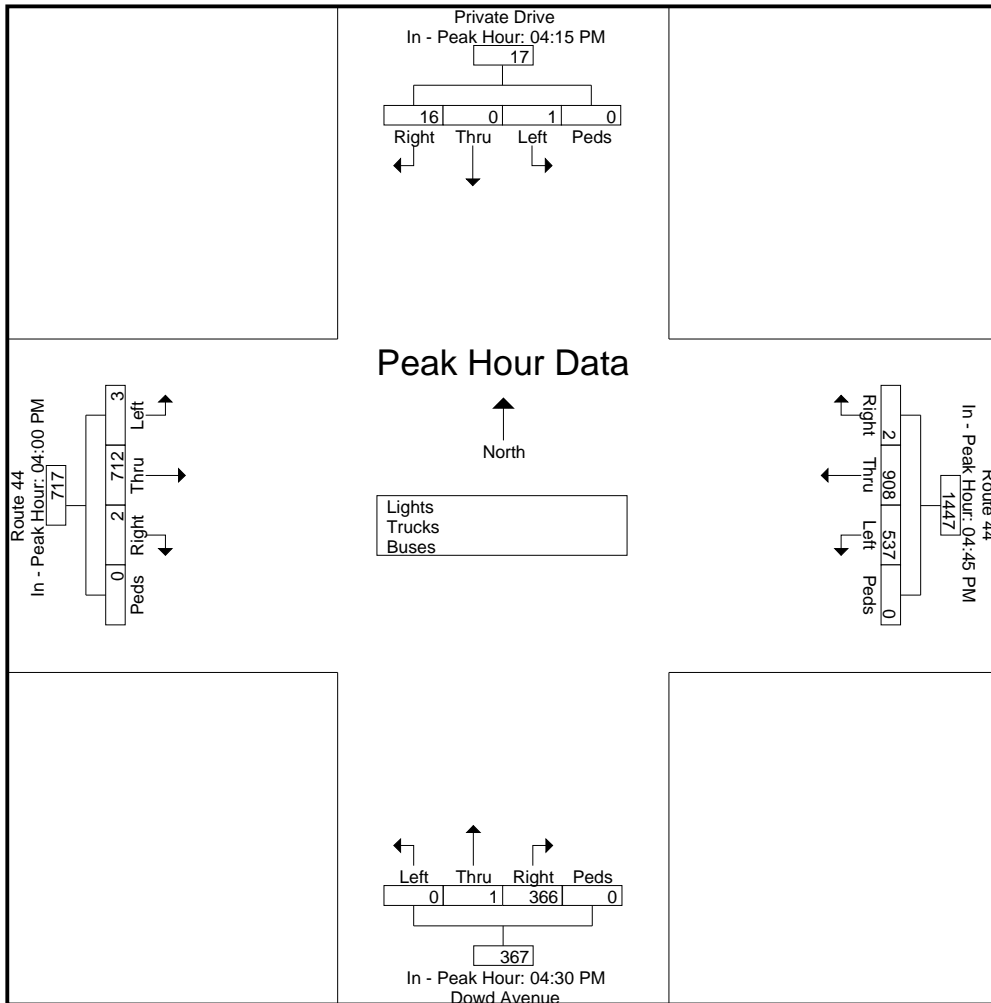
Kensington, Connecticut 06037
(860) 828-1693

File Name : 24635
Site Code : 24635
Start Date : 6/8/2023
Page No : 3

Start Time	Private Drive From North					Route 44 From East					Dowd Avenue From South					Route 44 From West					Int. Total
	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	Right	Thru	Left	Peds	App. Total	

Peak Hour Analysis From 04:00 PM to 05:45 PM - Peak 1 of 1
Peak Hour for Each Approach Begins at:

	04:15 PM					04:45 PM					04:30 PM					04:00 PM				
+0 mins.	2	0	0	0	2	2	213	121	0	336	96	0	0	0	96	1	176	0	0	177
+15 mins.	7	0	0	0	7	0	239	142	0	381	86	1	0	0	87	0	181	2	0	183
+30 mins.	1	0	0	0	1	0	233	114	0	347	93	0	0	0	93	0	196	1	0	197
+45 mins.	6	0	1	0	7	0	223	160	0	383	91	0	0	0	91	1	159	0	0	160
Total Volume	16	0	1	0	17	2	908	537	0	1447	366	1	0	0	367	2	712	3	0	717
% App. Total	94.1	0	5.9	0		0.1	62.8	37.1	0		99.7	0.3	0	0		0.3	99.3	0.4	0	
PHF	.571	.000	.250	.000	.607	.250	.950	.839	.000	.945	.953	.250	.000	.000	.956	.500	.908	.375	.000	.910



Route 44 at Lovely/Trailsend/Lawton - TMC

Thu Jun 8, 2023

Full Length (7 AM-9 AM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092404, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Lawton Road Southbound							Route 44 Westbound							Lovely Street Northbound							
	HR	R	T	L	U	App	Ped*	R	BR	T	L	U	App	Ped*	R	T	BL	L	U	App	Ped*	
Time																						
2023-06-08 7:00AM	0	30	4	13	0	47	0	4	0	80	1	0	85	0	23	12	0	16	0	51	1	
7:15AM	0	50	13	7	0	70	0	2	0	104	5	0	111	0	36	22	0	26	0	84	0	
7:30AM	0	34	11	7	0	52	0	4	1	100	13	0	118	0	35	22	0	26	0	83	0	
7:45AM	0	40	18	15	0	73	0	9	1	104	17	0	131	0	44	18	0	36	0	98	0	
Hourly Total	0	154	46	42	0	242	0	19	2	388	36	0	445	0	138	74	0	104	0	316	1	
8:00AM	0	19	14	14	0	47	0	3	0	84	14	0	101	0	39	21	0	22	0	82	0	
8:15AM	0	21	25	14	0	60	1	8	0	103	21	0	132	0	47	15	0	27	0	89	0	
8:30AM	1	22	18	18	0	59	0	4	0	123	13	0	140	0	40	15	0	32	0	87	0	
8:45AM	0	29	26	16	0	71	0	8	1	119	18	0	146	0	47	20	0	45	0	112	0	
Hourly Total	1	91	83	62	0	237	1	23	1	429	66	0	519	0	173	71	0	126	0	370	0	
Total	1	245	129	104	0	479	1	42	3	817	102	0	964	0	311	145	0	230	0	686	1	
% Approach	0.2%	51.1%	26.9%	21.7%	0%	-	-	4.4%	0.3%	84.8%	10.6%	0%	-	-	45.3%	21.1%	0%	33.5%	0%	-	-	
% Total	0%	5.4%	2.8%	2.3%	0%	10.6%	-	0.9%	0.1%	18.0%	2.2%	0%	21.2%	-	6.9%	3.2%	0%	5.1%	0%	15.1%	-	
Lights	1	240	127	104	0	472	-	40	3	761	97	0	901	-	303	142	0	224	0	669	-	
% Lights	100%	98.0%	98.4%	100%	0%	98.5%	-	95.2%	100%	93.1%	95.1%	0%	93.5%	-	97.4%	97.9%	0%	97.4%	0%	97.5%	-	
Articulated Trucks and Single-Unit Trucks	0	3	2	0	0	5	-	2	0	46	3	0	51	-	8	3	0	3	0	14	-	
% Articulated Trucks and Single-Unit Trucks	0%	1.2%	1.6%	0%	0%	1.0%	-	4.8%	0%	5.6%	2.9%	0%	5.3%	-	2.6%	2.1%	0%	1.3%	0%	2.0%	-	
Buses	0	2	0	0	0	2	-	0	0	10	2	0	12	-	0	0	0	3	0	3	-	
% Buses	0%	0.8%	0%	0%	0%	0.4%	-	0%	0%	1.2%	2.0%	0%	1.2%	-	0%	0%	0%	1.3%	0%	0.4%	-	
Pedestrians	-	-	-	-	-	-	1	-	-	-	-	-	0	-	-	-	-	-	-	0		
% Pedestrians	-	-	-	-	-	-	100%	-	-	-	-	-	0%	-	-	-	-	-	-	0%		
Bicycles on Crosswalk	-	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	-	1		
% Bicycles on Crosswalk	-	-	-	-	-	-	0%	-	-	-	-	-	0%	-	-	-	-	-	-	100%		

* Pedestrians and Bicycles on Crosswalk. BL: Bear left, BR: Bear right, HL: Hard left, HR: Hard right, L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Lovely/Trailsend/Lawton - TMC

Thu Jun 8, 2023

Full Length (7 AM-9 AM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092404, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Route 44 Eastbound							Trailsend Dr Southeastbound							Int
	T	L	HL	U	App	Ped*	HR	BR	BL	HL	U	App	Ped*		
2023-06-08 7:00AM	218	48	1	0	267	0	5	0	1	0	0	6	0	456	
7:15AM	273	48	0	0	321	0	8	0	0	1	0	9	0	595	
7:30AM	262	49	3	0	314	0	3	1	2	0	0	6	0	573	
7:45AM	274	44	1	0	319	0	3	0	2	1	0	6	0	627	
Hourly Total	1027	189	5	0	1221	0	19	1	5	2	0	27	0	2251	
8:00AM	264	35	0	0	299	2	0	1	2	1	0	4	1	533	
8:15AM	242	35	0	0	277	0	2	1	2	2	0	7	1	565	
8:30AM	252	42	0	0	294	0	1	2	0	0	0	3	0	583	
8:45AM	239	33	0	0	272	0	1	0	3	0	0	4	0	605	
Hourly Total	997	145	0	0	1142	2	4	4	7	3	0	18	2	2286	
Total	2024	334	5	0	2363	2	23	5	12	5	0	45	2	4537	
% Approach	85.7%	14.1%	0.2%	0%	-	-	51.1%	11.1%	26.7%	11.1%	0%	-	-	-	
% Total	44.6%	7.4%	0.1%	0%	52.1%	-	0.5%	0.1%	0.3%	0.1%	0%	1.0%	-	-	
Lights	1969	323	4	0	2296	-	20	5	12	5	0	42	-	4380	
% Lights	97.3%	96.7%	80.0%	0%	97.2%	-	87.0%	100%	100%	100%	0%	93.3%	-	96.5%	
Articulated Trucks and Single-Unit Trucks	38	4	0	0	42	-	0	0	0	0	0	0	-	112	
% Articulated Trucks and Single-Unit Trucks	1.9%	1.2%	0%	0%	1.8%	-	0%	0%	0%	0%	0%	0%	-	2.5%	
Buses	17	7	1	0	25	-	3	0	0	0	0	3	-	45	
% Buses	0.8%	2.1%	20.0%	0%	1.1%	-	13.0%	0%	0%	0%	0%	6.7%	-	1.0%	
Pedestrians	-	-	-	-	-	2	-	-	-	-	-	-	2	-	
% Pedestrians	-	-	-	-	-	100%	-	-	-	-	-	-	100%	-	
Bicycles on Crosswalk	-	-	-	-	-	0	-	-	-	-	-	-	0	-	
% Bicycles on Crosswalk	-	-	-	-	-	0%	-	-	-	-	-	-	0%	-	

* Pedestrians and Bicycles on Crosswalk. BL: Bear left, BR: Bear right, HL: Hard left, HR: Hard right, L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Lovely/Trailsend/Lawton - TMC

Provided by: Connecticut Counts LLC

Thu Jun 8, 2023

63 Sugar Maple Lane,

AM Peak (7:15 AM - 8:15 AM) - Overall Peak Hour

Kensington, CT, 12345, US

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092404, Location: 41.823949, -72.887559

Leg Direction	Lawton Road Southbound							Route 44 Westbound							Lovely Street Northbound							
	HR	R	T	L	U	App	Ped*	R	BR	T	L	U	App	Ped*	R	T	BL	L	U	App	Ped*	
Time	2023-06-08 7:15AM	0	50	13	7	0	70	0	2	0	104	5	0	111	0	36	22	0	26	0	84	0
	7:30AM	0	34	11	7	0	52	0	4	1	100	13	0	118	0	35	22	0	26	0	83	0
	7:45AM	0	40	18	15	0	73	0	9	1	104	17	0	131	0	44	18	0	36	0	98	0
	8:00AM	0	19	14	14	0	47	0	3	0	84	14	0	101	0	39	21	0	22	0	82	0
Total		0	143	56	43	0	242	0	18	2	392	49	0	461	0	154	83	0	110	0	347	0
% Approach		0%	59.1%	23.1%	17.8%	0%	-	-	3.9%	0.4%	85.0%	10.6%	0%	-	-	44.4%	23.9%	0%	31.7%	0%	-	-
% Total		0%	6.1%	2.4%	1.8%	0%	10.4%	-	0.8%	0.1%	16.8%	2.1%	0%	19.8%	-	6.6%	3.6%	0%	4.7%	0%	14.9%	-
PHF		-	0.715	0.778	0.717	-	0.829	-	0.500	0.500	0.942	0.721	-	0.880	-	0.875	0.943	-	0.764	-	0.885	-
Lights		0	139	55	43	0	237	-	17	2	362	46	0	427	-	149	83	0	107	0	339	-
% Lights		0%	97.2%	98.2%	100%	0%	97.9%	-	94.4%	100%	92.3%	93.9%	0%	92.6%	-	96.8%	100%	0%	97.3%	0%	97.7%	-
Articulated Trucks and Single-Unit Trucks		0	3	1	0	0	4	-	1	0	26	3	0	30	-	5	0	0	1	0	6	-
% Articulated Trucks and Single-Unit Trucks		0%	2.1%	1.8%	0%	0%	1.7%	-	5.6%	0%	6.6%	6.1%	0%	6.5%	-	3.2%	0%	0%	0.9%	0%	1.7%	-
Buses		0	1	0	0	0	1	-	0	0	4	0	0	4	-	0	0	0	2	0	2	-
% Buses		0%	0.7%	0%	0%	0%	0.4%	-	0%	0%	1.0%	0%	0%	0.9%	-	0%	0%	0%	1.8%	0%	0.6%	-
Pedestrians		-	-	-	-	-	0	-	-	-	-	-	-	0	-	-	-	-	-	-	-	0
% Pedestrians		-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Bicycles on Crosswalk		-	-	-	-	-	0	-	-	-	-	-	-	0	-	-	-	-	-	-	-	0
% Bicycles on Crosswalk		-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

* Pedestrians and Bicycles on Crosswalk. BL: Bear left, BR: Bear right, HL: Hard left, HR: Hard right, L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Lovely/Trailsend/Lawton - TMC

Thu Jun 8, 2023

AM Peak (7:15 AM - 8:15 AM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092404, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Route 44 Eastbound						Trailsend Dr Southeastbound						Int	
	T	L	HL	U	App	Ped*	HR	BR	BL	HL	U	App		Ped*
2023-06-08 7:15AM	273	48	0	0	321	0	8	0	0	1	0	9	0	595
7:30AM	262	49	3	0	314	0	3	1	2	0	0	6	0	573
7:45AM	274	44	1	0	319	0	3	0	2	1	0	6	0	627
8:00AM	264	35	0	0	299	2	0	1	2	1	0	4	1	533
Total	1073	176	4	0	1253	2	14	2	6	3	0	25	1	2328
% Approach	85.6%	14.0%	0.3%	0%	-	-	56.0%	8.0%	24.0%	12.0%	0%	-	-	-
% Total	46.1%	7.6%	0.2%	0%	53.8%	-	0.6%	0.1%	0.3%	0.1%	0%	1.1%	-	-
PHF	0.979	0.898	0.333	-	0.976	-	0.438	0.500	0.750	0.750	-	0.694	-	0.928
Lights	1040	170	3	0	1213	-	13	2	6	3	0	24	-	2240
% Lights	96.9%	96.6%	75.0%	0%	96.8%	-	92.9%	100%	100%	100%	0%	96.0%	-	96.2%
Articulated Trucks and Single-Unit Trucks	23	1	0	0	24	-	0	0	0	0	0	0	-	64
% Articulated Trucks and Single-Unit Trucks	2.1%	0.6%	0%	0%	1.9%	-	0%	0%	0%	0%	0%	0%	-	2.7%
Buses	10	5	1	0	16	-	1	0	0	0	0	1	-	24
% Buses	0.9%	2.8%	25.0%	0%	1.3%	-	7.1%	0%	0%	0%	0%	4.0%	-	1.0%
Pedestrians	-	-	-	-	-	2	-	-	-	-	-	-	1	-
% Pedestrians	-	-	-	-	-	100%	-	-	-	-	-	-	100%	-
Bicycles on Crosswalk	-	-	-	-	-	0	-	-	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	-	0%	-	-	-	-	-	-	0%	-

*Pedestrians and Bicycles on Crosswalk. BL: Bear left, BR: Bear right, HL: Hard left, HR: Hard right, L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Lovely/Trailsend/Lawton - TMC

Sat Jun 10, 2023

Full Length (11 AM-1 PM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092424, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Lawton Road Southbound							Route 44 Westbound							Lovely Street Northbound						
Time	HR	R	T	L	U	App	Ped*	R	BR	T	L	U	App	Ped*	R	T	BL	L	U	App	Ped*
2023-06-10 11:00AM	0	49	19	25	0	93	0	7	1	206	41	0	255	1	41	15	0	24	0	80	66
11:15AM	0	50	18	20	0	88	0	14	4	202	34	0	254	0	69	15	0	32	0	116	2
11:30AM	0	38	13	24	0	75	0	12	0	211	42	0	265	0	67	20	0	28	0	115	14
11:45AM	0	52	25	13	0	90	0	10	2	231	45	0	288	0	48	19	0	30	0	97	3
Hourly Total	0	189	75	82	0	346	0	43	7	850	162	0	1062	1	225	69	0	114	0	408	85
12:00PM	1	48	16	21	0	86	0	26	2	235	42	0	305	2	36	14	0	29	0	79	5
12:15PM	1	43	26	25	0	95	2	23	0	219	43	0	285	1	51	16	2	29	0	98	1
12:30PM	0	50	11	20	0	81	1	20	2	242	46	0	310	1	50	14	1	35	0	100	5
12:45PM	2	51	15	29	0	97	0	24	1	219	46	0	290	0	41	23	0	33	0	97	1
Hourly Total	4	192	68	95	0	359	3	93	5	915	177	0	1190	4	178	67	3	126	0	374	12
Total	4	381	143	177	0	705	3	136	12	1765	339	0	2252	5	403	136	3	240	0	782	97
% Approach	0.6%	54.0%	20.3%	25.1%	0%	-	-	6.0%	0.5%	78.4%	15.1%	0%	-	-	51.5%	17.4%	0.4%	30.7%	0%	-	-
% Total	0.1%	6.5%	2.4%	3.0%	0%	12.0%	-	2.3%	0.2%	30.0%	5.8%	0%	38.3%	-	6.9%	2.3%	0.1%	4.1%	0%	13.3%	-
Lights	4	378	142	177	0	701	-	135	11	1748	339	0	2233	-	401	136	3	240	0	780	-
% Lights	100%	99.2%	99.3%	100%	0%	99.4%	-	99.3%	91.7%	99.0%	100%	0%	99.2%	-	99.5%	100%	100%	100%	0%	99.7%	-
Articulated Trucks and Single-Unit Trucks	0	3	1	0	0	4	-	1	1	17	0	0	19	-	2	0	0	0	0	2	-
% Articulated Trucks and Single-Unit Trucks	0%	0.8%	0.7%	0%	0%	0.6%	-	0.7%	8.3%	1.0%	0%	0%	0.8%	-	0.5%	0%	0%	0%	0%	0.3%	-
Buses	0	0	0	0	0	0	-	0	0	0	0	0	0	-	0	0	0	0	0	0	-
% Buses	0%	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	0%	-
Pedestrians	-	-	-	-	-	-	1	-	-	-	-	-	-	1	-	-	-	-	-	-	91
% Pedestrians	-	-	-	-	-	-	33.3%	-	-	-	-	-	-	20.0%	-	-	-	-	-	-	93.8%
Bicycles on Crosswalk	-	-	-	-	-	-	2	-	-	-	-	-	-	4	-	-	-	-	-	-	6
% Bicycles on Crosswalk	-	-	-	-	-	-	66.7%	-	-	-	-	-	-	80.0%	-	-	-	-	-	-	6.2%

* Pedestrians and Bicycles on Crosswalk. BL: Bear left, BR: Bear right, HL: Hard left, HR: Hard right, L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Lovely/Trailsend/Lawton - TMC

Sat Jun 10, 2023

Full Length (11 AM-1 PM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092424, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Route 44 Eastbound						Trailsend Dr Southeastbound						Int	
	T	L	HL	U	App	Ped*	HR	BR	BL	HL	U	App		Ped*
2023-06-10 11:00AM	225	16	0	0	241	0	0	1	2	0	0	3	0	672
11:15AM	270	21	0	0	291	0	2	0	4	0	0	6	0	755
11:30AM	206	26	0	0	232	1	1	0	0	0	0	1	0	688
11:45AM	234	31	0	0	265	1	3	0	1	2	0	6	0	746
Hourly Total	935	94	0	0	1029	2	6	1	7	2	0	16	0	2861
12:00PM	253	31	0	0	284	0	0	1	0	0	0	1	0	755
12:15PM	206	24	1	0	231	4	3	0	2	2	0	7	0	716
12:30PM	235	43	3	0	281	0	2	0	1	0	0	3	0	775
12:45PM	245	27	2	0	274	0	3	0	5	1	0	9	0	767
Hourly Total	939	125	6	0	1070	4	8	1	8	3	0	20	0	3013
Total	1874	219	6	0	2099	6	14	2	15	5	0	36	0	5874
% Approach	89.3%	10.4%	0.3%	0%	-	-	38.9%	5.6%	41.7%	13.9%	0%	-	-	-
% Total	31.9%	3.7%	0.1%	0%	35.7%	-	0.2%	0%	0.3%	0.1%	0%	0.6%	-	-
Lights	1861	219	6	0	2086	-	14	2	15	5	0	36	-	5836
% Lights	99.3%	100%	100%	0%	99.4%	-	100%	100%	100%	100%	0%	100%	-	99.4%
Articulated Trucks and Single-Unit Trucks	12	0	0	0	12	-	0	0	0	0	0	0	-	37
% Articulated Trucks and Single-Unit Trucks	0.6%	0%	0%	0%	0.6%	-	0%	0%	0%	0%	0%	0%	-	0.6%
Buses	1	0	0	0	1	-	0	0	0	0	0	0	-	1
% Buses	0.1%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	0%	-	-	-	-	-	-	-	-
Bicycles on Crosswalk	-	-	-	-	-	6	-	-	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	-	100%	-	-	-	-	-	-	-	-

* Pedestrians and Bicycles on Crosswalk. BL: Bear left, BR: Bear right, HL: Hard left, HR: Hard right, L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Lovely/Trailsend/Lawton - TMC

Sat Jun 10, 2023

Full Length (11 AM-1 PM)

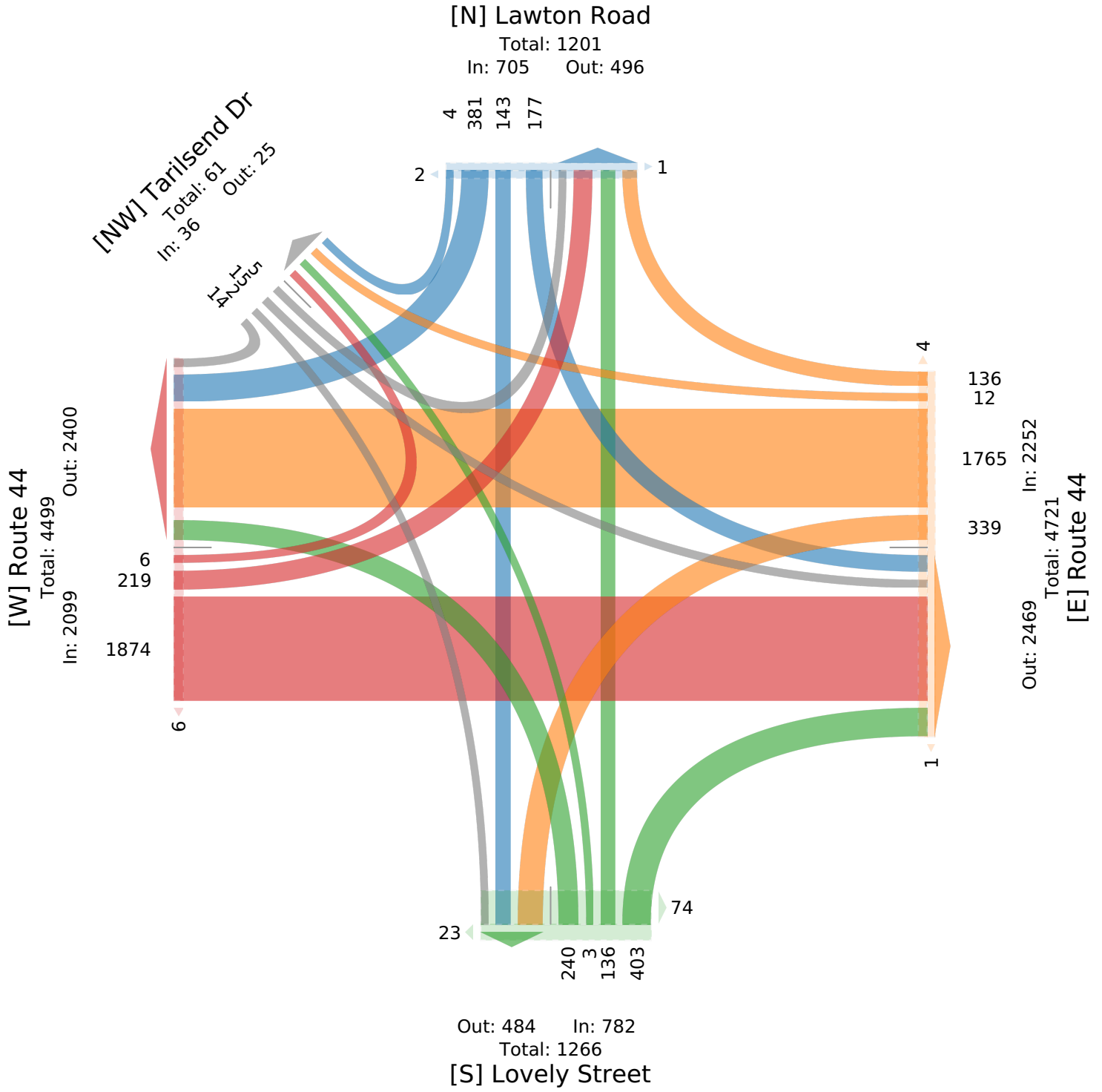
All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092424, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US



Route 44 at Lovely/Trailsend/Lawton - TMC

Sat Jun 10, 2023

Midday Peak (WKND) (12 PM - 1 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092424, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Lawton Road Southbound							Route 44 Westbound							Lovely Street Northbound						
	HR	R	T	L	U	App	Ped*	R	BR	T	L	U	App	Ped*	R	T	BL	L	U	App	Ped*
2023-06-10 12:00PM	1	48	16	21	0	86	0	26	2	235	42	0	305	2	36	14	0	29	0	79	5
12:15PM	1	43	26	25	0	95	2	23	0	219	43	0	285	1	51	16	2	29	0	98	1
12:30PM	0	50	11	20	0	81	1	20	2	242	46	0	310	1	50	14	1	35	0	100	5
12:45PM	2	51	15	29	0	97	0	24	1	219	46	0	290	0	41	23	0	33	0	97	1
Total	4	192	68	95	0	359	3	93	5	915	177	0	1190	4	178	67	3	126	0	374	12
% Approach	1.1%	53.5%	18.9%	26.5%	0%	-	-	7.8%	0.4%	76.9%	14.9%	0%	-	-	47.6%	17.9%	0.8%	33.7%	0%	-	-
% Total	0.1%	6.4%	2.3%	3.2%	0%	11.9%	-	3.1%	0.2%	30.4%	5.9%	0%	39.5%	-	5.9%	2.2%	0.1%	4.2%	0%	12.4%	-
PHF	0.500	0.941	0.654	0.819	-	0.925	-	0.894	0.625	0.945	0.962	-	0.960	-	0.873	0.728	0.375	0.900	-	0.935	-
Lights	4	191	67	95	0	357	-	93	5	909	177	0	1184	-	177	67	3	126	0	373	-
% Lights	100%	99.5%	98.5%	100%	0%	99.4%	-	100%	100%	99.3%	100%	0%	99.5%	-	99.4%	100%	100%	100%	0%	99.7%	-
Articulated Trucks and Single-Unit Trucks	0	1	1	0	0	2	-	0	0	6	0	0	6	-	1	0	0	0	0	1	-
% Articulated Trucks and Single-Unit Trucks	0%	0.5%	1.5%	0%	0%	0.6%	-	0%	0%	0.7%	0%	0%	0.5%	-	0.6%	0%	0%	0%	0%	0.3%	-
Buses	0	0	0	0	0	0	-	0	0	0	0	0	0	-	0	0	0	0	0	0	-
% Buses	0%	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	0%	-
Pedestrians	-	-	-	-	-	-	1	-	-	-	-	-	-	1	-	-	-	-	-	-	11
% Pedestrians	-	-	-	-	-	-	33.3%	-	-	-	-	-	-	25.0%	-	-	-	-	-	-	91.7%
Bicycles on Crosswalk	-	-	-	-	-	-	2	-	-	-	-	-	-	3	-	-	-	-	-	-	1
% Bicycles on Crosswalk	-	-	-	-	-	-	66.7%	-	-	-	-	-	-	75.0%	-	-	-	-	-	-	8.3%

*Pedestrians and Bicycles on Crosswalk. BL: Bear left, BR: Bear right, HL: Hard left, HR: Hard right, L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Lovely/Trailsend/Lawton - TMC

Sat Jun 10, 2023

Midday Peak (WKND) (12 PM - 1 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092424, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Route 44 Eastbound						Trailsend Dr Southeastbound						Int	
	T	L	HL	U	App	Ped*	HR	BR	BL	HL	U	App		Ped*
2023-06-10 12:00PM	253	31	0	0	284	0	0	1	0	0	0	1	0	755
12:15PM	206	24	1	0	231	4	3	0	2	2	0	7	0	716
12:30PM	235	43	3	0	281	0	2	0	1	0	0	3	0	775
12:45PM	245	27	2	0	274	0	3	0	5	1	0	9	0	767
Total	939	125	6	0	1070	4	8	1	8	3	0	20	0	3013
% Approach	87.8%	11.7%	0.6%	0%	-	-	40.0%	5.0%	40.0%	15.0%	0%	-	-	-
% Total	31.2%	4.1%	0.2%	0%	35.5%	-	0.3%	0%	0.3%	0.1%	0%	0.7%	-	-
PHF	0.928	0.727	0.500	-	0.942	-	0.667	0.250	0.400	0.375	-	0.556	-	0.972
Lights	932	125	6	0	1063	-	8	1	8	3	0	20	-	2997
% Lights	99.3%	100%	100%	0%	99.3%	-	100%	100%	100%	100%	0%	100%	-	99.5%
Articulated Trucks and Single-Unit Trucks	7	0	0	0	7	-	0	0	0	0	0	0	-	16
% Articulated Trucks and Single-Unit Trucks	0.7%	0%	0%	0%	0.7%	-	0%	0%	0%	0%	0%	0%	-	0.5%
Buses	0	0	0	0	0	-	0	0	0	0	0	0	-	0
% Buses	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	0%	-	-	-	-	-	-	-	-
Bicycles on Crosswalk	-	-	-	-	-	4	-	-	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	-	100%	-	-	-	-	-	-	-	-

*Pedestrians and Bicycles on Crosswalk. BL: Bear left, BR: Bear right, HL: Hard left, HR: Hard right, L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Lovely/Trailsend/Lawton - TMC

Sat Jun 10, 2023

Midday Peak (WKND) (12 PM - 1 PM) - Overall Peak Hour

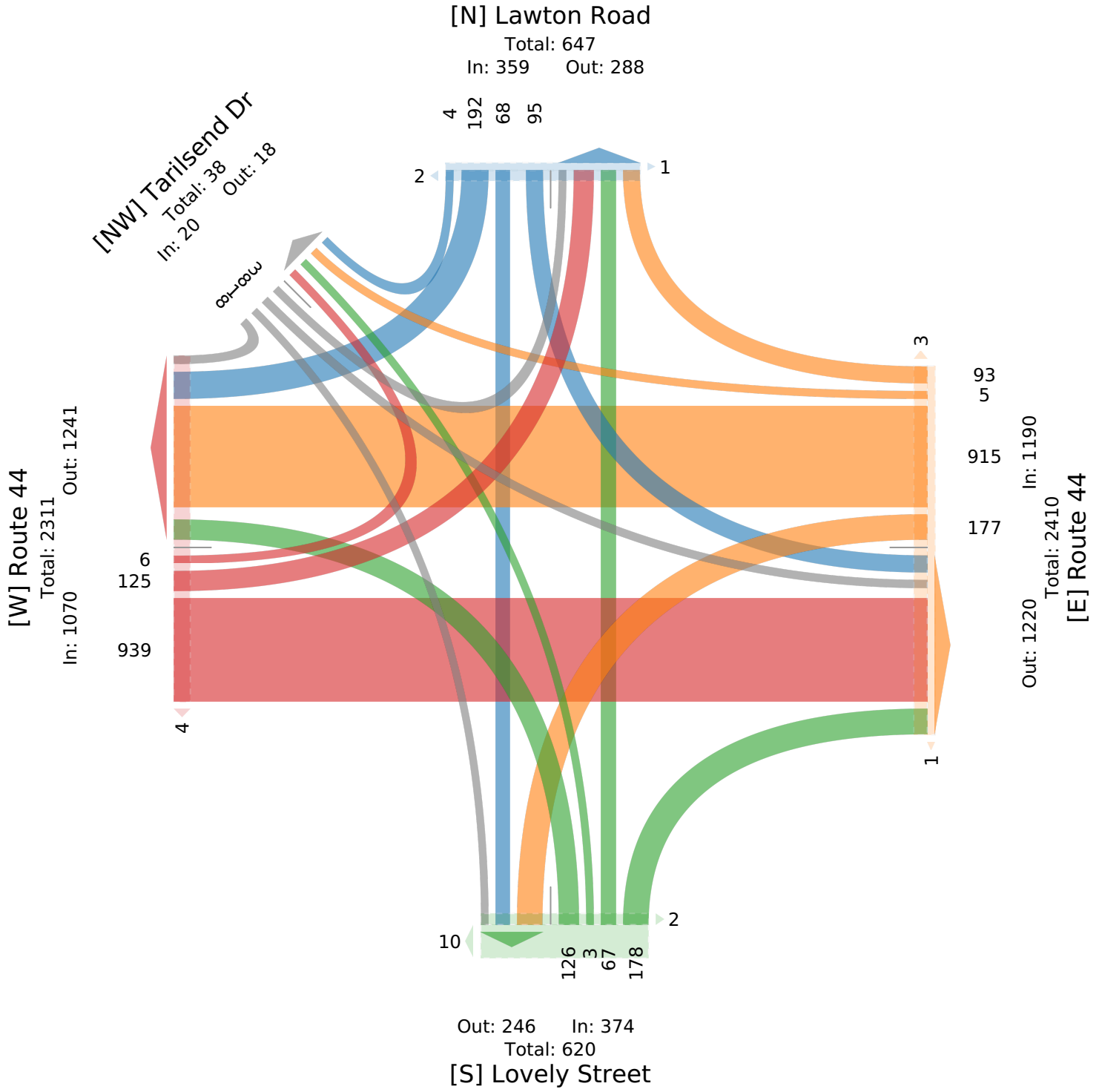
All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092424, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US



Route 44 at Lovely/Trailsend/Lawton - TMC

Thu Jun 8, 2023

Full Length (4 PM-6 PM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092416, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Lawton Road Southbound							Route 44 Westbound							Lovely Street Northbound						
	HR	R	T	L	U	App	Ped*	R	BR	T	L	U	App	Ped*	R	T	BL	L	U	App	Ped*
2023-06-08 4:00PM	2	50	23	21	0	96	0	16	3	318	50	0	387	0	45	19	0	41	0	105	1
4:15PM	1	85	30	18	0	134	0	15	1	270	39	0	325	0	35	17	0	35	0	87	1
4:30PM	0	76	30	17	0	123	0	21	3	295	40	0	359	0	55	23	0	51	0	129	0
4:45PM	2	64	26	16	0	108	1	23	5	284	36	0	348	0	49	17	0	46	0	112	0
Hourly Total	5	275	109	72	0	461	1	75	12	1167	165	0	1419	0	184	76	0	173	0	433	2
5:00PM	0	83	23	12	0	118	0	26	0	319	36	0	381	0	37	28	0	46	0	111	2
5:15PM	1	84	23	17	0	125	0	21	3	283	44	0	351	0	44	16	0	25	0	85	0
5:30PM	1	93	16	21	0	131	0	23	1	268	46	0	338	0	50	8	0	19	0	77	0
5:45PM	1	38	25	18	0	82	0	18	1	264	35	0	318	0	43	8	0	24	0	75	0
Hourly Total	3	298	87	68	0	456	0	88	5	1134	161	0	1388	0	174	60	0	114	0	348	2
Total	8	573	196	140	0	917	1	163	17	2301	326	0	2807	0	358	136	0	287	0	781	4
% Approach	0.9%	62.5%	21.4%	15.3%	0%	-	-	5.8%	0.6%	82.0%	11.6%	0%	-	-	45.8%	17.4%	0%	36.7%	0%	-	-
% Total	0.1%	8.9%	3.0%	2.2%	0%	14.2%	-	2.5%	0.3%	35.6%	5.0%	0%	43.4%	-	5.5%	2.1%	0%	4.4%	0%	12.1%	-
Lights	8	567	193	140	0	908	-	162	16	2271	325	0	2774	-	357	134	0	284	0	775	-
% Lights	100%	99.0%	98.5%	100%	0%	99.0%	-	99.4%	94.1%	98.7%	99.7%	0%	98.8%	-	99.7%	98.5%	0%	99.0%	0%	99.2%	-
Articulated Trucks and Single-Unit Trucks	0	1	1	0	0	2	-	0	0	8	1	0	9	-	1	2	0	0	0	3	-
% Articulated Trucks and Single-Unit Trucks	0%	0.2%	0.5%	0%	0%	0.2%	-	0%	0%	0.3%	0.3%	0%	0.3%	-	0.3%	1.5%	0%	0%	0%	0.4%	-
Buses	0	5	2	0	0	7	-	1	1	22	0	0	24	-	0	0	0	3	0	3	-
% Buses	0%	0.9%	1.0%	0%	0%	0.8%	-	0.6%	5.9%	1.0%	0%	0%	0.9%	-	0%	0%	0%	1.0%	0%	0.4%	-
Pedestrians	-	-	-	-	-	-	0	-	-	-	-	-	-	0	-	-	-	-	-	-	2
% Pedestrians	-	-	-	-	-	-	0%	-	-	-	-	-	-	-	-	-	-	-	-	-	50.0%
Bicycles on Crosswalk	-	-	-	-	-	-	1	-	-	-	-	-	-	0	-	-	-	-	-	-	2
% Bicycles on Crosswalk	-	-	-	-	-	-	100%	-	-	-	-	-	-	-	-	-	-	-	-	-	50.0%

* Pedestrians and Bicycles on Crosswalk. BL: Bear left, BR: Bear right, HL: Hard left, HR: Hard right, L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Lovely/Trailsend/Lawton - TMC

Thu Jun 8, 2023

Full Length (4 PM-6 PM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092416, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Route 44 Eastbound						Trailsend Dr Southeastbound							
Time	T	L	HL	U	App	Ped*	HR	BR	BL	HL	U	App	Ped*	Int
2023-06-08 4:00PM	194	48	3	0	245	0	0	0	0	0	0	0	0	833
4:15PM	198	45	1	0	244	0	2	2	1	0	0	5	0	795
4:30PM	200	42	3	0	245	0	0	1	0	0	0	1	0	857
4:45PM	220	43	0	0	263	0	2	0	0	0	0	2	0	833
Hourly Total	812	178	7	0	997	0	4	3	1	0	0	8	0	3318
5:00PM	225	43	2	0	270	1	1	1	1	0	0	3	0	883
5:15PM	207	47	1	0	255	0	0	0	1	0	0	1	0	817
5:30PM	173	36	0	0	209	0	2	0	0	0	0	2	0	757
5:45PM	182	22	5	0	209	0	3	2	4	0	0	9	0	693
Hourly Total	787	148	8	0	943	1	6	3	6	0	0	15	0	3150
Total	1599	326	15	0	1940	1	10	6	7	0	0	23	0	6468
% Approach	82.4%	16.8%	0.8%	0%	-	-	43.5%	26.1%	30.4%	0%	0%	-	-	-
% Total	24.7%	5.0%	0.2%	0%	30.0%	-	0.2%	0.1%	0.1%	0%	0%	0.4%	-	-
Lights	1567	321	15	0	1903	-	10	6	7	0	0	23	-	6383
% Lights	98.0%	98.5%	100%	0%	98.1%	-	100%	100%	100%	0%	0%	100%	-	98.7%
Articulated Trucks and Single-Unit Trucks	28	2	0	0	30	-	0	0	0	0	0	0	-	44
% Articulated Trucks and Single-Unit Trucks	1.8%	0.6%	0%	0%	1.5%	-	0%	0%	0%	0%	0%	0%	-	0.7%
Buses	4	3	0	0	7	-	0	0	0	0	0	0	-	41
% Buses	0.3%	0.9%	0%	0%	0.4%	-	0%	0%	0%	0%	0%	0%	-	0.6%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	0%	-	-	-	-	-	-	-	-
Bicycles on Crosswalk	-	-	-	-	-	1	-	-	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	-	100%	-	-	-	-	-	-	-	-

* Pedestrians and Bicycles on Crosswalk. BL: Bear left, BR: Bear right, HL: Hard left, HR: Hard right, L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Lovely/Trailsend/Lawton - TMC

Thu Jun 8, 2023

Full Length (4 PM-6 PM)

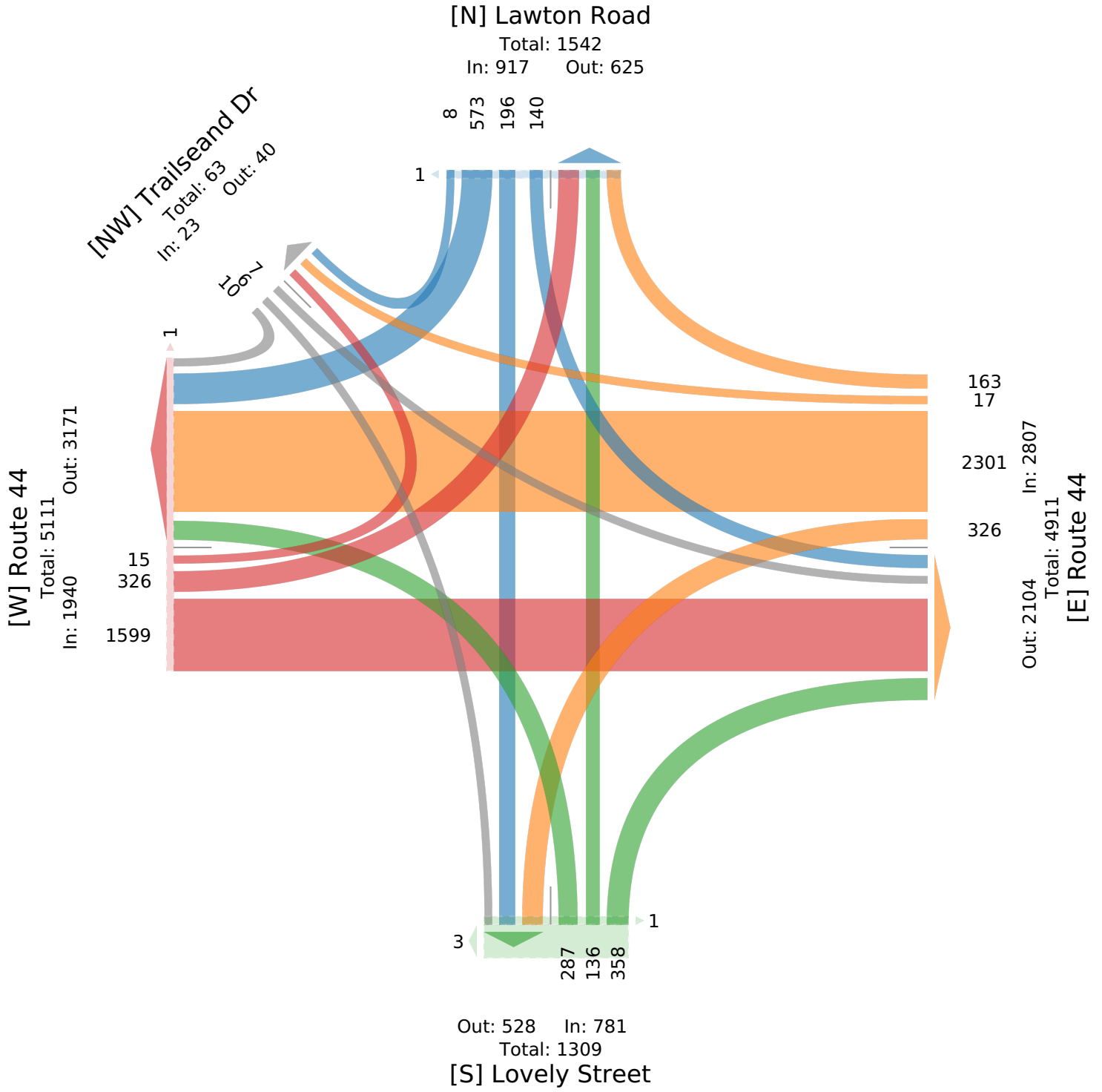
All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092416, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US



Route 44 at Lovely/Trailsend/Lawton - TMC

Thu Jun 8, 2023

PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092416, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Lawton Road Southbound							Route 44 Westbound							Lovely Street Northbound							
	HR	R	T	L	U	App	Ped*	R	BR	T	L	U	App	Ped*	R	T	BL	L	U	App	Ped*	
Time																						
2023-06-08 4:30PM	0	76	30	17	0	123	0	21	3	295	40	0	359	0	55	23	0	51	0	129	0	
4:45PM	2	64	26	16	0	108	1	23	5	284	36	0	348	0	49	17	0	46	0	112	0	
5:00PM	0	83	23	12	0	118	0	26	0	319	36	0	381	0	37	28	0	46	0	111	2	
5:15PM	1	84	23	17	0	125	0	21	3	283	44	0	351	0	44	16	0	25	0	85	0	
Total	3	307	102	62	0	474	1	91	11	1181	156	0	1439	0	185	84	0	168	0	437	2	
% Approach	0.6%	64.8%	21.5%	13.1%	0%	-	-	6.3%	0.8%	82.1%	10.8%	0%	-	-	42.3%	19.2%	0%	38.4%	0%	-	-	
% Total	0.1%	9.1%	3.0%	1.8%	0%	14.0%	-	2.7%	0.3%	34.8%	4.6%	0%	42.4%	-	5.5%	2.5%	0%	5.0%	0%	12.9%	-	
PHF	0.375	0.914	0.850	0.912	-	0.948	-	0.875	0.550	0.926	0.886	-	0.944	-	0.841	0.750	-	0.824	-	0.847	-	
Lights	3	304	101	62	0	470	-	90	11	1162	156	0	1419	-	185	83	0	167	0	435	-	
% Lights	100%	99.0%	99.0%	100%	0%	99.2%	-	98.9%	100%	98.4%	100%	0%	98.6%	-	100%	98.8%	0%	99.4%	0%	99.5%	-	
Articulated Trucks and Single-Unit Trucks	0	0	1	0	0	1	-	0	0	4	0	0	4	-	0	1	0	0	0	1	-	
% Articulated Trucks and Single-Unit Trucks	0%	0%	1.0%	0%	0%	0.2%	-	0%	0%	0.3%	0%	0%	0.3%	-	0%	1.2%	0%	0%	0%	0.2%	-	
Buses	0	3	0	0	0	3	-	1	0	15	0	0	16	-	0	0	0	1	0	1	-	
% Buses	0%	1.0%	0%	0%	0%	0.6%	-	1.1%	0%	1.3%	0%	0%	1.1%	-	0%	0%	0%	0.6%	0%	0.2%	-	
Pedestrians	-	-	-	-	-	-	0	-	-	-	-	-	-	0	-	-	-	-	-	-	2	
% Pedestrians	-	-	-	-	-	-	0%	-	-	-	-	-	-	-	-	-	-	-	-	-	100%	
Bicycles on Crosswalk	-	-	-	-	-	-	1	-	-	-	-	-	-	0	-	-	-	-	-	-	0	
% Bicycles on Crosswalk	-	-	-	-	-	-	100%	-	-	-	-	-	-	-	-	-	-	-	-	-	0%	

*Pedestrians and Bicycles on Crosswalk. BL: Bear left, BR: Bear right, HL: Hard left, HR: Hard right, L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Lovely/Trailsend/Lawton - TMC

Thu Jun 8, 2023

PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092416, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Route 44 Eastbound						Trailsend Dr Southeastbound						Int	
	T	L	HL	U	App	Ped*	HR	BR	BL	HL	U	App		Ped*
2023-06-08 4:30PM	200	42	3	0	245	0	0	1	0	0	0	1	0	857
4:45PM	220	43	0	0	263	0	2	0	0	0	0	2	0	833
5:00PM	225	43	2	0	270	1	1	1	1	0	0	3	0	883
5:15PM	207	47	1	0	255	0	0	0	1	0	0	1	0	817
Total	852	175	6	0	1033	1	3	2	2	0	0	7	0	3390
% Approach	82.5%	16.9%	0.6%	0%	-	-	42.9%	28.6%	28.6%	0%	0%	-	-	-
% Total	25.1%	5.2%	0.2%	0%	30.5%	-	0.1%	0.1%	0.1%	0%	0%	0.2%	-	-
PHF	0.947	0.931	0.500	-	0.956	-	0.375	0.500	0.500	-	-	0.583	-	0.960
Lights	833	170	6	0	1009	-	3	2	2	0	0	7	-	3340
% Lights	97.8%	97.1%	100%	0%	97.7%	-	100%	100%	100%	0%	0%	100%	-	98.5%
Articulated Trucks and Single-Unit Trucks	18	2	0	0	20	-	0	0	0	0	0	0	-	26
% Articulated Trucks and Single-Unit Trucks	2.1%	1.1%	0%	0%	1.9%	-	0%	0%	0%	0%	0%	0%	-	0.8%
Buses	1	3	0	0	4	-	0	0	0	0	0	0	-	24
% Buses	0.1%	1.7%	0%	0%	0.4%	-	0%	0%	0%	0%	0%	0%	-	0.7%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	0%	-	-	-	-	-	-	-	-
Bicycles on Crosswalk	-	-	-	-	-	1	-	-	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	-	100%	-	-	-	-	-	-	-	-

*Pedestrians and Bicycles on Crosswalk. BL: Bear left, BR: Bear right, HL: Hard left, HR: Hard right, L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Lovely/Trailsend/Lawton - TMC

Thu Jun 8, 2023

PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour

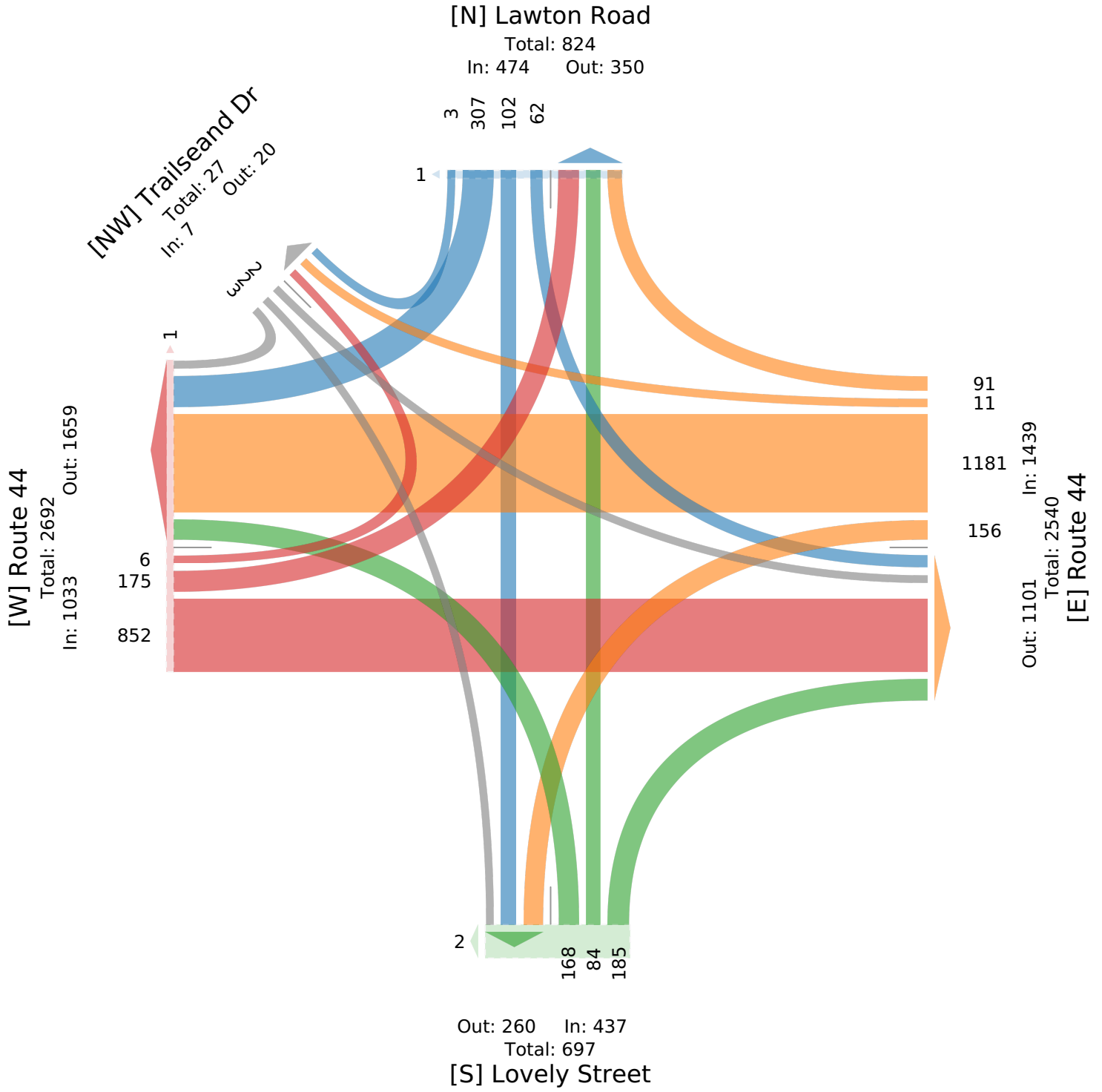
All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092416, Location: 41.823949, -72.887559

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US



Route 44 at CVS Drive - TMC

Thu Jun 8, 2023

Full Length (7 AM-9 AM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092493, Location: 41.824233, -72.884906

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	CVS Drive Southbound					Route 44 Westbound					Route 44 Eastbound					Int
	R	L	U	App	Ped*	R	T	U	App	Ped*	T	L	U	App	Ped*	
Time																
2023-06-08 7:00AM	0	0	0	0	0	0	87	0	87	0	234	1	0	235	0	322
7:15AM	1	2	0	3	0	2	111	0	113	0	289	4	0	293	0	409
7:30AM	0	2	0	2	1	1	123	0	124	0	304	2	0	306	0	432
7:45AM	0	0	0	0	0	5	143	0	148	0	309	1	0	310	0	458
Hourly Total	1	4	0	5	1	8	464	0	472	0	1136	8	0	1144	0	1621
8:00AM	1	1	0	2	0	1	132	0	133	0	302	2	0	304	0	439
8:15AM	1	5	0	6	1	6	154	0	160	0	316	3	1	320	0	486
8:30AM	1	2	0	3	0	5	162	0	167	0	287	6	0	293	0	463
8:45AM	1	6	0	7	0	8	174	0	182	0	292	6	0	298	0	487
Hourly Total	4	14	0	18	1	20	622	0	642	0	1197	17	1	1215	0	1875
Total	5	18	0	23	2	28	1086	0	1114	0	2333	25	1	2359	0	3496
% Approach	21.7%	78.3%	0%	-	-	2.5%	97.5%	0%	-	-	98.9%	1.1%	0%	-	-	-
% Total	0.1%	0.5%	0%	0.7%	-	0.8%	31.1%	0%	31.9%	-	66.7%	0.7%	0%	67.5%	-	-
Lights	5	17	0	22	-	26	1018	0	1044	-	2260	24	1	2285	-	3351
% Lights	100%	94.4%	0%	95.7%	-	92.9%	93.7%	0%	93.7%	-	96.9%	96.0%	100%	96.9%	-	95.9%
Articulated Trucks and Single-Unit Trucks	0	1	0	1	-	2	56	0	58	-	55	1	0	56	-	115
% Articulated Trucks and Single-Unit Trucks	0%	5.6%	0%	4.3%	-	7.1%	5.2%	0%	5.2%	-	2.4%	4.0%	0%	2.4%	-	3.3%
Buses	0	0	0	0	-	0	12	0	12	-	18	0	0	18	-	30
% Buses	0%	0%	0%	0%	-	0%	1.1%	0%	1.1%	-	0.8%	0%	0%	0.8%	-	0.9%
Pedestrians	-	-	-	-	1	-	-	-	-	0	-	-	-	-	0	-
% Pedestrians	-	-	-	-	50.0%	-	-	-	-	-	-	-	-	-	-	-
Bicycles on Crosswalk	-	-	-	-	1	-	-	-	-	0	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	50.0%	-	-	-	-	-	-	-	-	-	-	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at CVS Drive - TMC

Thu Jun 8, 2023

Full Length (7 AM-9 AM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092493, Location: 41.824233, -72.884906

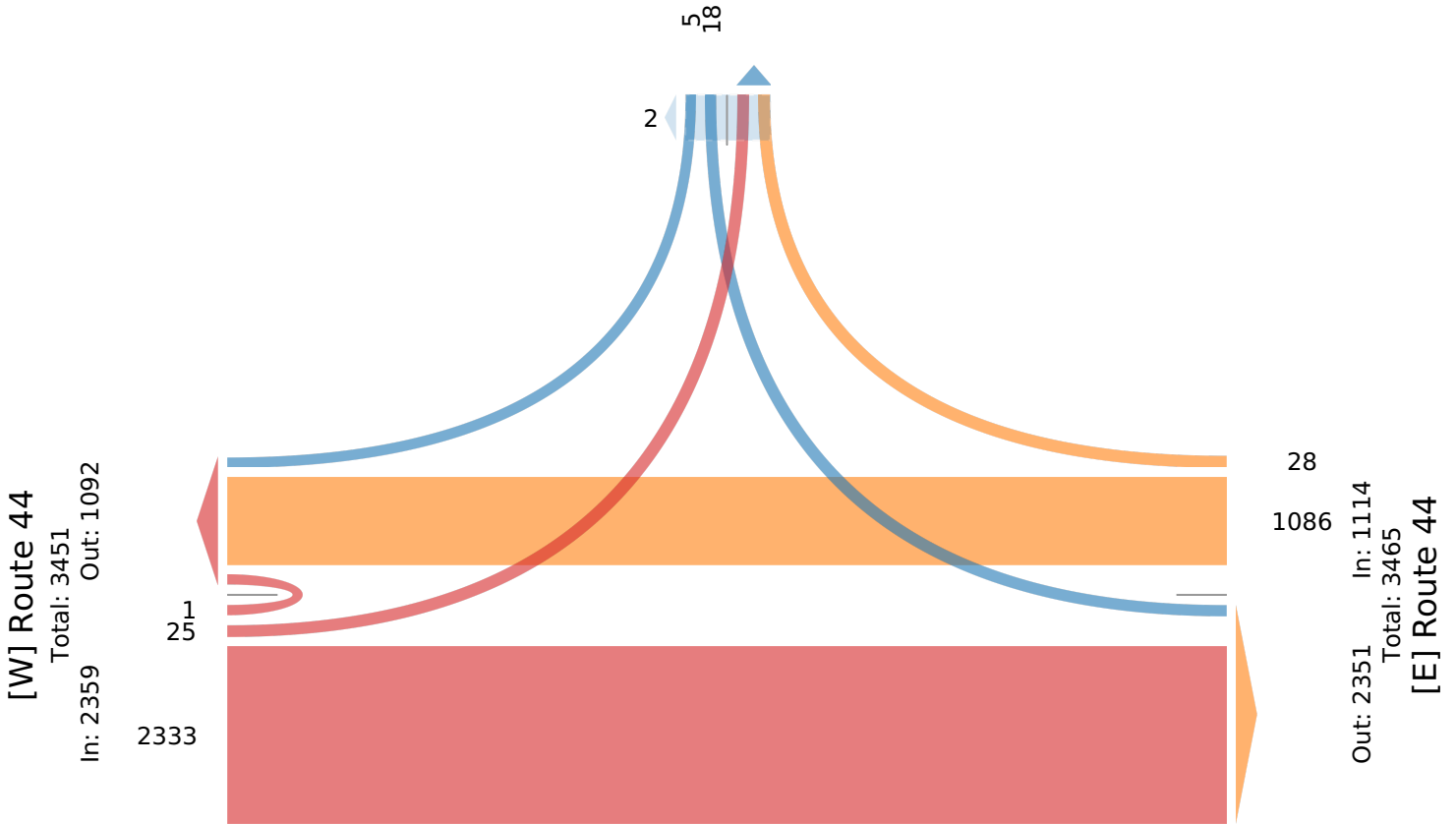
Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

[N] CVS Drive

Total: 76

In: 23 Out: 53



Route 44 at CVS Drive - TMC

Thu Jun 8, 2023

AM Peak (8 AM - 9 AM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092493, Location: 41.824233, -72.884906

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,

Kensington, CT, 12345, US

Leg Direction	CVS Drive Southbound					Route 44 Westbound					Route 44 Eastbound					Int
	R	L	U	App	Ped*	R	T	U	App	Ped*	T	L	U	App	Ped*	
Time																
2023-06-08 8:00AM	1	1	0	2	0	1	132	0	133	0	302	2	0	304	0	439
8:15AM	1	5	0	6	1	6	154	0	160	0	316	3	1	320	0	486
8:30AM	1	2	0	3	0	5	162	0	167	0	287	6	0	293	0	463
8:45AM	1	6	0	7	0	8	174	0	182	0	292	6	0	298	0	487
Total	4	14	0	18	1	20	622	0	642	0	1197	17	1	1215	0	1875
% Approach	22.2%	77.8%	0%	-	-	3.1%	96.9%	0%	-	-	98.5%	1.4%	0.1%	-	-	-
% Total	0.2%	0.7%	0%	1.0%	-	1.1%	33.2%	0%	34.2%	-	63.8%	0.9%	0.1%	64.8%	-	-
PHF	1.000	0.583	-	0.643	-	0.625	0.894	-	0.882	-	0.947	0.708	0.250	0.949	-	0.963
Lights	4	13	0	17	-	18	592	0	610	-	1154	16	1	1171	-	1798
% Lights	100%	92.9%	0%	94.4%	-	90.0%	95.2%	0%	95.0%	-	96.4%	94.1%	100%	96.4%	-	95.9%
Articulated Trucks and Single-Unit Trucks	0	1	0	1	-	2	23	0	25	-	36	1	0	37	-	63
% Articulated Trucks and Single-Unit Trucks	0%	7.1%	0%	5.6%	-	10.0%	3.7%	0%	3.9%	-	3.0%	5.9%	0%	3.0%	-	3.4%
Buses	0	0	0	0	-	0	7	0	7	-	7	0	0	7	-	14
% Buses	0%	0%	0%	0%	-	0%	1.1%	0%	1.1%	-	0.6%	0%	0%	0.6%	-	0.7%
Pedestrians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	-
% Pedestrians	-	-	-	-	0%	-	-	-	-	-	-	-	-	-	-	-
Bicycles on Crosswalk	-	-	-	-	1	-	-	-	-	0	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	100%	-	-	-	-	-	-	-	-	-	-	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at CVS Drive - TMC

Thu Jun 8, 2023

AM Peak (8 AM - 9 AM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092493, Location: 41.824233, -72.884906

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

[N] CVS Drive

Total: 55

In: 18 Out: 37

4
14

1

[W] Route 44

Total: 1842

In: 1215

Out: 627

1
17

1197

20

622

Out: 1211

In: 642

Total: 1853

[E] Route 44

Route 44 at CVS Drive - TMC

Sat Jun 10, 2023

Full Length (11 AM-1 PM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092500, Location: 41.824233, -72.884906

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	CVS Drive Southbound					Route 44 Westbound					Route 44 Eastbound					Int
	R	L	U	App	Ped*	R	T	U	App	Ped*	T	L	U	App	Ped*	
Time																
2023-06-10 11:00AM	10	10	0	20	0	17	251	0	268	0	311	12	0	323	0	611
11:15AM	13	14	0	27	0	17	236	0	253	0	315	8	1	324	0	604
11:30AM	11	11	0	22	0	26	259	0	285	0	294	10	0	304	0	611
11:45AM	11	17	0	28	0	15	270	0	285	0	303	7	0	310	0	623
Hourly Total	45	52	0	97	0	75	1016	0	1091	0	1223	37	1	1261	0	2449
12:00PM	12	14	0	26	0	26	296	0	322	0	301	6	0	307	0	655
12:15PM	11	7	0	18	0	23	327	0	350	0	268	10	0	278	0	646
12:30PM	6	10	0	16	0	17	299	0	316	0	282	12	0	294	0	626
12:45PM	13	17	0	30	0	27	313	0	340	0	278	13	0	291	0	661
Hourly Total	42	48	0	90	0	93	1235	0	1328	0	1129	41	0	1170	0	2588
Total	87	100	0	187	0	168	2251	0	2419	0	2352	78	1	2431	0	5037
% Approach	46.5%	53.5%	0%	-	-	6.9%	93.1%	0%	-	-	96.8%	3.2%	0%	-	-	-
% Total	1.7%	2.0%	0%	3.7%	-	3.3%	44.7%	0%	48.0%	-	46.7%	1.5%	0%	48.3%	-	-
Lights	87	100	0	187	-	167	2228	0	2395	-	2334	78	1	2413	-	4995
% Lights	100%	100%	0%	100%	-	99.4%	99.0%	0%	99.0%	-	99.2%	100%	100%	99.3%	-	99.2%
Articulated Trucks and Single-Unit Trucks	0	0	0	0	-	1	21	0	22	-	17	0	0	17	-	39
% Articulated Trucks and Single-Unit Trucks	0%	0%	0%	0%	-	0.6%	0.9%	0%	0.9%	-	0.7%	0%	0%	0.7%	-	0.8%
Buses	0	0	0	0	-	0	2	0	2	-	1	0	0	1	-	3
% Buses	0%	0%	0%	0%	-	0%	0.1%	0%	0.1%	-	0%	0%	0%	0%	-	0.1%
Pedestrians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

* Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at CVS Drive - TMC

Sat Jun 10, 2023

Full Length (11 AM-1 PM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092500, Location: 41.824233, -72.884906

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

[N] CVS Drive

Total: 433

In: 187 Out: 246

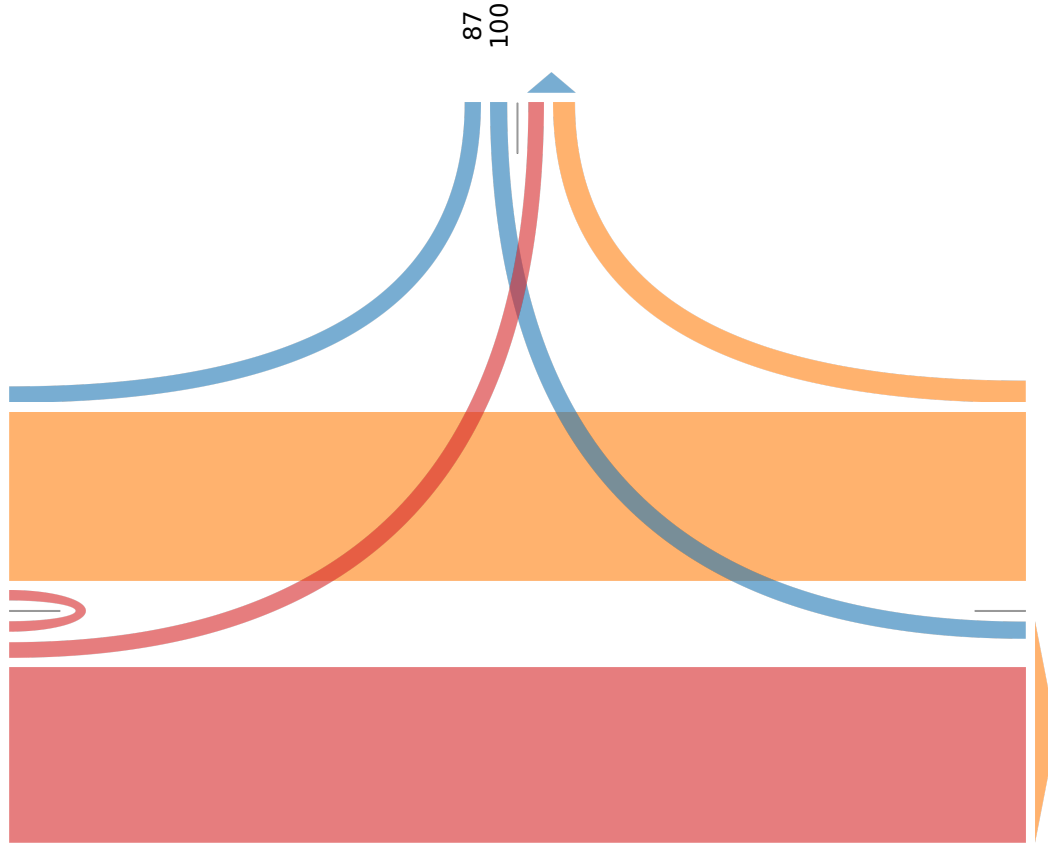
87
100

[W] Route 44
Total: 4770
In: 2431 Out: 2339

1
78
2352

168
2251

Out: 2452 In: 2419
Total: 4871
[E] Route 44



Route 44 at CVS Drive - TMC

Sat Jun 10, 2023

Midday Peak (WKND) (12 PM - 1 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092500, Location: 41.824233, -72.884906

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	CVS Drive Southbound					Route 44 Westbound					Route 44 Eastbound					Int
	R	L	U	App	Ped*	R	T	U	App	Ped*	T	L	U	App	Ped*	
Time																
2023-06-10 12:00PM	12	14	0	26	0	26	296	0	322	0	301	6	0	307	0	655
12:15PM	11	7	0	18	0	23	327	0	350	0	268	10	0	278	0	646
12:30PM	6	10	0	16	0	17	299	0	316	0	282	12	0	294	0	626
12:45PM	13	17	0	30	0	27	313	0	340	0	278	13	0	291	0	661
Total	42	48	0	90	0	93	1235	0	1328	0	1129	41	0	1170	0	2588
% Approach	46.7%	53.3%	0%	-	-	7.0%	93.0%	0%	-	-	96.5%	3.5%	0%	-	-	-
% Total	1.6%	1.9%	0%	3.5%	-	3.6%	47.7%	0%	51.3%	-	43.6%	1.6%	0%	45.2%	-	-
PHF	0.808	0.706	-	0.750	-	0.861	0.944	-	0.949	-	0.938	0.788	-	0.953	-	0.979
Lights	42	48	0	90	-	92	1225	0	1317	-	1118	41	0	1159	-	2566
% Lights	100%	100%	0%	100%	-	98.9%	99.2%	0%	99.2%	-	99.0%	100%	0%	99.1%	-	99.1%
Articulated Trucks and Single-Unit Trucks	0	0	0	0	-	1	9	0	10	-	11	0	0	11	-	21
% Articulated Trucks and Single-Unit Trucks	0%	0%	0%	0%	-	1.1%	0.7%	0%	0.8%	-	1.0%	0%	0%	0.9%	-	0.8%
Buses	0	0	0	0	-	0	1	0	1	-	0	0	0	0	-	1
% Buses	0%	0%	0%	0%	-	0%	0.1%	0%	0.1%	-	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at CVS Drive - TMC

Sat Jun 10, 2023

Midday Peak (WKND) (12 PM - 1 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092500, Location: 41.824233, -72.884906

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

[N] CVS Drive

Total: 224

In: 90 Out: 134

42
48

[W] Route 44

Total: 2447

In: 1170 Out: 1277

41

1129

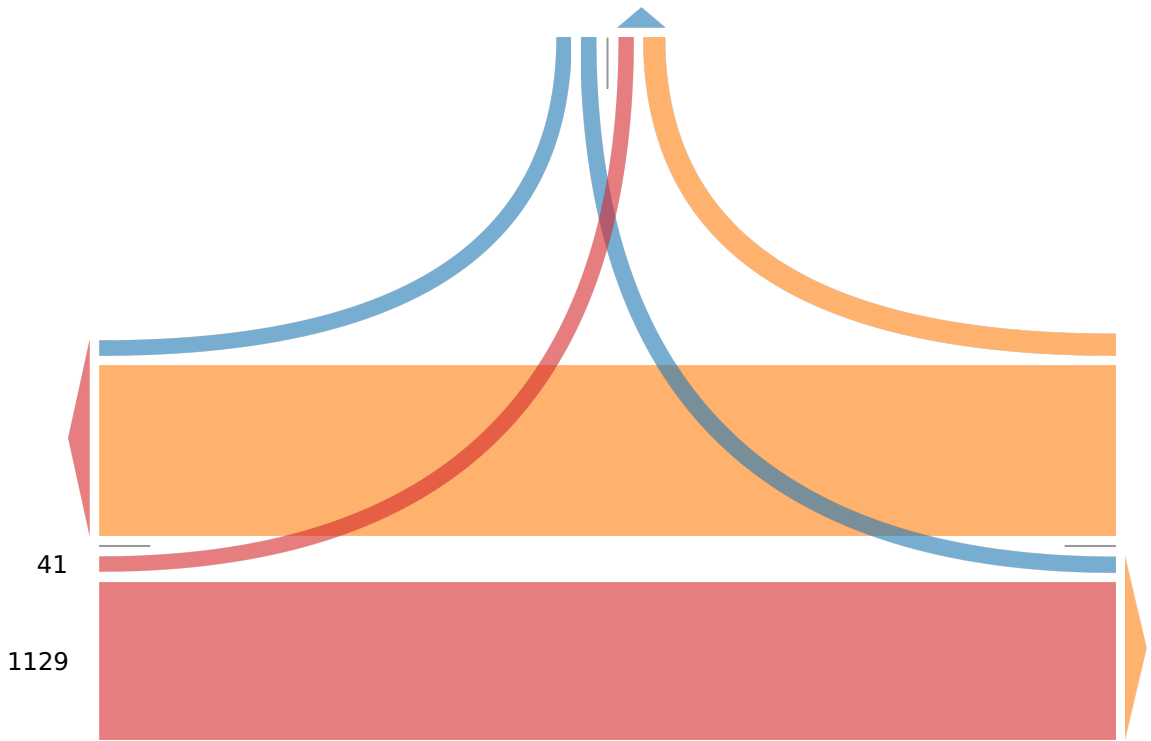
93

1235

Out: 1177 In: 1328

Total: 2505

[E] Route 44



Route 44 at CVS Drive - TMC

Thu Jun 8, 2023

Full Length (4 PM-6 PM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092496, Location: 41.824233, -72.884906

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	CVS Drive Southbound					Route 44 Westbound					Route 44 Eastbound					Int
	R	L	U	App	Ped*	R	T	U	App	Ped*	T	L	U	App	Ped*	
Time																
2023-06-08 4:00PM	16	13	0	29	0	33	380	1	414	0	262	6	0	268	0	711
4:15PM	11	13	0	24	0	23	330	1	354	0	247	7	0	254	0	632
4:30PM	8	11	0	19	0	27	336	0	363	0	270	7	0	277	0	659
4:45PM	12	14	0	26	0	25	319	0	344	0	295	7	0	302	1	672
Hourly Total	47	51	0	98	0	108	1365	2	1475	0	1074	27	0	1101	1	2674
5:00PM	6	14	0	20	0	29	366	0	395	0	258	8	0	266	0	681
5:15PM	11	10	0	21	0	23	334	0	357	0	268	4	0	272	1	650
5:30PM	1	10	0	11	0	17	309	0	326	0	227	13	0	240	0	577
5:45PM	8	7	0	15	0	19	315	0	334	0	221	8	0	229	0	578
Hourly Total	26	41	0	67	0	88	1324	0	1412	0	974	33	0	1007	1	2486
Total	73	92	0	165	0	196	2689	2	2887	0	2048	60	0	2108	2	5160
% Approach	44.2%	55.8%	0%	-	-	6.8%	93.1%	0.1%	-	-	97.2%	2.8%	0%	-	-	-
% Total	1.4%	1.8%	0%	3.2%	-	3.8%	52.1%	0%	55.9%	-	39.7%	1.2%	0%	40.9%	-	-
Lights	73	92	0	165	-	195	2651	2	2848	-	2016	60	0	2076	-	5089
% Lights	100%	100%	0%	100%	-	99.5%	98.6%	100%	98.6%	-	98.4%	100%	0%	98.5%	-	98.6%
Articulated Trucks and Single-Unit Trucks	0	0	0	0	-	1	26	0	27	-	28	0	0	28	-	55
% Articulated Trucks and Single-Unit Trucks	0%	0%	0%	0%	-	0.5%	1.0%	0%	0.9%	-	1.4%	0%	0%	1.3%	-	1.1%
Buses	0	0	0	0	-	0	12	0	12	-	4	0	0	4	-	16
% Buses	0%	0%	0%	0%	-	0%	0.4%	0%	0.4%	-	0.2%	0%	0%	0.2%	-	0.3%
Pedestrians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0%	-
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	0	-	-	-	-	2	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	-	-	-	-	100%	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at CVS Drive - TMC

Thu Jun 8, 2023

Full Length (4 PM-6 PM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092496, Location: 41.824233, -72.884906

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

[N] CVS Drive

Total: 421

In: 165 Out: 256

73 92

[W] Route 44

Total: 4870

In: 2108 Out: 2762

60
2048

1

1

196
2689

2

Out: 2142 In: 2887

Total: 5029

[E] Route 44

Route 44 at CVS Drive - TMC

Thu Jun 8, 2023

PM Peak (4 PM - 5 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092496, Location: 41.824233, -72.884906

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	CVS Drive Southbound					Route 44 Westbound					Route 44 Eastbound					Int
	R	L	U	App	Ped*	R	T	U	App	Ped*	T	L	U	App	Ped*	
Time																
2023-06-08 4:00PM	16	13	0	29	0	33	380	1	414	0	262	6	0	268	0	711
4:15PM	11	13	0	24	0	23	330	1	354	0	247	7	0	254	0	632
4:30PM	8	11	0	19	0	27	336	0	363	0	270	7	0	277	0	659
4:45PM	12	14	0	26	0	25	319	0	344	0	295	7	0	302	1	672
Total	47	51	0	98	0	108	1365	2	1475	0	1074	27	0	1101	1	2674
% Approach	48.0%	52.0%	0%	-	-	7.3%	92.5%	0.1%	-	-	97.5%	2.5%	0%	-	-	-
% Total	1.8%	1.9%	0%	3.7%	-	4.0%	51.0%	0.1%	55.2%	-	40.2%	1.0%	0%	41.2%	-	-
PHF	0.734	0.911	-	0.845	-	0.818	0.898	0.500	0.891	-	0.910	0.964	-	0.911	-	0.940
Lights	47	51	0	98	-	107	1341	2	1450	-	1054	27	0	1081	-	2629
% Lights	100%	100%	0%	100%	-	99.1%	98.2%	100%	98.3%	-	98.1%	100%	0%	98.2%	-	98.3%
Articulated Trucks and Single-Unit Trucks	0	0	0	0	-	1	14	0	15	-	18	0	0	18	-	33
% Articulated Trucks and Single-Unit Trucks	0%	0%	0%	0%	-	0.9%	1.0%	0%	1.0%	-	1.7%	0%	0%	1.6%	-	1.2%
Buses	0	0	0	0	-	0	10	0	10	-	2	0	0	2	-	12
% Buses	0%	0%	0%	0%	-	0%	0.7%	0%	0.7%	-	0.2%	0%	0%	0.2%	-	0.4%
Pedestrians	-	-	-	-	0	-	-	-	-	0	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0%	-
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	0	-	-	-	-	1	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	-	-	-	-	100%	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at CVS Drive - TMC

Thu Jun 8, 2023

PM Peak (4 PM - 5 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092496, Location: 41.824233, -72.884906

Provided by: Connecticut Counts LLC

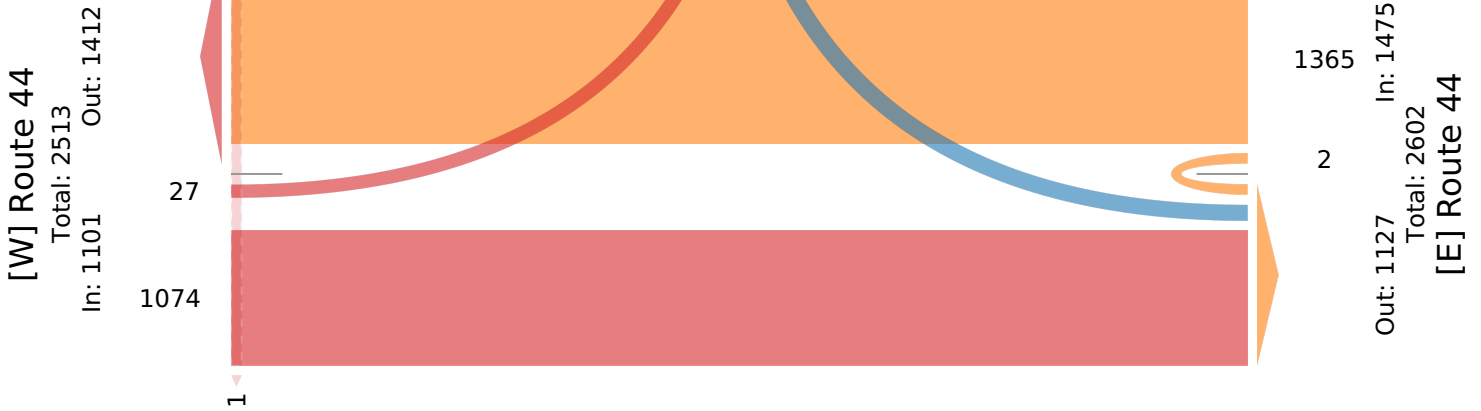
63 Sugar Maple Lane,
Kensington, CT, 12345, US

[N] CVS Drive

Total: 233

In: 98 Out: 135

47
51



Route 44 at Shops at Farmington Valley - TMC

Thu Jun 8, 2023

Full Length (7 AM-9 AM)

All Classes (Motorcycles, Lights, Heavy, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092401, Location: 41.823653, -72.882434

Provided by: Connecticut Counts LLC
63 Sugar Maple Lane, Kensington, CT, 12345, US

Leg Direction	Plaza Drive Southbound						Route 44 Westbound						Shops at Farmington Valley Northbound						Route 44 Eastbound						
Time	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	Int
2023-06-08 7:00AM	0	0	0	0	0	0	3	82	4	0	89	0	5	1	3	0	9	0	16	245	1	0	262	0	360
7:15AM	0	0	1	0	1	0	7	116	2	0	125	0	2	0	5	0	7	0	12	284	3	0	299	1	432
7:30AM	1	0	1	0	2	0	8	121	5	0	134	0	5	0	7	0	12	0	21	284	1	0	306	0	454
7:45AM	0	0	2	0	2	0	5	138	13	0	156	0	5	0	6	0	11	0	21	316	0	0	337	0	506
Hourly Total	1	0	4	0	5	0	23	457	24	0	504	0	17	1	21	0	39	0	70	1129	5	0	1204	1	1752
8:00AM	0	0	1	0	1	0	3	115	16	0	134	0	3	0	13	0	16	0	23	285	2	0	310	0	461
8:15AM	1	0	1	0	2	0	3	158	17	1	179	0	3	0	11	0	14	0	25	298	0	0	323	1	518
8:30AM	1	0	2	0	3	0	4	145	13	0	162	0	11	0	16	0	27	0	36	266	0	0	302	0	494
8:45AM	0	1	3	0	4	2	5	163	22	2	192	0	15	0	22	0	37	0	52	265	5	0	322	0	555
Hourly Total	2	1	7	0	10	2	15	581	68	3	667	0	32	0	62	0	94	0	136	1114	7	0	1257	1	2028
Total	3	1	11	0	15	2	38	1038	92	3	1171	0	49	1	83	0	133	0	206	2243	12	0	2461	2	3780
% Approach	20.0%	6.7%	73.3%	0%	-	-	3.2%	88.6%	7.9%	0.3%	-	-	36.8%	0.8%	62.4%	0%	-	-	8.4%	91.1%	0.5%	0%	-	-	-
% Total	0.1%	0%	0.3%	0%	0.4%	-	1.0%	27.5%	2.4%	0.1%	31.0%	-	1.3%	0%	2.2%	0%	3.5%	-	5.4%	59.3%	0.3%	0%	65.1%	-	-
Motorcycles	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	1	0	0	1	-	1
% Motorcycles	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%
Lights	3	1	11	0	15	-	37	966	88	3	1094	-	46	1	81	0	128	-	203	2168	12	0	2383	-	3620
% Lights	100%	100%	100%	0%	100%	-	97.4%	93.1%	95.7%	100%	93.4%	-	93.9%	100%	97.6%	0%	96.2%	-	98.5%	96.7%	100%	0%	96.8%	-	95.8%
Heavy	0	0	0	0	0	-	1	72	4	0	77	-	3	0	2	0	5	-	3	74	0	0	77	-	159
% Heavy	0%	0%	0%	0%	0%	-	2.6%	6.9%	4.3%	0%	6.6%	-	6.1%	0%	2.4%	0%	3.8%	-	1.5%	3.3%	0%	0%	3.1%	-	4.2%
Pedestrians	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	2	
% Pedestrians	-	-	-	-	-	100%	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	100%	-
Bicycles on Crosswalk	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	
% Bicycles on Crosswalk	-	-	-	-	-	0%	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0%	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Shops at Farmington Valley - TMC

Thu Jun 8, 2023

Full Length (7 AM-9 AM)

All Classes (Motorcycles, Lights, Heavy, Pedestrians, Bicycles on Crosswalk)

All Movements

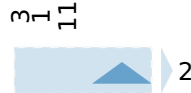
ID: 1092401, Location: 41.823653, -72.882434

Provided by: Connecticut Counts LLC
63 Sugar Maple Lane, Kensington, CT, 12345, US

[N] Plaza Drive

Total: 66

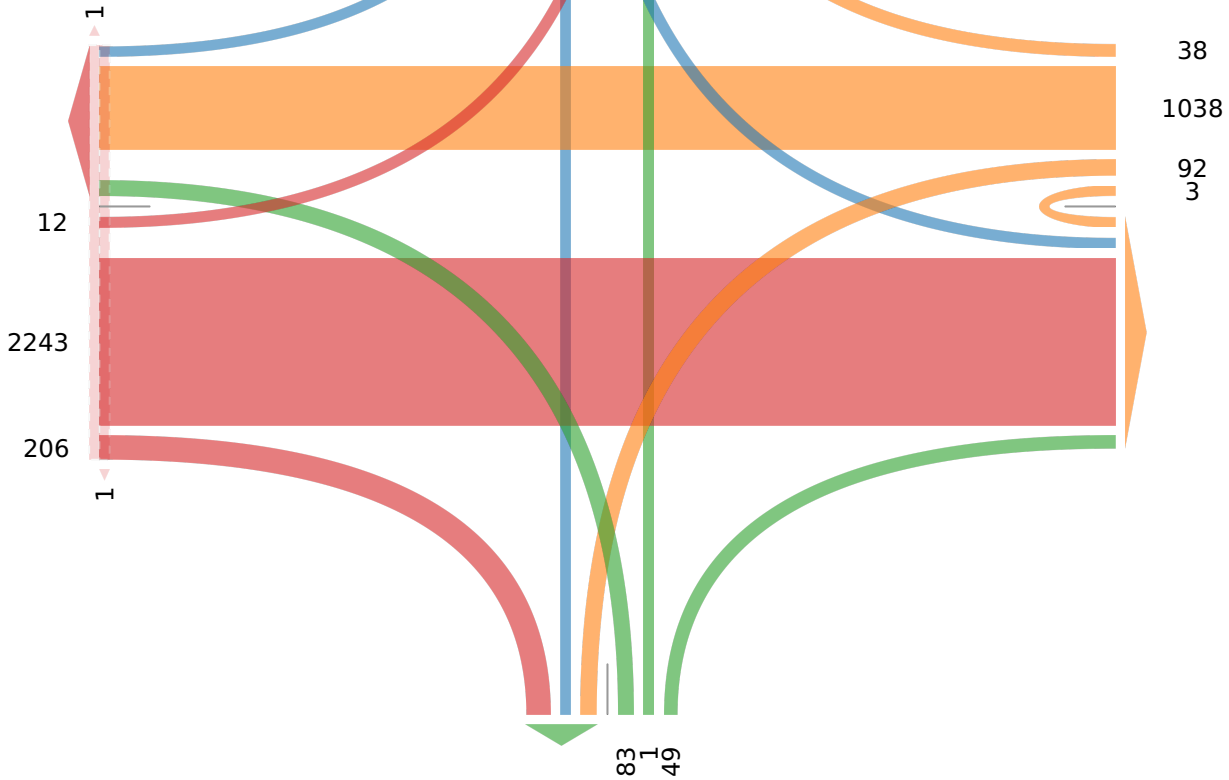
In: 15 Out: 51



[W] Route 44

Total: 3585

In: 2461 Out: 1124



Out: 2306 In: 1171

Total: 3477

[E] Route 44

Out: 299 In: 133
Total: 432

[S] Shops at Farmington Valley

Route 44 at Shops at Farmington Valley - TMC

Provided by: Connecticut Counts LLC
63 Sugar Maple Lane, Kensington, CT, 12345, US

Thu Jun 8, 2023

AM Peak (8 AM - 9 AM) - Overall Peak Hour

All Classes (Motorcycles, Lights, Heavy, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092401, Location: 41.823653, -72.882434

Leg Direction	Plaza Drive Southbound						Route 44 Westbound						Shops at Farmington Valley Northbound						Route 44 Eastbound						
Time	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	Int
2023-06-08 8:00AM	0	0	1	0	1	0	3	115	16	0	134	0	3	0	13	0	16	0	23	285	2	0	310	0	461
8:15AM	1	0	1	0	2	0	3	158	17	1	179	0	3	0	11	0	14	0	25	298	0	0	323	1	518
8:30AM	1	0	2	0	3	0	4	145	13	0	162	0	11	0	16	0	27	0	36	266	0	0	302	0	494
8:45AM	0	1	3	0	4	2	5	163	22	2	192	0	15	0	22	0	37	0	52	265	5	0	322	0	555
Total	2	1	7	0	10	2	15	581	68	3	667	0	32	0	62	0	94	0	136	1114	7	0	1257	1	2028
% Approach	20.0%	10.0%	70.0%	0%	-	-	2.2%	87.1%	10.2%	0.4%	-	-	34.0%	0%	66.0%	0%	-	-	10.8%	88.6%	0.6%	0%	-	-	-
% Total	0.1%	0%	0.3%	0%	0.5%	-	0.7%	28.6%	3.4%	0.1%	32.9%	-	1.6%	0%	3.1%	0%	4.6%	-	6.7%	54.9%	0.3%	0%	62.0%	-	-
PHF	0.500	0.250	0.583	-	0.625	-	0.750	0.891	0.773	0.375	0.868	-	0.533	-	0.705	-	0.635	-	0.654	0.935	0.350	-	0.973	-	0.914
Motorcycles	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0	0	0	0	0	-	0
% Motorcycles	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%
Lights	2	1	7	0	10	-	14	548	65	3	630	-	31	0	61	0	92	-	134	1080	7	0	1221	-	1953
% Lights	100%	100%	100%	0%	100%	-	93.3%	94.3%	95.6%	100%	94.5%	-	96.9%	0%	98.4%	0%	97.9%	-	98.5%	96.9%	100%	0%	97.1%	-	96.3%
Heavy	0	0	0	0	0	-	1	33	3	0	37	-	1	0	1	0	2	-	2	34	0	0	36	-	75
% Heavy	0%	0%	0%	0%	0%	-	6.7%	5.7%	4.4%	0%	5.5%	-	3.1%	0%	1.6%	0%	2.1%	-	1.5%	3.1%	0%	0%	2.9%	-	3.7%
Pedestrians	-	-	-	-	-	2	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	1	-
% Pedestrians	-	-	-	-	-	100%	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	100%	-
Bicycles on Crosswalk	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	-	0%	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0%	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Shops at Farmington Valley - TMC

Thu Jun 8, 2023

AM Peak (8 AM - 9 AM) - Overall Peak Hour

All Classes (Motorcycles, Lights, Heavy, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092401, Location: 41.823653, -72.882434

Provided by: Connecticut Counts LLC
63 Sugar Maple Lane, Kensington, CT, 12345, US

[N] Plaza Drive

Total: 32

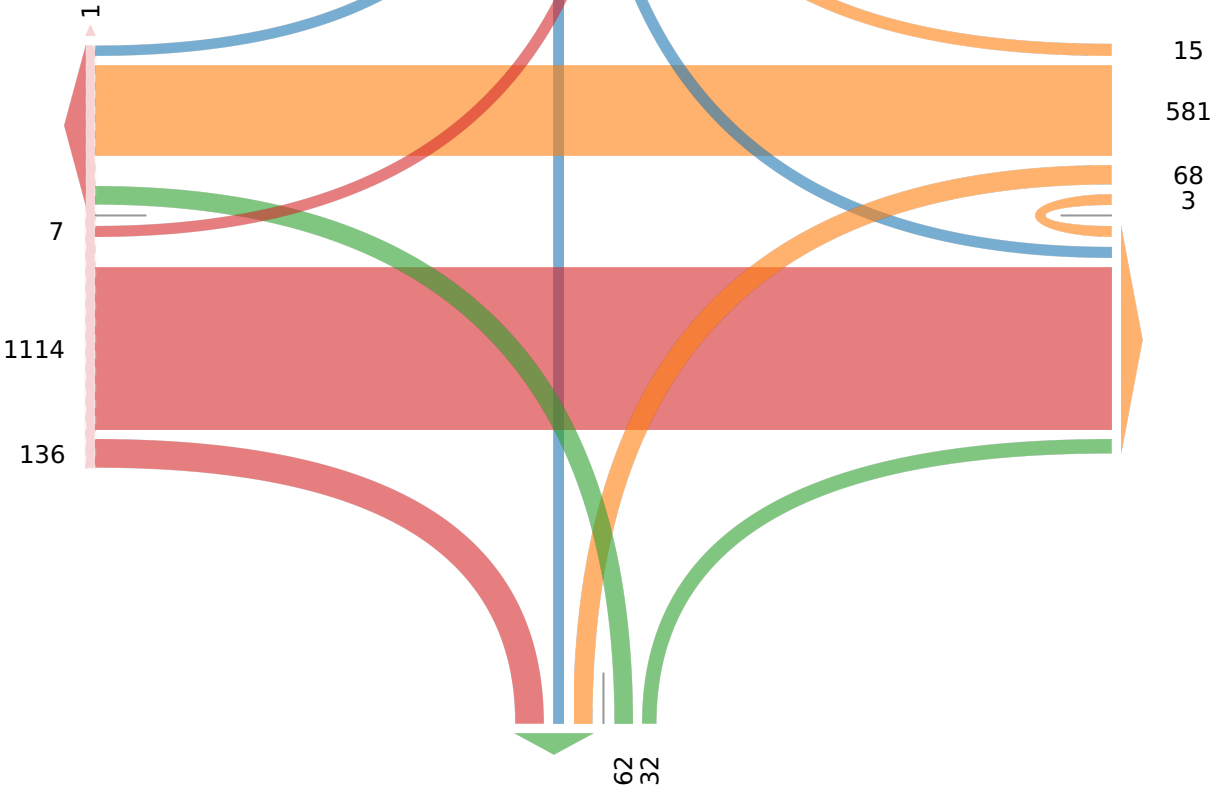
In: 10 Out: 22

2 1 7

[W] Route 44

Total: 1902

In: 1257 Out: 645



[S] Shops at Farmington Valley

Out: 205 In: 94
Total: 299

Route 44 at Shops at Farmington Valley - TMC

Provided by: Connecticut Counts LLC

Sat Jun 10, 2023

63 Sugar Maple Lane,

Full Length (11 AM-1 PM)

Kensington, CT, 12345, US

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092418, Location: 41.823653, -72.882434

Leg Direction	Plaza Drive Southbound						Route 44 Westbound						Shops at Farmington Valley Northbound						Route 44 Eastbound						Int
	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	
2023-06-10 11:00AM	2	0	0	0	2	0	1	200	52	2	255	0	58	1	76	0	135	0	90	221	1	0	312	0	704
11:15AM	0	0	0	0	0	0	0	197	58	0	255	0	56	0	61	0	117	0	101	242	0	0	343	0	715
11:30AM	1	1	1	0	3	0	0	208	55	1	264	0	65	1	76	0	142	0	94	206	2	0	302	0	711
11:45AM	0	1	0	0	1	0	1	230	81	0	312	0	73	0	80	0	153	0	109	238	0	0	347	0	813
Hourly Total	3	2	1	0	6	0	2	835	246	3	1086	0	252	2	293	0	547	0	394	907	3	0	1304	0	2943
12:00PM	6	0	1	0	7	0	2	246	85	0	333	0	79	0	83	0	162	1	79	226	0	0	305	0	807
12:15PM	2	0	0	0	2	0	1	193	90	0	284	0	85	0	87	0	172	0	91	176	1	0	268	1	726
12:30PM	1	0	3	0	4	0	3	217	62	0	282	0	97	0	101	0	198	0	113	205	1	1	320	0	804
12:45PM	1	1	5	0	7	0	2	232	64	1	299	0	75	2	81	0	158	0	93	219	1	0	313	0	777
Hourly Total	10	1	9	0	20	0	8	888	301	1	1198	0	336	2	352	0	690	1	376	826	3	1	1206	1	3114
Total	13	3	10	0	26	0	10	1723	547	4	2284	0	588	4	645	0	1237	1	770	1733	6	1	2510	1	6057
% Approach	50.0%	11.5%	38.5%	0%	-	-	0.4%	75.4%	23.9%	0.2%	-	-	47.5%	0.3%	52.1%	0%	-	-	30.7%	69.0%	0.2%	0%	-	-	-
% Total	0.2%	0%	0.2%	0%	0.4%	-	0.2%	28.4%	9.0%	0.1%	37.7%	-	9.7%	0.1%	10.6%	0%	20.4%	-	12.7%	28.6%	0.1%	0%	41.4%	-	-
Lights	13	3	10	0	26	-	10	1707	545	4	2266	-	582	4	643	0	1229	-	764	1722	6	1	2493	-	6014
% Lights	100%	100%	100%	0%	100%	-	100%	99.1%	99.6%	100%	99.2%	-	99.0%	100%	99.7%	0%	99.4%	-	99.2%	99.4%	100%	100%	99.3%	-	99.3%
Articulated Trucks and Single-Unit Trucks	0	0	0	0	0	-	0	14	2	0	16	-	6	0	1	0	7	-	6	10	0	0	16	-	39
% Articulated Trucks and Single-Unit Trucks	0%	0%	0%	0%	0%	-	0%	0.8%	0.4%	0%	0.7%	-	1.0%	0%	0.2%	0%	0.6%	-	0.8%	0.6%	0%	0%	0.6%	-	0.6%
Buses	0	0	0	0	0	-	0	2	0	0	2	-	0	0	1	0	1	-	0	1	0	0	1	-	4
% Buses	0%	0%	0%	0%	0%	-	0%	0.1%	0%	0%	0.1%	-	0%	0%	0.2%	0%	0.1%	-	0%	0.1%	0%	0%	0%	-	0.1%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	1	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	100%	-	-	-	-	-	100%	-
Bicycles on Crosswalk	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0%	-	-	-	-	-	0%	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Shops at Farmington Valley - TMC

Sat Jun 10, 2023

Full Length (11 AM-1 PM)

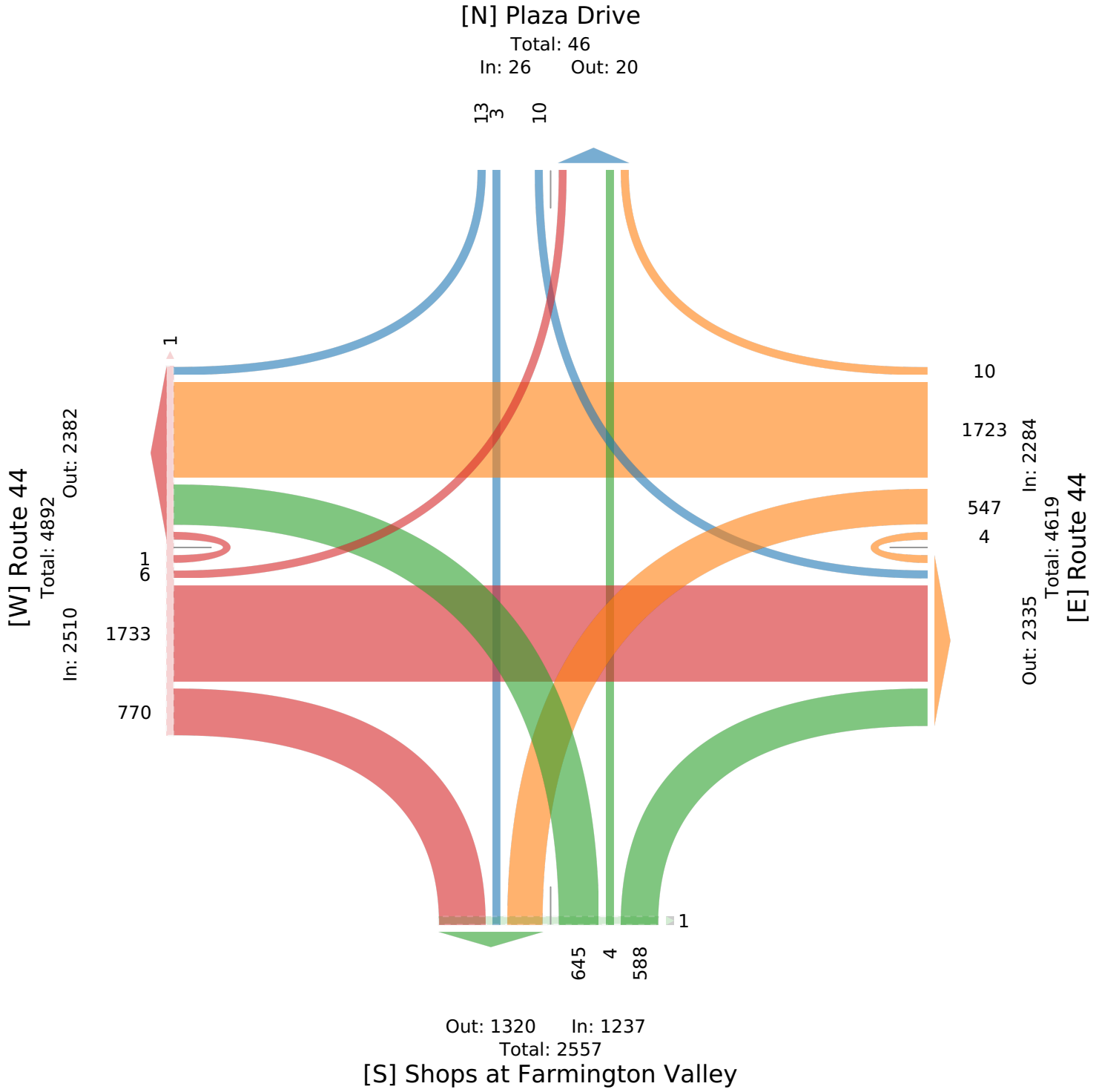
All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092418, Location: 41.823653, -72.882434

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US



Route 44 at Shops at Farmington Valley - TMC

Sat Jun 10, 2023

Midday Peak (WKND) (11:45 AM - 12:45 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092418, Location: 41.823653, -72.882434

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,

Kensington, CT, 12345, US

Leg Direction	Plaza Drive Southbound					Route 44 Westbound					Shops at Farmington Valley Northbound					Route 44 Eastbound									
Time	R	T	L	U	App Ped*	R	T	L	U	App Ped*	R	T	L	U	App Ped*	R	T	L	U	App Ped*	Int				
2023-06-10 11:45AM	0	1	0	0	1	0	1	230	81	0	312	0	73	0	80	0	153	0	109	238	0	0	347	0	813
12:00PM	6	0	1	0	7	0	2	246	85	0	333	0	79	0	83	0	162	1	79	226	0	0	305	0	807
12:15PM	2	0	0	0	2	0	1	193	90	0	284	0	85	0	87	0	172	0	91	176	1	0	268	1	726
12:30PM	1	0	3	0	4	0	3	217	62	0	282	0	97	0	101	0	198	0	113	205	1	1	320	0	804
Total	9	1	4	0	14	0	7	886	318	0	1211	0	334	0	351	0	685	1	392	845	2	1	1240	1	3150
% Approach	64.3%	7.1%	28.6%	0%	-	-	0.6%	73.2%	26.3%	0%	-	-	48.8%	0%	51.2%	0%	-	-	31.6%	68.1%	0.2%	0.1%	-	-	-
% Total	0.3%	0%	0.1%	0%	0.4%	-	0.2%	28.1%	10.1%	0%	38.4%	-	10.6%	0%	11.1%	0%	21.7%	-	12.4%	26.8%	0.1%	0%	39.4%	-	-
PHF	0.375	0.250	0.333	-	0.500	-	0.583	0.900	0.883	-	0.909	-	0.861	-	0.869	-	0.865	-	0.867	0.888	0.500	0.250	0.893	-	0.969
Lights	9	1	4	0	14	-	7	880	317	0	1204	-	331	0	350	0	681	-	388	839	2	1	1230	-	3129
% Lights	100%	100%	100%	0%	100%	-	100%	99.3%	99.7%	0%	99.4%	-	99.1%	0%	99.7%	0%	99.4%	-	99.0%	99.3%	100%	100%	99.2%	-	99.3%
Articulated Trucks and Single-Unit Trucks	0	0	0	0	0	-	0	5	1	0	6	-	3	0	0	0	3	-	4	6	0	0	10	-	19
% Articulated Trucks and Single-Unit Trucks	0%	0%	0%	0%	0%	-	0%	0.6%	0.3%	0%	0.5%	-	0.9%	0%	0%	0%	0.4%	-	1.0%	0.7%	0%	0%	0.8%	-	0.6%
Buses	0	0	0	0	0	-	0	1	0	0	1	-	0	0	1	0	1	-	0	0	0	0	0	-	2
% Buses	0%	0%	0%	0%	0%	-	0%	0.1%	0%	0%	0.1%	-	0%	0%	0.3%	0%	0.1%	-	0%	0%	0%	0%	0%	-	0.1%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	1	-	-	-	-	-	-	1
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	100%	-	-	-	-	-	-	100%
Bicycles on Crosswalk	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	-	0
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	0%	-	-	-	-	-	-	0%

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Shops at Farmington Valley - TMC

Sat Jun 10, 2023

Midday Peak (WKND) (11:45 AM - 12:45 PM) - Overall Peak Hour

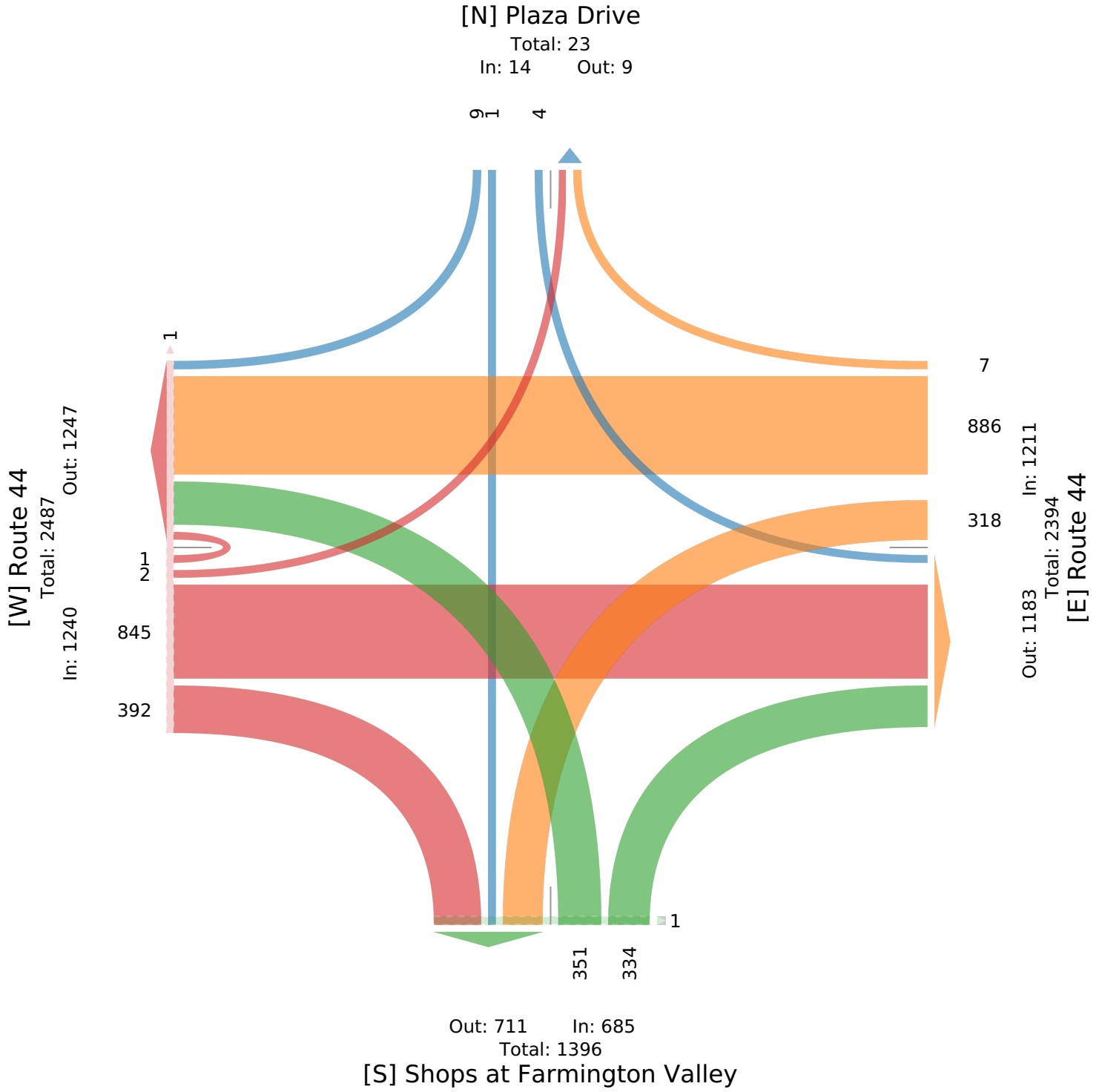
All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092418, Location: 41.823653, -72.882434

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US



Route 44 at Shops at Farmington Valley - TMC

Thu Jun 8, 2023

Full Length (4 PM-6 PM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092408, Location: 41.823653, -72.882434

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,

Kensington, CT, 12345, US

Leg Direction	Plaza Drive Southbound					Route 44 Westbound					Shops at Farmington Valley Northbound					Route 44 Eastbound									
Time	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	Int						
2023-06-08 4:00PM	1	1	3	0	5	0	2	303	48	0	353	0	63	0	71	0	134	0	76	190	1	0	267	0	759
4:15PM	2	0	7	0	9	0	2	261	55	0	318	0	54	0	68	0	122	0	78	177	0	0	255	1	704
4:30PM	0	0	2	0	2	0	2	292	37	0	331	0	57	0	74	0	131	0	58	205	3	0	266	0	730
4:45PM	3	0	5	0	8	0	3	287	63	2	355	0	55	0	77	0	132	0	78	227	0	0	305	0	800
Hourly Total	6	1	17	0	24	0	9	1143	203	2	1357	0	229	0	290	0	519	0	290	799	4	0	1093	1	2993
5:00PM	2	0	5	0	7	0	0	267	62	0	329	0	66	0	79	0	145	0	59	227	2	0	288	0	769
5:15PM	1	0	1	0	2	0	1	274	47	0	322	0	50	0	62	0	112	0	69	215	0	0	284	0	720
5:30PM	3	1	2	0	6	0	2	253	52	1	308	0	45	0	71	0	116	0	77	178	0	0	255	0	685
5:45PM	0	1	1	0	2	0	3	257	56	0	316	0	53	0	77	0	130	0	86	166	0	0	252	1	700
Hourly Total	6	2	9	0	17	0	6	1051	217	1	1275	0	214	0	289	0	503	0	291	786	2	0	1079	1	2874
Total	12	3	26	0	41	0	15	2194	420	3	2632	0	443	0	579	0	1022	0	581	1585	6	0	2172	2	5867
% Approach	29.3%	7.3%	63.4%	0%	-	-	0.6%	83.4%	16.0%	0.1%	-	-	43.3%	0%	56.7%	0%	-	-	26.7%	73.0%	0.3%	0%	-	-	-
% Total	0.2%	0.1%	0.4%	0%	0.7%	-	0.3%	37.4%	7.2%	0.1%	44.9%	-	7.6%	0%	9.9%	0%	17.4%	-	9.9%	27.0%	0.1%	0%	37.0%	-	-
Lights	12	3	25	0	40	-	15	2171	420	3	2609	-	442	0	578	0	1020	-	579	1549	6	0	2134	-	5803
% Lights	100%	100%	96.2%	0%	97.6%	-	100%	99.0%	100%	100%	99.1%	-	99.8%	0%	99.8%	0%	99.8%	-	99.7%	97.7%	100%	0%	98.3%	-	98.9%
Articulated Trucks and Single-Unit Trucks	0	0	1	0	1	-	0	12	0	0	12	-	1	0	1	0	2	-	2	31	0	0	33	-	48
% Articulated Trucks and Single-Unit Trucks	0%	0%	3.8%	0%	2.4%	-	0%	0.5%	0%	0%	0.5%	-	0.2%	0%	0.2%	0%	0.2%	-	0.3%	2.0%	0%	0%	1.5%	-	0.8%
Buses	0	0	0	0	0	-	0	11	0	0	11	-	0	0	0	0	0	-	0	5	0	0	5	-	16
% Buses	0%	0%	0%	0%	0%	-	0%	0.5%	0%	0%	0.4%	-	0%	0%	0%	0%	0%	-	0%	0.3%	0%	0%	0.2%	-	0.3%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	1	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-50.0%	-
Bicycles on Crosswalk	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	1	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-50.0%	-

* Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Shops at Farmington Valley - TMC

Thu Jun 8, 2023

Full Length (4 PM-6 PM)

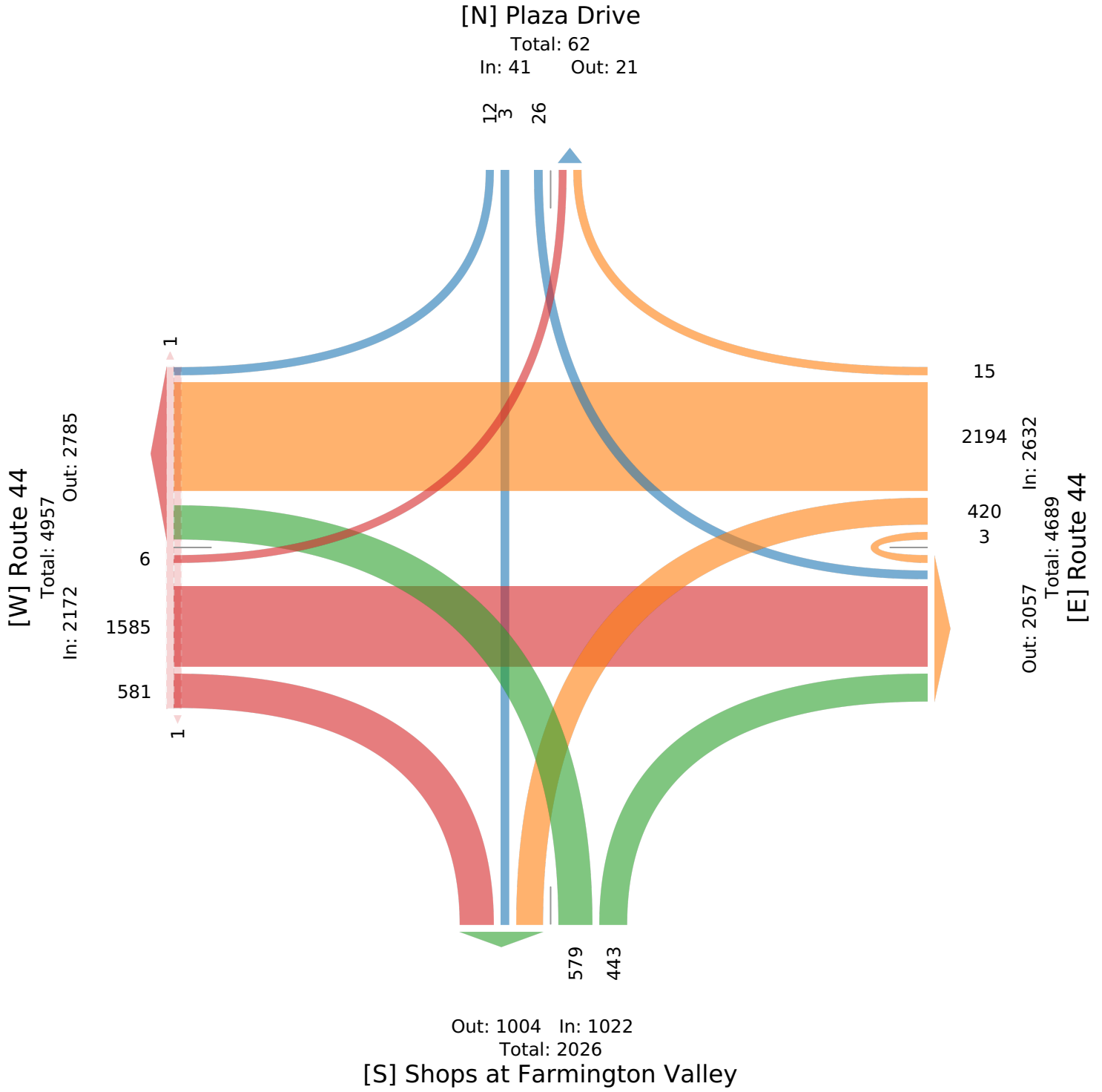
All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092408, Location: 41.823653, -72.882434

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US



Route 44 at Shops at Farmington Valley - TMC

Thu Jun 8, 2023

PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092408, Location: 41.823653, -72.882434

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Plaza Drive Southbound						Route 44 Westbound						Shops at Farmington Valley Northbound						Route 44 Eastbound						
Time	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	Int
2023-06-08 4:30PM	0	0	2	0	2	0	2	292	37	0	331	0	57	0	74	0	131	0	58	205	3	0	266	0	730
4:45PM	3	0	5	0	8	0	3	287	63	2	355	0	55	0	77	0	132	0	78	227	0	0	305	0	800
5:00PM	2	0	5	0	7	0	0	267	62	0	329	0	66	0	79	0	145	0	59	227	2	0	288	0	769
5:15PM	1	0	1	0	2	0	1	274	47	0	322	0	50	0	62	0	112	0	69	215	0	0	284	0	720
Total	6	0	13	0	19	0	6	1120	209	2	1337	0	228	0	292	0	520	0	264	874	5	0	1143	0	3019
% Approach	31.6%	0%	68.4%	0%	-	-	0.4%	83.8%	15.6%	0.1%	-	-	43.8%	0%	56.2%	0%	-	-	23.1%	76.5%	0.4%	0%	-	-	-
% Total	0.2%	0%	0.4%	0%	0.6%	-	0.2%	37.1%	6.9%	0.1%	44.3%	-	7.6%	0%	9.7%	0%	17.2%	-	8.7%	28.9%	0.2%	0%	37.9%	-	-
PHF	0.500	-	0.650	-	0.594	-	0.500	0.959	0.829	0.250	0.942	-	0.864	-	0.924	-	0.897	-	0.846	0.963	0.417	-	0.937	-	0.943
Lights	6	0	13	0	19	-	6	1108	209	2	1325	-	227	0	291	0	518	-	262	853	5	0	1120	-	2982
% Lights	100%	0%	100%	0%	100%	-	100%	98.9%	100%	100%	99.1%	-	99.6%	0%	99.7%	0%	99.6%	-	99.2%	97.6%	100%	0%	98.0%	-	98.8%
Articulated Trucks and Single-Unit Trucks	0	0	0	0	0	-	0	6	0	0	6	-	1	0	1	0	2	-	2	20	0	0	22	-	30
% Articulated Trucks and Single-Unit Trucks	0%	0%	0%	0%	0%	-	0%	0.5%	0%	0%	0.4%	-	0.4%	0%	0.3%	0%	0.4%	-	0.8%	2.3%	0%	0%	1.9%	-	1.0%
Buses	0	0	0	0	0	-	0	6	0	0	6	-	0	0	0	0	0	-	0	1	0	0	1	-	7
% Buses	0%	0%	0%	0%	0%	-	0%	0.5%	0%	0%	0.4%	-	0%	0%	0%	0%	0%	-	0%	0.1%	0%	0%	0.1%	-	0.2%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Bicycles on Crosswalk	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Shops at Farmington Valley - TMC

Thu Jun 8, 2023

PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092408, Location: 41.823653, -72.882434

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

[N] Plaza Drive

Total: 30

In: 19 Out: 11

6
13

[W] Route 44
Total: 2561
In: 1143 Out: 1418

5
874
264

6
1120
209
2
Out: 1117 In: 1337

Total: 2454
[E] Route 44

Out: 473 In: 520
Total: 993

[S] Shops at Farmington Valley

292
228

Route 44 at Secret Lake Road - TMC

Thu Jun 8, 2023

Full Length (7 AM-9 AM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092488, Location: 41.823104, -72.879655

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Plaza Drive Southbound						Route 44 Westbound						Secret Lake Road Northbound						Route 44 Eastbound						
Time	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	Int
2023-06-08 7:00AM	0	0	0	0	0	0	2	91	0	0	93	0	5	0	1	0	6	0	0	249	0	0	249	0	348
7:15AM	3	0	0	0	3	0	3	111	0	0	114	0	12	1	8	0	21	0	2	281	4	1	288	0	426
7:30AM	0	0	0	0	0	0	4	131	0	0	135	0	4	0	5	0	9	0	0	303	0	0	303	0	447
7:45AM	0	0	1	0	1	0	3	151	1	0	155	0	1	0	5	0	6	1	11	302	3	0	316	0	478
Hourly Total	3	0	1	0	4	0	12	484	1	0	497	0	22	1	19	0	42	1	13	1135	7	1	1156	0	1699
8:00AM	2	0	1	0	3	0	5	137	1	0	143	0	5	0	7	0	12	0	4	284	3	0	291	0	449
8:15AM	2	0	0	0	2	1	4	179	4	0	187	1	3	0	4	0	7	0	3	311	0	0	314	0	510
8:30AM	1	0	0	0	1	0	5	173	7	0	185	0	6	0	2	0	8	0	2	282	3	0	287	0	481
8:45AM	3	0	0	0	3	0	3	184	3	0	190	0	8	0	10	0	18	1	3	274	4	0	281	0	492
Hourly Total	8	0	1	0	9	1	17	673	15	0	705	1	22	0	23	0	45	1	12	1151	10	0	1173	0	1932
Total	11	0	2	0	13	1	29	1157	16	0	1202	1	44	1	42	0	87	2	25	2286	17	1	2329	0	3631
% Approach	84.6%	0%	15.4%	0%	-	-	2.4%	96.3%	1.3%	0%	-	-	50.6%	1.1%	48.3%	0%	-	-	1.1%	98.2%	0.7%	0%	-	-	-
% Total	0.3%	0%	0.1%	0%	0.4%	-	0.8%	31.9%	0.4%	0%	33.1%	-	1.2%	0%	1.2%	0%	2.4%	-	0.7%	63.0%	0.5%	0%	64.1%	-	-
Lights	9	0	2	0	11	-	26	1086	15	0	1127	-	44	1	39	0	84	-	25	2206	17	1	2249	-	3471
% Lights	81.8%	0%	100%	0%	84.6%	-	89.7%	93.9%	93.8%	0%	93.8%	-	100%	100%	92.9%	0%	96.6%	-	100%	96.5%	100%	100%	96.6%	-	95.6%
Articulated Trucks and Single-Unit Trucks	2	0	0	0	2	-	3	62	1	0	66	-	0	0	0	0	0	-	0	62	0	0	62	-	130
% Articulated Trucks and Single-Unit Trucks	18.2%	0%	0%	0%	15.4%	-	10.3%	5.4%	6.3%	0%	5.5%	-	0%	0%	0%	0%	0%	-	0%	2.7%	0%	0%	2.7%	-	3.6%
Buses	0	0	0	0	0	-	0	9	0	0	9	-	0	0	3	0	3	-	0	18	0	0	18	-	30
% Buses	0%	0%	0%	0%	0%	-	0%	0.8%	0%	0%	0.7%	-	0%	0%	7.1%	0%	3.4%	-	0%	0.8%	0%	0%	0.8%	-	0.8%
Pedestrians	-	-	-	-	-	1	-	-	-	-	-	1	-	-	-	-	-	2	-	-	-	-	-	0	
% Pedestrians	-	-	-	-	-	100%	-	-	-	-	-	100%	-	-	-	-	-	100%	-	-	-	-	-	-	
Bicycles on Crosswalk	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	
% Bicycles on Crosswalk	-	-	-	-	-	0%	-	-	-	-	-	0%	-	-	-	-	-	0%	-	-	-	-	-	-	

* Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Secret Lake Road - TMC

Thu Jun 8, 2023

Full Length (7 AM-9 AM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092488, Location: 41.823104, -72.879655

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

[N] Plaza Drive

Total: 60

In: 13 Out: 47

1 1
2

1

[W] Route 44

Total: 3540

In: 2329 Out: 1211

1
17

2286

25

2

42
1
44

Out: 41 In: 87

Total: 128

[S] Secret Lake Road

29

1157

16

Out: 2332 In: 1202

Total: 3534

[E] Route 44

1

Route 44 at Secret Lake Road - TMC

Thu Jun 8, 2023

AM Peak (8 AM - 9 AM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092488, Location: 41.823104, -72.879655

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Plaza Drive Southbound					Route 44 Westbound					Secret Lake Road Northbound					Route 44 Eastbound									
Time	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	Int
2023-06-08 8:00AM	2	0	1	0	3	0	5	137	1	0	143	0	5	0	7	0	12	0	4	284	3	0	291	0	449
8:15AM	2	0	0	0	2	1	4	179	4	0	187	1	3	0	4	0	7	0	3	311	0	0	314	0	510
8:30AM	1	0	0	0	1	0	5	173	7	0	185	0	6	0	2	0	8	0	2	282	3	0	287	0	481
8:45AM	3	0	0	0	3	0	3	184	3	0	190	0	8	0	10	0	18	1	3	274	4	0	281	0	492
Total	8	0	1	0	9	1	17	673	15	0	705	1	22	0	23	0	45	1	12	1151	10	0	1173	0	1932
% Approach	88.9%	0%	11.1%	0%	-	-	2.4%	95.5%	2.1%	0%	-	-	48.9%	0%	51.1%	0%	-	-	1.0%	98.1%	0.9%	0%	-	-	-
% Total	0.4%	0%	0.1%	0%	0.5%	-	0.9%	34.8%	0.8%	0%	36.5%	-	1.1%	0%	1.2%	0%	2.3%	-	0.6%	59.6%	0.5%	0%	60.7%	-	-
PHF	0.667	-	0.250	-	0.750	-	0.850	0.914	0.536	-	0.928	-	0.688	-	0.575	-	0.625	-	0.750	0.925	0.625	-	0.934	-	0.947
Lights	7	0	1	0	8	-	14	641	14	0	669	-	22	0	22	0	44	-	12	1107	10	0	1129	-	1850
% Lights	87.5%	0%	100%	0%	88.9%	-	82.4%	95.2%	93.3%	0%	94.9%	-	100%	0%	95.7%	0%	97.8%	-	100%	96.2%	100%	0%	96.2%	-	95.8%
Articulated Trucks and Single-Unit Trucks	1	0	0	0	1	-	3	26	1	0	30	-	0	0	0	0	0	-	0	39	0	0	39	-	70
% Articulated Trucks and Single-Unit Trucks	12.5%	0%	0%	0%	11.1%	-	17.6%	3.9%	6.7%	0%	4.3%	-	0%	0%	0%	0%	0%	-	0%	3.4%	0%	0%	3.3%	-	3.6%
Buses	0	0	0	0	0	-	0	6	0	0	6	-	0	0	1	0	1	-	0	5	0	0	5	-	12
% Buses	0%	0%	0%	0%	0%	-	0%	0.9%	0%	0%	0.9%	-	0%	0%	4.3%	0%	2.2%	-	0%	0.4%	0%	0%	0.4%	-	0.6%
Pedestrians	-	-	-	-	-	1	-	-	-	-	-	1	-	-	-	-	-	1	-	-	-	-	-	-	0
% Pedestrians	-	-	-	-	-	100%	-	-	-	-	-	100%	-	-	-	-	-	100%	-	-	-	-	-	-	-
Bicycles on Crosswalk	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	-	0
% Bicycles on Crosswalk	-	-	-	-	-	0%	-	-	-	-	-	0%	-	-	-	-	-	0%	-	-	-	-	-	-	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Secret Lake Road - TMC

Thu Jun 8, 2023

AM Peak (8 AM - 9 AM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092488, Location: 41.823104, -72.879655

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

[N] Plaza Drive

Total: 36

In: 9 Out: 27

8-1

1

[W] Route 44

Total: 1877
In: 1173 Out: 704

10

1151

12

1

23
22

Out: 27 In: 45

Total: 72

[S] Secret Lake Road

17

673

15

Out: 1174 In: 705

Total: 1879

[E] Route 44

1

Route 44 at Secret Lake Road - TMC

Sat Jun 10, 2023

Full Length (11 AM-1 PM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092495, Location: 41.823104, -72.879655

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,

Kensington, CT, 12345, US

Leg Direction	Plaza Drive Southbound						Route 44 Westbound						Secret Lake Road Northbound						Route 44 Eastbound						
Time	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	Int
2023-06-10 11:00AM	1	0	0	0	1	0	1	243	2	0	246	0	4	0	7	0	11	0	7	274	0	0	281	0	539
11:15AM	1	0	1	0	2	0	0	253	4	0	257	0	5	0	6	0	11	0	6	299	3	0	308	0	578
11:30AM	1	0	1	1	3	0	0	266	6	0	272	0	3	0	6	0	9	0	4	262	1	0	267	0	551
11:45AM	0	0	3	0	3	0	0	290	3	0	293	0	7	0	9	0	16	0	11	302	1	1	315	0	627
Hourly Total	3	0	5	1	9	0	1	1052	15	0	1068	0	19	0	28	0	47	0	28	1137	5	1	1171	0	2295
12:00PM	5	0	0	0	5	0	0	299	6	0	305	0	3	0	6	0	9	1	9	303	0	0	312	0	631
12:15PM	1	0	2	0	3	0	3	289	2	0	294	0	4	0	3	0	7	0	5	254	0	0	259	0	563
12:30PM	2	0	1	0	3	0	2	278	6	0	286	2	5	0	7	0	12	0	5	304	1	0	310	0	611
12:45PM	4	0	1	0	5	0	1	285	8	0	294	0	5	0	5	0	10	0	7	289	3	0	299	0	608
Hourly Total	12	0	4	0	16	0	6	1151	22	0	1179	2	17	0	21	0	38	1	26	1150	4	0	1180	0	2413
Total	15	0	9	1	25	0	7	2203	37	0	2247	2	36	0	49	0	85	1	54	2287	9	1	2351	0	4708
% Approach	60.0%	0%	36.0%	4.0%	-	-	0.3%	98.0%	1.6%	0%	-	-	42.4%	0%	57.6%	0%	-	-	2.3%	97.3%	0.4%	0%	-	-	-
% Total	0.3%	0%	0.2%	0%	0.5%	-	0.1%	46.8%	0.8%	0%	47.7%	-	0.8%	0%	1.0%	0%	1.8%	-	1.1%	48.6%	0.2%	0%	49.9%	-	-
Lights	15	0	9	1	25	-	7	2180	37	0	2224	-	36	0	49	0	85	-	52	2273	9	1	2335	-	4669
% Lights	100%	0%	100%	100%	100%	-	100%	99.0%	100%	0%	99.0%	-	100%	0%	100%	0%	100%	-	96.3%	99.4%	100%	100%	99.3%	-	99.2%
Articulated Trucks and Single-Unit Trucks	0	0	0	0	0	-	0	21	0	0	21	-	0	0	0	0	0	-	2	13	0	0	15	-	36
% Articulated Trucks and Single-Unit Trucks	0%	0%	0%	0%	0%	-	0%	1.0%	0%	0%	0.9%	-	0%	0%	0%	0%	0%	-	3.7%	0.6%	0%	0%	0.6%	-	0.8%
Buses	0	0	0	0	0	-	0	2	0	0	2	-	0	0	0	0	0	-	0	1	0	0	1	-	3
% Buses	0%	0%	0%	0%	0%	-	0%	0.1%	0%	0%	0.1%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0.1%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	2	-	-	-	-	-	1	-	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	100%	-	-	-	-	-	100%	-	-	-	-	-	-	-
Bicycles on Crosswalk	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	-	0%	-	-	-	-	-	0%	-	-	-	-	-	-	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Secret Lake Road - TMC

Sat Jun 10, 2023

Full Length (11 AM-1 PM)

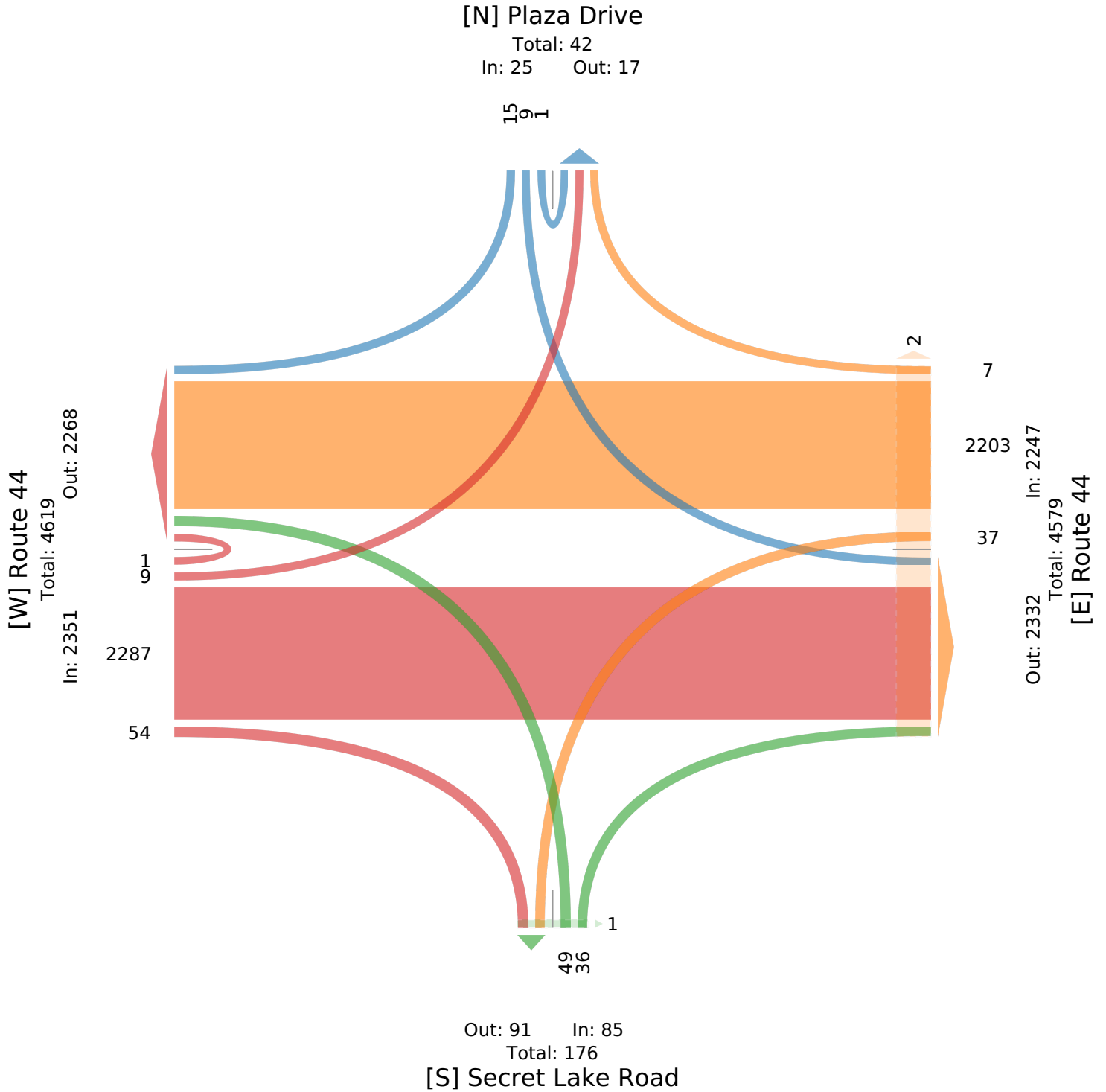
All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092495, Location: 41.823104, -72.879655

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US



Route 44 at Secret Lake Road - TMC

Sat Jun 10, 2023

Midday Peak (WKND) (11:45 AM - 12:45 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092495, Location: 41.823104, -72.879655

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,

Kensington, CT, 12345, US

Leg Direction	Plaza Drive Southbound						Route 44 Westbound						Secret Lake Road Northbound						Route 44 Eastbound						
Time	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	Int
2023-06-10 11:45AM	0	0	3	0	3	0	0	290	3	0	293	0	7	0	9	0	16	0	11	302	1	1	315	0	627
12:00PM	5	0	0	0	5	0	0	299	6	0	305	0	3	0	6	0	9	1	9	303	0	0	312	0	631
12:15PM	1	0	2	0	3	0	3	289	2	0	294	0	4	0	3	0	7	0	5	254	0	0	259	0	563
12:30PM	2	0	1	0	3	0	2	278	6	0	286	2	5	0	7	0	12	0	5	304	1	0	310	0	611
Total	8	0	6	0	14	0	5	1156	17	0	1178	2	19	0	25	0	44	1	30	1163	2	1	1196	0	2432
% Approach	57.1%	0%	42.9%	0%	-	-	0.4%	98.1%	1.4%	0%	-	-	43.2%	0%	56.8%	0%	-	-	2.5%	97.2%	0.2%	0.1%	-	-	-
% Total	0.3%	0%	0.2%	0%	0.6%	-	0.2%	47.5%	0.7%	0%	48.4%	-	0.8%	0%	1.0%	0%	1.8%	-	1.2%	47.8%	0.1%	0%	49.2%	-	-
PHF	0.400	-	0.500	-	0.700	-	0.417	0.967	0.708	-	0.966	-	0.679	-	0.694	-	0.688	-	0.682	0.956	0.500	0.250	0.949	-	0.964
Lights	8	0	6	0	14	-	5	1148	17	0	1170	-	19	0	25	0	44	-	29	1157	2	1	1189	-	2417
% Lights	100%	0%	100%	0%	100%	-	100%	99.3%	100%	0%	99.3%	-	100%	0%	100%	0%	100%	-	96.7%	99.5%	100%	100%	99.4%	-	99.4%
Articulated Trucks and Single-Unit Trucks	0	0	0	0	0	-	0	7	0	0	7	-	0	0	0	0	0	-	1	6	0	0	7	-	14
% Articulated Trucks and Single-Unit Trucks	0%	0%	0%	0%	0%	-	0%	0.6%	0%	0%	0.6%	-	0%	0%	0%	0%	0%	-	3.3%	0.5%	0%	0%	0.6%	-	0.6%
Buses	0	0	0	0	0	-	0	1	0	0	1	-	0	0	0	0	0	-	0	0	0	0	0	-	1
% Buses	0%	0%	0%	0%	0%	-	0%	0.1%	0%	0%	0.1%	-	0%	0%	0%	0%	0%	-	0%	0%	0%	0%	0%	-	0%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	2	-	-	-	-	-	1	-	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	100%	-	-	-	-	-	100%	-	-	-	-	-	-	-
Bicycles on Crosswalk	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	-	0%	-	-	-	-	-	0%	-	-	-	-	-	-	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Secret Lake Road - TMC

Sat Jun 10, 2023

Midday Peak (WKND) (11:45 AM - 12:45 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092495, Location: 41.823104, -72.879655

Provided by: Connecticut Counts LLC

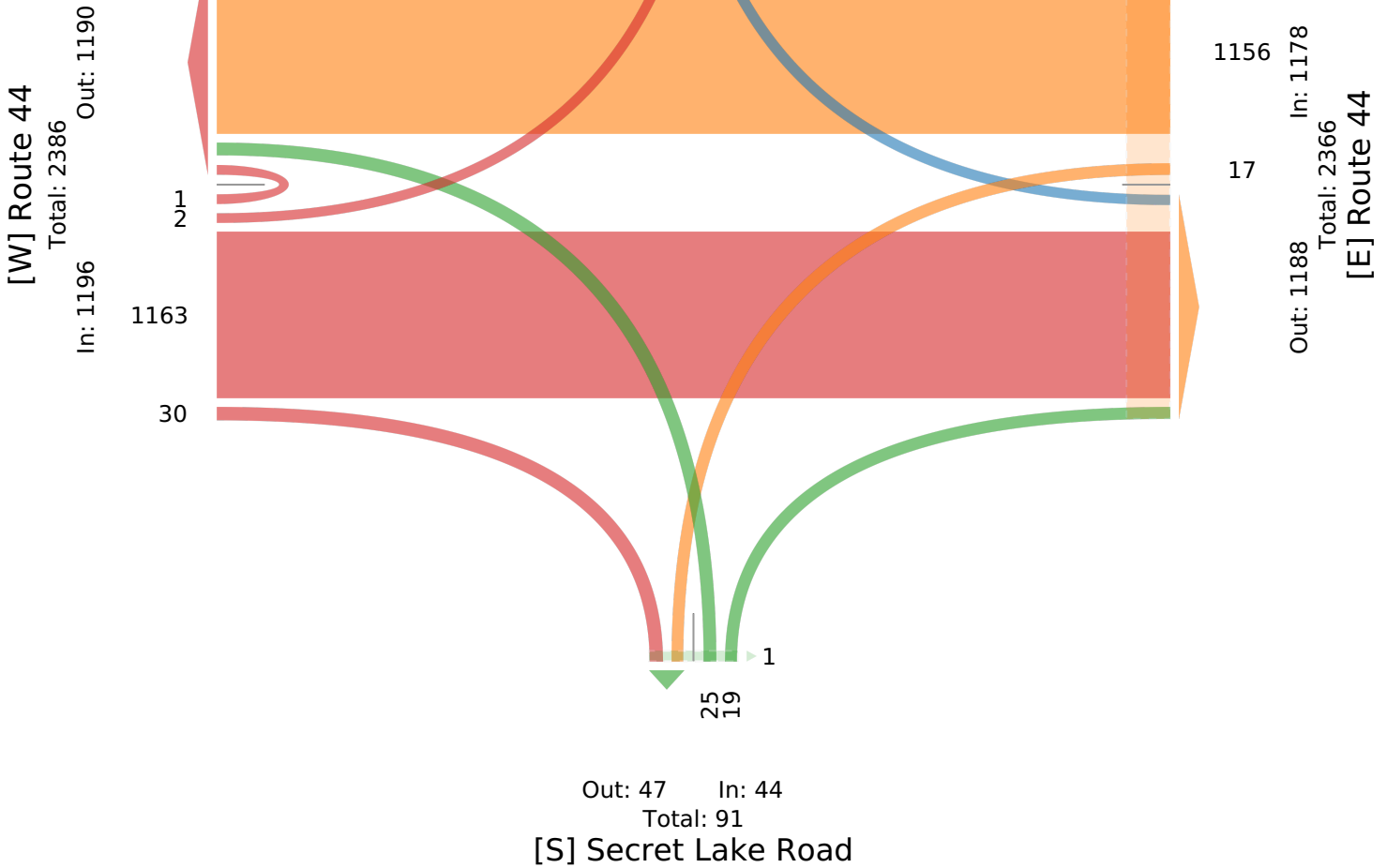
63 Sugar Maple Lane,
Kensington, CT, 12345, US

[N] Plaza Drive

Total: 21

In: 14 Out: 7

06



Route 44 at Secret Lake Road - TMC

Thu Jun 8, 2023

Full Length (4 PM-6 PM)

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092491, Location: 41.823104, -72.879655

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Plaza Drive Southbound						Route 44 Westbound						Secret Lake Road Northbound						Route 44 Eastbound						
Time	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	R	T	L	U	App	Ped*	Int
2023-06-08 4:00PM	2	1	2	0	5	0	1	370	4	0	375	0	5	0	4	0	9	0	2	260	3	0	265	0	654
4:15PM	1	1	0	0	2	0	1	328	7	0	336	0	4	0	7	0	11	0	7	231	1	0	239	0	588
4:30PM	7	0	3	0	10	0	2	329	5	0	336	0	3	0	7	0	10	0	4	278	4	0	286	0	642
4:45PM	3	0	1	0	4	0	0	375	10	0	385	0	4	0	11	0	15	0	4	294	1	0	299	0	703
Hourly Total	13	2	6	0	21	0	4	1402	26	0	1432	0	16	0	29	0	45	0	17	1063	9	0	1089	0	2587
5:00PM	3	0	4	0	7	0	2	318	5	0	325	0	14	0	7	0	21	0	5	297	0	0	302	0	655
5:15PM	3	0	5	0	8	0	1	325	10	0	336	0	5	0	2	0	7	0	6	245	1	1	253	0	604
5:30PM	1	0	2	0	3	0	0	325	7	0	332	0	5	0	9	0	14	0	6	230	0	1	237	0	586
5:45PM	2	0	1	0	3	0	0	307	9	0	316	0	4	0	7	0	11	0	2	219	0	0	221	0	551
Hourly Total	9	0	12	0	21	0	3	1275	31	0	1309	0	28	0	25	0	53	0	19	991	1	2	1013	0	2396
Total	22	2	18	0	42	0	7	2677	57	0	2741	0	44	0	54	0	98	0	36	2054	10	2	2102	0	4983
% Approach	52.4%	4.8%	42.9%	0%	-	-	0.3%	97.7%	2.1%	0%	-	-	44.9%	0%	55.1%	0%	-	-	1.7%	97.7%	0.5%	0.1%	-	-	-
% Total	0.4%	0%	0.4%	0%	0.8%	-	0.1%	53.7%	1.1%	0%	55.0%	-	0.9%	0%	1.1%	0%	2.0%	-	0.7%	41.2%	0.2%	0%	42.2%	-	-
Lights	22	2	18	0	42	-	7	2652	57	0	2716	-	44	0	52	0	96	-	36	2019	10	2	2067	-	4921
% Lights	100%	100%	100%	0%	100%	-	100%	99.1%	100%	0%	99.1%	-	100%	0%	96.3%	0%	98.0%	-	100%	98.3%	100%	100%	98.3%	-	98.8%
Articulated Trucks and Single-Unit Trucks	0	0	0	0	0	-	0	15	0	0	15	-	0	0	1	0	1	-	0	24	0	0	24	-	40
% Articulated Trucks and Single-Unit Trucks	0%	0%	0%	0%	0%	-	0%	0.6%	0%	0%	0.5%	-	0%	0%	1.9%	0%	1.0%	-	0%	1.2%	0%	0%	1.1%	-	0.8%
Buses	0	0	0	0	0	-	0	10	0	0	10	-	0	0	1	0	1	-	0	11	0	0	11	-	22
% Buses	0%	0%	0%	0%	0%	-	0%	0.4%	0%	0%	0.4%	-	0%	0%	1.9%	0%	1.0%	-	0%	0.5%	0%	0%	0.5%	-	0.4%
Pedestrians	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Bicycles on Crosswalk	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Secret Lake Road - TMC

Thu Jun 8, 2023

Full Length (4 PM-6 PM)

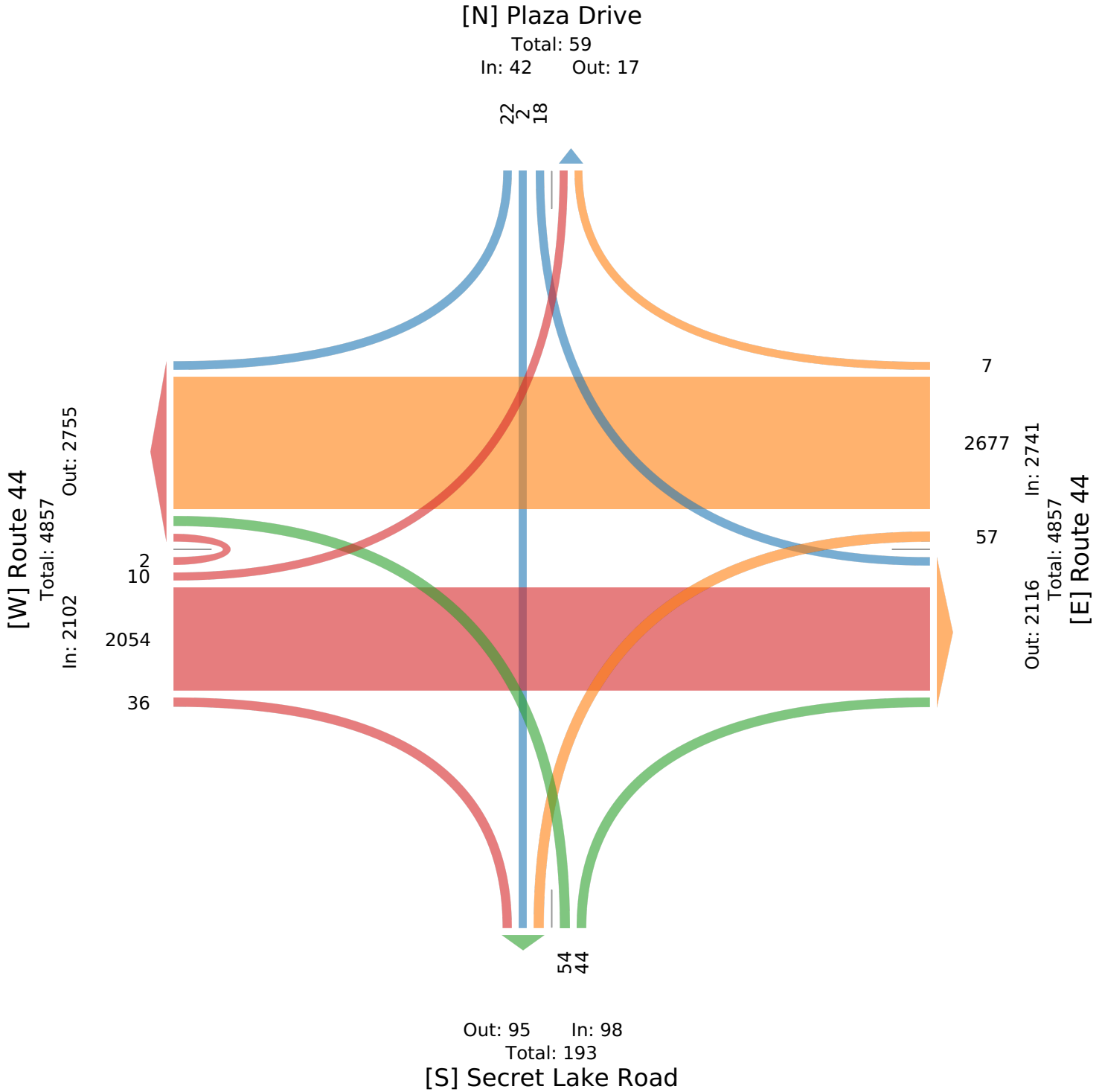
All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092491, Location: 41.823104, -72.879655

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US



Route 44 at Secret Lake Road - TMC

Thu Jun 8, 2023

PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour

All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092491, Location: 41.823104, -72.879655

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US

Leg Direction	Plaza Drive Southbound					Route 44 Westbound					Secret Lake Road Northbound					Route 44 Eastbound									
Time	R	T	L	U	App Ped*	R	T	L	U	App Ped*	R	T	L	U	App Ped*	R	T	L	U	App Ped*	Int				
2023-06-08 4:30PM	7	0	3	0	10	0	2	329	5	0	336	0	3	0	7	0	10	0	4	278	4	0	286	0	642
4:45PM	3	0	1	0	4	0	0	375	10	0	385	0	4	0	11	0	15	0	4	294	1	0	299	0	703
5:00PM	3	0	4	0	7	0	2	318	5	0	325	0	14	0	7	0	21	0	5	297	0	0	302	0	655
5:15PM	3	0	5	0	8	0	1	325	10	0	336	0	5	0	2	0	7	0	6	245	1	1	253	0	604
Total	16	0	13	0	29	0	5	1347	30	0	1382	0	26	0	27	0	53	0	19	1114	6	1	1140	0	2604
% Approach	55.2%	0%	44.8%	0%	-	-	0.4%	97.5%	2.2%	0%	-	-	49.1%	0%	50.9%	0%	-	-	1.7%	97.7%	0.5%	0.1%	-	-	-
% Total	0.6%	0%	0.5%	0%	1.1%	-	0.2%	51.7%	1.2%	0%	53.1%	-	1.0%	0%	1.0%	0%	2.0%	-	0.7%	42.8%	0.2%	0%	43.8%	-	-
PHF	0.571	-	0.650	-	0.725	-	0.625	0.898	0.750	-	0.897	-	0.464	-	0.614	-	0.631	-	0.792	0.938	0.375	0.250	0.944	-	0.926
Lights	16	0	13	0	29	-	5	1334	30	0	1369	-	26	0	27	0	53	-	19	1094	6	1	1120	-	2571
% Lights	100%	0%	100%	0%	100%	-	100%	99.0%	100%	0%	99.1%	-	100%	0%	100%	0%	100%	-	100%	98.2%	100%	100%	98.2%	-	98.7%
Articulated Trucks and Single-Unit Trucks	0	0	0	0	0	-	0	7	0	0	7	-	0	0	0	0	0	-	0	13	0	0	13	-	20
% Articulated Trucks and Single-Unit Trucks	0%	0%	0%	0%	0%	-	0%	0.5%	0%	0%	0.5%	-	0%	0%	0%	0%	0%	-	0%	1.2%	0%	0%	1.1%	-	0.8%
Buses	0	0	0	0	0	-	0	6	0	0	6	-	0	0	0	0	0	-	0	7	0	0	7	-	13
% Buses	0%	0%	0%	0%	0%	-	0%	0.4%	0%	0%	0.4%	-	0%	0%	0%	0%	0%	-	0%	0.6%	0%	0%	0.6%	-	0.5%
Pedestrians	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Pedestrians	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-
Bicycles on Crosswalk	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-	-	-	-	0	-	-
% Bicycles on Crosswalk	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

*Pedestrians and Bicycles on Crosswalk. L: Left, R: Right, T: Thru, U: U-Turn

Route 44 at Secret Lake Road - TMC

Thu Jun 8, 2023

PM Peak (4:30 PM - 5:30 PM) - Overall Peak Hour

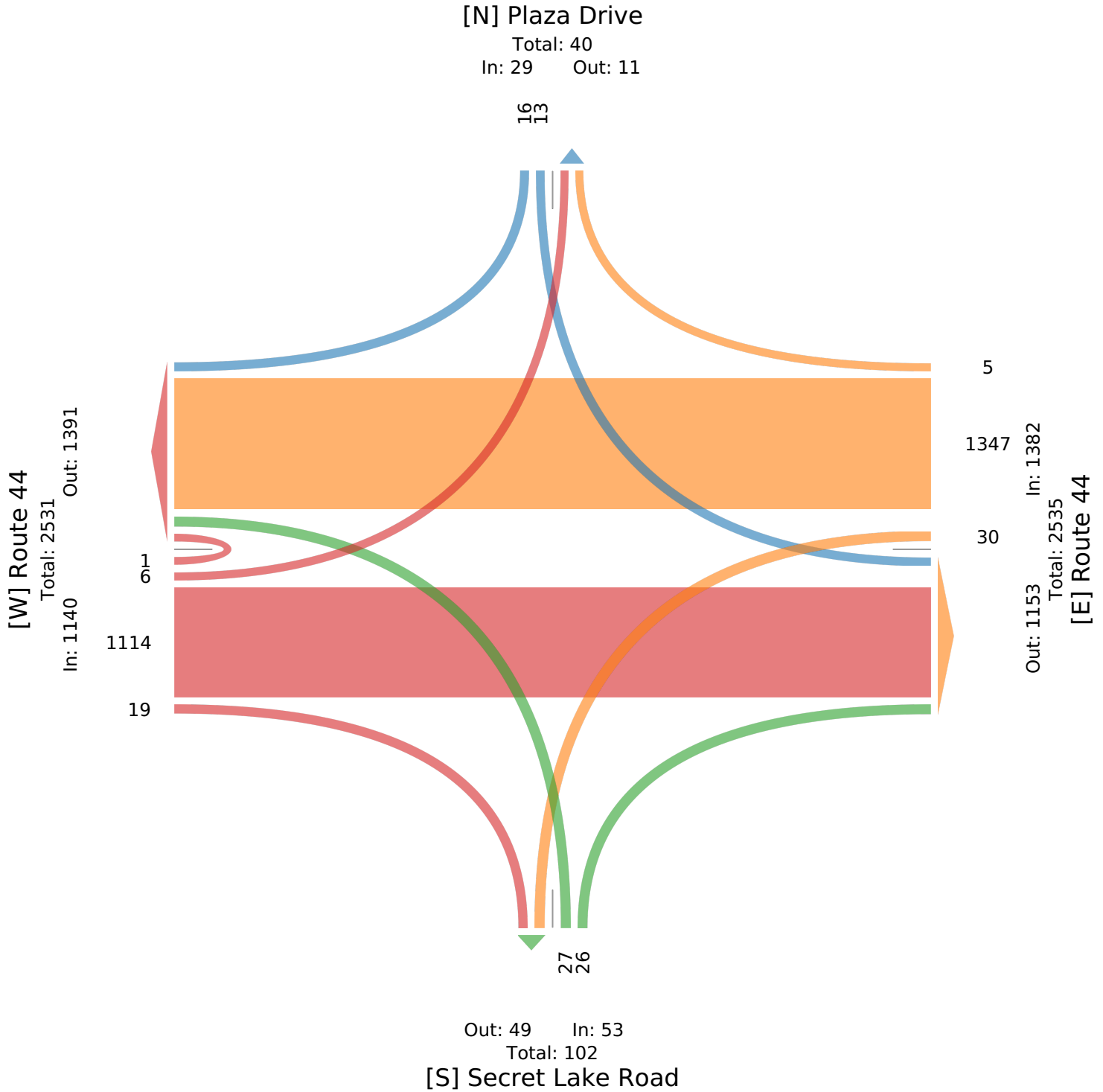
All Classes (Lights, Articulated Trucks and Single-Unit Trucks, Buses, Pedestrians, Bicycles on Crosswalk)

All Movements

ID: 1092491, Location: 41.823104, -72.879655

Provided by: Connecticut Counts LLC

63 Sugar Maple Lane,
Kensington, CT, 12345, US



Route 44 West of Route 177 (EB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5927

Latitude: 0' 0.0000 Undefined

Eastbound															
Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
10/02/23	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
12 PM	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
13:00	5	599	100	3	11	6	3	17	5	3	2	5	7	0	766
14:00	6	658	90	7	3	11	16	27	3	11	0	4	10	0	846
15:00	6	532	85	8	13	9	13	22	0	13	2	6	22	0	731
16:00	2	666	70	3	9	6	8	29	3	14	0	8	18	0	836
17:00	10	624	72	3	5	8	10	10	0	10	1	10	10	0	773
18:00	6	556	73	3	3	1	6	4	2	5	0	2	0	0	661
19:00	2	356	36	1	1	0	1	3	0	1	0	0	0	0	401
20:00	0	184	13	1	1	0	0	0	1	0	0	0	0	0	200
21:00	2	126	8	0	0	0	1	0	0	0	0	0	0	0	137
22:00	1	77	6	0	1	0	0	0	0	0	0	0	0	0	85
23:00	0	42	4	0	1	0	0	0	0	0	0	0	0	0	47
Total	40	4420	557	29	48	41	58	112	14	57	5	35	67	0	5483
Percent	0.7%	80.6%	10.2%	0.5%	0.9%	0.7%	1.1%	2.0%	0.3%	1.0%	0.1%	0.6%	1.2%	0.0%	
AM Peak															
Vol.	17:00	16:00	13:00	15:00	15:00	14:00	14:00	16:00	13:00	16:00	13:00	17:00	15:00		
Vol.	10	666	100	8	13	11	16	29	5	14	2	10	22		

Route 44 West of Route 177 (EB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5927

Latitude: 0' 0.0000 Undefined

Eastbound															
Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
10/03/23	2	17	2	0	0	0	0	1	0	0	0	0	0	0	22
01:00	0	9	1	0	0	0	0	0	0	0	0	0	0	0	10
02:00	0	14	1	0	1	0	0	0	0	0	0	0	0	0	16
03:00	0	24	3	0	0	0	0	0	0	0	0	0	0	0	27
04:00	0	59	10	2	3	2	0	0	0	0	0	0	0	0	76
05:00	3	280	57	1	3	0	0	5	3	1	0	1	0	0	354
06:00	10	514	97	6	5	5	6	20	1	11	1	4	6	0	686
07:00	10	636	75	6	4	6	24	28	7	22	1	12	36	0	867
08:00	6	704	105	1	12	10	17	24	5	17	1	8	25	0	935
09:00	3	758	95	2	11	10	11	7	8	10	1	2	1	0	919
10:00	6	727	84	0	9	7	8	15	4	6	0	3	5	0	874
11:00	4	698	76	5	7	7	10	15	5	3	0	5	0	0	835
12 PM	8	628	86	2	10	12	7	9	6	6	0	3	9	0	786
13:00	6	602	93	0	6	10	7	17	2	12	0	5	10	0	770
14:00	6	650	81	15	8	8	7	20	3	13	1	6	14	0	832
15:00	5	643	77	9	14	12	10	41	2	19	1	11	17	0	861
16:00	8	623	67	2	6	4	14	21	5	16	0	6	28	0	800
17:00	7	626	74	4	5	6	6	14	2	16	0	6	12	0	778
18:00	9	549	56	2	5	3	7	9	2	2	1	3	7	0	655
19:00	4	354	33	1	1	2	2	5	1	1	0	0	0	0	404
20:00	1	217	15	0	3	1	0	1	0	0	0	0	1	0	239
21:00	0	163	8	0	2	0	0	0	0	0	0	0	0	0	173
22:00	1	64	11	1	1	0	0	0	0	0	0	0	0	0	78
23:00	0	38	3	0	1	0	0	0	1	0	0	0	0	0	43
Total	99	9597	1210	59	117	105	136	252	57	155	7	75	171	0	12040
Percent	0.8%	79.7%	10.0%	0.5%	1.0%	0.9%	1.1%	2.1%	0.5%	1.3%	0.1%	0.6%	1.4%	0.0%	
AM Peak	06:00	09:00	08:00	06:00	08:00	08:00	07:00	07:00	09:00	07:00	06:00	07:00	07:00		
Vol.	10	758	105	6	12	10	24	28	8	22	1	12	36		
PM Peak	18:00	14:00	13:00	14:00	15:00	12:00	16:00	15:00	12:00	15:00	14:00	15:00	16:00		
Vol.	9	650	93	15	14	12	14	41	6	19	1	11	28		

Route 44 West of Route 177 (EB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5927

Latitude: 0' 0.0000 Undefined

Eastbound															
Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
10/04/23	0	20	3	0	0	0	0	1	0	0	0	0	0	0	24
01:00	0	10	1	0	0	0	0	0	3	0	0	0	0	0	14
02:00	0	15	0	0	0	0	0	0	0	0	0	0	0	0	15
03:00	0	23	2	1	0	0	0	0	0	0	0	0	0	0	26
04:00	0	67	10	2	2	1	0	1	0	0	0	0	0	0	83
05:00	2	250	59	1	5	2	2	4	2	1	0	0	1	0	329
06:00	6	539	102	5	11	4	2	13	6	6	0	6	5	0	705
07:00	7	682	90	6	11	13	14	27	7	22	3	6	47	0	935
08:00	8	585	80	2	8	12	16	25	10	19	0	4	35	0	804
09:00	8	665	83	3	15	9	14	23	7	7	1	6	8	0	849
10:00	3	640	90	4	12	10	8	14	15	8	0	7	8	0	819
11:00	5	622	88	2	10	6	6	10	5	7	0	8	9	0	778
12 PM	5	611	69	2	10	8	15	17	4	11	0	5	16	0	773
13:00	4	607	85	3	9	11	4	19	6	11	0	4	3	0	766
14:00	3	577	84	9	8	17	8	20	2	5	1	8	20	0	762
15:00	8	644	90	10	6	11	11	19	5	11	2	10	21	0	848
16:00	9	626	69	4	14	15	10	25	8	17	1	8	31	0	837
17:00	11	648	78	5	5	14	17	18	1	8	0	9	9	0	823
18:00	6	551	60	2	7	2	2	9	3	2	0	1	3	0	648
19:00	5	398	30	3	2	2	2	3	0	0	0	2	2	0	449
20:00	2	225	18	0	0	0	0	0	1	0	0	1	1	0	248
21:00	1	176	12	0	1	1	0	1	1	1	0	0	0	0	194
22:00	0	74	5	1	0	0	0	1	0	0	0	0	0	0	81
23:00	0	53	3	0	1	0	0	0	0	0	0	0	0	0	57
Total	93	9308	1211	65	137	138	131	250	86	136	8	85	219	0	11867
Percent	0.8%	78.4%	10.2%	0.5%	1.2%	1.2%	1.1%	2.1%	0.7%	1.1%	0.1%	0.7%	1.8%	0.0%	
AM Peak	08:00	07:00	06:00	07:00	09:00	07:00	08:00	07:00	10:00	07:00	07:00	11:00	07:00		
Vol.	8	682	102	6	15	13	16	27	15	22	3	8	47		
PM Peak	17:00	17:00	15:00	15:00	16:00	14:00	17:00	16:00	16:00	16:00	15:00	15:00	16:00		
Vol.	11	648	90	10	14	17	17	25	8	17	2	10	31		

Route 44 West of Route 177 (EB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5927

Latitude: 0' 0.0000 Undefined

Eastbound															
Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
10/05/23	0	19	2	0	0	0	0	1	0	0	0	0	0	0	22
01:00	0	4	0	0	2	0	0	0	1	0	0	0	0	0	7
02:00	0	13	1	0	0	0	0	0	1	0	0	0	0	0	15
03:00	0	20	3	0	1	0	0	0	0	0	0	0	0	0	24
04:00	0	65	10	1	6	1	0	1	2	0	0	0	0	0	86
05:00	3	246	58	2	2	2	1	6	3	1	0	1	0	0	325
06:00	8	520	90	8	15	5	5	21	2	6	0	3	8	0	691
07:00	6	656	104	8	6	23	21	24	10	17	5	14	36	0	930
08:00	7	647	90	2	8	13	13	25	8	23	4	8	32	0	880
09:00	2	618	79	2	8	9	13	23	4	20	0	5	13	0	796
10:00	3	623	82	2	11	11	16	11	5	11	1	8	12	0	796
11:00	5	630	75	4	9	7	6	16	5	9	1	5	15	0	787
12 PM	5	624	77	0	6	7	15	20	5	12	1	4	14	0	790
13:00	7	623	84	3	8	8	9	17	7	9	0	3	7	0	785
14:00	8	538	63	8	9	7	13	17	1	14	2	8	20	0	708
15:00	6	520	68	6	7	4	17	16	5	12	1	8	28	0	698
16:00	7	611	60	2	10	10	12	20	5	17	0	9	23	0	786
17:00	8	499	50	2	6	5	7	18	1	9	1	7	22	0	635
18:00	0	561	65	4	5	5	5	10	2	7	0	4	5	0	673
19:00	3	435	21	0	3	4	3	3	0	2	0	0	0	0	474
20:00	1	235	14	0	0	2	0	3	2	0	0	0	0	0	257
21:00	0	188	14	0	1	1	0	0	0	0	0	0	0	0	204
22:00	1	78	8	1	2	0	0	1	1	0	0	0	0	0	92
23:00	1	51	3	1	1	0	0	0	0	0	0	0	0	0	57
Total	81	9024	1121	56	126	124	156	253	70	169	16	87	235	0	11518
Percent	0.7%	78.3%	9.7%	0.5%	1.1%	1.1%	1.4%	2.2%	0.6%	1.5%	0.1%	0.8%	2.0%	0.0%	
AM Peak	06:00	07:00	07:00	06:00	06:00	07:00	07:00	08:00	07:00	08:00	07:00	07:00	07:00	07:00	
Vol.	8	656	104	8	15	23	21	25	10	23	5	14	36		
PM Peak	14:00	12:00	13:00	14:00	16:00	16:00	15:00	12:00	13:00	16:00	14:00	16:00	15:00		
Vol.	8	624	84	8	10	10	17	20	7	17	2	9	28		

Route 44 West of Route 177 (EB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5927

Latitude: 0' 0.0000 Undefined

Eastbound															
Start Time	Class 1	Class 2	Class 3	Class 4	Class 5	Class 6	Class 7	Class 8	Class 9	Class 10	Class 11	Class 12	Class 13	Class 14	Total
10/06/23	0	27	1	0	1	0	0	1	0	0	0	0	0	0	30
01:00	0	11	2	0	0	0	0	0	2	0	0	0	0	0	15
02:00	0	14	3	0	2	0	0	0	0	0	0	0	0	0	19
03:00	0	26	3	1	0	0	0	0	1	0	0	0	0	0	31
04:00	0	58	9	1	2	1	0	0	2	0	0	0	0	0	73
05:00	0	223	59	2	5	0	0	7	2	1	1	0	0	0	300
06:00	1	482	102	7	3	7	5	14	2	7	0	1	4	0	635
07:00	6	704	111	8	5	9	15	22	5	20	4	6	22	0	937
08:00	6	631	88	8	10	13	10	22	5	17	1	7	21	0	839
09:00	6	643	96	2	7	5	11	16	4	11	1	8	9	0	819
10:00	3	635	77	5	12	5	10	25	10	17	1	6	10	0	816
11:00	6	569	79	18	21	12	12	19	6	9	2	9	16	0	778
12 PM	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
Total	28	4023	630	52	68	52	63	126	39	82	10	37	82	0	5292
Percent	0.5%	76.0%	11.9%	1.0%	1.3%	1.0%	1.2%	2.4%	0.7%	1.5%	0.2%	0.7%	1.5%	0.0%	
AM Peak	07:00	07:00	07:00	11:00	11:00	08:00	07:00	10:00	10:00	07:00	07:00	11:00	07:00		
Vol.	6	704	111	18	21	13	15	25	10	20	4	9	22		
PM Peak															
Vol.															
Grand Total	341	36372	4729	261	496	460	544	993	266	599	46	319	774	0	46200
Percent	0.7%	78.7%	10.2%	0.6%	1.1%	1.0%	1.2%	2.1%	0.6%	1.3%	0.1%	0.7%	1.7%	0.0%	

Route 44 West of Route 177 (EB)
Canton, Connecticut

Connecticut Counts LLC

Kensington, Connecticut 06037

(860) 828-1693

Site Code:
Station ID: 5927

Latitude: 0' 0.0000 Undefined

Eastbound																	
Start Time	0	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75	9999			
10/02/23	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
12 PM	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
13:00	13	24	62	210	312	136	16	1	0	0	0	0	0	0	774	26-35	523
14:00	17	50	109	250	301	116	10	2	0	0	0	0	0	0	855	26-35	552
15:00	34	33	83	210	263	109	8	0	0	0	0	0	0	0	740	26-35	474
16:00	25	26	61	236	344	141	10	1	0	0	0	0	0	0	844	26-35	581
17:00	7	25	62	201	328	140	18	0	0	0	0	0	0	0	781	26-35	529
18:00	1	9	20	120	341	157	16	1	1	0	0	0	0	0	666	31-40	498
19:00	0	7	12	80	186	97	21	0	0	0	0	0	0	0	403	31-40	283
20:00	2	3	1	25	77	72	20	2	0	0	0	0	0	0	202	31-40	149
21:00	0	0	2	17	48	52	16	2	0	0	0	0	0	0	137	31-40	100
22:00	0	1	0	7	20	30	24	2	0	0	1	0	0	0	85	36-45	54
23:00	0	0	0	4	11	21	7	3	0	0	1	0	0	0	47	31-40	32
Total	99	178	412	1360	2231	1071	166	14	1	0	2	0	0	0	5534		
Percent	1.8%	3.2%	7.4%	24.6%	40.3%	19.4%	3.0%	0.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
AM Peak																	
Vol.																	
PM Peak	15:00	14:00	14:00	14:00	16:00	18:00	22:00	23:00	18:00		22:00				14:00		
Vol.	34	50	109	250	344	157	24	3	1		1				855		

Route 44 West of Route 177 (EB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5927

Latitude: 0' 0.0000 Undefined

Eastbound																Pace	Number
Start Time	0	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Speed	in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75	9999			
10/03/23	0	0	0	0	6	9	3	1	1	1	1	0	0	0	22	31-40	15
01:00	0	0	0	1	4	2	2	1	0	0	0	0	0	0	10	31-40	6
02:00	0	0	1	1	7	6	0	1	0	0	0	0	0	0	16	31-40	13
03:00	0	0	0	1	3	14	8	1	0	0	0	0	0	0	27	36-45	22
04:00	0	0	0	0	17	31	25	2	1	0	0	0	0	0	76	36-45	56
05:00	1	14	14	7	85	155	65	10	2	2	0	0	0	0	355	31-40	240
06:00	12	28	37	102	258	198	53	3	1	0	0	0	0	0	692	31-40	456
07:00	46	77	138	235	265	106	10	0	0	0	0	0	0	0	877	26-35	502
08:00	46	70	129	263	322	102	9	1	0	0	0	0	0	0	942	26-35	587
09:00	12	42	83	242	417	120	12	0	0	0	0	0	0	0	928	26-35	660
10:00	18	32	96	265	343	115	11	1	0	0	0	0	0	0	881	26-35	609
11:00	15	31	66	293	340	89	9	0	0	0	0	0	0	0	843	26-35	634
12 PM	19	45	108	284	247	84	9	0	0	0	0	0	0	0	796	26-35	532
13:00	20	29	69	243	316	91	11	0	0	0	0	0	0	0	779	26-35	560
14:00	13	42	70	260	319	127	8	1	0	0	0	0	0	0	840	26-35	580
15:00	19	59	99	308	279	97	9	0	0	0	0	0	0	0	870	26-35	588
16:00	26	50	92	232	299	98	13	0	0	0	0	0	0	0	810	26-35	532
17:00	18	35	43	218	331	130	10	2	0	0	0	0	0	0	787	26-35	550
18:00	1	16	29	187	275	137	15	0	0	0	0	0	0	0	660	26-35	462
19:00	1	5	2	85	182	119	12	1	0	0	0	0	0	0	407	31-40	301
20:00	0	2	4	30	107	72	22	4	0	0	0	0	0	0	241	31-40	179
21:00	0	0	5	19	71	66	10	2	0	0	0	0	0	0	173	31-40	137
22:00	0	1	1	5	25	28	11	5	1	1	0	0	0	0	78	31-40	53
23:00	0	0	0	1	9	21	10	1	1	0	0	0	0	0	43	34-43	31
Total	267	578	1086	3282	4527	2017	347	37	7	4	1	0	0	0	12153		
Percent	2.2%	4.8%	8.9%	27.0%	37.3%	16.6%	2.9%	0.3%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%			
AM Peak	07:00	07:00	07:00	11:00	09:00	06:00	05:00	05:00	05:00	05:00	00:00				08:00		
Vol.	46	77	138	293	417	198	65	10	2	2	1				942		
PM Peak	16:00	15:00	12:00	15:00	17:00	18:00	20:00	22:00	22:00	22:00					15:00		
Vol.	26	59	108	308	331	137	22	5	1	1					870		

Route 44 West of Route 177 (EB)
Canton, Connecticut

Connecticut Counts LLC Kensington, Connecticut 06037 (860) 828-1693

Site Code:
Station ID: 5927

Latitude: 0' 0.0000 Undefined

Eastbound																Pace	Number
Start	0	16	21	26	31	36	41	46	51	56	61	66	71	76			
Time	15	20	25	30	35	40	45	50	55	60	65	70	75	9999	Total	Speed	in Pace
10/04/23	0	0	2	7	9	5	1	0	0	0	0	0	0	0	24	26-35	16
01:00	0	0	2	4	6	1	1	0	0	0	0	0	0	0	14	26-35	10
02:00	0	0	2	6	7	0	0	0	0	0	0	0	0	0	15	26-35	13
03:00	0	0	0	12	12	2	0	0	0	0	0	0	0	0	26	26-35	24
04:00	0	0	4	13	28	21	14	3	0	0	0	0	0	0	83	31-40	49
05:00	0	11	13	19	87	129	56	14	1	1	0	0	0	0	331	31-40	216
06:00	2	32	46	105	236	233	56	1	0	0	0	0	0	0	711	31-40	469
07:00	47	82	125	264	312	102	11	1	0	0	0	0	0	0	944	26-35	578
08:00	62	80	128	238	202	94	6	0	0	0	0	0	0	0	810	26-35	442
09:00	28	44	85	235	342	106	16	0	0	1	0	0	0	0	857	26-35	578
10:00	20	39	83	233	306	138	9	0	0	0	0	0	0	0	828	26-35	540
11:00	15	42	75	234	313	90	15	1	0	0	0	0	0	0	785	26-35	548
12 PM	39	48	95	233	247	105	16	0	0	0	0	0	0	0	783	26-35	481
13:00	6	40	64	207	318	128	13	0	0	0	0	0	0	0	776	26-35	525
14:00	34	38	85	217	292	89	13	2	0	0	0	0	0	0	770	26-35	510
15:00	32	58	96	261	275	119	12	1	0	0	0	0	0	0	854	26-35	537
16:00	44	60	101	241	283	103	9	1	2	0	0	0	0	0	844	26-35	526
17:00	23	40	90	245	304	122	7	0	0	0	0	0	0	0	831	26-35	550
18:00	0	12	36	156	313	121	15	1	0	1	0	0	0	0	655	26-35	469
19:00	0	9	13	87	204	118	19	0	0	0	0	0	0	0	450	31-40	322
20:00	1	2	4	45	112	68	17	1	1	0	0	0	0	0	251	31-40	180
21:00	0	2	4	10	82	70	21	5	0	0	0	0	0	0	194	31-40	152
22:00	1	0	1	5	26	30	11	7	0	0	0	0	0	0	81	31-40	56
23:00	0	0	0	4	10	29	9	3	1	1	0	0	0	0	57	31-40	39
Total	354	639	1154	3081	4326	2023	347	41	5	4	0	0	0	0	11974		
Percent	3.0%	5.3%	9.6%	25.7%	36.1%	16.9%	2.9%	0.3%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
AM Peak	08:00	07:00	08:00	07:00	09:00	06:00	05:00	05:00	05:00	05:00					07:00		
Vol.	62	82	128	264	342	233	56	14	1	1					944		
PM Peak	16:00	16:00	16:00	15:00	13:00	13:00	21:00	22:00	16:00	18:00					15:00		
Vol.	44	60	101	261	318	128	21	7	2	1					854		

Route 44 West of Route 177 (EB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5927

Latitude: 0' 0.0000 Undefined

Eastbound																		
Start Time	0	16	21	26	31	36	41	46	51	56	61	66	71	76	9999	Total	Pace Speed	Number in Pace
10/05/23	0	0	0	2	7	6	5	2	0	0	0	0	0	0	0	22	31-40	13
01:00	0	0	0	1	3	1	2	0	0	0	0	0	0	0	0	7	31-40	4
02:00	0	0	0	3	3	3	5	1	0	0	0	0	0	0	0	15	36-45	8
03:00	0	0	0	0	5	8	10	1	0	0	0	0	0	0	0	24	36-45	18
04:00	0	0	2	4	25	31	19	4	1	0	0	0	0	0	0	86	31-40	56
05:00	1	12	12	23	95	105	64	14	1	0	0	0	0	0	0	327	31-40	200
06:00	4	28	51	117	251	190	49	5	2	0	0	0	0	0	0	697	31-40	441
07:00	48	83	131	291	246	115	22	1	0	0	0	0	0	0	0	937	26-35	539
08:00	48	76	126	279	264	82	11	2	0	0	0	0	0	0	0	888	26-35	545
09:00	2	49	96	257	299	95	7	0	0	0	0	0	0	0	0	805	26-35	556
10:00	17	53	95	256	275	99	8	1	0	0	0	0	0	0	0	804	26-35	532
11:00	30	39	97	274	261	81	13	1	0	0	0	0	0	0	0	796	26-35	536
12 PM	28	40	102	243	272	104	8	0	0	0	0	0	0	0	0	797	26-35	516
13:00	17	37	77	212	333	106	12	1	0	0	0	0	0	0	0	795	26-35	546
14:00	76	48	113	248	194	32	4	1	0	0	0	0	0	0	0	716	26-35	445
15:00	96	72	108	208	169	52	2	0	0	0	0	0	0	0	0	707	26-35	380
16:00	78	65	108	233	227	74	10	0	0	0	0	0	0	0	0	795	26-35	463
17:00	82	64	85	189	170	46	6	0	0	0	0	0	0	0	0	642	26-35	362
18:00	16	31	63	239	241	79	10	0	0	0	0	0	0	0	0	679	26-35	481
19:00	1	9	22	128	220	85	10	1	0	0	0	0	0	0	0	476	26-35	348
20:00	0	3	4	29	109	102	10	1	0	0	0	0	0	0	0	258	31-40	211
21:00	0	1	2	25	89	67	20	1	0	0	0	0	0	0	0	205	31-40	156
22:00	0	1	1	7	28	41	13	1	0	0	0	0	0	0	0	92	31-40	69
23:00	0	0	0	0	13	29	8	5	2	0	0	0	0	0	0	57	31-40	42
Total	544	711	1295	3268	3799	1633	328	43	6	0	0	0	0	0	0	11627		
Percent	4.7%	6.1%	11.1%	28.1%	32.7%	14.0%	2.8%	0.4%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
AM Peak	07:00	07:00	07:00	07:00	09:00	06:00	05:00	05:00	06:00							07:00		
Vol.	48	83	131	291	299	190	64	14	2							937		
PM Peak	15:00	15:00	14:00	14:00	13:00	13:00	21:00	23:00	23:00							12:00		
Vol.	96	72	113	248	333	106	20	5	2							797		

Route 44 West of Route 177 (EB)
Canton, Connecticut

Connecticut Counts LLC

Kensington, Connecticut 06037

(860) 828-1693

Site Code:
Station ID: 5927

Latitude: 0' 0.0000 Undefined

Eastbound																	
Start Time	0	16	21	26	31	36	41	46	51	56	61	66	71	76	Pace	Number	
	15	20	25	30	35	40	45	50	55	60	65	70	75	9999	Total	Speed	in Pace
10/06/23	0	0	1	0	9	10	8	2	0	0	0	0	0	0	30	31-40	19
01:00	0	0	1	1	5	3	5	0	0	0	0	0	0	0	15	30-39	8
02:00	0	0	2	1	5	11	0	0	0	0	0	0	0	0	19	31-40	16
03:00	0	0	1	1	7	13	8	0	1	0	0	0	0	0	31	34-43	21
04:00	0	0	0	3	13	33	18	4	1	0	1	0	0	0	73	36-45	51
05:00	3	19	15	13	65	135	41	8	1	0	0	0	0	0	300	31-40	200
06:00	2	32	36	118	217	194	37	4	0	0	0	0	0	0	640	31-40	411
07:00	19	62	109	310	304	125	14	0	0	0	0	0	0	0	943	26-35	615
08:00	31	86	94	244	303	84	9	0	0	0	0	0	0	0	851	26-35	548
09:00	16	50	88	292	276	98	7	2	0	0	0	0	0	0	829	26-35	569
10:00	22	41	133	272	265	86	6	1	0	0	0	0	0	0	826	26-35	538
11:00	38	60	85	246	244	68	9	2	7	11	3	3	4	0	780	26-35	491
12 PM	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
Total	131	350	565	1501	1713	860	162	23	10	11	4	3	4	0	5337		
Percent	2.5%	6.6%	10.6%	28.1%	32.1%	16.1%	3.0%	0.4%	0.2%	0.2%	0.1%	0.1%	0.1%	0.0%			
AM Peak	11:00	08:00	10:00	07:00	07:00	06:00	05:00	05:00	11:00	11:00	11:00	11:00	11:00		07:00		
Vol.	38	86	133	310	304	194	41	8	7	11	3	3	4		943		
PM Peak																	
Vol.																	
Total	1395	2456	4512	12492	16596	7604	1350	158	29	19	7	3	4	0	46625		
Percent	3.0%	5.3%	9.7%	26.8%	35.6%	16.3%	2.9%	0.3%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%			

15th Percentile : 23 MPH
 50th Percentile : 30 MPH
 85th Percentile : 36 MPH
 95th Percentile : 39 MPH

Stats
 10 MPH Pace Speed : 26-35 MPH
 Number in Pace : 29142
 Percent in Pace : 62.5%
 Number of Vehicles > 35 MPH : 9174
 Percent of Vehicles > 35 MPH : 19.7%
 Mean Speed(Average) : 30 MPH

Route 44 West of Route 177 (EB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5927

Latitude: 0' 0.0000 Undefined

Start Time	Mon 02-Oct-23	Tue 03-Oct-23	Wed 04-Oct-23	Thu 05-Oct-23	Fri 06-Oct-23	Week Day Average	Sat 07-Oct-23	Sun 08-Oct-23	Week Average
12:00 AM	*	22	24	22	30	24	*	*	24
01:00	*	10	14	7	15	12	*	*	12
02:00	*	16	15	15	19	16	*	*	16
03:00	*	27	26	24	31	27	*	*	27
04:00	*	76	83	86	73	80	*	*	80
05:00	*	355	331	327	300	328	*	*	328
06:00	*	692	711	697	640	685	*	*	685
07:00	*	877	944	937	943	925	*	*	925
08:00	*	942	810	888	851	873	*	*	873
09:00	*	928	857	805	829	855	*	*	855
10:00	*	881	828	804	826	835	*	*	835
11:00	*	843	785	796	780	801	*	*	801
12:00 PM	*	796	783	797	*	792	*	*	792
01:00	774	779	776	795	*	781	*	*	781
02:00	855	840	770	716	*	795	*	*	795
03:00	740	870	854	707	*	793	*	*	793
04:00	844	810	844	795	*	823	*	*	823
05:00	781	787	831	642	*	760	*	*	760
06:00	666	660	655	679	*	665	*	*	665
07:00	403	407	450	476	*	434	*	*	434
08:00	202	241	251	258	*	238	*	*	238
09:00	137	173	194	205	*	177	*	*	177
10:00	85	78	81	92	*	84	*	*	84
11:00	47	43	57	57	*	51	*	*	51
Total	5534	12153	11974	11627	5337	11854	0	0	11854
Daily Total									
Date									
03-Oct-23	12153								
04-Oct-23	11974								
05-Oct-23	11627								
Average	11918								
Grand Total	5534	12153	11974	11627	5337	11854	0	0	11854

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5928

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/02/23	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
12 PM	6	411	79	3	38	7	2	4	2	1	0	0	1	0	554
13:00	2	699	142	2	41	2	0	12	1	1	1	0	0	0	903
14:00	2	789	156	10	67	5	2	26	1	2	1	1	0	0	1062
15:00	9	877	203	8	74	5	2	25	2	4	1	0	0	0	1210
16:00	6	1098	219	3	76	2	0	21	1	2	1	1	1	0	1431
17:00	22	1064	172	1	70	1	0	23	2	0	0	0	0	0	1355
18:00	7	737	113	1	42	2	0	7	1	2	1	0	1	0	914
19:00	4	486	83	1	32	0	0	6	0	0	0	0	0	0	612
20:00	0	314	50	2	19	0	0	1	0	0	0	0	0	0	386
21:00	1	206	39	0	9	0	0	1	0	0	0	0	0	0	256
22:00	1	114	14	1	4	0	0	0	0	0	0	0	0	0	134
23:00	1	54	12	0	0	0	0	1	0	0	0	0	0	0	68
Total	61	6849	1282	32	472	24	6	127	10	12	5	2	3	0	8885
Percent	0.7%	77.1%	14.4%	0.4%	5.3%	0.3%	0.1%	1.4%	0.1%	0.1%	0.1%	0.0%	0.0%	0.0%	
AM Peak Vol.															
PM Peak Vol.	17:00	16:00	16:00	14:00	16:00	12:00	12:00	14:00	12:00	15:00	13:00	14:00	12:00		
	22	1098	219	10	76	7	2	26	2	4	1	1	1		

Route 44 West of Route 177 (WB)
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Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
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Latitude: 0' 0.0000 Undefined

Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/03/23	1	20	6	0	3	0	0	1	0	0	0	0	0	0	31
01:00	0	16	4	0	0	0	0	0	0	0	0	0	0	0	20
02:00	0	6	3	0	0	0	0	0	0	0	0	0	0	0	9
03:00	0	12	2	1	0	0	0	0	0	0	0	0	0	0	15
04:00	1	24	4	3	3	1	0	1	0	0	0	0	0	0	37
05:00	1	72	20	3	13	2	0	3	1	0	0	0	0	0	115
06:00	0	226	57	4	30	2	0	1	4	0	1	0	0	0	325
07:00	4	412	111	12	53	3	0	7	3	0	0	0	0	0	605
08:00	2	474	119	8	37	4	0	15	2	2	2	0	0	0	665
09:00	7	471	135	2	55	2	0	17	3	1	0	1	0	0	694
10:00	6	561	123	6	50	2	2	12	5	1	0	0	0	0	768
11:00	5	630	144	7	52	4	2	12	1	5	1	0	0	0	863
12 PM	15	755	131	4	49	6	0	13	0	4	0	0	0	0	977
13:00	6	712	161	4	52	3	3	17	3	0	1	0	1	0	963
14:00	14	807	186	6	65	5	4	14	2	0	1	1	0	0	1105
15:00	23	937	207	11	69	5	1	28	1	0	0	1	0	0	1283
16:00	19	1109	223	3	54	7	2	21	0	3	1	0	0	0	1442
17:00	9	1101	187	2	51	5	0	14	2	4	0	0	1	0	1376
18:00	17	843	137	1	35	2	0	8	0	3	0	0	0	0	1046
19:00	7	534	84	1	22	0	0	4	0	2	0	0	0	0	654
20:00	4	371	67	0	12	0	0	2	0	0	0	0	0	0	456
21:00	0	236	40	0	11	0	0	1	0	0	0	0	0	0	288
22:00	1	152	6	1	5	0	0	0	0	0	0	0	0	0	165
23:00	0	58	16	0	4	0	0	0	1	0	0	0	0	0	79
Total	142	10539	2173	79	725	53	14	191	28	25	7	3	2	0	13981
Percent	1.0%	75.4%	15.5%	0.6%	5.2%	0.4%	0.1%	1.4%	0.2%	0.2%	0.1%	0.0%	0.0%	0.0%	
AM Peak	09:00	11:00	11:00	07:00	09:00	08:00	10:00	09:00	10:00	11:00	08:00	09:00			
Vol.	7	630	144	12	55	4	2	17	5	5	2	1			
PM Peak	15:00	16:00	16:00	15:00	15:00	16:00	14:00	15:00	13:00	12:00	13:00	14:00	13:00		
Vol.	23	1109	223	11	69	7	4	28	3	4	1	1	1		

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
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Latitude: 0' 0.0000 Undefined

Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/04/23	2	22	6	0	2	0	0	1	0	0	0	0	0	0	33
01:00	0	14	3	0	0	0	0	0	1	0	0	0	0	0	18
02:00	0	13	2	0	0	0	0	0	0	0	0	0	0	0	15
03:00	0	10	3	1	0	1	0	0	4	0	0	0	0	0	19
04:00	0	18	6	2	0	0	0	1	0	0	0	0	0	0	27
05:00	1	70	20	5	18	1	0	1	0	0	0	0	1	0	117
06:00	2	228	53	4	37	1	0	2	1	0	0	0	0	0	328
07:00	5	451	124	8	40	7	8	6	2	1	0	0	0	0	652
08:00	8	487	160	6	68	0	8	14	1	2	0	0	0	0	754
09:00	11	521	131	7	54	4	5	8	2	2	1	0	0	0	746
10:00	10	549	140	6	54	6	3	10	5	1	0	1	0	0	785
11:00	5	667	145	3	52	1	5	13	2	1	0	0	0	0	894
12 PM	5	666	122	5	45	3	4	17	1	1	1	0	0	0	870
13:00	6	734	140	3	56	2	2	10	3	2	0	1	3	0	962
14:00	11	764	178	6	69	5	3	17	1	2	1	0	0	0	1057
15:00	13	916	207	8	68	6	1	23	1	3	1	1	0	0	1248
16:00	18	1117	219	3	80	7	3	21	3	2	2	0	1	0	1476
17:00	18	1098	174	7	56	4	0	14	0	1	3	0	0	0	1375
18:00	15	843	138	2	34	1	0	12	0	1	0	1	0	0	1047
19:00	7	566	88	1	28	0	1	2	0	0	0	0	0	0	693
20:00	4	414	63	0	16	1	0	0	0	0	1	0	0	0	499
21:00	1	234	30	0	15	1	0	0	0	0	0	0	0	0	281
22:00	1	143	20	0	6	0	0	1	0	0	0	0	0	0	171
23:00	1	76	12	0	2	0	0	0	1	0	0	0	0	0	92
Total	144	10621	2184	77	800	51	43	173	28	19	10	4	5	0	14159
Percent	1.0%	75.0%	15.4%	0.5%	5.7%	0.4%	0.3%	1.2%	0.2%	0.1%	0.1%	0.0%	0.0%	0.0%	
AM Peak	09:00	11:00	08:00	07:00	08:00	07:00	07:00	08:00	10:00	08:00	09:00	10:00	05:00		
Vol.	11	667	160	8	68	7	8	14	5	2	1	1	1		
PM Peak	16:00	16:00	16:00	15:00	16:00	16:00	12:00	15:00	13:00	15:00	17:00	13:00	13:00		
Vol.	18	1117	219	8	80	7	4	23	3	3	3	1	3		

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5928

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/05/23	2	27	7	1	4	0	0	1	1	0	0	0	0	0	43
01:00	0	13	1	0	2	0	0	0	0	0	0	0	0	0	16
02:00	0	7	2	0	1	0	0	0	0	0	0	0	0	0	10
03:00	0	11	0	0	1	1	0	1	1	0	0	0	0	0	15
04:00	0	21	3	3	3	0	0	0	1	0	0	0	0	0	31
05:00	2	63	18	2	6	2	0	2	0	0	0	0	0	0	95
06:00	2	241	59	6	30	4	1	3	1	0	1	0	0	0	348
07:00	4	451	113	13	54	4	3	11	4	1	0	0	1	0	659
08:00	2	491	147	6	49	3	3	12	5	1	1	0	0	0	720
09:00	1	470	138	6	60	3	3	13	2	1	1	0	0	0	698
10:00	8	536	148	0	52	2	3	12	0	1	1	0	0	0	763
11:00	13	635	142	4	60	8	4	16	2	0	2	0	0	0	886
12 PM	12	711	153	4	40	4	5	12	1	1	1	1	0	0	945
13:00	9	758	164	5	54	5	1	12	3	2	0	1	0	0	1014
14:00	14	798	162	6	61	5	1	16	0	4	1	0	1	0	1069
15:00	36	919	186	13	57	7	4	23	0	3	0	0	1	0	1249
16:00	15	1041	200	5	66	8	2	23	2	1	2	1	0	0	1366
17:00	18	1021	146	3	45	4	2	18	0	1	3	0	0	0	1261
18:00	9	792	107	2	43	1	0	12	0	0	1	0	2	0	969
19:00	10	490	89	1	22	1	0	15	2	1	1	0	1	0	633
20:00	2	387	71	0	14	1	0	4	1	0	0	0	0	0	480
21:00	0	281	48	0	12	0	0	0	0	0	0	0	0	0	341
22:00	0	134	18	2	7	0	0	0	1	0	0	0	0	0	162
23:00	0	70	14	0	5	0	0	0	1	0	0	0	0	0	90
Total	159	10368	2136	82	748	63	32	206	28	17	15	3	6	0	13863
Percent	1.1%	74.8%	15.4%	0.6%	5.4%	0.5%	0.2%	1.5%	0.2%	0.1%	0.1%	0.0%	0.0%	0.0%	
AM Peak	11:00	11:00	10:00	07:00	09:00	11:00	11:00	11:00	08:00	07:00	11:00		07:00		
Vol.	13	635	148	13	60	8	4	16	5	1	2		1		
PM Peak	15:00	16:00	16:00	15:00	16:00	16:00	12:00	15:00	13:00	14:00	17:00	12:00	18:00		
Vol.	36	1041	200	13	66	8	5	23	3	4	3	1	2		

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
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Latitude: 0' 0.0000 Undefined

Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/06/23	2	30	6	0	3	0	0	1	0	0	0	0	0	0	42
01:00	0	14	4	0	1	0	0	0	0	0	0	0	0	0	19
02:00	0	10	4	0	1	0	0	0	0	0	0	0	0	0	15
03:00	0	11	1	1	0	1	0	0	0	0	0	0	0	0	14
04:00	0	17	6	3	2	0	0	1	0	0	0	0	0	0	29
05:00	2	64	25	4	11	2	0	0	1	0	0	0	0	0	109
06:00	3	220	49	6	29	3	0	5	3	0	0	0	0	0	318
07:00	7	450	94	11	48	5	0	8	0	1	0	0	0	0	624
08:00	6	488	150	8	67	5	0	10	3	1	0	0	0	0	738
09:00	3	485	134	6	69	4	0	6	0	1	1	0	0	0	709
10:00	1	553	169	4	58	2	0	15	3	1	0	0	0	0	806
11:00	10	707	177	3	48	3	1	14	1	1	0	0	0	0	965
12 PM	2	706	164	6	65	5	1	17	2	5	1	1	1	0	976
13:00	4	711	156	10	59	2	1	17	3	3	1	0	1	0	968
14:00	10	801	182	5	57	5	2	18	1	7	1	0	3	0	1092
15:00	9	930	212	7	82	0	0	21	3	3	2	0	0	0	1269
16:00	8	1058	187	5	69	2	0	27	0	3	0	0	0	0	1359
17:00	9	1005	161	5	54	4	0	22	1	5	0	0	0	0	1266
18:00	7	695	130	4	51	0	0	13	0	2	0	0	0	0	902
19:00	9	524	98	0	29	2	0	7	1	0	0	0	0	0	670
20:00	1	395	70	0	10	0	0	4	0	0	0	0	0	0	480
21:00	3	349	57	1	10	0	0	6	0	0	0	0	0	0	426
22:00	3	179	36	2	15	0	0	0	0	0	0	0	0	0	235
23:00	1	139	25	1	4	0	0	0	0	0	0	0	0	0	170
Total	100	10541	2297	92	842	45	5	212	22	33	6	1	5	0	14201
Percent	0.7%	74.2%	16.2%	0.6%	5.9%	0.3%	0.0%	1.5%	0.2%	0.2%	0.0%	0.0%	0.0%	0.0%	
AM Peak	11:00	11:00	11:00	07:00	09:00	07:00	11:00	10:00	06:00	07:00	09:00				
Vol.	10	707	177	11	69	5	1	15	3	1	1				
PM Peak	14:00	16:00	15:00	13:00	15:00	12:00	14:00	16:00	13:00	14:00	15:00	12:00	14:00		
Vol.	10	1058	212	10	82	5	2	27	3	7	2	1	3		

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Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/07/23	1	53	8	0	4	0	0	1	1	0	0	0	0	0	68
01:00	0	32	3	0	1	0	0	0	0	0	0	0	0	0	36
02:00	0	27	1	0	1	0	0	0	0	0	0	0	0	0	29
03:00	0	14	4	0	1	0	0	0	0	0	0	0	0	0	19
04:00	0	12	5	3	3	0	0	1	0	0	0	0	0	0	24
05:00	1	31	6	0	4	0	0	2	0	0	0	0	0	0	44
06:00	0	65	22	2	10	0	0	0	0	0	0	0	0	0	99
07:00	1	177	49	2	20	0	0	2	1	0	0	0	0	0	252
08:00	1	318	83	1	28	1	0	5	1	0	0	0	0	0	438
09:00	2	501	119	0	51	0	0	7	0	1	0	0	0	0	681
10:00	4	693	172	3	40	1	0	10	0	0	0	0	0	0	923
11:00	11	756	168	1	52	1	1	17	1	2	0	0	0	0	1010
12 PM	7	848	177	2	62	2	0	18	0	4	0	0	0	0	1120
13:00	11	786	149	1	55	1	1	17	0	3	3	0	1	0	1028
14:00	1	798	159	4	44	1	0	11	0	4	0	1	1	0	1024
15:00	3	826	140	0	41	1	0	7	0	2	0	1	0	0	1021
16:00	9	783	126	0	48	3	0	5	2	0	1	0	0	0	977
17:00	6	644	113	0	32	0	0	11	1	0	0	0	0	0	807
18:00	2	541	95	1	37	1	0	13	0	1	0	0	0	0	691
19:00	4	373	78	0	27	1	0	5	0	1	0	0	0	0	489
20:00	1	326	63	1	20	0	0	2	0	0	0	0	0	0	413
21:00	2	238	57	0	20	0	0	3	0	0	0	0	0	0	320
22:00	0	160	41	1	14	0	0	0	0	0	0	0	0	0	216
23:00	0	127	20	0	7	0	0	0	0	0	0	0	0	0	154
Total	67	9129	1858	22	622	13	2	137	7	18	4	2	2	0	11883
Percent	0.6%	76.8%	15.6%	0.2%	5.2%	0.1%	0.0%	1.2%	0.1%	0.2%	0.0%	0.0%	0.0%	0.0%	
AM Peak	11:00	11:00	10:00	04:00	11:00	08:00	11:00	11:00	00:00	11:00					
Vol.	11	756	172	3	52	1	1	17	1	2					
PM Peak	13:00	12:00	12:00	14:00	12:00	16:00	13:00	12:00	16:00	12:00	13:00	14:00	13:00		
Vol.	11	848	177	4	62	3	1	18	2	4	3	1	1		

Route 44 West of Route 177 (WB)
Canton, Connecticut

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Site Code:
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Latitude: 0' 0.0000 Undefined

Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/08/23	0	50	9	0	4	0	0	0	1	0	0	0	0	0	64
01:00	0	27	4	0	1	0	0	0	0	0	0	0	0	0	32
02:00	0	20	1	0	1	0	0	0	0	0	0	0	0	0	22
03:00	0	10	1	0	1	0	0	1	0	0	0	0	0	0	13
04:00	0	10	3	1	0	0	0	0	0	0	0	0	0	0	14
05:00	0	21	4	0	3	0	0	0	0	0	0	0	0	0	28
06:00	1	67	17	0	7	0	0	1	1	0	0	0	0	0	94
07:00	1	122	54	0	12	0	0	1	0	0	0	0	0	0	190
08:00	0	261	64	0	21	0	0	2	0	0	1	0	0	0	349
09:00	4	458	111	0	26	0	0	5	0	0	0	0	0	0	604
10:00	7	620	128	0	31	2	0	13	1	0	1	0	0	0	803
11:00	6	770	108	0	41	1	0	6	0	0	0	0	0	0	932
12 PM	9	793	136	1	36	1	0	13	0	3	1	0	1	0	994
13:00	14	798	120	0	28	5	0	13	0	0	0	2	0	0	980
14:00	10	767	121	0	20	2	0	11	0	0	2	0	0	0	933
15:00	12	790	111	0	33	1	0	3	0	3	0	0	0	0	953
16:00	15	669	100	2	27	2	0	7	1	1	1	0	0	0	825
17:00	13	610	101	0	25	5	1	10	1	0	0	0	0	0	766
18:00	6	472	100	1	26	1	0	6	0	2	1	0	0	0	615
19:00	0	335	61	0	25	0	0	3	1	0	0	0	0	0	425
20:00	5	266	49	0	21	0	0	4	2	0	1	0	0	0	348
21:00	0	203	45	2	10	0	0	1	0	0	0	0	0	0	261
22:00	0	109	15	0	8	0	0	0	0	0	0	0	0	0	132
23:00	0	64	7	1	3	0	0	0	0	0	0	0	0	0	75
Total	103	8312	1470	8	410	20	1	100	8	9	8	2	1	0	10452
Percent	1.0%	79.5%	14.1%	0.1%	3.9%	0.2%	0.0%	1.0%	0.1%	0.1%	0.1%	0.0%	0.0%	0.0%	
AM Peak	10:00	11:00	10:00	04:00	11:00	10:00		10:00	00:00		08:00				
Vol.	7	770	128	1	41	2		13	1		1				
PM Peak	16:00	13:00	12:00	16:00	12:00	13:00	17:00	12:00	20:00	12:00	14:00	13:00	12:00		
Vol.	15	798	136	2	36	5	1	13	2	3	2	2	1		

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5928

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/09/23	0	26	4	0	1	0	0	0	0	0	0	0	0	0	31
01:00	0	13	4	0	3	0	0	0	1	0	0	0	0	0	21
02:00	1	5	2	0	1	0	0	0	1	0	0	0	0	0	10
03:00	0	8	2	0	0	0	0	1	1	0	0	0	0	0	12
04:00	0	13	9	3	1	0	0	0	0	0	0	0	0	0	26
05:00	1	43	13	1	9	0	0	0	1	0	0	0	0	0	68
06:00	4	131	44	4	26	1	0	3	3	1	0	0	0	0	217
07:00	1	241	84	2	41	0	0	7	0	0	0	0	0	0	376
08:00	4	373	107	1	56	4	0	6	2	0	0	0	0	0	553
09:00	8	471	113	4	58	0	0	13	0	3	0	0	0	0	670
10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
12 PM	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
Total	19	1324	382	15	196	5	0	30	9	4	0	0	0	0	1984
Percent	1.0%	66.7%	19.3%	0.8%	9.9%	0.3%	0.0%	1.5%	0.5%	0.2%	0.0%	0.0%	0.0%	0.0%	
AM Peak	09:00	09:00	09:00	06:00	09:00	08:00		09:00	06:00	09:00					
Vol.	8	471	113	4	58	4		13	3	3					
PM Peak															
Vol.															
Grand Total	795	67683	13782	407	4815	274	103	1176	140	137	55	17	24	0	89408
Percent	0.9%	75.7%	15.4%	0.5%	5.4%	0.3%	0.1%	1.3%	0.2%	0.2%	0.1%	0.0%	0.0%	0.0%	

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC

Kensington, Connecticut 06037

(860) 828-1693

Site Code:
Station ID: 5928

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
10/02/23	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
12 PM	0	2	25	149	248	96	25	8	1	0	0	0	0	0	554	26-35	397
13:00	0	4	27	160	424	201	60	23	4	0	0	0	0	0	903	31-40	625
14:00	0	1	39	226	473	233	67	19	4	0	0	0	0	0	1062	31-40	706
15:00	0	0	26	267	546	259	90	20	2	0	0	0	0	0	1210	26-35	813
16:00	0	5	31	379	687	269	54	3	3	0	0	0	0	0	1431	26-35	1066
17:00	0	4	49	290	604	357	42	7	1	1	0	0	0	0	1355	31-40	961
18:00	0	2	15	127	393	279	81	15	2	0	0	0	0	0	914	31-40	672
19:00	0	1	4	70	239	157	95	39	4	2	1	0	0	0	612	31-40	396
20:00	0	0	2	37	101	100	85	48	12	1	0	0	0	0	386	31-40	201
21:00	0	1	4	31	45	65	63	36	9	2	0	0	0	0	256	36-45	128
22:00	0	0	0	14	27	21	32	24	14	2	0	0	0	0	134	41-50	56
23:00	0	0	1	6	9	18	14	15	5	0	0	0	0	0	68	36-45	32
Total	0	20	223	1756	3796	2055	708	257	61	8	1	0	0	0	8885		
Percent	0.0%	0.2%	2.5%	19.8%	42.7%	23.1%	8.0%	2.9%	0.7%	0.1%	0.0%	0.0%	0.0%	0.0%			
AM Peak Vol.																	
PM Peak Vol.		16:00	17:00	16:00	16:00	17:00	19:00	20:00	22:00	19:00	19:00				16:00		
		5	49	379	687	357	95	48	14	2	1				1431		

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC

Kensington, Connecticut 06037

(860) 828-1693

Site Code:
Station ID: 5928

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
10/03/23	0	0	0	3	7	4	8	2	4	3	0	0	0	0	31	36-45	12
01:00	0	0	0	2	3	3	5	6	1	0	0	0	0	0	20	41-50	11
02:00	0	0	0	1	2	1	4	0	1	0	0	0	0	0	9	35-44	5
03:00	0	0	0	1	1	5	3	2	3	0	0	0	0	0	15	35-44	8
04:00	0	0	1	5	5	7	11	4	3	0	1	0	0	0	37	36-45	18
05:00	0	1	3	24	30	31	6	10	7	3	0	0	0	0	115	31-40	61
06:00	0	0	9	70	94	71	27	37	12	5	0	0	0	0	325	31-40	165
07:00	0	3	23	116	254	113	47	37	8	2	2	0	0	0	605	26-35	370
08:00	0	1	20	181	253	115	59	28	8	0	0	0	0	0	665	26-35	434
09:00	0	4	33	195	251	130	52	25	4	0	0	0	0	0	694	26-35	446
10:00	0	0	40	201	334	139	38	12	4	0	0	0	0	0	768	26-35	535
11:00	0	3	41	282	345	119	53	15	5	0	0	0	0	0	863	26-35	627
12 PM	0	0	39	338	412	130	43	13	2	0	0	0	0	0	977	26-35	750
13:00	0	4	66	334	358	140	48	11	2	0	0	0	0	0	963	26-35	692
14:00	0	6	60	310	473	200	42	13	1	0	0	0	0	0	1105	26-35	783
15:00	1	12	124	490	463	149	35	8	1	0	0	0	0	0	1283	26-35	953
16:00	0	1	47	390	689	270	42	3	1	0	0	0	0	0	1443	26-35	1079
17:00	0	1	36	269	655	334	73	6	1	1	0	0	0	0	1376	31-40	989
18:00	3	6	29	194	496	235	65	12	6	0	0	0	0	0	1046	31-40	731
19:00	0	1	10	82	304	151	80	22	4	0	0	0	0	0	654	31-40	455
20:00	0	2	6	31	126	127	111	38	10	5	0	0	0	0	456	31-40	253
21:00	0	0	3	23	81	61	71	38	7	4	0	0	0	0	288	31-40	142
22:00	0	3	1	10	26	38	45	30	8	3	1	0	0	0	165	36-45	83
23:00	1	0	1	6	14	11	17	15	11	3	0	0	0	0	79	41-50	32
Total	5	48	592	3558	5676	2584	985	387	114	29	4	0	0	0	13982		
Percent	0.0%	0.3%	4.2%	25.4%	40.6%	18.5%	7.0%	2.8%	0.8%	0.2%	0.0%	0.0%	0.0%	0.0%			
AM Peak		09:00	11:00	11:00	11:00	10:00	08:00	06:00	06:00	06:00	07:00				11:00		
Vol.		4	41	282	345	139	59	37	12	5	2				863		
PM Peak	18:00	15:00	15:00	15:00	16:00	17:00	20:00	20:00	23:00	20:00	22:00				16:00		
Vol.	3	12	124	490	689	334	111	38	11	5	1				1443		

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5928

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
10/04/23	0	0	1	5	9	11	5	1	1	0	0	0	0	0	33	31-40	20
01:00	0	0	0	5	6	4	1	2	0	0	0	0	0	0	18	26-35	11
02:00	0	0	0	2	6	4	2	0	0	0	0	1	0	0	15	31-40	10
03:00	0	0	0	4	3	8	1	3	0	0	0	0	0	0	19	31-40	11
04:00	0	0	1	2	4	5	6	5	4	0	0	0	0	0	27	41-50	11
05:00	0	0	6	23	28	16	19	15	6	2	1	0	1	0	117	26-35	51
06:00	0	1	17	62	96	80	38	17	11	6	0	0	0	0	328	31-40	176
07:00	0	10	39	170	189	115	84	35	9	1	0	0	0	0	652	26-35	359
08:00	0	6	44	198	291	132	56	22	5	0	0	0	0	0	754	26-35	489
09:00	0	9	27	177	295	151	61	25	1	0	0	0	0	0	746	26-35	472
10:00	0	2	32	168	332	183	53	12	1	1	1	0	0	0	785	31-40	515
11:00	1	2	31	192	378	210	55	17	7	1	0	0	0	0	894	31-40	588
12 PM	0	1	19	163	379	216	65	21	5	1	0	0	0	0	870	31-40	595
13:00	0	11	38	199	422	207	70	15	0	0	0	0	0	0	962	31-40	629
14:00	3	2	54	232	479	210	55	16	6	0	0	0	0	0	1057	26-35	711
15:00	3	7	41	278	624	245	37	12	1	0	0	0	0	0	1248	26-35	902
16:00	0	4	53	367	702	323	26	0	1	0	0	0	0	0	1476	26-35	1069
17:00	5	7	45	353	642	270	48	5	0	0	0	0	0	0	1375	26-35	995
18:00	2	19	38	213	474	236	50	14	0	0	0	1	0	0	1047	31-40	710
19:00	0	0	7	96	298	185	79	23	5	0	0	0	0	0	693	31-40	483
20:00	0	0	3	59	173	119	86	44	12	2	1	0	0	0	499	31-40	292
21:00	0	0	3	20	74	56	65	48	14	1	0	0	0	0	281	31-40	130
22:00	0	0	3	11	24	42	41	32	12	4	2	0	0	0	171	36-45	83
23:00	0	0	0	7	15	27	15	18	8	2	0	0	0	0	92	36-45	42
Total	14	81	502	3006	5943	3055	1018	402	109	21	5	2	1	0	14159		
Percent	0.1%	0.6%	3.5%	21.2%	42.0%	21.6%	7.2%	2.8%	0.8%	0.1%	0.0%	0.0%	0.0%	0.0%			
AM Peak	11:00	07:00	08:00	08:00	11:00	11:00	07:00	08:00	06:00	06:00	05:00	02:00	05:00		11:00		
Vol.	1	10	44	198	378	210	84	35	11	6	1	1	1		894		
PM Peak	17:00	18:00	14:00	16:00	16:00	16:00	20:00	21:00	21:00	22:00	22:00	18:00			16:00		
Vol.	5	19	54	367	702	323	86	48	14	4	2	1			1476		

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC Kensington, Connecticut 06037 (860) 828-1693

Site Code:
Station ID: 5928

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
10/05/23	0	0	0	2	7	8	12	8	6	0	0	0	0	0	43	41-50	20
01:00	0	0	0	3	2	1	8	0	2	0	0	0	0	0	16	36-45	9
02:00	0	0	1	0	1	3	1	3	1	0	0	0	0	0	10	46-55	4
03:00	0	0	0	2	1	5	4	1	0	2	0	0	0	0	15	36-45	9
04:00	0	0	1	2	6	11	4	5	1	0	1	0	0	0	31	31-40	17
05:00	0	1	4	19	22	20	12	12	4	0	0	1	0	0	95	29-38	42
06:00	0	1	15	78	101	94	32	15	11	1	0	0	0	0	348	31-40	195
07:00	1	3	34	151	245	150	46	18	10	1	0	0	0	0	659	26-35	396
08:00	0	4	27	199	286	148	38	14	3	1	0	0	0	0	720	26-35	485
09:00	0	1	23	140	284	168	63	17	2	0	0	0	0	0	698	31-40	452
10:00	1	0	20	146	276	206	79	32	2	1	0	0	0	0	763	31-40	482
11:00	0	4	46	214	391	176	42	11	2	0	0	0	0	0	886	26-35	605
12 PM	0	4	21	175	398	236	83	21	5	2	0	0	0	0	945	31-40	634
13:00	1	2	37	193	507	202	55	17	0	0	0	0	0	0	1014	31-40	709
14:00	2	4	79	368	444	150	19	3	0	0	0	0	0	0	1069	26-35	812
15:00	5	18	120	476	506	114	11	0	0	0	0	0	0	0	1250	26-35	982
16:00	1	2	57	474	631	181	20	0	0	0	0	0	0	0	1366	26-35	1105
17:00	5	11	96	492	525	121	10	1	0	0	0	0	0	0	1261	26-35	1017
18:00	1	3	63	316	434	136	16	1	0	0	0	0	0	0	970	26-35	750
19:00	0	0	22	142	322	104	33	8	2	0	0	0	0	0	633	26-35	464
20:00	0	2	6	37	171	135	84	34	8	0	3	0	0	0	480	31-40	306
21:00	0	0	5	20	78	85	82	50	16	1	2	2	0	0	341	36-45	167
22:00	0	0	2	9	30	26	38	28	22	6	1	0	0	0	162	41-50	66
23:00	0	0	2	6	12	14	22	15	15	2	1	1	0	0	90	41-50	37
Total	17	60	681	3664	5680	2494	814	314	112	17	8	4	0	0	13865		
Percent	0.1%	0.4%	4.9%	26.4%	41.0%	18.0%	5.9%	2.3%	0.8%	0.1%	0.1%	0.0%	0.0%	0.0%			
AM Peak	07:00	08:00	11:00	11:00	11:00	10:00	10:00	10:00	10:00	06:00	03:00	04:00	05:00		11:00		
Vol.	1	4	46	214	391	206	79	32	11	2	1	1			886		
PM Peak	15:00	15:00	15:00	17:00	16:00	12:00	20:00	21:00	22:00	22:00	20:00	21:00			16:00		
Vol.	5	18	120	492	631	236	84	50	22	6	3	2			1366		

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC Kensington, Connecticut 06037 (860) 828-1693

Site Code:
Station ID: 5928

Latitude: 0' 0.0000 Undefined

Westbound																	
Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
10/06/23	0	0	0	2	9	11	9	7	3	1	0	0	0	0	42	31-40	20
01:00	0	0	1	3	1	3	6	4	1	0	0	0	0	0	19	41-50	10
02:00	0	0	2	1	2	4	4	1	1	0	0	0	0	0	15	36-45	8
03:00	0	0	0	0	1	4	3	3	2	1	0	0	0	0	14	35-44	7
04:00	0	0	4	1	5	6	4	4	3	2	0	0	0	0	29	31-40	11
05:00	0	1	7	21	28	17	18	12	3	0	2	0	0	0	109	26-35	49
06:00	0	1	8	53	109	70	56	8	10	3	0	0	0	0	318	31-40	179
07:00	0	0	42	134	216	132	58	31	10	1	0	0	0	0	624	26-35	350
08:00	0	3	28	186	284	153	63	17	4	0	0	0	0	0	738	26-35	470
09:00	0	2	35	156	257	166	57	27	9	0	0	0	0	0	709	31-40	423
10:00	0	5	21	167	368	148	64	29	3	1	0	0	0	0	806	26-35	535
11:00	0	2	31	229	401	230	47	20	5	0	0	0	0	0	965	29-38	631
12 PM	1	3	34	187	454	196	74	24	2	1	0	0	0	0	976	31-40	650
13:00	2	1	33	202	452	209	53	14	0	1	1	0	0	0	968	31-40	661
14:00	0	2	30	240	511	238	58	13	0	0	0	0	0	0	1092	26-35	751
15:00	1	11	35	287	634	247	42	9	3	0	0	0	0	0	1269	26-35	921
16:00	0	3	31	315	678	275	49	8	0	0	0	0	0	0	1359	26-35	993
17:00	0	3	31	257	541	356	59	16	3	0	0	0	0	0	1266	31-40	897
18:00	0	1	14	140	420	229	81	13	3	1	0	0	0	0	902	31-40	649
19:00	0	1	9	109	246	196	82	23	4	0	0	0	0	0	670	31-40	442
20:00	0	0	5	59	159	113	92	42	8	2	0	0	0	0	480	31-40	272
21:00	0	0	2	41	94	121	102	57	8	0	1	0	0	0	426	36-45	223
22:00	0	2	6	12	41	56	45	49	22	2	0	0	0	0	235	36-45	101
23:00	0	1	1	11	27	29	42	41	17	0	1	0	0	0	170	41-50	83
Total	4	42	410	2813	5938	3209	1168	472	124	16	5	0	0	0	14201		
Percent	0.0%	0.3%	2.9%	19.8%	41.8%	22.6%	8.2%	3.3%	0.9%	0.1%	0.0%	0.0%	0.0%	0.0%			
AM Peak		10:00	07:00	11:00	11:00	11:00	10:00	07:00	06:00	06:00	05:00				11:00		
Vol.		5	42	229	401	230	64	31	10	3	2				965		
PM Peak	13:00	15:00	15:00	16:00	16:00	17:00	21:00	21:00	22:00	20:00	13:00				16:00		
Vol.	2	11	35	315	678	356	102	57	22	2	1				1359		

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC

Kensington, Connecticut 06037

(860) 828-1693

Site Code:
Station ID: 5928

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
10/07/23	0	0	0	8	12	13	17	13	4	1	0	0	0	0	68	41-50	30
01:00	0	0	0	0	5	6	17	5	1	1	0	0	0	0	36	36-45	23
02:00	0	0	1	1	4	2	9	9	0	2	1	0	0	0	29	41-50	18
03:00	0	0	1	1	1	3	6	4	1	2	0	0	0	0	19	39-48	10
04:00	0	1	2	3	5	2	6	3	2	0	0	0	0	0	24	39-48	9
05:00	0	0	2	8	6	4	7	8	7	1	1	0	0	0	44	41-50	15
06:00	0	0	2	21	21	20	17	14	2	2	0	0	0	0	99	26-35	42
07:00	0	0	8	39	68	72	28	22	13	2	0	0	0	0	252	31-40	140
08:00	0	1	11	69	140	93	83	26	12	3	0	0	0	0	438	31-40	233
09:00	0	0	15	156	275	128	80	24	3	0	0	0	0	0	681	26-35	431
10:00	0	1	32	218	437	154	54	27	0	0	0	0	0	0	923	26-35	655
11:00	3	6	36	250	462	191	45	13	4	0	0	0	0	0	1010	26-35	712
12 PM	0	6	39	315	508	184	54	13	1	0	0	0	0	0	1120	26-35	823
13:00	0	2	24	204	550	191	49	8	0	0	0	0	0	0	1028	26-35	754
14:00	1	0	33	198	468	228	73	19	4	0	0	0	0	0	1024	31-40	696
15:00	0	5	13	182	496	205	89	28	2	1	0	0	0	0	1021	31-40	701
16:00	0	3	25	165	447	233	77	25	2	0	0	0	0	0	977	31-40	680
17:00	0	0	7	119	350	237	59	31	4	0	0	0	0	0	807	31-40	587
18:00	0	0	15	99	280	182	91	20	4	0	0	0	0	0	691	31-40	462
19:00	0	0	8	62	130	171	88	25	2	3	0	0	0	0	489	31-40	301
20:00	0	1	4	30	128	118	80	38	9	3	1	1	0	0	413	31-40	246
21:00	0	0	6	22	80	70	77	43	14	6	1	0	1	0	320	31-40	150
22:00	0	0	1	16	41	51	54	36	12	3	2	0	0	0	216	36-45	105
23:00	0	0	0	8	12	28	40	45	15	4	2	0	0	0	154	41-50	85
Total	4	26	285	2194	4926	2586	1200	499	118	34	9	1	1	0	11883		
Percent	0.0%	0.2%	2.4%	18.5%	41.5%	21.8%	10.1%	4.2%	1.0%	0.3%	0.1%	0.0%	0.0%	0.0%			
AM Peak	11:00	11:00	11:00	11:00	11:00	11:00	08:00	10:00	07:00	08:00	01:00				11:00		
Vol.	3	6	36	250	462	191	83	27	13	3	1				1010		
PM Peak	14:00	12:00	12:00	12:00	13:00	17:00	18:00	23:00	23:00	21:00	22:00	20:00	21:00		12:00		
Vol.	1	6	39	315	550	237	91	45	15	6	2	1	1		1120		

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC

Kensington, Connecticut 06037

(860) 828-1693

Site Code:
Station ID: 5928

Latitude: 0' 0.0000 Undefined

Westbound																	
Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
10/08/23	0	0	0	6	6	16	12	18	6	0	0	0	0	0	64	41-50	30
01:00	0	0	1	0	4	3	6	10	6	2	0	0	0	0	32	41-50	16
02:00	0	0	1	3	1	3	7	5	2	0	0	0	0	0	22	41-50	12
03:00	0	0	0	2	0	2	3	4	1	1	0	0	0	0	13	41-50	7
04:00	0	0	0	1	1	3	4	3	1	1	0	0	0	0	14	36-45	7
05:00	0	0	1	7	4	3	3	9	1	0	0	0	0	0	28	41-50	12
06:00	0	0	1	9	16	19	18	13	12	4	2	0	0	0	94	35-44	37
07:00	0	1	7	33	52	40	27	17	8	4	1	0	0	0	190	31-40	92
08:00	0	0	6	55	129	94	36	23	5	1	0	0	0	0	349	31-40	223
09:00	0	1	13	105	225	150	73	29	8	0	0	0	0	0	604	31-40	375
10:00	0	0	15	109	349	222	72	31	5	0	0	0	0	0	803	31-40	571
11:00	2	4	21	186	429	185	75	29	1	0	0	0	0	0	932	26-35	615
12 PM	0	1	25	218	482	193	62	11	2	0	0	0	0	0	994	26-35	700
13:00	0	0	21	157	492	230	57	22	1	0	0	0	0	0	980	31-40	722
14:00	0	0	14	183	439	193	76	24	4	0	0	0	0	0	933	31-40	632
15:00	0	0	21	164	448	225	74	18	1	1	1	1	0	0	954	31-40	673
16:00	0	0	23	149	372	200	49	24	7	1	0	0	0	0	825	31-40	572
17:00	0	0	17	107	301	234	72	28	7	0	0	0	0	0	766	31-40	535
18:00	0	0	5	70	249	164	83	34	8	2	0	0	0	0	615	31-40	413
19:00	0	0	2	42	148	123	71	33	5	1	0	0	0	0	425	31-40	271
20:00	0	1	4	17	78	103	70	50	23	1	1	0	0	0	348	31-40	181
21:00	0	0	3	17	47	76	64	34	13	2	3	1	1	0	261	36-45	140
22:00	0	0	2	10	22	24	30	27	12	3	1	0	1	0	132	41-50	57
23:00	0	0	0	6	12	9	26	12	7	1	1	0	1	0	75	41-50	38
Total	2	8	203	1656	4306	2514	1070	508	146	25	10	2	3	0	10453		
Percent	0.0%	0.1%	1.9%	15.8%	41.2%	24.1%	10.2%	4.9%	1.4%	0.2%	0.1%	0.0%	0.0%	0.0%			
AM Peak	11:00	11:00	11:00	11:00	11:00	10:00	11:00	10:00	06:00	06:00	06:00				11:00		
Vol.	2	4	21	186	429	222	75	31	12	4	2				932		
PM Peak		12:00	12:00	12:00	13:00	17:00	18:00	20:00	20:00	22:00	21:00	15:00	21:00		12:00		
Vol.		1	25	218	492	234	83	50	23	3	3	1	1		994		

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC Kensington, Connecticut 06037 (860) 828-1693

Site Code:
Station ID: 5928

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
10/09/23	0	0	0	2	2	4	11	6	4	0	0	2	0	0	31	40-49	17
01:00	0	0	0	0	3	4	5	7	1	1	0	0	0	0	21	41-50	12
02:00	0	0	1	1	0	1	2	2	2	1	0	0	0	0	10	46-55	4
03:00	0	0	0	0	1	5	4	2	0	0	0	0	0	0	12	36-45	9
04:00	0	0	3	1	2	4	3	6	1	3	3	0	0	0	26	40-49	9
05:00	0	0	4	11	19	8	10	10	4	1	1	0	0	0	68	26-35	30
06:00	0	0	11	35	51	44	33	24	15	4	0	0	0	0	217	31-40	95
07:00	0	1	11	59	131	88	41	20	16	7	2	0	0	0	376	31-40	219
08:00	0	1	18	109	219	105	50	34	14	2	0	1	0	0	553	26-35	328
09:00	0	1	26	136	269	136	54	39	7	2	0	0	0	0	670	26-35	405
10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
12 PM	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
Total	0	3	74	354	697	399	213	150	64	21	6	3	0	0	1984		
Percent	0.0%	0.2%	3.7%	17.8%	35.1%	20.1%	10.7%	7.6%	3.2%	1.1%	0.3%	0.2%	0.0%	0.0%			
AM Peak		07:00	09:00	09:00	09:00	09:00	09:00	09:00	07:00	07:00	04:00	00:00			09:00		
Vol.		1	26	136	269	136	54	39	16	7	3	2			670		
PM Peak																	
Vol.																	
Total	46	288	2970	19001	36962	18896	7176	2989	848	171	48	12	5	0	89412		
Percent	0.1%	0.3%	3.3%	21.3%	41.3%	21.1%	8.0%	3.3%	0.9%	0.2%	0.1%	0.0%	0.0%	0.0%			

15th Percentile : 27 MPH
50th Percentile : 33 MPH
85th Percentile : 39 MPH
95th Percentile : 44 MPH













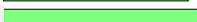
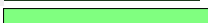



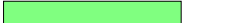






Stats
10 MPH Pace Speed : 26-35 MPH
Number in Pace : 55963
Percent in Pace : 62.6%
Number of Vehicles > 40 MPH : 11249
Percent of Vehicles > 40 MPH : 12.6%
Mean Speed(Average) : 34 MPH

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5928

Latitude: 0' 0.0000 Undefined

Start Time	Mon 02-Oct-23	Tue 03-Oct-23	Wed 04-Oct-23	Thu 05-Oct-23	Fri 06-Oct-23	Week Day Average	Sat 07-Oct-23	Sun 08-Oct-23	Week Average
12:00 AM	*	31	33	43	42	37	68	64	47 
01:00	*	20	18	16	19	18	36	32	24 
02:00	*	9	15	10	15	12	29	22	17 
03:00	*	15	19	15	14	16	19	13	16 
04:00	*	37	27	31	29	31	24	14	27 
05:00	*	115	117	95	109	109	44	28	85 
06:00	*	325	328	348	318	330	99	94	252 
07:00	*	605	652	659	624	635	252	190	497 
08:00	*	665	754	720	738	719	438	349	611 
09:00	*	694	746	698	709	712	681	604	689 
10:00	*	768	785	763	806	780	923	803	808 
11:00	*	863	894	886	965	902	1010	932	925 
12:00 PM	554	977	870	945	976	864	1120	994	919 
01:00	903	963	962	1014	968	962	1028	980	974 
02:00	1062	1105	1057	1069	1092	1077	1024	933	1049 
03:00	1210	1283	1248	1250	1269	1252	1021	954	1176 
04:00	1431	1443	1476	1366	1359	1415	977	825	1268 
05:00	1355	1376	1375	1261	1266	1327	807	766	1172 
06:00	914	1046	1047	970	902	976	691	615	884 
07:00	612	654	693	633	670	652	489	425	597 
08:00	386	456	499	480	480	460	413	348	437 
09:00	256	288	281	341	426	318	320	261	310 
10:00	134	165	171	162	235	173	216	132	174 
11:00	68	79	92	90	170	100	154	75	104 
Total	8885	13982	14159	13865	14201	13877	11883	10453	13062











Date	Daily Total
03-Oct-23	13982
04-Oct-23	14159
05-Oct-23	13865
06-Oct-23	14201
Average	14052

Route 44 West of Route 177 (WB)
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5928

Latitude: 0' 0.0000 Undefined

Start Time	Mon 09-Oct-23	Tue 10-Oct-23	Wed 11-Oct-23	Thu 12-Oct-23	Fri 13-Oct-23	Week Day Average	Sat 14-Oct-23	Sun 15-Oct-23	Week Average
12:00 AM	31	*	*	*	*	31	*	*	31 
01:00	21	*	*	*	*	21	*	*	21 
02:00	10	*	*	*	*	10	*	*	10 
03:00	12	*	*	*	*	12	*	*	12 
04:00	26	*	*	*	*	26	*	*	26 
05:00	68	*	*	*	*	68	*	*	68 
06:00	217	*	*	*	*	217	*	*	217 
07:00	376	*	*	*	*	376	*	*	376 
08:00	553	*	*	*	*	553	*	*	553 
09:00	670	*	*	*	*	670	*	*	670 
10:00	*	*	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	*	*	*
12:00 PM	*	*	*	*	*	*	*	*	*
01:00	*	*	*	*	*	*	*	*	*
02:00	*	*	*	*	*	*	*	*	*
03:00	*	*	*	*	*	*	*	*	*
04:00	*	*	*	*	*	*	*	*	*
05:00	*	*	*	*	*	*	*	*	*
06:00	*	*	*	*	*	*	*	*	*
07:00	*	*	*	*	*	*	*	*	*
08:00	*	*	*	*	*	*	*	*	*
09:00	*	*	*	*	*	*	*	*	*
10:00	*	*	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	*	*	*
Total	1984	0	0	0	0	1984	0	0	1984
Grand Total	10869	13982	14159	13865	14201	15861	11883	10453	15046

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Eastbound															
Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/02/23	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
12 PM	2	343	66	5	20	2	0	4	4	0	1	0	0	0	447
13:00	4	606	109	4	37	3	0	6	0	1	0	0	0	0	770
14:00	3	642	109	4	38	1	1	7	1	0	1	0	1	0	808
15:00	4	615	106	10	41	6	0	7	0	1	0	0	0	0	790
16:00	9	578	109	3	41	3	0	7	0	0	0	0	0	0	750
17:00	3	580	88	2	29	0	1	6	0	0	0	0	0	0	709
18:00	2	463	88	2	36	3	0	5	2	1	0	0	0	0	602
19:00	3	374	73	1	23	0	0	3	1	0	0	0	0	0	478
20:00	0	196	61	2	13	1	0	0	0	0	0	0	0	0	273
21:00	1	138	23	0	7	0	0	0	0	0	0	0	0	0	169
22:00	0	70	19	0	6	0	0	0	0	0	0	0	0	0	95
23:00	0	21	14	1	7	0	0	0	0	0	0	0	0	0	43
Total	31	4626	865	34	298	19	2	45	8	3	2	0	1	0	5934
Percent	0.5%	78.0%	14.6%	0.6%	5.0%	0.3%	0.0%	0.8%	0.1%	0.1%	0.0%	0.0%	0.0%	0.0%	
AM Peak Vol.															
PM Peak Vol.	16:00	14:00	13:00	15:00	15:00	15:00	14:00	14:00	12:00	13:00	12:00		14:00		
	9	642	109	10	41	6	1	7	4	1	1		1		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Eastbound															
Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/03/23	1	18	7	0	2	0	0	1	0	0	0	0	0	0	29
01:00	0	7	3	0	2	0	0	0	0	0	0	0	0	0	12
02:00	0	10	4	0	2	0	0	0	0	0	0	0	0	0	16
03:00	0	13	2	0	2	0	0	0	0	0	0	0	0	0	17
04:00	0	28	19	2	7	1	0	1	0	0	0	0	0	0	58
05:00	2	130	68	3	38	1	0	0	0	0	0	0	0	0	242
06:00	3	362	147	4	78	0	0	7	1	0	0	0	0	0	602
07:00	9	666	238	3	72	1	0	20	1	1	0	0	0	0	1011
08:00	7	687	250	6	56	4	1	17	1	0	0	0	0	0	1029
09:00	6	588	158	4	56	4	0	7	3	2	0	0	0	0	828
10:00	5	624	159	5	41	3	0	7	1	1	1	0	0	0	847
11:00	3	494	120	6	31	1	0	10	0	0	0	0	0	0	665
12 PM	5	585	86	6	34	3	0	7	2	0	0	1	0	0	729
13:00	2	590	103	3	44	2	0	9	0	0	0	0	1	0	754
14:00	3	566	136	6	33	2	0	12	0	1	3	0	0	0	762
15:00	7	621	126	10	52	4	0	6	0	0	0	0	0	0	826
16:00	6	611	114	4	34	4	1	6	1	0	0	0	0	0	781
17:00	3	588	98	4	36	3	0	8	0	1	0	0	0	0	741
18:00	1	471	88	4	20	0	0	4	0	1	0	0	1	0	590
19:00	2	364	71	1	16	0	0	1	1	0	0	0	0	0	456
20:00	2	253	60	0	10	1	0	1	0	0	0	0	0	0	327
21:00	0	161	33	1	12	0	0	0	0	0	0	0	0	0	207
22:00	0	76	13	1	4	0	0	1	0	0	0	0	0	0	95
23:00	0	40	4	0	3	0	0	0	0	0	0	0	0	0	47
Total	67	8553	2107	73	685	34	2	125	11	7	4	1	2	0	11671
Percent	0.6%	73.3%	18.1%	0.6%	5.9%	0.3%	0.0%	1.1%	0.1%	0.1%	0.0%	0.0%	0.0%	0.0%	
AM Peak	07:00	08:00	08:00	08:00	06:00	08:00	08:00	07:00	09:00	09:00	10:00				
Vol.	9	687	250	6	78	4	1	20	3	2	1				
PM Peak	15:00	15:00	14:00	15:00	15:00	15:00	16:00	14:00	12:00	14:00	14:00	12:00	13:00		
Vol.	7	621	136	10	52	4	1	12	2	1	3	1	1		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Eastbound															
Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/04/23	0	16	6	1	3	0	0	1	1	0	0	0	0	0	28
01:00	0	4	6	0	2	0	0	0	0	0	0	0	0	0	12
02:00	1	14	7	0	0	1	0	0	0	0	0	0	0	0	23
03:00	0	14	2	1	2	0	0	0	0	0	0	0	0	0	19
04:00	0	30	14	2	9	0	0	0	0	0	0	0	0	0	55
05:00	2	126	53	1	39	2	0	2	0	0	0	0	0	0	225
06:00	4	350	104	5	66	1	0	5	0	0	0	0	0	0	535
07:00	10	687	210	3	75	3	0	11	0	2	0	0	0	0	1001
08:00	8	693	227	6	76	7	0	19	1	1	2	0	0	0	1040
09:00	3	565	162	1	44	4	0	9	1	0	1	0	0	0	790
10:00	6	575	121	6	47	2	0	6	2	1	0	0	0	0	766
11:00	3	584	101	4	41	4	1	7	1	0	0	1	0	0	747
12 PM	5	577	106	5	55	4	2	9	0	0	1	0	0	0	764
13:00	3	629	86	3	42	6	1	4	0	2	0	0	0	0	776
14:00	5	549	80	7	36	5	0	9	0	0	1	0	0	0	692
15:00	2	633	95	5	44	6	0	9	1	0	1	0	0	0	796
16:00	3	616	106	5	45	1	0	11	0	0	0	0	0	0	787
17:00	2	562	98	1	36	2	1	7	0	1	0	0	0	0	710
18:00	4	464	88	2	30	0	0	5	0	0	1	0	0	0	594
19:00	0	403	62	2	15	0	0	1	0	0	0	0	0	0	483
20:00	1	236	43	1	14	1	0	0	0	0	0	0	0	0	296
21:00	0	187	35	0	6	0	0	1	0	0	0	0	0	0	229
22:00	0	84	14	0	8	0	0	0	0	0	0	0	0	0	106
23:00	0	48	9	1	2	0	0	0	0	0	0	0	0	0	60
Total	62	8646	1835	62	737	49	5	116	7	7	7	1	0	0	11534
Percent	0.5%	75.0%	15.9%	0.5%	6.4%	0.4%	0.0%	1.0%	0.1%	0.1%	0.1%	0.0%	0.0%	0.0%	
AM Peak	07:00	08:00	08:00	08:00	08:00	08:00	11:00	08:00	10:00	07:00	08:00	11:00			
Vol.	10	693	227	6	76	7	1	19	2	2	2	1			
PM Peak	12:00	15:00	12:00	14:00	12:00	13:00	12:00	16:00	15:00	13:00	12:00				
Vol.	5	633	106	7	55	6	2	11	1	2	1				

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Eastbound															
Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/05/23	0	17	4	0	0	0	0	1	0	0	0	0	0	0	22
01:00	0	11	2	0	1	0	0	0	0	0	0	0	0	0	14
02:00	0	10	3	0	3	0	0	0	0	0	0	0	0	0	16
03:00	1	12	2	0	3	0	0	0	0	0	0	0	0	0	18
04:00	0	40	8	1	7	0	0	0	0	0	0	0	0	0	56
05:00	0	163	50	1	28	0	0	1	1	0	0	0	0	0	244
06:00	1	380	101	5	66	0	0	5	0	1	0	0	0	0	559
07:00	5	602	193	3	82	3	0	9	1	0	1	0	0	0	899
08:00	8	637	235	8	74	3	0	12	0	1	1	0	0	0	979
09:00	7	534	166	5	44	1	0	13	0	0	0	0	0	0	770
10:00	3	562	125	3	47	1	1	12	0	0	0	0	1	0	755
11:00	5	569	92	6	30	0	1	10	0	1	0	0	0	0	714
12 PM	4	571	104	5	45	3	1	6	1	0	1	0	0	0	741
13:00	4	592	89	2	36	2	1	8	0	0	0	1	0	0	735
14:00	8	538	69	3	31	0	0	7	0	1	0	0	0	0	657
15:00	3	568	72	4	17	0	0	5	1	0	0	0	0	0	670
16:00	1	360	11	2	7	0	0	1	0	0	0	0	0	0	382
17:00	6	348	19	0	2	0	0	0	0	0	0	0	0	0	375
18:00	2	423	59	3	21	1	0	5	0	0	0	0	0	0	514
19:00	3	385	68	0	13	0	1	4	0	0	0	0	0	0	474
20:00	2	285	33	0	9	1	0	1	1	0	0	0	0	0	332
21:00	1	192	39	0	8	2	0	0	0	0	0	0	0	0	242
22:00	0	95	14	1	4	0	0	0	0	0	0	0	0	0	114
23:00	0	60	3	0	2	0	0	0	1	0	0	0	0	0	66
Total	64	7954	1561	52	580	17	5	100	6	4	3	1	1	0	10348
Percent	0.6%	76.9%	15.1%	0.5%	5.6%	0.2%	0.0%	1.0%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	
AM Peak	08:00	08:00	08:00	08:00	07:00	07:00	10:00	09:00	05:00	06:00	07:00			10:00	
Vol.	8	637	235	8	82	3	1	13	1	1	1		1		
PM Peak	14:00	13:00	12:00	12:00	12:00	12:00	12:00	13:00	12:00	14:00	12:00	13:00			
Vol.	8	592	104	5	45	3	1	8	1	1	1	1			

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Eastbound															
Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/06/23	0	27	1	1	0	0	0	1	0	0	0	0	0	0	30
01:00	0	16	3	0	2	0	0	0	1	0	0	0	0	0	22
02:00	0	13	0	0	3	0	0	0	0	0	0	0	0	0	16
03:00	0	24	1	0	0	0	0	0	0	0	0	0	0	0	25
04:00	0	38	8	1	3	0	0	0	0	0	0	0	0	0	50
05:00	0	141	26	0	24	0	0	2	0	0	0	0	0	0	193
06:00	0	367	65	3	40	0	0	3	1	0	0	0	0	0	479
07:00	3	540	129	7	47	2	0	2	0	0	0	0	0	0	730
08:00	0	510	93	1	33	1	0	7	0	0	0	0	0	0	645
09:00	1	471	54	2	21	0	0	0	0	0	1	0	0	0	550
10:00	4	517	62	1	23	1	0	5	1	1	0	0	0	0	615
11:00	2	500	67	6	18	0	0	5	0	0	0	0	0	0	598
12 PM	1	477	76	2	16	1	0	5	0	0	0	0	0	0	578
13:00	2	505	56	0	12	1	0	1	0	0	0	0	0	0	577
14:00	4	503	42	2	15	1	0	2	0	0	0	0	0	0	569
15:00	0	533	69	1	10	0	0	4	0	0	0	0	0	0	617
16:00	6	408	127	1	19	1	0	4	0	0	0	0	0	0	566
17:00	1	408	94	1	14	0	0	3	1	0	0	0	0	0	522
18:00	1	387	50	0	13	0	0	1	0	0	0	0	0	0	452
19:00	0	378	67	2	14	0	0	0	0	0	0	0	0	0	461
20:00	0	314	36	0	8	0	0	1	0	0	0	0	0	0	359
21:00	0	219	30	0	4	0	0	0	0	0	0	0	0	0	253
22:00	0	129	20	2	5	0	0	0	0	0	0	0	0	0	156
23:00	0	81	6	0	11	0	0	0	1	0	0	0	0	0	99
Total	25	7506	1182	33	355	8	0	46	5	1	1	0	0	0	9162
Percent	0.3%	81.9%	12.9%	0.4%	3.9%	0.1%	0.0%	0.5%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	
AM Peak	10:00	07:00	07:00	07:00	07:00	07:00		08:00	01:00	10:00	09:00				
Vol.	4	540	129	7	47	2		7	1	1	1				
PM Peak	16:00	15:00	16:00	12:00	16:00	12:00		12:00	17:00						
Vol.	6	533	127	2	19	1		5	1						

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Eastbound															
Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/07/23	0	51	7	0	3	0	0	1	0	0	0	0	0	0	62
01:00	0	19	2	0	1	0	0	0	0	0	0	0	0	0	22
02:00	0	21	2	0	2	0	0	0	1	0	0	0	0	0	26
03:00	0	9	4	0	0	0	0	0	0	0	0	0	0	0	13
04:00	0	25	2	0	5	0	0	0	0	0	0	0	0	0	32
05:00	0	55	6	2	7	0	0	0	0	0	0	0	0	0	70
06:00	1	121	9	2	10	0	0	0	0	0	0	0	0	0	143
07:00	0	244	38	4	18	0	0	0	0	0	0	0	0	0	304
08:00	4	378	46	1	12	0	0	3	0	0	0	0	0	0	444
09:00	0	450	60	1	25	0	0	4	1	0	0	0	0	0	541
10:00	1	536	87	0	22	0	0	8	0	0	1	0	0	0	655
11:00	4	548	103	0	24	2	0	5	1	1	0	0	0	0	688
12 PM	1	414	153	1	28	0	0	6	0	0	0	0	0	0	603
13:00	2	470	170	1	34	2	0	3	0	0	0	0	0	0	682
14:00	2	550	115	5	13	0	0	5	0	0	1	0	1	0	692
15:00	3	497	106	1	15	1	0	7	0	1	0	0	0	0	631
16:00	2	443	162	0	35	0	0	6	0	0	0	0	0	0	648
17:00	2	267	235	0	87	1	0	1	0	0	0	0	0	0	593
18:00	0	202	258	1	49	0	0	3	0	0	1	0	0	0	514
19:00	0	185	203	1	34	0	0	2	0	0	0	0	0	0	425
20:00	0	102	178	0	23	0	0	3	0	0	0	0	0	0	306
21:00	0	157	67	0	14	0	0	0	1	0	0	0	0	0	239
22:00	0	118	42	0	5	0	0	0	0	0	0	0	0	0	165
23:00	0	77	29	0	8	0	0	0	0	0	0	0	0	0	114
Total	22	5939	2084	20	474	6	0	57	4	2	3	0	1	0	8612
Percent	0.3%	69.0%	24.2%	0.2%	5.5%	0.1%	0.0%	0.7%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
AM Peak	08:00	11:00	11:00	07:00	09:00	11:00		10:00	02:00	11:00	10:00				
Vol.	4	548	103	4	25	2		8	1	1	1				
PM Peak	15:00	14:00	18:00	14:00	17:00	13:00		15:00	21:00	15:00	14:00		14:00		
Vol.	3	550	258	5	87	2		7	1	1	1		1		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Eastbound															
Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/08/23	0	27	10	0	5	0	0	0	0	0	0	0	0	0	42
01:00	0	18	10	0	2	0	0	0	0	0	0	0	0	0	30
02:00	1	14	0	1	3	0	0	0	0	0	0	0	0	0	19
03:00	0	8	5	0	5	0	0	0	0	0	0	0	0	0	18
04:00	0	12	10	0	1	0	0	1	0	0	0	0	0	0	24
05:00	1	29	9	1	7	0	0	1	0	0	0	0	0	0	48
06:00	0	79	24	0	8	0	0	0	0	0	0	0	0	0	111
07:00	1	118	61	0	15	0	0	1	1	0	0	0	0	0	197
08:00	1	196	87	1	18	0	0	2	0	0	0	0	0	0	305
09:00	1	321	83	3	24	0	0	3	0	0	0	0	0	0	435
10:00	0	458	128	0	37	0	0	3	0	0	0	0	0	0	626
11:00	2	506	129	0	24	0	0	2	0	0	0	0	0	0	663
12 PM	0	560	120	1	22	1	0	3	0	1	0	0	0	0	708
13:00	1	524	102	0	16	0	0	7	0	0	0	0	0	0	650
14:00	1	544	110	0	12	0	0	4	0	0	0	0	0	0	671
15:00	4	638	100	0	28	0	0	4	0	0	0	0	0	0	774
16:00	4	566	83	0	14	1	0	1	0	0	0	0	0	0	669
17:00	3	509	82	1	21	0	0	6	0	0	0	0	0	0	622
18:00	1	463	60	0	16	0	0	4	0	0	0	0	0	0	544
19:00	3	337	50	0	13	0	0	1	0	0	0	0	0	0	404
20:00	1	221	40	1	5	0	0	0	0	0	0	0	0	0	268
21:00	0	140	25	0	9	0	0	0	0	0	0	0	0	0	174
22:00	0	101	12	1	7	0	0	0	0	0	0	0	0	0	121
23:00	0	45	11	0	5	0	0	0	0	0	0	0	0	0	61
Total	25	6434	1351	10	317	2	0	43	1	1	0	0	0	0	8184
Percent	0.3%	78.6%	16.5%	0.1%	3.9%	0.0%	0.0%	0.5%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
AM Peak	11:00	11:00	11:00	09:00	10:00			09:00	07:00						
Vol.	2	506	129	3	37			3	1						
PM Peak	15:00	15:00	12:00	12:00	15:00	12:00		13:00		12:00					
Vol.	4	638	120	1	28	1		7		1					

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Eastbound															
Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/09/23	1	31	5	0	3	0	0	0	0	0	0	0	0	0	40
01:00	0	16	3	0	1	0	0	0	0	0	0	0	0	0	20
02:00	0	14	2	0	0	0	0	0	0	0	0	0	0	0	16
03:00	1	14	5	0	0	1	0	0	0	0	0	0	0	0	21
04:00	0	29	11	1	3	0	0	0	0	0	0	0	0	0	44
05:00	0	142	38	0	19	0	0	0	0	0	0	0	0	0	199
06:00	2	258	86	1	48	0	0	0	0	0	0	0	0	0	395
07:00	3	396	163	4	59	3	0	8	0	0	0	0	0	0	636
08:00	0	540	142	2	57	3	0	8	0	0	0	0	0	0	752
09:00	2	484	144	3	49	1	2	4	0	0	0	0	0	0	689
10:00	1	486	108	4	35	1	0	7	0	0	0	0	0	0	642
11:00	2	269	47	3	11	0	0	3	0	0	0	0	0	0	335
12 PM	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
Total	12	2679	754	18	285	9	2	30	0	0	0	0	0	0	3789
Percent	0.3%	70.7%	19.9%	0.5%	7.5%	0.2%	0.1%	0.8%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%	
AM Peak	07:00	08:00	07:00	07:00	07:00	07:00	09:00	07:00							
Vol.	3	540	163	4	59	3	2	8							
PM Peak															
Vol.															
Grand Total	308	52337	11739	302	3731	144	16	562	42	25	20	3	5	0	69234
Percent	0.4%	75.6%	17.0%	0.4%	5.4%	0.2%	0.0%	0.8%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/02/23	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
12 PM	11	336	72	1	24	4	1	6	1	1	0	1	0	0	458
13:00	6	625	110	3	27	3	0	11	0	2	0	1	0	0	788
14:00	10	695	113	4	39	4	2	9	2	0	0	0	1	0	879
15:00	10	727	151	5	38	3	3	10	2	1	2	0	0	0	952
16:00	30	874	152	4	34	4	0	15	0	2	1	0	0	0	1116
17:00	21	883	112	1	48	2	2	10	0	4	1	0	0	0	1084
18:00	20	682	91	2	20	2	0	7	2	0	0	0	0	0	826
19:00	9	503	67	0	27	1	0	5	1	0	0	0	0	0	613
20:00	3	308	53	0	13	0	0	4	0	1	0	0	0	0	382
21:00	4	187	30	0	8	0	0	4	0	1	0	0	0	0	234
22:00	0	108	12	0	2	0	0	0	0	0	0	0	0	0	122
23:00	2	53	14	1	0	1	0	1	0	0	0	0	0	0	72
Total	126	5981	977	21	280	24	8	82	8	12	4	2	1	0	7526
Percent	1.7%	79.5%	13.0%	0.3%	3.7%	0.3%	0.1%	1.1%	0.1%	0.2%	0.1%	0.0%	0.0%	0.0%	
AM Peak Vol.															
PM Peak Vol.	16:00 30	17:00 883	16:00 152	15:00 5	17:00 48	12:00 4	15:00 3	16:00 15	14:00 2	17:00 4	15:00 2	12:00 1	14:00 1		

Route 44 West of Secret Lake Road
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Site Code:
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Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/03/23	0	25	3	0	2	0	0	2	0	0	0	0	0	0	32
01:00	0	17	7	0	0	0	0	0	0	0	0	0	0	0	24
02:00	0	8	0	0	0	0	0	0	0	0	0	0	0	0	8
03:00	0	10	1	0	0	0	0	0	0	0	0	0	0	0	11
04:00	0	15	2	3	3	0	0	0	1	0	0	0	0	0	24
05:00	0	45	7	2	7	0	0	3	2	0	0	0	0	0	66
06:00	3	140	26	4	19	4	0	0	4	0	0	0	0	0	200
07:00	6	242	48	4	28	1	0	4	0	0	0	0	0	0	333
08:00	4	362	77	8	36	1	2	5	4	1	0	0	0	0	500
09:00	9	441	92	4	38	4	0	8	2	0	1	0	0	0	599
10:00	6	480	85	4	34	3	3	12	2	2	0	0	0	0	631
11:00	10	566	87	4	40	1	1	7	1	4	0	0	0	0	721
12 PM	5	610	93	5	32	3	1	9	1	0	1	0	0	0	760
13:00	9	645	101	4	21	7	2	7	1	0	2	0	0	0	799
14:00	19	646	105	2	38	1	2	12	0	1	0	0	0	0	826
15:00	9	766	136	8	37	3	1	6	2	2	1	0	0	0	971
16:00	18	928	125	1	32	2	0	13	0	2	1	0	0	0	1122
17:00	21	899	117	3	20	3	1	6	0	2	0	0	0	0	1072
18:00	18	745	91	1	6	2	0	3	1	0	0	0	0	0	867
19:00	16	524	67	1	14	1	1	2	0	0	0	0	0	0	626
20:00	7	412	35	0	4	1	0	3	0	0	0	0	0	0	462
21:00	7	252	22	0	5	0	0	1	0	0	0	0	0	0	287
22:00	1	158	9	1	5	0	0	0	0	0	0	0	0	0	174
23:00	0	92	8	0	0	0	0	0	0	0	0	0	0	0	100
Total	168	9028	1344	59	421	37	14	103	21	14	6	0	0	0	11215
Percent	1.5%	80.5%	12.0%	0.5%	3.8%	0.3%	0.1%	0.9%	0.2%	0.1%	0.1%	0.0%	0.0%	0.0%	
AM Peak	11:00	11:00	09:00	08:00	11:00	06:00	10:00	10:00	06:00	11:00	09:00				
Vol.	10	566	92	8	40	4	3	12	4	4	1				
PM Peak	17:00	16:00	15:00	15:00	14:00	13:00	13:00	16:00	15:00	15:00	13:00				
Vol.	21	928	136	8	38	7	2	13	2	2	2				

Route 44 West of Secret Lake Road
Canton, Connecticut

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Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/04/23	1	20	6	0	0	1	0	2	1	0	0	0	0	0	31
01:00	0	15	3	0	0	0	0	0	1	0	0	0	0	0	19
02:00	0	18	0	0	0	0	0	0	0	0	0	0	0	0	18
03:00	1	11	1	0	1	1	0	0	1	0	0	0	0	0	16
04:00	1	13	0	1	0	2	0	2	0	0	0	0	0	0	19
05:00	0	44	9	4	2	1	0	1	0	0	0	0	0	0	61
06:00	3	145	19	5	16	3	0	0	1	0	0	0	0	0	192
07:00	8	265	45	3	15	6	4	2	1	1	0	0	0	0	350
08:00	6	396	83	3	30	4	8	3	1	1	0	0	0	0	535
09:00	10	503	78	6	32	7	1	2	0	1	0	0	0	0	640
10:00	15	499	76	4	22	5	3	4	1	0	0	0	0	0	629
11:00	16	612	95	1	16	3	4	6	2	1	0	0	0	0	756
12 PM	16	630	101	5	24	2	3	2	1	1	0	0	0	0	785
13:00	22	620	77	5	24	7	6	5	0	0	1	0	1	0	768
14:00	18	649	83	4	22	10	2	10	1	1	0	0	0	0	800
15:00	19	704	117	2	30	4	3	6	3	2	0	0	1	0	891
16:00	16	888	125	2	30	7	0	7	0	1	0	0	0	0	1076
17:00	25	883	109	3	20	4	0	10	1	3	0	0	0	0	1058
18:00	27	727	72	3	13	4	1	2	1	0	1	1	1	0	853
19:00	10	546	29	2	10	0	1	3	0	0	0	0	0	0	601
20:00	4	413	48	1	4	0	0	0	0	0	1	0	0	0	471
21:00	5	225	16	1	5	0	0	0	0	0	1	0	0	0	253
22:00	0	157	17	0	3	0	0	0	0	0	0	0	0	0	177
23:00	0	91	8	0	1	0	0	1	0	0	0	0	0	0	101
Total	223	9074	1217	55	320	71	36	68	16	12	4	1	3	0	11100
Percent	2.0%	81.7%	11.0%	0.5%	2.9%	0.6%	0.3%	0.6%	0.1%	0.1%	0.0%	0.0%	0.0%	0.0%	
AM Peak	11:00	11:00	11:00	09:00	09:00	09:00	08:00	11:00	11:00	07:00					
Vol.	16	612	95	6	32	7	8	6	2	1					
PM Peak	18:00	16:00	16:00	12:00	15:00	14:00	13:00	14:00	15:00	17:00	13:00	18:00	13:00		
Vol.	27	888	125	5	30	10	6	10	3	3	1	1	1		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/05/23	1	43	2	0	0	1	0	1	1	0	0	0	0	0	49
01:00	0	11	3	1	1	0	0	1	0	0	0	0	0	0	17
02:00	0	12	1	0	1	0	0	0	0	0	0	0	0	0	14
03:00	0	7	1	0	0	0	0	0	1	0	0	0	0	0	9
04:00	1	13	2	1	0	2	0	0	0	0	0	0	0	0	19
05:00	0	38	7	2	2	2	0	0	0	0	0	0	0	0	51
06:00	5	143	25	4	15	6	0	0	1	0	0	0	0	0	199
07:00	9	279	43	3	12	2	0	1	3	0	0	0	0	0	352
08:00	3	384	72	5	14	3	3	2	2	0	0	0	0	0	488
09:00	8	438	82	0	22	4	1	6	2	0	0	0	0	0	563
10:00	9	487	68	1	18	5	2	6	0	0	0	0	0	0	596
11:00	7	612	72	3	16	4	3	6	2	2	1	0	0	0	728
12 PM	8	663	83	6	12	6	1	9	1	0	0	1	0	0	790
13:00	11	619	78	4	16	7	2	6	1	0	0	0	0	0	744
14:00	19	637	86	4	18	0	2	4	0	2	0	0	0	0	772
15:00	26	698	104	8	27	2	1	8	1	2	2	0	0	0	879
16:00	13	467	27	2	5	6	0	3	1	0	0	0	0	0	524
17:00	10	413	25	1	4	3	0	2	0	0	0	0	1	0	459
18:00	23	574	46	1	15	1	0	1	1	1	0	0	0	0	663
19:00	13	540	48	1	10	4	0	7	1	0	0	0	0	0	624
20:00	6	377	36	0	2	0	0	1	0	0	0	0	0	0	422
21:00	3	288	19	1	1	1	0	1	0	0	0	0	0	0	314
22:00	0	152	10	1	2	0	0	0	1	0	0	0	0	0	166
23:00	1	88	11	1	1	2	0	0	0	0	0	0	0	0	104
Total	176	7983	951	50	214	61	15	65	19	7	3	1	1	0	9546
Percent	1.8%	83.6%	10.0%	0.5%	2.2%	0.6%	0.2%	0.7%	0.2%	0.1%	0.0%	0.0%	0.0%	0.0%	
AM Peak	07:00	11:00	09:00	08:00	09:00	06:00	08:00	09:00	07:00	11:00	11:00				
Vol.	9	612	82	5	22	6	3	6	3	2	1				
PM Peak	15:00	15:00	15:00	15:00	15:00	13:00	13:00	12:00	12:00	14:00	15:00	12:00	17:00		
Vol.	26	698	104	8	27	7	2	9	1	2	2	1	1		

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Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/06/23	0	36	5	0	1	0	0	2	0	0	0	0	0	0	44
01:00	0	20	3	0	0	0	0	0	0	0	0	0	0	0	23
02:00	1	13	1	0	2	0	0	0	0	0	0	0	0	0	17
03:00	0	11	1	1	0	0	0	0	0	0	0	0	0	0	13
04:00	0	14	1	2	0	1	0	0	0	0	0	0	0	0	18
05:00	1	38	9	3	4	1	0	0	1	0	0	0	0	0	57
06:00	1	131	21	4	7	5	0	0	1	0	0	0	0	0	170
07:00	96	222	7	1	4	5	0	1	1	0	0	0	0	0	337
08:00	133	319	4	1	3	4	1	1	0	1	0	0	0	0	467
09:00	104	376	4	0	2	3	0	0	1	0	0	0	0	0	490
10:00	84	446	6	0	5	5	0	0	0	1	0	0	0	0	547
11:00	74	476	7	0	3	3	2	1	2	0	0	0	0	0	568
12 PM	60	537	6	0	1	3	1	0	3	1	0	0	1	0	613
13:00	56	530	3	1	3	4	1	0	1	0	0	0	0	0	599
14:00	63	599	6	1	8	6	3	0	0	0	0	0	0	0	686
15:00	57	640	12	3	2	3	0	0	1	0	0	0	0	0	718
16:00	46	736	9	2	2	7	0	1	0	0	0	0	0	0	803
17:00	66	640	3	0	2	5	0	0	1	0	0	0	0	0	717
18:00	45	493	3	0	0	3	0	0	0	1	0	0	1	0	546
19:00	47	454	5	1	0	3	0	0	0	0	0	0	0	0	510
20:00	29	369	3	0	0	3	0	0	1	0	0	0	0	0	405
21:00	14	327	2	0	1	1	0	1	0	0	0	0	0	0	346
22:00	28	191	4	0	0	1	0	0	0	0	0	0	0	0	224
23:00	15	136	0	0	3	0	0	0	0	0	0	0	0	0	154
Total	1020	7754	125	20	53	66	8	7	13	4	0	0	2	0	9072
Percent	11.2%	85.5%	1.4%	0.2%	0.6%	0.7%	0.1%	0.1%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	
AM Peak	08:00	11:00	06:00	06:00	06:00	06:00	11:00	00:00	11:00	08:00					
Vol.	133	476	21	4	7	5	2	2	2	1					
PM Peak	17:00	16:00	15:00	15:00	14:00	16:00	14:00	16:00	12:00	12:00			12:00		
Vol.	66	736	12	3	8	7	3	1	3	1			1		

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Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/07/23	13	67	2	0	0	0	0	2	0	0	0	0	0	0	84
01:00	7	34	1	0	0	0	0	0	0	0	0	0	0	0	42
02:00	4	22	0	0	0	0	0	0	0	0	0	0	0	0	26
03:00	3	17	0	0	0	0	0	0	0	0	0	0	0	0	20
04:00	2	14	0	0	3	0	0	0	0	0	0	0	0	0	19
05:00	3	21	0	0	0	0	0	0	0	0	0	0	0	0	24
06:00	12	57	2	0	1	0	0	0	1	0	0	0	0	0	73
07:00	14	130	2	0	3	0	0	0	1	0	0	0	0	0	150
08:00	20	240	3	0	2	0	0	0	1	0	0	0	0	0	266
09:00	19	448	6	0	0	0	0	0	0	0	0	0	0	0	473
10:00	30	504	6	0	0	2	0	0	1	0	0	0	0	0	543
11:00	28	575	9	0	1	3	3	0	0	0	0	0	0	0	619
12 PM	50	614	3	0	4	4	1	0	2	0	0	0	0	0	678
13:00	37	630	8	0	4	2	0	0	1	0	0	0	0	0	682
14:00	33	652	10	1	0	1	0	0	0	0	0	0	1	0	698
15:00	37	620	6	0	1	7	0	0	3	0	0	0	0	0	674
16:00	22	668	14	0	0	3	0	0	1	0	0	0	0	0	708
17:00	26	575	6	0	0	6	0	0	0	0	0	0	0	0	613
18:00	28	550	3	0	0	4	0	0	0	0	0	0	0	0	585
19:00	23	362	5	0	0	5	1	0	2	0	0	0	0	0	398
20:00	30	332	3	0	0	1	0	0	0	0	0	0	0	0	366
21:00	31	246	1	0	0	4	0	0	0	0	0	0	0	0	282
22:00	21	185	2	0	0	0	0	0	0	0	0	0	0	0	208
23:00	10	150	1	0	1	0	0	0	0	0	0	0	0	0	162
Total	503	7713	93	1	20	42	5	2	13	0	0	0	1	0	8393
Percent	6.0%	91.9%	1.1%	0.0%	0.2%	0.5%	0.1%	0.0%	0.2%	0.0%	0.0%	0.0%	0.0%	0.0%	
AM Peak	10:00	11:00	11:00		04:00	11:00	11:00	00:00	06:00						
Vol.	30	575	9		3	3	3	2	1						
PM Peak	12:00	16:00	16:00	14:00	12:00	15:00	12:00		15:00				14:00		
Vol.	50	668	14	1	4	7	1		3				1		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/08/23	9	73	1	1	0	0	0	0	0	0	0	0	0	0	84
01:00	2	41	0	0	0	0	0	0	0	0	0	0	0	0	43
02:00	1	18	0	0	0	0	0	1	0	0	0	0	0	0	20
03:00	1	13	0	0	0	0	0	0	0	0	0	0	0	0	14
04:00	0	11	0	0	1	0	0	0	0	0	0	0	0	0	12
05:00	2	15	0	0	0	1	0	0	0	0	0	0	0	0	18
06:00	4	58	1	0	0	0	0	0	0	0	0	0	0	0	63
07:00	10	107	6	0	0	0	0	0	1	0	0	0	0	0	124
08:00	20	228	4	0	0	0	0	0	0	0	0	0	0	0	252
09:00	22	370	5	0	0	1	0	0	2	0	0	0	0	0	400
10:00	27	540	11	0	2	4	0	1	1	0	0	0	0	0	586
11:00	26	640	15	0	0	3	0	0	0	1	0	0	0	0	685
12 PM	21	643	14	2	1	1	1	0	0	0	0	0	0	0	683
13:00	27	671	20	2	0	5	0	0	0	0	0	0	0	0	725
14:00	18	649	17	0	0	0	0	0	0	0	0	0	0	0	684
15:00	12	678	14	0	0	4	0	1	0	0	0	0	0	0	709
16:00	17	608	21	0	0	4	0	0	1	0	0	1	0	0	652
17:00	12	527	20	1	2	5	1	0	1	0	0	0	0	0	569
18:00	14	501	13	0	0	4	0	1	0	1	0	0	0	0	534
19:00	9	382	13	0	0	3	0	0	0	0	1	0	0	0	408
20:00	3	306	7	0	0	0	0	0	2	0	0	0	0	0	318
21:00	5	208	3	0	1	0	0	0	1	0	0	0	0	0	218
22:00	0	127	6	0	0	0	0	0	0	0	0	0	0	0	133
23:00	1	79	2	0	1	0	0	0	0	0	0	0	0	0	83
Total	263	7493	193	6	8	35	2	4	9	2	1	1	0	0	8017
Percent	3.3%	93.5%	2.4%	0.1%	0.1%	0.4%	0.0%	0.0%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	
AM Peak	10:00	11:00	11:00	00:00	10:00	10:00		02:00	09:00	11:00					
Vol.	27	640	15	1	2	4		1	2	1					
PM Peak	13:00	15:00	16:00	12:00	17:00	13:00	12:00	15:00	20:00	18:00	19:00	16:00			
Vol.	27	678	21	2	2	5	1	1	2	1	1	1			

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
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Westbound

Start Time	Bikes	Cars & Trailers	2 Axle Long	Buses	2 Axle 6 Tire	3 Axle Single	4 Axle Single	<5 Axl Double	5 Axle Double	>6 Axl Double	<6 Axl Multi	6 Axle Multi	>6 Axl Multi	Not Classed	Total
10/09/23	1	38	0	0	1	0	0	0	0	0	0	0	0	0	40
01:00	0	26	1	0	0	0	0	0	0	0	0	0	0	0	27
02:00	1	7	0	0	0	0	0	0	0	0	0	0	0	0	8
03:00	0	6	0	0	1	0	0	0	0	0	0	0	0	0	7
04:00	1	19	0	0	1	1	0	0	0	0	0	0	0	0	22
05:00	1	27	1	0	1	2	0	0	0	0	0	0	0	0	32
06:00	6	103	15	2	2	4	0	0	0	0	0	0	0	0	132
07:00	8	223	10	0	2	2	1	0	1	0	0	0	0	0	247
08:00	7	314	22	1	4	3	0	0	2	0	0	0	0	0	353
09:00	17	433	23	1	7	6	0	1	2	0	0	0	0	0	490
10:00	7	509	23	2	5	3	1	1	2	0	0	0	0	0	553
11:00	7	292	12	0	2	2	0	1	1	0	0	0	0	0	317
12 PM	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
Total	56	1997	107	6	26	23	2	3	8	0	0	0	0	0	2228
Percent	2.5%	89.6%	4.8%	0.3%	1.2%	1.0%	0.1%	0.1%	0.4%	0.0%	0.0%	0.0%	0.0%	0.0%	
AM Peak	09:00	10:00	09:00	06:00	09:00	09:00	07:00	09:00	08:00						
Vol.	17	509	23	2	7	6	1	1	2						
PM Peak															
Vol.															
Grand Total	2535	57023	5007	218	1342	359	90	334	107	51	18	5	8	0	67097
Percent	3.8%	85.0%	7.5%	0.3%	2.0%	0.5%	0.1%	0.5%	0.2%	0.1%	0.0%	0.0%	0.0%	0.0%	

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC

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Site Code:
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Eastbound	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace	Number
Start Time	15	20	25	30	35	40	45	50	55	60	65	70	75	999		Speed	in Pace
10/02/23	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
12 PM	8	20	17	24	67	152	111	43	3	2	0	0	0	0	447	36-45	263
13:00	10	25	19	40	84	220	259	87	24	1	0	1	0	0	770	36-45	479
14:00	12	24	46	57	97	203	236	105	24	3	0	0	1	0	808	36-45	439
15:00	9	41	42	42	106	236	193	90	28	2	1	0	0	0	790	36-45	429
16:00	6	26	44	31	122	196	223	81	18	2	0	1	0	0	750	36-45	419
17:00	12	28	35	57	75	178	206	77	33	6	2	0	0	0	709	36-45	384
18:00	5	14	16	50	71	151	161	106	26	1	1	0	0	0	602	36-45	312
19:00	9	11	11	21	65	143	137	62	15	3	1	0	0	0	478	36-45	280
20:00	7	4	8	10	23	71	92	34	15	7	2	0	0	0	273	36-45	163
21:00	3	5	7	8	18	36	41	29	13	3	3	3	0	0	169	36-45	77
22:00	0	0	2	0	3	13	28	25	17	5	2	0	0	0	95	41-50	53
23:00	0	0	0	1	1	5	7	16	7	4	0	2	0	0	43	41-50	23
Total	81	198	247	341	732	1604	1694	755	223	39	12	7	1	0	5934		
Percent	1.4%	3.3%	4.2%	5.7%	12.3%	27.0%	28.5%	12.7%	3.8%	0.7%	0.2%	0.1%	0.0%	0.0%			
AM Peak																	
Vol.																	
PM Peak	14:00	15:00	14:00	14:00	16:00	15:00	13:00	18:00	17:00	20:00	21:00	21:00	14:00		14:00		
Vol.	12	41	46	57	122	236	259	106	33	7	3	3	1		808		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Eastbound																	
Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
10/03/23	0	0	0	1	0	2	8	8	7	2	0	1	0	0	29	41-50	16
01:00	0	0	0	0	1	2	3	3	2	1	0	0	0	0	12	41-50	6
02:00	0	0	0	0	1	5	4	4	2	0	0	0	0	0	16	36-45	9
03:00	0	0	1	0	0	2	3	6	2	2	1	0	0	0	17	41-50	9
04:00	0	0	1	0	0	0	10	19	11	13	2	0	2	0	58	44-53	30
05:00	1	2	0	1	3	6	38	57	64	42	19	5	1	0	239	46-55	121
06:00	2	13	16	24	39	46	92	150	134	65	20	1	0	0	602	46-55	284
07:00	2	16	31	62	78	136	255	204	147	70	8	2	0	0	1011	41-50	459
08:00	7	18	39	78	93	200	282	204	91	11	5	0	0	0	1028	41-50	486
09:00	5	17	27	19	74	172	300	151	52	9	2	0	0	0	828	36-45	472
10:00	11	33	32	31	70	214	291	123	35	5	2	0	0	0	847	36-45	505
11:00	7	15	20	22	36	157	226	137	39	4	0	0	0	0	663	36-45	383
12 PM	10	25	39	41	103	218	217	56	17	3	0	0	0	0	729	36-45	435
13:00	3	28	28	45	82	222	254	78	12	2	0	0	0	0	754	36-45	476
14:00	11	30	33	59	105	222	198	80	19	4	1	0	0	0	762	36-45	420
15:00	12	24	48	46	70	246	245	106	26	3	0	0	0	0	826	36-45	491
16:00	8	33	36	43	108	202	205	113	27	5	1	0	0	0	781	36-45	407
17:00	8	18	29	48	88	228	197	85	32	8	0	0	0	0	741	36-45	425
18:00	7	24	25	80	116	151	119	54	8	5	0	1	0	0	590	36-45	270
19:00	7	10	14	41	60	125	119	63	14	3	0	0	0	0	456	36-45	244
20:00	6	6	6	10	38	85	105	47	21	3	0	0	0	0	327	36-45	190
21:00	4	3	9	13	16	45	61	37	10	7	2	0	0	0	207	36-45	106
22:00	0	0	3	3	6	17	27	31	4	2	0	1	0	0	94	41-50	58
23:00	0	0	0	1	1	1	18	12	8	4	1	1	0	0	47	41-50	30
Total	111	315	437	668	1188	2704	3277	1828	784	273	64	12	3	0	11664		
Percent	1.0%	2.7%	3.7%	5.7%	10.2%	23.2%	28.1%	15.7%	6.7%	2.3%	0.5%	0.1%	0.0%	0.0%			
AM Peak	10:00	10:00	08:00	08:00	08:00	10:00	09:00	07:00	07:00	07:00	06:00	05:00	04:00		08:00		
Vol.	11	33	39	78	93	214	300	204	147	70	20	5	2		1028		
PM Peak	15:00	16:00	15:00	18:00	18:00	15:00	13:00	16:00	17:00	17:00	21:00	18:00			15:00		
Vol.	12	33	48	80	116	246	254	113	32	8	2	1			826		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
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(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Eastbound																Pace	Number
Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Speed	in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
10/04/23	0	0	0	0	1	2	9	11	2	3	0	0	0	0	28	41-50	20
01:00	0	0	0	0	0	1	2	4	4	1	0	0	0	0	12	45-54	8
02:00	0	1	0	0	0	3	8	7	4	0	0	0	0	0	23	41-50	15
03:00	0	0	1	1	0	1	1	4	7	3	1	0	0	0	19	46-55	11
04:00	0	0	0	0	0	0	9	20	11	10	2	3	0	0	55	45-54	31
05:00	0	0	0	1	0	5	18	53	79	47	14	6	2	0	225	46-55	132
06:00	2	14	16	11	37	48	87	151	112	37	18	2	0	0	535	46-55	263
07:00	6	13	34	66	83	119	211	269	154	34	10	2	0	0	1001	41-50	480
08:00	5	15	39	84	126	170	267	229	86	14	5	0	0	0	1040	41-50	496
09:00	9	9	19	40	71	166	267	155	46	8	0	0	0	0	790	36-45	433
10:00	13	23	34	58	94	186	210	117	28	3	0	0	0	0	766	36-45	396
11:00	7	24	19	48	90	243	238	65	10	2	0	0	0	0	746	36-45	481
12 PM	8	19	25	34	80	258	218	83	33	3	3	0	0	0	764	36-45	476
13:00	9	17	28	44	90	244	234	86	20	3	1	0	0	0	776	36-45	478
14:00	10	34	42	65	82	178	206	61	13	1	0	0	0	0	692	36-45	384
15:00	9	22	44	72	103	232	224	70	17	2	1	0	0	0	796	36-45	456
16:00	9	15	33	52	60	263	234	91	29	1	0	0	0	0	787	36-45	497
17:00	7	24	50	51	84	210	186	81	15	2	0	0	0	0	710	36-45	396
18:00	14	17	20	77	76	152	161	57	18	2	0	0	0	0	594	36-45	313
19:00	8	11	7	40	77	155	117	55	13	0	0	0	0	0	483	36-45	272
20:00	3	7	6	16	38	80	72	56	9	7	1	0	1	0	296	36-45	152
21:00	6	6	6	5	14	63	65	38	18	5	3	0	0	0	229	36-45	128
22:00	0	0	0	2	7	10	34	26	20	4	2	1	0	0	106	41-50	60
23:00	0	0	0	2	2	6	24	13	6	4	3	0	0	0	60	41-50	37
Total	125	271	423	769	1215	2795	3102	1802	754	196	64	14	3	0	11533		
Percent	1.1%	2.3%	3.7%	6.7%	10.5%	24.2%	26.9%	15.6%	6.5%	1.7%	0.6%	0.1%	0.0%	0.0%			
AM Peak	10:00	11:00	08:00	08:00	08:00	11:00	08:00	07:00	07:00	05:00	06:00	05:00	05:00	05:00	08:00		
Vol.	13	24	39	84	126	243	267	269	154	47	18	6	2		1040		
PM Peak	18:00	14:00	17:00	18:00	15:00	16:00	13:00	16:00	12:00	20:00	12:00	22:00	20:00	15:00			
Vol.	14	34	50	77	103	263	234	91	33	7	3	1	1		796		

Route 44 West of Secret Lake Road
Canton, Connecticut

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Site Code:
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Eastbound	Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
10/05/23	0	0	1	0	0	2	11	4	3	1	0	0	0	0	0	22	40-49	15
01:00	0	0	1	0	1	2	5	2	2	1	0	0	0	0	0	14	41-50	7
02:00	0	0	1	0	0	1	9	3	2	0	0	0	0	0	0	16	40-49	12
03:00	0	0	0	2	0	1	4	4	2	4	1	0	0	0	0	18	41-50	8
04:00	0	0	0	0	0	2	11	27	5	6	4	0	1	0	0	56	41-50	38
05:00	0	0	1	1	1	5	36	75	73	33	13	6	0	0	0	244	46-55	148
06:00	3	11	8	18	28	67	120	150	87	58	8	1	0	0	0	559	41-50	270
07:00	2	13	31	51	64	128	227	229	109	38	5	1	0	0	0	898	41-50	456
08:00	5	8	36	74	95	197	274	184	86	15	4	1	0	0	0	979	36-45	471
09:00	13	19	14	67	91	173	188	144	51	7	3	0	0	0	0	770	36-45	361
10:00	12	18	20	32	111	226	199	88	45	4	0	0	0	0	0	755	36-45	425
11:00	9	24	30	66	119	208	175	64	14	3	1	0	1	0	0	714	36-45	383
12 PM	8	24	23	45	98	251	184	79	23	5	1	0	0	0	0	741	36-45	435
13:00	16	23	20	38	100	202	216	91	23	5	0	1	0	0	0	735	36-45	418
14:00	13	20	48	77	114	172	150	45	12	5	1	0	0	0	0	657	36-45	322
15:00	9	29	78	71	138	155	142	40	7	1	0	0	0	0	0	670	36-45	297
16:00	11	135	134	64	10	16	4	7	0	1	0	0	0	0	0	382	16-25	269
17:00	10	214	96	18	6	15	10	5	1	0	0	0	0	0	0	375	16-25	310
18:00	9	63	71	70	96	85	64	42	10	4	0	0	0	0	0	514	31-40	181
19:00	9	11	21	37	76	147	103	51	14	4	0	1	0	0	0	474	36-45	250
20:00	6	6	7	15	67	98	80	38	12	3	0	0	0	0	0	332	36-45	178
21:00	6	0	6	8	35	70	52	41	16	7	0	1	0	0	0	242	36-45	122
22:00	0	0	3	3	5	19	51	24	7	1	1	0	0	0	0	114	41-50	75
23:00	0	0	0	1	0	8	28	19	4	4	1	1	0	0	0	66	41-50	47
Total	141	618	650	758	1255	2250	2343	1456	608	210	43	13	2	0	0	10347		
Percent	1.4%	6.0%	6.3%	7.3%	12.1%	21.7%	22.6%	14.1%	5.9%	2.0%	0.4%	0.1%	0.0%	0.0%	0.0%			
AM Peak	09:00	11:00	08:00	08:00	11:00	10:00	08:00	07:00	07:00	06:00	05:00	05:00	04:00	07:00	04:00	08:00		
Vol.	13	24	36	74	119	226	274	229	109	58	13	6	1	1	1	979		
PM Peak	13:00	17:00	16:00	14:00	15:00	12:00	13:00	13:00	12:00	21:00	12:00	13:00	12:00	13:00	12:00	12:00		
Vol.	16	214	134	77	138	251	216	91	23	7	1	1	1	1	1	741		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Eastbound																	
Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
10/06/23	0	0	0	1	0	5	10	9	4	0	0	1	0	0	30	41-50	19
01:00	0	0	0	0	1	1	9	5	3	3	0	0	0	0	22	41-50	14
02:00	0	0	0	0	0	9	4	3	0	0	0	0	0	0	16	36-45	13
03:00	0	0	0	0	1	1	7	12	1	1	1	1	0	0	25	41-50	19
04:00	0	0	0	0	0	0	13	21	10	5	1	0	0	0	50	41-50	34
05:00	0	0	1	1	1	2	31	58	74	17	7	0	1	0	193	46-55	132
06:00	2	5	14	12	18	84	119	124	63	26	10	2	0	0	479	41-50	243
07:00	2	10	15	44	77	142	178	152	87	17	3	1	2	0	730	41-50	330
08:00	5	9	77	95	106	137	113	68	25	6	4	0	0	0	645	36-45	250
09:00	8	21	125	107	85	101	68	26	7	1	0	1	0	0	550	21-30	232
10:00	7	17	92	144	105	125	69	41	11	2	2	0	0	0	615	26-35	249
11:00	11	36	134	131	98	94	66	23	3	0	2	0	0	0	598	21-30	265
12 PM	15	31	119	122	92	93	65	31	8	2	0	0	0	0	578	21-30	241
13:00	12	54	144	142	104	60	39	13	7	2	0	0	0	0	577	21-30	286
14:00	11	41	131	148	90	68	52	20	5	3	0	0	0	0	569	21-30	279
15:00	8	29	134	132	110	102	66	32	3	1	0	0	0	0	617	21-30	266
16:00	7	27	160	132	75	72	45	18	21	6	2	1	0	0	566	21-30	292
17:00	6	33	154	107	62	60	49	34	15	2	0	0	0	0	522	21-30	261
18:00	8	34	167	87	56	44	32	12	9	3	0	0	0	0	452	21-30	254
19:00	7	14	112	112	74	63	49	28	1	1	0	0	0	0	461	21-30	224
20:00	3	6	34	104	84	51	37	28	8	4	0	0	0	0	359	26-35	188
21:00	2	7	9	53	60	51	28	30	8	4	0	0	0	0	252	26-35	113
22:00	0	1	4	2	23	38	45	26	13	3	0	1	0	0	156	36-45	83
23:00	0	0	1	5	24	22	16	14	9	5	3	0	0	0	99	31-40	46
Total	114	375	1627	1681	1346	1425	1210	828	395	114	35	8	3	0	9161		
Percent	1.2%	4.1%	17.8%	18.3%	14.7%	15.6%	13.2%	9.0%	4.3%	1.2%	0.4%	0.1%	0.0%	0.0%			
AM Peak	11:00	11:00	11:00	10:00	08:00	07:00	07:00	07:00	07:00	06:00	06:00	06:00	07:00		07:00		
Vol.	11	36	134	144	106	142	178	152	87	26	10	2	2		730		
PM Peak	12:00	13:00	18:00	14:00	15:00	15:00	15:00	17:00	16:00	16:00	23:00	16:00			15:00		
Vol.	15	54	167	148	110	102	66	34	21	6	3	1			617		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Eastbound	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace	Number
Start Time	15	20	25	30	35	40	45	50	55	60	65	70	75	999		Speed	in Pace
10/07/23	0	0	1	1	20	12	9	14	4	1	0	0	0	0	62	31-40	32
01:00	0	0	0	0	3	7	5	3	1	1	2	0	0	0	22	35-44	12
02:00	0	0	0	0	2	7	6	7	3	0	1	0	0	0	26	36-45	13
03:00	0	0	0	0	0	2	3	5	3	0	0	0	0	0	13	41-50	8
04:00	0	0	0	0	1	5	11	7	5	2	0	1	0	0	32	40-49	18
05:00	0	0	0	0	2	10	21	22	6	8	1	0	0	0	70	41-50	43
06:00	2	1	6	12	23	38	30	13	10	6	2	0	0	0	143	36-45	68
07:00	3	6	4	22	48	79	59	44	28	8	3	0	0	0	304	36-45	138
08:00	5	6	23	47	76	74	101	78	26	6	2	0	0	0	444	41-50	179
09:00	5	11	56	60	98	133	96	61	18	2	1	0	0	0	541	31-40	231
10:00	13	14	59	80	96	163	126	70	28	6	0	0	0	0	655	36-45	289
11:00	6	42	137	119	99	157	98	21	8	0	1	0	0	0	688	31-40	256
12 PM	4	34	152	123	81	86	66	45	9	3	0	0	0	0	603	21-30	275
13:00	8	52	188	134	76	76	63	54	23	5	2	1	0	0	682	21-30	322
14:00	14	21	146	128	109	121	101	41	11	0	0	0	0	0	692	21-30	274
15:00	12	48	136	85	60	117	100	52	19	2	0	0	0	0	631	21-30	221
16:00	8	38	103	102	78	105	96	77	28	9	3	1	0	0	648	21-30	205
17:00	5	22	61	85	70	66	81	76	57	44	18	6	1	0	592	41-50	157
18:00	3	16	42	72	55	69	65	93	65	20	12	1	1	0	514	41-50	158
19:00	4	7	47	59	54	66	71	57	33	20	6	1	0	0	425	36-45	137
20:00	1	3	12	34	30	51	50	60	38	21	5	1	0	0	306	41-50	110
21:00	1	5	14	26	23	37	57	44	23	6	3	0	0	0	239	41-50	101
22:00	0	0	1	1	7	18	61	35	32	8	0	2	0	0	165	41-50	96
23:00	0	0	0	0	5	15	20	32	23	17	2	0	0	0	114	46-55	55
Total	94	326	1188	1190	1116	1514	1396	1011	501	195	64	14	2	0	8611		
Percent	1.1%	3.8%	13.8%	13.8%	13.0%	17.6%	16.2%	11.7%	5.8%	2.3%	0.7%	0.2%	0.0%	0.0%			
AM Peak	10:00	11:00	11:00	11:00	11:00	10:00	10:00	08:00	07:00	05:00	07:00	04:00			11:00		
Vol.	13	42	137	119	99	163	126	78	28	8	3	1			688		
PM Peak	14:00	13:00	13:00	13:00	14:00	14:00	14:00	18:00	18:00	17:00	17:00	17:00	17:00		14:00		
Vol.	14	52	188	134	109	121	101	93	65	44	18	6	1		692		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Eastbound																	
Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
10/08/23	0	1	0	3	1	2	4	14	10	5	2	0	0	0	42	46-55	24
01:00	0	0	1	1	3	6	4	6	5	3	1	0	0	0	30	44-53	11
02:00	0	0	0	0	1	0	7	5	3	3	0	0	0	0	19	41-50	12
03:00	0	0	0	0	0	1	3	7	3	3	1	0	0	0	18	41-50	10
04:00	0	0	0	0	0	0	8	6	7	3	0	0	0	0	24	41-50	14
05:00	0	0	0	0	1	11	6	10	11	7	2	0	0	0	48	46-55	21
06:00	4	1	3	2	10	15	22	21	18	9	4	2	0	0	111	41-50	43
07:00	4	7	9	7	16	17	39	34	36	21	4	1	1	0	196	41-50	73
08:00	2	9	11	25	17	36	59	81	47	17	0	1	0	0	305	41-50	140
09:00	4	13	11	49	47	58	100	104	35	11	3	0	0	0	435	41-50	204
10:00	6	8	25	74	56	97	179	115	47	15	4	0	0	0	626	41-50	294
11:00	9	20	41	79	78	123	184	95	28	5	1	0	0	0	663	36-45	307
12 PM	9	31	68	95	93	168	133	76	26	7	1	1	0	0	708	36-45	301
13:00	7	25	48	109	89	138	132	60	35	6	1	0	0	0	650	36-45	270
14:00	9	18	63	80	86	170	162	54	27	2	0	0	0	0	671	36-45	332
15:00	16	28	55	76	109	231	156	72	27	4	0	0	0	0	774	36-45	387
16:00	7	27	64	107	103	129	126	74	28	4	0	0	0	0	669	36-45	255
17:00	8	21	28	96	100	160	111	64	28	5	0	1	0	0	622	36-45	271
18:00	4	9	32	66	82	135	131	62	21	2	0	0	0	0	544	36-45	266
19:00	7	14	20	62	83	87	65	43	19	3	1	0	0	0	404	31-40	170
20:00	3	7	16	27	64	55	59	20	14	3	0	0	0	0	268	31-40	119
21:00	2	5	3	16	31	43	51	17	6	0	0	0	0	0	174	36-45	94
22:00	0	0	0	1	4	22	38	24	21	9	1	1	0	0	121	41-50	62
23:00	0	0	0	1	0	13	10	17	13	3	1	1	1	0	60	46-55	30
Total	101	244	498	976	1074	1717	1789	1081	515	150	27	8	2	0	8182		
Percent	1.2%	3.0%	6.1%	11.9%	13.1%	21.0%	21.9%	13.2%	6.3%	1.8%	0.3%	0.1%	0.0%	0.0%			
AM Peak	11:00	11:00	11:00	11:00	11:00	11:00	11:00	10:00	08:00	07:00	06:00	06:00	07:00		11:00		
Vol.	9	20	41	79	78	123	184	115	47	21	4	2	1		663		
PM Peak	15:00	12:00	12:00	13:00	15:00	15:00	14:00	12:00	13:00	22:00	12:00	12:00	23:00		15:00		
Vol.	16	31	68	109	109	231	162	76	35	9	1	1	1		774		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC Kensington, Connecticut 06037 (860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Eastbound																	
Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
10/09/23	0	0	0	0	3	7	14	10	3	2	1	0	0	0	40	41-50	24
01:00	0	0	0	1	3	6	5	2	1	0	0	0	1	0	19	35-44	11
02:00	0	0	0	0	5	7	3	0	1	0	0	0	0	0	16	31-40	12
03:00	0	0	0	2	1	5	5	2	4	1	1	0	0	0	21	36-45	10
04:00	0	0	0	1	0	5	7	16	5	5	3	1	0	0	43	41-50	23
05:00	0	0	0	0	1	3	6	61	74	37	13	3	1	0	199	46-55	135
06:00	1	15	7	9	16	19	64	97	89	58	13	6	0	0	394	46-55	186
07:00	4	14	11	24	38	75	151	158	93	53	14	1	0	0	636	41-50	309
08:00	0	12	24	50	72	120	210	162	77	21	4	0	0	0	752	41-50	372
09:00	8	12	34	64	84	139	149	125	56	15	3	0	0	0	689	36-45	288
10:00	14	17	47	83	129	157	113	56	22	4	0	0	0	0	642	31-40	286
11:00	7	16	26	53	70	83	48	29	3	0	0	0	0	0	335	31-40	153
12 PM	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
Total	34	86	149	287	422	626	775	718	428	196	52	11	2	0	3786		
Percent	0.9%	2.3%	3.9%	7.6%	11.1%	16.5%	20.5%	19.0%	11.3%	5.2%	1.4%	0.3%	0.1%	0.0%			
AM Peak	10:00	10:00	10:00	10:00	10:00	10:00	08:00	08:00	07:00	06:00	07:00	06:00	01:00		08:00		
Vol.	14	17	47	83	129	157	210	162	93	58	14	6	1		752		
PM Peak																	
Vol.	801	2433	5219	6670	8348	14635	15586	9479	4208	1373	361	87	18	0	69218		
Percent	1.2%	3.5%	7.5%	9.6%	12.1%	21.1%	22.5%	13.7%	6.1%	2.0%	0.5%	0.1%	0.0%	0.0%			

15th Percentile : 26 MPH
50th Percentile : 38 MPH
85th Percentile : 47 MPH
95th Percentile : 53 MPH

Stats
10 MPH Pace Speed : 36-45 MPH
Number in Pace : 30221
Percent in Pace : 43.7%
Number of Vehicles > 40 MPH : 31112
Percent of Vehicles > 40 MPH : 44.9%
Mean Speed(Average) : 38 MPH

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC Kensington, Connecticut 06037 (860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
10/02/23	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
12 PM	0	2	24	144	101	85	78	21	1	2	0	0	0	0	458	26-35	245
13:00	2	3	45	195	234	128	116	49	16	0	0	0	0	0	788	26-35	429
14:00	0	1	65	225	220	164	150	46	7	1	0	0	0	0	879	26-35	445
15:00	0	0	50	234	246	177	189	47	6	2	0	1	0	0	952	26-35	480
16:00	4	1	48	276	369	206	157	46	7	2	0	0	0	0	1116	26-35	645
17:00	2	1	34	296	359	211	113	53	14	1	0	0	0	0	1084	26-35	655
18:00	4	2	59	231	246	172	85	27	0	0	0	0	0	0	826	26-35	477
19:00	0	1	31	205	144	111	97	20	3	1	0	0	0	0	613	26-35	349
20:00	1	1	14	99	67	81	86	28	5	0	0	0	0	0	382	36-45	167
21:00	0	0	12	62	30	41	59	25	4	1	0	0	0	0	234	36-45	100
22:00	0	0	1	1	2	12	41	51	12	2	0	0	0	0	122	41-50	92
23:00	0	0	0	0	3	7	30	21	9	2	0	0	0	0	72	41-50	51
Total	13	12	383	1968	2021	1395	1201	434	84	14	0	1	0	0	7526		
Percent	0.2%	0.2%	5.1%	26.1%	26.9%	18.5%	16.0%	5.8%	1.1%	0.2%	0.0%	0.0%	0.0%	0.0%			
AM Peak Vol.																	
PM Peak Vol.	16:00	13:00	14:00	17:00	16:00	17:00	15:00	17:00	13:00	12:00		15:00			16:00		
	4	3	65	296	369	211	189	53	16	2		1			1116		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
10/03/23	0	0	0	1	3	4	12	8	3	1	0	0	0	0	32	41-50	20
01:00	0	0	1	0	1	2	11	7	1	1	0	0	0	0	24	41-50	18
02:00	0	0	0	0	1	3	3	1	0	0	0	0	0	0	8	36-45	6
03:00	0	0	0	0	0	1	3	5	2	0	0	0	0	0	11	41-50	8
04:00	0	0	0	1	0	5	10	4	3	1	0	0	0	0	24	36-45	15
05:00	0	0	1	2	2	7	17	23	9	4	1	0	0	0	66	41-50	40
06:00	1	1	6	39	22	27	32	42	22	8	0	0	0	0	200	41-50	74
07:00	0	0	14	81	45	34	59	69	20	7	4	0	0	0	333	41-50	128
08:00	0	1	44	98	82	71	102	74	22	6	0	0	0	0	500	26-35	180
09:00	0	2	38	185	115	96	102	45	13	3	0	0	0	0	599	26-35	300
10:00	0	0	35	213	122	109	103	41	6	2	0	0	0	0	631	26-35	335
11:00	1	2	45	177	109	163	147	62	10	4	0	0	0	0	720	36-45	310
12 PM	4	5	50	205	169	172	110	37	6	2	0	0	0	0	760	26-35	374
13:00	0	3	52	232	215	141	113	36	6	1	0	0	0	0	799	26-35	447
14:00	2	11	53	247	190	162	115	40	5	0	1	0	0	0	826	26-35	437
15:00	1	4	54	233	250	207	157	49	15	0	0	1	0	0	971	26-35	483
16:00	3	1	58	252	323	262	156	61	5	1	0	0	0	0	1122	31-40	585
17:00	2	6	48	270	306	245	142	40	10	3	0	0	0	0	1072	26-35	576
18:00	0	4	77	340	241	149	49	5	2	0	0	0	0	0	867	26-35	581
19:00	0	3	63	212	116	132	80	18	2	0	0	0	0	0	626	26-35	328
20:00	0	0	27	122	67	105	100	37	4	0	0	0	0	0	462	36-45	205
21:00	0	0	15	92	51	38	53	27	11	0	0	0	0	0	287	26-35	143
22:00	0	1	2	3	10	37	71	39	10	1	0	0	0	0	174	41-50	110
23:00	0	0	1	1	4	26	30	27	10	0	0	0	1	0	100	41-50	57
Total	14	44	684	3006	2444	2198	1777	797	197	45	6	1	1	0	11214		
Percent	0.1%	0.4%	6.1%	26.8%	21.8%	19.6%	15.8%	7.1%	1.8%	0.4%	0.1%	0.0%	0.0%	0.0%			
AM Peak	06:00	09:00	11:00	10:00	10:00	11:00	11:00	08:00	06:00	06:00	07:00				11:00		
Vol.	1	2	45	213	122	163	147	74	22	8	4				720		
PM Peak	12:00	14:00	18:00	18:00	16:00	16:00	15:00	16:00	15:00	17:00	14:00	15:00	23:00		16:00		
Vol.	4	11	77	340	323	262	157	61	15	3	1	1	1		1122		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Westbound																	
Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
10/04/23	0	0	1	0	2	10	10	6	1	0	1	0	0	0	31	36-45	20
01:00	0	0	0	0	4	6	5	2	2	0	0	0	0	0	19	36-45	11
02:00	0	1	1	1	3	3	3	4	2	0	0	0	0	0	18	41-50	7
03:00	0	0	1	2	1	6	1	3	2	0	0	0	0	0	16	35-44	7
04:00	0	0	0	0	1	5	9	2	2	0	0	0	0	0	19	36-45	14
05:00	0	0	1	0	2	10	16	20	9	2	1	0	0	0	61	41-50	36
06:00	0	2	9	39	15	21	49	41	13	3	0	0	0	0	192	41-50	90
07:00	0	0	18	81	41	50	86	49	19	5	1	0	0	0	350	36-45	136
08:00	0	2	34	142	111	99	87	43	17	0	0	0	0	0	535	26-35	253
09:00	0	3	67	168	101	128	135	27	9	2	0	0	0	0	640	26-35	269
10:00	2	2	52	188	113	123	100	36	12	0	1	0	0	0	629	26-35	301
11:00	1	10	65	232	156	139	113	30	9	1	0	0	0	0	756	26-35	388
12 PM	1	2	50	251	174	147	122	34	4	0	0	0	0	0	785	26-35	425
13:00	1	2	49	197	181	177	119	34	6	2	0	0	0	0	768	26-35	378
14:00	2	5	55	248	180	179	100	22	8	1	0	0	0	0	800	26-35	428
15:00	3	5	53	231	210	199	129	49	10	3	0	0	0	0	892	26-35	441
16:00	0	2	64	254	283	248	173	39	10	3	0	0	0	0	1076	26-35	537
17:00	0	13	71	290	365	212	88	14	3	2	0	0	0	0	1058	26-35	655
18:00	2	5	73	343	234	122	57	13	3	0	1	0	0	0	853	26-35	577
19:00	0	0	53	195	136	131	72	14	0	0	0	0	0	0	601	26-35	331
20:00	0	2	41	126	100	106	74	18	2	2	0	0	0	0	471	26-35	226
21:00	1	0	18	66	36	41	56	27	7	1	0	0	0	0	253	26-35	102
22:00	0	0	0	1	9	52	60	44	9	2	0	0	0	0	177	36-45	112
23:00	0	0	1	1	9	21	34	23	11	1	0	0	0	0	101	41-50	57
Total	13	56	777	3056	2467	2235	1698	594	170	30	5	0	0	0	11101		
Percent	0.1%	0.5%	7.0%	27.5%	22.2%	20.1%	15.3%	5.4%	1.5%	0.3%	0.0%	0.0%	0.0%	0.0%			
AM Peak	10:00	11:00	09:00	11:00	11:00	11:00	09:00	07:00	07:00	07:00	00:00				11:00		
Vol.	2	10	67	232	156	139	135	49	19	5	1				756		
PM Peak	15:00	17:00	18:00	18:00	17:00	16:00	16:00	15:00	23:00	15:00	18:00				16:00		
Vol.	3	13	73	343	365	248	173	49	11	3	1				1076		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC Kensington, Connecticut 06037 (860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
10/05/23	0	0	1	0	3	16	20	4	4	1	0	0	0	0	49	36-45	36
01:00	0	0	0	1	2	7	5	2	0	0	0	0	0	0	17	36-45	12
02:00	0	0	0	0	4	3	4	3	0	0	0	0	0	0	14	31-40	7
03:00	0	0	0	0	1	3	1	2	2	0	0	0	0	0	9	46-55	4
04:00	0	0	1	0	4	5	5	3	1	0	0	0	0	0	19	34-43	10
05:00	0	0	0	0	3	12	19	12	4	0	1	0	0	0	51	36-45	31
06:00	0	1	22	27	28	36	47	22	14	2	0	0	0	0	199	36-45	83
07:00	0	2	33	67	43	67	71	53	12	4	0	0	0	0	352	36-45	138
08:00	2	1	32	112	93	83	97	51	16	0	0	1	0	0	488	26-35	205
09:00	0	2	55	167	102	108	94	31	3	1	0	0	0	0	563	26-35	269
10:00	0	2	43	181	152	108	78	30	1	1	0	0	0	0	596	26-35	333
11:00	1	5	79	259	184	119	72	7	1	1	0	0	0	0	728	26-35	443
12 PM	1	8	52	252	184	169	102	18	4	0	0	0	0	0	790	26-35	436
13:00	1	5	52	220	137	164	130	31	4	0	0	0	0	0	744	26-35	357
14:00	3	9	84	261	145	158	82	22	7	1	0	0	0	0	772	26-35	406
15:00	10	19	76	308	205	150	88	20	3	0	0	0	0	0	879	26-35	513
16:00	76	182	194	66	6	0	0	0	0	0	0	0	0	0	524	16-25	376
17:00	107	208	129	11	0	4	0	0	0	0	0	0	0	0	459	16-25	337
18:00	30	81	115	243	106	59	24	5	1	0	0	0	0	0	664	21-30	358
19:00	0	3	59	196	144	141	63	13	5	0	0	0	0	0	624	26-35	340
20:00	0	1	36	127	82	106	50	17	2	1	0	0	0	0	422	26-35	209
21:00	0	0	30	104	41	55	54	20	6	3	1	0	0	0	314	26-35	145
22:00	0	0	1	3	9	45	63	33	10	2	0	0	0	0	166	36-45	108
23:00	0	0	0	1	12	23	27	24	15	1	1	0	0	0	104	41-50	51
Total	231	529	1094	2606	1690	1641	1196	423	115	18	3	1	0	0	9547		
Percent	2.4%	5.5%	11.5%	27.3%	17.7%	17.2%	12.5%	4.4%	1.2%	0.2%	0.0%	0.0%	0.0%	0.0%			
AM Peak	08:00	11:00	11:00	11:00	11:00	11:00	08:00	07:00	08:00	07:00	05:00	08:00			11:00		
Vol.	2	5	79	259	184	119	97	53	16	4	1	1			728		
PM Peak	17:00	17:00	16:00	15:00	15:00	12:00	13:00	22:00	23:00	21:00	21:00				15:00		
Vol.	107	208	194	308	205	169	130	33	15	3	1				879		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
10/06/23	0	0	0	0	3	19	17	3	1	1	0	0	0	0	44	36-45	36
01:00	0	0	0	0	4	10	9	0	0	0	0	0	0	0	23	36-45	19
02:00	0	0	1	1	7	5	2	1	0	0	0	0	0	0	17	31-40	12
03:00	0	0	0	0	0	1	5	5	2	0	0	0	0	0	13	41-50	10
04:00	0	0	0	0	2	6	5	4	1	0	0	0	0	0	18	36-45	11
05:00	0	0	0	2	3	9	27	9	4	1	2	0	0	0	57	36-45	36
06:00	0	1	6	40	14	35	31	25	13	3	2	0	0	0	170	36-45	66
07:00	3	36	61	70	77	40	25	22	2	0	0	1	0	0	337	26-35	147
08:00	7	91	147	105	91	26	0	0	0	0	0	0	0	0	467	21-30	252
09:00	5	122	159	118	67	16	3	0	0	0	0	0	0	0	490	16-25	281
10:00	4	100	191	130	101	17	4	0	0	0	0	0	0	0	547	21-30	321
11:00	7	108	203	159	70	19	2	0	0	0	0	0	0	0	568	21-30	362
12 PM	2	105	264	166	59	16	1	0	0	0	0	0	0	0	613	21-30	430
13:00	4	101	269	147	71	6	1	0	0	0	0	0	0	0	599	21-30	416
14:00	14	104	273	189	90	14	2	0	0	0	0	0	0	0	686	21-30	462
15:00	7	102	262	218	108	20	1	0	0	0	0	0	0	0	718	21-30	480
16:00	11	126	327	238	85	14	2	0	0	0	0	0	0	0	803	21-30	565
17:00	12	153	301	164	73	14	1	0	0	0	0	0	0	0	718	21-30	465
18:00	7	127	270	103	30	8	1	0	0	0	0	0	0	0	546	16-25	397
19:00	4	105	171	148	67	14	1	0	0	0	0	0	0	0	510	21-30	319
20:00	0	56	126	113	89	18	3	0	0	0	0	0	0	0	405	21-30	239
21:00	0	47	82	112	78	25	2	0	0	0	0	0	0	0	346	21-30	194
22:00	0	3	4	68	101	42	6	0	0	0	0	0	0	0	224	26-35	169
23:00	0	2	4	44	71	27	5	1	0	0	0	0	0	0	154	26-35	115
Total	87	1489	3121	2335	1361	421	156	70	23	5	4	1	0	0	9073		
Percent	1.0%	16.4%	34.4%	25.7%	15.0%	4.6%	1.7%	0.8%	0.3%	0.1%	0.0%	0.0%	0.0%	0.0%			
AM Peak	08:00	09:00	11:00	11:00	10:00	07:00	06:00	06:00	06:00	06:00	05:00	07:00			11:00		
Vol.	7	122	203	159	101	40	31	25	13	3	2	1			568		
PM Peak	14:00	17:00	16:00	16:00	15:00	22:00	22:00	23:00							16:00		
Vol.	14	153	327	238	108	42	6	1							803		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
10/07/23	0	1	7	29	34	13	0	0	0	0	0	0	0	0	84	26-35	63
01:00	0	0	4	13	16	8	1	0	0	0	0	0	0	0	42	26-35	29
02:00	0	1	0	6	9	8	2	0	0	0	0	0	0	0	26	31-40	17
03:00	0	0	1	6	8	5	0	0	0	0	0	0	0	0	20	26-35	14
04:00	0	1	3	7	4	3	1	0	0	0	0	0	0	0	19	24-33	11
05:00	0	0	2	2	8	9	2	1	0	0	0	0	0	0	24	31-40	17
06:00	1	12	12	17	18	9	3	1	0	0	0	0	0	0	73	26-35	35
07:00	1	22	36	29	44	15	3	0	0	0	0	0	0	0	150	26-35	73
08:00	2	30	62	68	61	31	10	2	0	0	0	0	0	0	266	21-30	130
09:00	2	58	146	114	101	49	3	0	0	0	0	0	0	0	473	21-30	260
10:00	3	76	170	130	116	41	7	0	0	0	0	0	0	0	543	21-30	300
11:00	4	107	220	183	86	18	2	0	0	0	0	0	0	0	620	21-30	403
12 PM	10	138	290	166	68	6	1	0	0	0	0	0	0	0	679	21-30	456
13:00	6	137	277	150	93	18	1	0	0	0	0	0	0	0	682	21-30	427
14:00	7	102	265	183	110	29	2	0	0	0	0	0	0	0	698	21-30	448
15:00	4	89	232	198	112	36	3	0	0	0	0	0	0	0	674	21-30	430
16:00	3	94	269	182	114	41	5	0	0	0	0	0	0	0	708	21-30	451
17:00	0	72	222	169	105	26	14	5	0	0	0	0	0	0	613	21-30	391
18:00	0	74	183	184	112	28	4	0	0	0	0	0	0	0	585	21-30	367
19:00	3	66	148	117	46	15	1	2	0	0	0	0	0	0	398	21-30	265
20:00	2	57	102	96	77	19	13	0	0	0	0	0	0	0	366	21-30	198
21:00	3	47	85	77	51	18	1	0	0	0	0	0	0	0	282	21-30	162
22:00	0	0	9	77	89	26	7	0	0	0	0	0	0	0	208	26-35	166
23:00	0	2	2	37	64	46	8	1	2	0	0	0	0	0	162	31-40	110
Total	51	1186	2747	2240	1546	517	94	12	2	0	0	0	0	0	8395		
Percent	0.6%	14.1%	32.7%	26.7%	18.4%	6.2%	1.1%	0.1%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
AM Peak	11:00	11:00	11:00	11:00	10:00	09:00	08:00	08:00							11:00		
Vol.	4	107	220	183	116	49	10	2							620		
PM Peak	12:00	12:00	12:00	15:00	16:00	23:00	17:00	17:00	23:00						16:00		
Vol.	10	138	290	198	114	46	14	5	2						708		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
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Site Code:
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Latitude: 0' 0.0000 Undefined

Westbound																	
Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
	15	20	25	30	35	40	45	50	55	60	65	70	75	999			
10/08/23	0	1	4	21	38	17	1	2	0	0	0	0	0	0	84	26-35	59
01:00	0	0	2	11	18	10	1	1	0	0	0	0	0	0	43	26-35	29
02:00	0	0	0	7	7	5	1	0	0	0	0	0	0	0	20	26-35	14
03:00	0	0	0	5	8	1	0	0	0	0	0	0	0	0	14	26-35	13
04:00	0	0	2	3	6	1	0	0	0	0	0	0	0	0	12	26-35	9
05:00	0	0	3	3	6	5	1	0	0	0	0	0	0	0	18	30-39	11
06:00	1	10	15	9	15	6	6	1	0	0	0	0	0	0	63	16-25	25
07:00	0	17	30	29	26	15	4	1	1	0	1	0	0	0	124	21-30	59
08:00	0	34	84	43	50	31	10	0	0	0	0	0	0	0	252	21-30	127
09:00	2	33	130	80	103	40	8	4	0	0	0	0	0	0	400	21-30	210
10:00	1	45	175	131	146	70	17	1	0	0	0	0	0	0	586	21-30	306
11:00	1	82	205	190	139	61	6	0	1	0	0	0	0	0	685	21-30	395
12 PM	1	47	222	188	170	45	10	0	0	0	0	0	0	0	683	21-30	410
13:00	0	32	210	206	194	73	10	0	0	0	0	0	0	0	725	21-30	416
14:00	1	38	223	196	141	73	11	1	0	0	0	0	0	0	684	21-30	419
15:00	2	33	201	201	172	87	13	0	0	0	0	0	0	0	709	21-30	402
16:00	0	29	227	174	150	63	9	0	0	0	0	0	0	0	652	21-30	401
17:00	1	28	142	159	148	75	16	0	0	0	0	0	0	0	569	26-35	307
18:00	0	24	157	136	140	67	10	0	0	0	0	0	0	0	534	21-30	293
19:00	1	48	141	83	98	34	2	0	0	0	1	0	0	0	408	21-30	224
20:00	0	29	115	48	74	43	7	2	0	0	0	0	0	0	318	21-30	163
21:00	0	31	66	45	49	21	6	0	0	0	0	0	0	0	218	21-30	111
22:00	0	0	3	22	50	37	15	4	1	1	0	0	0	0	133	31-40	87
23:00	0	0	0	13	34	23	11	2	0	0	0	0	0	0	83	31-40	57
Total	11	561	2357	2003	1982	903	175	19	3	1	2	0	0	0	8017		
Percent	0.1%	7.0%	29.4%	25.0%	24.7%	11.3%	2.2%	0.2%	0.0%	0.0%	0.0%	0.0%	0.0%	0.0%			
AM Peak	09:00	11:00	11:00	11:00	10:00	10:00	10:00	09:00	07:00		07:00				11:00		
Vol.	2	82	205	190	146	70	17	4	1		1				685		
PM Peak	15:00	19:00	16:00	13:00	13:00	15:00	17:00	22:00	22:00	22:00	19:00				13:00		
Vol.	2	48	227	206	194	87	16	4	1	1	1				725		

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC Kensington, Connecticut 06037 (860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Westbound

Start Time	1	16	21	26	31	36	41	46	51	56	61	66	71	76	Total	Pace Speed	Number in Pace
10/09/23	1	1	0	4	12	17	3	0	2	0	0	0	0	0	40	31-40	29
01:00	0	1	1	4	11	8	1	1	0	0	0	0	0	0	27	31-40	19
02:00	0	0	0	3	2	2	0	1	0	0	0	0	0	0	8	26-35	5
03:00	0	0	0	1	5	1	0	0	0	0	0	0	0	0	7	26-35	6
04:00	0	1	0	6	5	4	6	0	0	0	0	0	0	0	22	26-35	11
05:00	0	0	0	4	10	9	5	3	1	0	0	0	0	0	32	31-40	19
06:00	0	4	16	20	36	36	14	6	0	0	0	0	0	0	132	31-40	72
07:00	1	15	55	35	50	51	32	5	3	0	0	0	0	0	247	31-40	101
08:00	0	21	92	50	85	78	26	1	0	0	0	0	0	0	353	31-40	163
09:00	4	33	145	123	108	64	9	4	0	0	0	0	0	0	490	21-30	268
10:00	1	38	178	151	109	67	6	3	0	0	0	0	0	0	553	21-30	329
11:00	3	19	116	80	80	18	1	0	0	0	0	0	0	0	317	21-30	196
12 PM	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
13:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
14:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
15:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
16:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
17:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
18:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
19:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
20:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
21:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
22:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
23:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
Total	10	133	603	481	513	355	103	24	6	0	0	0	0	0	2228		
Percent	0.4%	6.0%	27.1%	21.6%	23.0%	15.9%	4.6%	1.1%	0.3%	0.0%	0.0%	0.0%	0.0%	0.0%			
AM Peak	09:00	10:00	10:00	10:00	10:00	08:00	07:00	06:00	07:00						10:00		
Vol.	4	38	178	151	109	78	32	6	3						553		
PM Peak																	
Vol.	430	4010	11766	17695	14024	9665	6400	2373	600	113	20	4	1	0	67101		
Percent	0.6%	6.0%	17.5%	26.4%	20.9%	14.4%	9.5%	3.5%	0.9%	0.2%	0.0%	0.0%	0.0%	0.0%			

15th Percentile : 22 MPH
50th Percentile : 29 MPH
85th Percentile : 39 MPH
95th Percentile : 44 MPH

Stats
10 MPH Pace Speed : 26-35 MPH
Number in Pace : 31719
Percent in Pace : 47.3%
Number of Vehicles > 40 MPH : 9511
Percent of Vehicles > 40 MPH : 14.2%
Mean Speed(Average) : 31 MPH

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC

Kensington, Connecticut 06037

(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Start Time	02-Oct-23		Tue		Wed		Thu		Fri		Sat		Sun		Week Average	
	Eastbound	Westbou	Eastbound	Westbou	Eastbound	Westbou	Eastbound	Westbou	Eastbound	Westbou	Eastbound	Westbou	Eastbound	Westbou	Eastbound	Westbou
12:00 AM	*	*	29	32	28	31	22	49	30	44	62	84	42	84	36	54
01:00	*	*	12	24	12	19	14	17	22	23	22	42	30	43	19	28
02:00	*	*	16	8	23	18	16	14	16	17	26	26	19	20	19	17
03:00	*	*	17	11	19	16	18	9	25	13	13	20	18	14	18	14
04:00	*	*	58	24	55	19	56	19	50	18	32	19	24	12	46	18
05:00	*	*	239	66	225	61	244	51	193	57	70	24	48	18	170	46
06:00	*	*	602	200	535	192	559	199	479	170	143	73	111	63	405	150
07:00	*	*	1011	333	1001	350	898	352	730	337	304	150	196	124	690	274
08:00	*	*	1028	500	1040	535	979	488	645	467	444	266	305	252	740	418
09:00	*	*	828	599	790	640	770	563	550	490	541	473	435	400	652	528
10:00	*	*	847	631	766	629	755	596	615	547	655	543	626	586	711	589
11:00	*	*	663	720	746	756	714	728	598	568	688	620	663	685	679	680
12:00 PM	447	458	729	760	764	785	741	790	578	613	603	679	708	683	653	681
01:00	770	788	754	799	776	768	735	744	577	599	682	682	650	725	706	729
02:00	808	879	762	826	692	800	657	772	569	686	692	698	671	684	693	764
03:00	790	952	826	971	796	892	670	879	617	718	631	674	774	709	729	828
04:00	750	1116	781	1122	787	1076	382	524	566	803	648	708	669	652	655	857
05:00	709	1084	741	1072	710	1058	375	459	522	718	592	613	622	569	610	796
06:00	602	826	590	867	594	853	514	664	452	546	514	585	544	534	544	696
07:00	478	613	456	626	483	601	474	624	461	510	425	398	404	408	454	540
08:00	273	382	327	462	296	471	332	422	359	405	306	366	268	318	309	404
09:00	169	234	207	287	229	253	242	314	252	346	239	282	174	218	216	276
10:00	95	122	94	174	106	177	114	166	156	224	165	208	121	133	122	172
11:00	43	72	47	100	60	101	66	104	99	154	114	162	60	83	70	111
Lane	5934	7526	11664	11214	11533	11101	10347	9547	9161	9073	8611	8395	8182	8017	9946	9670
Day	13460		22878		22634		19894		18234		17006		16199		19616	
AM Peak	-	-	08:00	11:00	08:00	11:00	08:00	11:00	07:00	11:00	11:00	11:00	11:00	11:00	08:00	11:00
Vol.	-	-	1028	720	1040	756	979	728	730	568	688	620	663	685	740	680
PM Peak	14:00	16:00	15:00	16:00	15:00	16:00	12:00	15:00	15:00	16:00	14:00	16:00	15:00	13:00	15:00	16:00
Vol.	808	1116	826	1122	796	1076	741	879	617	803	692	708	774	725	729	857

Route 44 West of Secret Lake Road
Canton, Connecticut

Connecticut Counts LLC
Kensington, Connecticut 06037
(860) 828-1693

Site Code:
Station ID: 5929

Latitude: 0' 0.0000 Undefined

Start Time	09-Oct-23		Tue		Wed		Thu		Fri		Sat		Sun		Week Average	
	Eastbound	Westbou	Eastboun	Westbou	Eastboun	Westbou	Eastboun	Westbou	Eastboun	Westbou	Eastboun	Westbou	Eastboun	Westbou	Eastboun	Westbou
12:00 AM	40	40	*	*	*	*	*	*	*	*	*	*	*	*	40	40
01:00	19	27	*	*	*	*	*	*	*	*	*	*	*	*	19	27
02:00	16	8	*	*	*	*	*	*	*	*	*	*	*	*	16	8
03:00	21	7	*	*	*	*	*	*	*	*	*	*	*	*	21	7
04:00	43	22	*	*	*	*	*	*	*	*	*	*	*	*	43	22
05:00	199	32	*	*	*	*	*	*	*	*	*	*	*	*	199	32
06:00	394	132	*	*	*	*	*	*	*	*	*	*	*	*	394	132
07:00	636	247	*	*	*	*	*	*	*	*	*	*	*	*	636	247
08:00	752	353	*	*	*	*	*	*	*	*	*	*	*	*	752	353
09:00	689	490	*	*	*	*	*	*	*	*	*	*	*	*	689	490
10:00	642	553	*	*	*	*	*	*	*	*	*	*	*	*	642	553
11:00	335	317	*	*	*	*	*	*	*	*	*	*	*	*	335	317
12:00 PM	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
01:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
02:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
03:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
04:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
05:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
06:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
07:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
08:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
09:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
10:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
11:00	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*	*
Lane Day	3786	2228	0	0	0	0	0	0	0	0	0	0	0	0	3786	2228
AM Peak Vol.	08:00	10:00	-	-	-	-	-	-	-	-	-	-	-	-	08:00	10:00
PM Peak Vol.	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-

Comb. Total	19474	22878	22634	19894	18234	17006	16199	25630
ADT	ADT 20,910	AADT 20,910						

C Crash Data

2018 - June 2023 Crash Data: Route 44 Corridor Study

CrashId	Town Name	Date of Crash	Time of Crash	Crash Severity	Manner of Crash / Collision Impact	Weather Condition	Light Condition	Road Surface Condition
374827	Canton	1/6/2018	10:07:00	Property Damage Only	Angle	Clear	Daylight	Dry
377044	Canton	1/3/2018	9:51:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
377469	Canton	1/11/2018	17:08:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Wet
379043	Canton	1/10/2018	8:09:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
379839	Canton	1/11/2018	17:50:00	Property Damage Only	Sideswipe, same direction	Clear	Dark-Lighted	Dry
379840	Canton	1/13/2018	12:47:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
379841	Canton	1/15/2018	17:34:00	Property Damage Only	Front to rear	Clear	Dusk	Dry
382248	Canton	1/23/2018	7:50:00	Property Damage Only	Front to rear	Clear	Other	Wet
382249	Canton	1/23/2018	8:46:00	Property Damage Only	Front to rear	Rain	Daylight	Wet
382252	Canton	1/24/2018	14:50:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
382253	Canton	1/25/2018	11:32:00	Property Damage Only	Angle	Clear	Daylight	Dry
389573	Canton	2/7/2018	8:28:00	Property Damage Only	Front to rear	Clear	Daylight	Slush
389574	Canton	2/9/2018	17:43:00	Property Damage Only	Front to rear	Snow	Dusk	Wet
391691	Canton	2/14/2018	11:50:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
392367	Canton	2/17/2018	21:29:00	Property Damage Only	Front to front	Snow	Dark-Lighted	Snow
392960	Canton	2/20/2018	14:23:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
393350	Canton	2/20/2018	14:15:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
393416	Canton	1/19/2018	19:33:00	Injury of any type (Serious, Minor, Possible)	Not Applicable	Clear	Dark-Lighted	Dry
393661	Canton	2/24/2018	23:25:00	Property Damage Only	Not Applicable	Cloudy	Dark-Not Lighted	Wet
395589	Canton	2/26/2018	6:36:00	Property Damage Only	Front to rear	Cloudy	Daylight	Wet
396586	Canton	2/21/2018	13:19:00	Property Damage Only	Angle	Clear	Daylight	Dry
397370	Canton	3/9/2018	9:20:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
401118	Canton	3/16/2018	15:34:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
402746	Canton	3/27/2018	15:38:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
404927	Canton	3/31/2018	11:49:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
404929	Canton	4/3/2018	19:28:00	Property Damage Only	Front to rear	Rain	Dark-Lighted	Wet
417775	Canton	5/15/2018	15:30:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
417806	Canton	5/9/2018	16:31:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
420832	Canton	5/25/2018	14:53:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
420833	Canton	5/26/2018	13:18:00	Property Damage Only	Angle	Clear	Daylight	Dry
421793	Canton	5/21/2018	16:43:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
421795	Canton	5/24/2018	21:06:00	Property Damage Only	Angle	Clear	Dark-Lighted	Dry
421796	Canton	5/27/2018	12:36:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
425377	Canton	6/5/2018	7:55:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
425699	Canton	6/5/2018	10:25:00	Property Damage Only	Rear to side	Clear	Daylight	Dry
427547	Canton	5/14/2018	11:57:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
429039	Canton	6/24/2018	20:38:00	Property Damage Only	Not Applicable	Cloudy	Dusk	Dry
429785	Canton	6/25/2018	5:57:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
431930	Canton	5/23/2018	18:55:00	Property Damage Only	Not Applicable	Clear	Daylight	Dry
431931	Canton	6/29/2018	15:26:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
432667	Canton	7/3/2018	18:21:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
436321	Canton	7/12/2018	16:48:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
438051	Canton	7/14/2018	11:39:00	Property Damage Only	Angle	Clear	Daylight	Dry
438331	Canton	7/20/2018	20:31:00	Property Damage Only	Angle	Clear	Dusk	Dry
439157	Canton	7/5/2018	12:45:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
439470	Canton	7/12/2018	16:14:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
442314	Canton	7/18/2018	16:26:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
442315	Canton	7/30/2018	11:07:00	Property Damage Only	Other	Clear	Daylight	Dry
443379	Canton	8/4/2018	22:20:00	Property Damage Only	Sideswipe, same direction	Clear	Dark-Lighted	Dry
443553	Canton	8/2/2018	9:37:00	Property Damage Only	Rear to side	Clear	Daylight	Dry
443556	Canton	8/6/2018	14:16:00	Property Damage Only	Sideswipe, opposite direction	Clear	Daylight	Dry
446328	Canton	8/13/2018	13:43:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
446329	Canton	8/13/2018	15:25:00	Property Damage Only	Front to rear	Rain	Daylight	Wet
446330	Canton	8/18/2018	17:29:00	Property Damage Only	Angle	Cloudy	Daylight	Wet
463846	Canton	8/14/2018	15:08:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry

CrashId	Town Name	Date of Crash	Time of Crash	Crash Severity	Manner of Crash / Collision Impact	Weather Condition	Light Condition	Road Surface Condition
463847	Canton	8/16/2018	14:17:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
463854	Canton	8/26/2018	16:04:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
463855	Canton	8/28/2018	8:20:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
463856	Canton	8/30/2018	13:34:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
463857	Canton	9/3/2018	15:41:00	Property Damage Only	Rear to rear	Clear	Daylight	Dry
463860	Canton	9/11/2018	10:16:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
463864	Canton	9/14/2018	12:01:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
463865	Canton	9/14/2018	14:57:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
463866	Canton	9/17/2018	9:07:00	Property Damage Only	Not Applicable	Clear	Daylight	Dry
463867	Canton	9/20/2018	10:10:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
463872	Canton	9/23/2018	16:27:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
463873	Canton	9/27/2018	14:38:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
463875	Canton	9/27/2018	19:23:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
463876	Canton	9/27/2018	21:14:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Dark-Lighted	Dry
465932	Canton	9/29/2018	13:08:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
465974	Canton	9/25/2018	18:20:00	Property Damage Only	Front to rear	Rain	Dark-Lighted	Wet
466139	Canton	10/5/2018	17:07:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
466140	Canton	10/13/2018	14:36:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
467554	Canton	9/26/2018	10:54:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
468896	Canton	10/19/2018	11:17:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
469429	Canton	10/19/2018	15:45:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
472777	Canton	9/28/2018	13:35:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
472778	Canton	10/26/2018	16:50:00	Property Damage Only	Angle	Clear	Daylight	Dry
474824	Canton	9/27/2018	16:35:00	Property Damage Only	Sideswipe, opposite direction	Clear	Daylight	Dry
476304	Canton	11/8/2018	18:14:00	Property Damage Only	Sideswipe, same direction	Clear	Dark-Not Lighted	Dry
479691	Canton	11/6/2018	17:37:00	Property Damage Only	Angle	Rain	Dark-Lighted	Wet
479692	Canton	11/10/2018	15:01:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
482556	Canton	10/25/2018	16:20:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
484985	Canton	11/20/2018	16:23:00	Property Damage Only	Front to rear	Clear	Dusk	Dry
484987	Canton	11/21/2018	17:16:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Dark-Not Lighted	Dry
485249	Canton	11/24/2018	10:32:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
488625	Canton	12/5/2018	16:03:00	Property Damage Only	Sideswipe, opposite direction	Clear	Daylight	Dry
493240	Canton	12/7/2018	19:16:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
494561	Canton	11/27/2018	7:45:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
494873	Canton	12/14/2018	13:40:00	Property Damage Only	Front to front	Clear	Daylight	Dry
494874	Canton	12/14/2018	15:28:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
497610	Canton	12/18/2018	12:55:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
497611	Canton	12/18/2018	17:50:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
497613	Canton	12/26/2018	13:24:00	Property Damage Only	Angle	Clear	Daylight	Dry
498218	Canton	12/31/2018	11:19:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
499917	Canton	12/29/2018	9:41:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
499918	Canton	1/4/2019	10:49:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
500564	Canton	12/26/2018	17:11:00	Fatal (Kill)	Not Applicable	Clear	Dark-Lighted	Dry
503596	Canton	1/15/2019	07:29:00	Property Damage Only	Angle	Clear	Daylight	Dry
508621	Canton	1/29/2019	15:54:00	Property Damage Only	Front to rear	Snow	Daylight	Wet
508622	Canton	1/29/2019	17:11:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Snow	Dark-Lighted	Snow
511512	Canton	1/21/2019	07:09:00	Property Damage Only	Front to rear	Clear	Daylight	Ice / Frost
511879	Canton	2/3/2019	11:15:00	Property Damage Only	Other	Clear	Daylight	Wet
512308	Canton	1/29/2019	08:13:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
512309	Canton	2/4/2019	08:44:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Wet
514143	Canton	1/31/2019	18:29:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
516556	Canton	2/19/2019	14:26:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
517049	Canton	2/20/2019	15:13:00	Property Damage Only	Angle	Clear	Daylight	Dry
519855	Canton	1/24/2019	11:46:00	Property Damage Only	Angle	Rain	Daylight	Wet
526097	Canton	3/25/2019	19:48:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
526782	Canton	3/11/2019	16:54:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
530778	Canton	4/3/2019	07:57:00	Property Damage Only	Front to rear	Clear	Daylight	Dry

CrashId	Town Name	Date of Crash	Time of Crash	Crash Severity	Manner of Crash / Collision Impact	Weather Condition	Light Condition	Road Surface Condition
532094	Canton	4/2/2019	11:38:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
534290	Canton	4/8/2019	14:48:00	Property Damage Only	Angle	Clear	Daylight	Dry
534914	Canton	4/9/2019	12:25:00	Property Damage Only	Other	Clear	Daylight	Dry
536938	Canton	4/16/2019	16:30:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
537385	Canton	4/20/2019	11:19:00	Property Damage Only	Front to rear	Rain	Daylight	Wet
541169	Canton	5/6/2019	07:20:00	Property Damage Only	Angle	Clear	Daylight	Dry
541170	Canton	5/6/2019	07:54:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
542106	Canton	5/2/2019	12:19:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
542662	Canton	4/26/2019	09:54:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
543055	Canton	5/7/2019	08:18:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
546396	Canton	5/17/2019	13:48:00	Property Damage Only	Angle	Rain	Daylight	Wet
548837	Canton	5/26/2019	00:39:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
550285	Canton	5/18/2019	22:27:00	Property Damage Only	Sideswipe, same direction	Clear	Dark-Lighted	Dry
552470	Canton	12/21/2018	19:37:00	Property Damage Only	Sideswipe, same direction	Rain	Dark-Lighted	Wet
553898	Canton	5/29/2019	16:55:00	Property Damage Only	Front to rear	Cloudy	Daylight	Dry
554041	Canton	6/7/2019	12:26:00	Property Damage Only	Rear to side	Clear	Daylight	Dry
554042	Canton	6/9/2019	15:16:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
554054	Canton	6/4/2019	11:40:00	Property Damage Only	Not Applicable	Clear	Daylight	Dry
554704	Canton	6/11/2019	16:38:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
554721	Canton	6/11/2019	14:53:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
556389	Canton	6/12/2019	16:15:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
557086	Canton	6/18/2019	13:34:00	Property Damage Only	Front to rear	Rain	Daylight	Wet
557570	Canton	6/14/2019	14:26:00	Property Damage Only	Angle	Clear	Daylight	Dry
563138	Canton	7/6/2019	13:51:00	Property Damage Only	Sideswipe, opposite direction	Clear	Daylight	Dry
565069	Canton	7/10/2019	06:41:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
568088	Canton	7/15/2019	13:11:00	Property Damage Only	Not Applicable	Clear	Daylight	Dry
568156	Canton	7/15/2019	18:57:00	Property Damage Only	Other	Clear	Daylight	Dry
569197	Canton	7/24/2019	15:32:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
570686	Canton	7/24/2019	17:21:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
571630	Canton	7/24/2019	11:05:00	Property Damage Only	Angle	Clear	Daylight	Dry
571631	Canton	7/30/2019	11:33:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
572271	Canton	7/29/2019	18:58:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
572349	Canton	7/30/2019	13:33:00	Property Damage Only	Angle	Clear	Daylight	Dry
572983	Canton	8/5/2019	12:58:00	Property Damage Only	Angle	Clear	Daylight	Dry
573538	Canton	8/1/2019	16:47:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
573631	Canton	8/11/2019	11:14:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
575539	Canton	8/4/2019	15:19:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
575540	Canton	8/13/2019	08:20:00	Property Damage Only	Angle	Clear	Daylight	Dry
575981	Canton	8/16/2019	11:01:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
576151	Canton	8/18/2019	10:04:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
576210	Canton	8/8/2019	18:39:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
576870	Canton	8/20/2019	08:31:00	Property Damage Only	Sideswipe, opposite direction	Clear	Daylight	Dry
576871	Canton	8/20/2019	11:55:00	Injury of any type (Serious, Minor, Possible)	Sideswipe, same direction	Clear	Daylight	Dry
577244	Canton	8/19/2019	13:26:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
578637	Canton	8/13/2019	19:26:00	Property Damage Only	Other	Clear	Dusk	Dry
579104	Canton	8/22/2019	11:38:00	Property Damage Only	Angle	Clear	Daylight	Dry
580562	Canton	8/27/2019	08:39:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
581875	Canton	8/30/2019	21:30:00	Property Damage Only	Angle	Clear	Dark-Not Lighted	Dry
582392	Canton	9/5/2019	06:54:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
582941	Canton	8/26/2019	17:20:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
582942	Canton	9/3/2019	16:14:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
584964	Canton	9/17/2019	13:18:00	Property Damage Only	Not Applicable	Clear	Daylight	Dry
588720	Canton	9/26/2019	16:53:00	Property Damage Only	Other	Clear	Daylight	Wet
590208	Canton	9/19/2019	16:57:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
590858	Canton	10/2/2019	11:00:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
590881	Canton	9/30/2019	09:33:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
592127	Canton	9/30/2019	18:02:00	Property Damage Only	Angle	Clear	Daylight	Dry

CrashId	Town Name	Date of Crash	Time of Crash	Crash Severity	Manner of Crash / Collision Impact	Weather Condition	Light Condition	Road Surface Condition
593801	Canton	9/30/2019	17:24:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
593803	Canton	10/7/2019	18:46:00	Property Damage Only	Rear to side	Rain	Dark-Lighted	Wet
607038	Canton	10/27/2019	13:46:00	Property Damage Only	Front to rear	Rain	Daylight	Wet
607554	Canton	11/2/2019	17:12:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
608095	Canton	10/21/2019	16:58:00	Property Damage Only	Angle	Clear	Daylight	Dry
608672	Canton	11/4/2019	10:10:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
610028	Canton	11/2/2019	01:45:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Dark-Lighted	Dry
610029	Canton	11/8/2019	11:35:00	Property Damage Only	Front to front	Clear	Daylight	Dry
613041	Canton	11/12/2019	15:50:00	Property Damage Only	Not Applicable	Clear	Daylight	Dry
615404	Canton	11/20/2019	17:17:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
619445	Canton	11/23/2019	20:20:00	Property Damage Only	Not Applicable	Clear	Dark-Lighted	Dry
620345	Canton	11/27/2019	16:32:00	Property Damage Only	Angle	Rain	Dark-Not Lighted	Wet
622103	Canton	12/4/2019	17:47:00	Property Damage Only	Angle	Clear	Dark-Lighted	Wet
624736	Canton	12/4/2019	13:03:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
624737	Canton	12/14/2019	08:02:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Rain	Daylight	Wet
627346	Canton	12/19/2019	10:03:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
628222	Canton	12/12/2019	16:47:00	Property Damage Only	Angle	Clear	Dark-Not Lighted	Dry
630375	Canton	12/23/2019	19:30:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Dark-Lighted	Wet
630649	Canton	12/23/2019	12:59:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
631063	Canton	12/6/2019	11:51:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Ice / Frost
631064	Canton	12/23/2019	17:15:00	Property Damage Only	Angle	Clear	Dark-Lighted	Dry
631065	Canton	12/24/2019	13:22:00	Property Damage Only	Angle	Clear	Daylight	Dry
631767	Canton	12/29/2019	16:51:00	Injury of any type (Serious, Minor, Possible)	Other	Rain	Dark-Lighted	Wet
631986	Canton	12/27/2019	12:06:00	Property Damage Only	Angle	Clear	Daylight	Ice / Frost
633280	Canton	1/1/2020	15:17:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
635891	Canton	1/6/2020	14:52:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
635893	Canton	1/8/2020	16:01:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
636723	Canton	1/10/2020	14:46:00	Property Damage Only	Angle	Clear	Daylight	Dry
639004	Canton	1/15/2020	14:30:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
641626	Canton	1/15/2020	13:22:00	Property Damage Only	Angle	Clear	Daylight	Dry
642438	Canton	1/20/2020	13:21:00	Injury of any type (Serious, Minor, Possible)	Sideswipe, same direction	Clear	Daylight	Dry
644981	Canton	1/22/2020	15:45:00	Property Damage Only	Rear to rear	Clear	Daylight	Dry
648027	Canton	2/5/2020	17:37:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
651742	Canton	2/5/2020	07:13:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
652758	Canton	2/17/2020	15:13:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
655381	Canton	2/21/2020	21:29:00	Property Damage Only	Front to rear	Clear	Dark-Not Lighted	Dry
656124	Canton	2/6/2020	12:49:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
656899	Canton	2/25/2020	14:38:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
662083	Canton	2/14/2020	21:12:00	Property Damage Only	Sideswipe, opposite direction	Clear	Dark-Not Lighted	Dry
662919	Canton	3/5/2020	13:15:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
664485	Canton	3/7/2020	14:00:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
665092	Canton	3/2/2020	15:28:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
667921	Canton	3/21/2020	00:40:00	Property Damage Only	Not Applicable	Clear	Dark-Lighted	Dry
668859	Canton	3/21/2020	11:35:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
678398	Canton	5/7/2020	15:05:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
681377	Canton	5/18/2020	14:24:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
687851	Canton	5/27/2020	16:14:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
688555	Canton	6/3/2020	13:41:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
690532	Canton	6/21/2020	15:14:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
694925	Canton	6/27/2020	15:14:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Rain	Daylight	Wet
706889	Canton	7/24/2020	11:36:00	Property Damage Only	Angle	Clear	Daylight	Dry
706890	Canton	7/29/2020	17:25:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
706892	Canton	7/31/2020	21:05:00	Property Damage Only	Not Applicable	Clear	Dark-Lighted	Dry
709074	Canton	7/30/2020	09:56:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
713543	Canton	8/24/2020	15:22:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
714359	Canton	7/24/2020	16:18:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
717726	Canton	8/24/2020	15:12:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry

CrashId	Town Name	Date of Crash	Time of Crash	Crash Severity	Manner of Crash / Collision Impact	Weather Condition	Light Condition	Road Surface Condition
718465	Canton	9/6/2020	14:35:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
718966	Canton	9/3/2020	21:27:00	Property Damage Only	Not Applicable	Clear	Dark-Lighted	Dry
723926	Canton	9/18/2020	08:34:00	Property Damage Only	Angle	Clear	Daylight	Dry
724237	Canton	9/18/2020	19:18:00	Property Damage Only	Angle	Clear	Daylight	Dry
727718	Canton	9/23/2020	13:09:00	Property Damage Only	Not Applicable	Clear	Daylight	Dry
728803	Canton	9/23/2020	14:48:00	Property Damage Only	Front to front	Clear	Daylight	Dry
729897	Canton	10/1/2020	20:16:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Dark-Unknown Lighting	Dry
729898	Canton	10/2/2020	14:52:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
729900	Canton	10/6/2020	18:42:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Dark-Lighted	Dry
733503	Canton	10/23/2020	08:01:00	Property Damage Only	Angle	Clear	Daylight	Dry
734625	Canton	10/27/2020	07:09:00	Property Damage Only	Front to rear	Clear	Dawn	Dry
737936	Canton	10/30/2020	18:57:00	Property Damage Only	Front to rear	Cloudy	Dark-Lighted	Wet
737937	Canton	10/30/2020	19:55:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Cloudy	Dark-Lighted	Wet
752997	Canton	10/31/2020	16:21:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
753392	Canton	11/5/2020	09:58:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
753651	Canton	10/31/2020	18:01:00	Injury of any type (Serious, Minor, Possible)	Sideswipe, opposite direction	Clear	Daylight	Dry
756133	Canton	11/9/2020	10:36:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
758863	Canton	11/15/2020	20:09:00	Property Damage Only	Not Applicable	Rain	Dark-Lighted	Wet
761803	Canton	11/18/2020	11:05:00	Property Damage Only	Not Applicable	Severe Crosswinds	Daylight	Dry
762196	Canton	11/16/2020	21:34:00	Injury of any type (Serious, Minor, Possible)	Not Applicable	Clear	Dark-Not Lighted	Dry
762197	Canton	11/18/2020	08:16:00	Property Damage Only	Sideswipe, opposite direction	Clear	Daylight	Dry
764506	Canton	11/29/2020	17:21:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
770407	Canton	12/15/2020	17:31:00	Property Damage Only	Sideswipe, same direction	Clear	Dark-Lighted	Dry
774821	Canton	12/16/2020	17:14:00	Injury of any type (Serious, Minor, Possible)	Not Applicable	Clear	Dark-Not Lighted	Dry
775174	Canton	12/21/2020	11:01:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
775257	Canton	12/22/2020	15:26:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
776506	Canton	12/31/2020	11:57:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
776507	Canton	1/1/2021	14:43:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
777989	Canton	1/3/2021	12:26:00	Property Damage Only	Front to rear	Cloudy	Daylight	Dry
778368	Canton	1/6/2021	12:16:00	Property Damage Only	Front to front	Clear	Daylight	Dry
783808	Canton	1/27/2021	18:07:00	Property Damage Only	Angle	Clear	Dark-Lighted	Dry
785582	Canton	2/3/2021	15:58:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
786271	Canton	2/6/2021	16:56:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
793018	Canton	3/1/2021	14:02:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
793291	Canton	3/5/2021	14:14:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
795542	Canton	2/18/2021	16:48:00	Property Damage Only	Not Applicable	Clear	Daylight	Wet
796916	Canton	3/8/2021	08:58:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
798547	Canton	3/16/2021	15:03:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
799374	Canton	3/18/2021	15:14:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
799377	Canton	3/26/2021	14:53:00	Property Damage Only	Other	Clear	Daylight	Dry
799378	Canton	3/27/2021	08:26:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
800444	Canton	4/2/2021	14:30:00	Property Damage Only	Angle	Clear	Daylight	Dry
800445	Canton	4/9/2021	08:22:00	Injury of any type (Serious, Minor, Possible)	Sideswipe, same direction	Clear	Daylight	Dry
800991	Canton	4/8/2021	11:31:00	Injury of any type (Serious, Minor, Possible)	Unknown	Clear	Daylight	Dry
806193	Canton	4/16/2021	16:24:00	Property Damage Only	Front to rear	Cloudy	Daylight	Wet
807054	Canton	4/25/2021	14:46:00	Property Damage Only	Front to rear	Cloudy	Daylight	Dry
809143	Canton	4/29/2021	13:48:00	Property Damage Only	Front to rear	Cloudy	Daylight	Wet
809145	Canton	4/30/2021	15:33:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
809263	Canton	5/4/2021	11:43:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
812814	Canton	5/14/2021	16:38:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
814341	Canton	5/13/2021	14:36:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
815108	Canton	5/22/2021	12:18:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
815109	Canton	5/22/2021	13:02:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
816141	Canton	5/26/2021	11:56:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
816142	Canton	5/26/2021	13:21:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
817325	Canton	5/19/2021	15:35:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
817807	Canton	6/3/2021	23:21:00	Property Damage Only	Not Applicable	Clear	Dark-Lighted	Dry

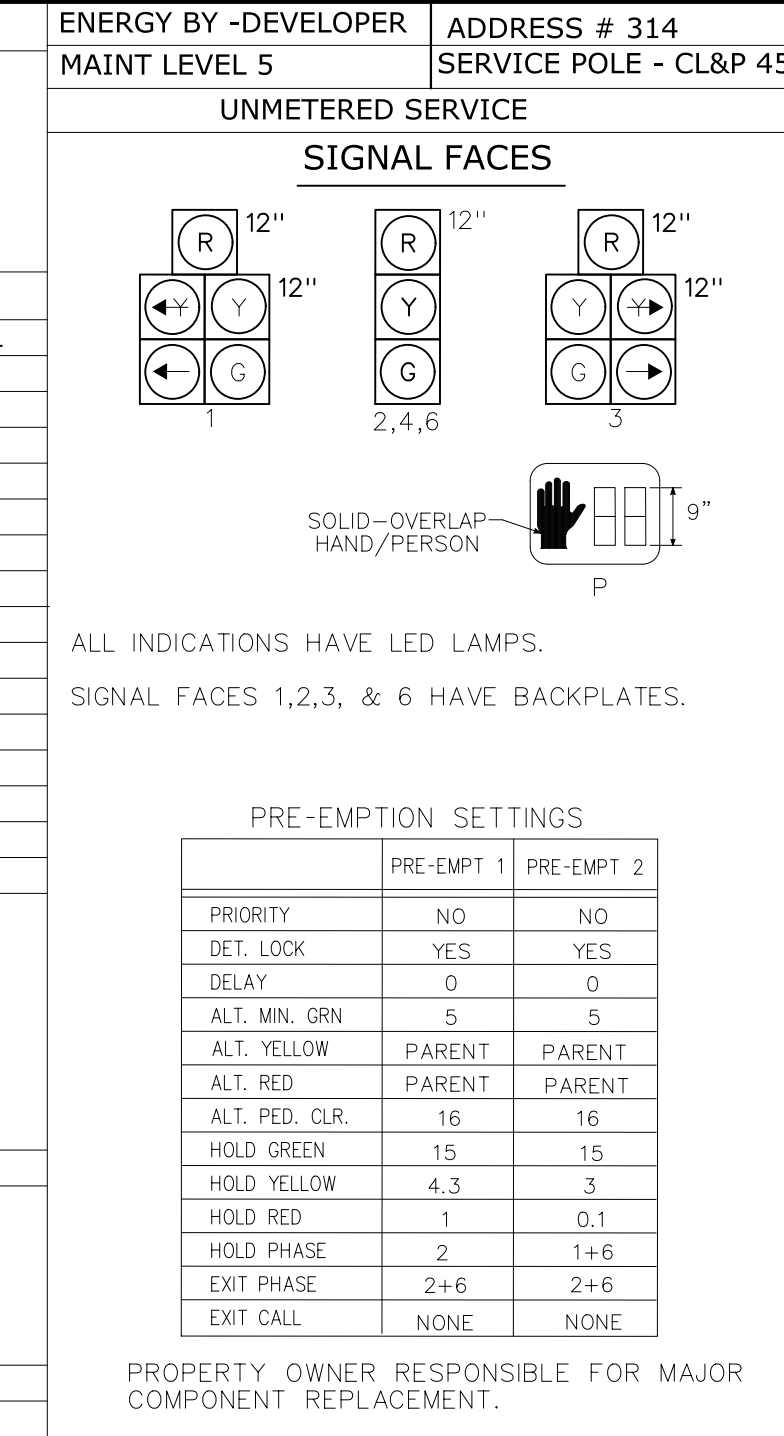
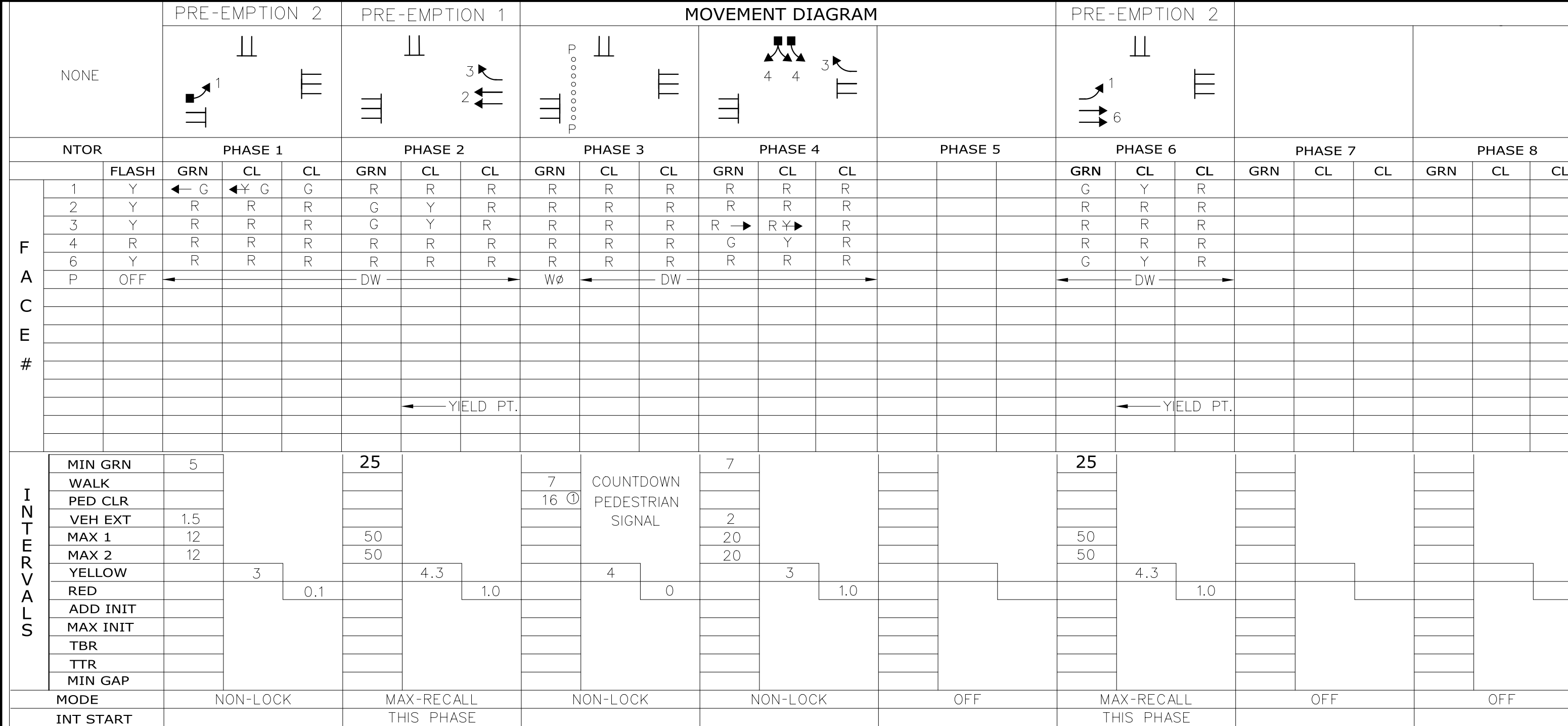
CrashId	Town Name	Date of Crash	Time of Crash	Crash Severity	Manner of Crash / Collision Impact	Weather Condition	Light Condition	Road Surface Condition
818469	Canton	6/2/2021	09:23:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
819185	Canton	6/11/2021	11:56:00	Property Damage Only	Front to front	Clear	Daylight	Dry
819186	Canton	6/11/2021	12:19:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
820790	Canton	6/16/2021	06:06:00	Property Damage Only	Not Applicable	Clear	Daylight	Dry
821112	Canton	6/19/2021	12:48:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
821887	Canton	6/14/2021	14:03:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
822537	Canton	6/23/2021	17:13:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
826619	Canton	6/25/2021	13:08:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
827308	Canton	7/1/2021	12:55:00	Property Damage Only	Rear to side	Rain	Daylight	Wet
827941	Canton	7/9/2021	13:10:00	Property Damage Only	Not Applicable	Clear	Daylight	Wet
829468	Canton	7/9/2021	16:53:00	Property Damage Only	Front to rear	Clear	Daylight	Wet
832282	Canton	7/22/2021	14:34:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
835188	Canton	7/29/2021	16:36:00	Property Damage Only	Front to rear	Rain	Daylight	Wet
835717	Canton	7/23/2021	17:11:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
837812	Canton	7/31/2021	18:13:00	Property Damage Only	Not Applicable	Clear	Daylight	Dry
837814	Canton	8/13/2021	16:21:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
840017	Canton	7/27/2021	06:54:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
841605	Canton	8/19/2021	06:21:00	Property Damage Only	Not Applicable	Rain	Daylight	Wet
841606	Canton	8/27/2021	17:49:00	Property Damage Only	Front to rear	Cloudy	Daylight	Dry
843548	Canton	8/25/2021	09:04:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
844406	Canton	9/5/2021	16:30:00	Property Damage Only	Not Applicable	Rain	Daylight	Wet
846371	Canton	9/7/2021	12:56:00	Injury of any type (Serious, Minor, Possible)	Front to front	Clear	Daylight	Dry
847911	Canton	9/14/2021	06:14:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Dawn	Dry
848905	Canton	9/21/2021	14:56:00	Property Damage Only	Angle	Clear	Daylight	Dry
851642	Canton	9/23/2021	14:25:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
852882	Canton	10/1/2021	15:40:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
854459	Canton	10/5/2021	14:35:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
861396	Canton	10/16/2021	13:57:00	Property Damage Only	Angle	Clear	Daylight	Dry
863146	Canton	10/25/2021	16:44:00	Property Damage Only	Front to front	Rain	Daylight	Wet
863609	Canton	10/26/2021	14:30:00	Property Damage Only	Front to rear	Rain	Daylight	Wet
863806	Canton	10/22/2021	12:40:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
864195	Canton	10/22/2021	15:50:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
864440	Canton	10/29/2021	11:19:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
865744	Canton	10/31/2021	19:14:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
867625	Canton	11/4/2021	16:50:00	Property Damage Only	Angle	Clear	Daylight	Dry
867989	Canton	11/1/2021	18:25:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
868313	Canton	11/9/2021	14:03:00	Injury of any type (Serious, Minor, Possible)	Sideswipe, opposite direction	Clear	Daylight	Dry
871390	Canton	11/13/2021	14:16:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
872392	Canton	11/21/2021	14:39:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
873271	Canton	11/20/2021	12:08:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
873345	Canton	11/17/2021	17:36:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
875747	Canton	11/22/2021	12:33:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
877511	Canton	12/3/2021	18:00:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
880524	Canton	11/27/2021	18:11:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
881851	Canton	12/7/2021	12:27:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
883755	Canton	12/20/2021	14:23:00	Property Damage Only	Angle	Clear	Daylight	Dry
885005	Canton	12/23/2021	19:08:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
890298	Canton	12/27/2021	22:35:00	Property Damage Only	Not Applicable	Snow	Dark-Not Lighted	Snow
893112	Canton	1/2/2022	17:58:00	Property Damage Only	Sideswipe, same direction	Clear	Dark-Lighted	Dry
895521	Canton	1/15/2022	13:01:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
897621	Canton	1/19/2022	17:50:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Dark-Lighted	Wet
898573	Canton	1/19/2022	11:55:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
899066	Canton	1/12/2022	16:28:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
900156	Canton	1/24/2022	13:09:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
900157	Canton	1/26/2022	11:34:00	Property Damage Only	Front to front	Clear	Daylight	Dry
907583	Canton	2/3/2022	18:36:00	Property Damage Only	Front to rear	Rain	Dark-Not Lighted	Wet
908736	Canton	2/15/2022	18:00:00	Property Damage Only	Angle	Clear	Dark-Not Lighted	Dry

CrashId	Town Name	Date of Crash	Time of Crash	Crash Severity	Manner of Crash / Collision Impact	Weather Condition	Light Condition	Road Surface Condition
911211	Canton	2/18/2022	11:23:00	Property Damage Only	Other	Clear	Daylight	Dry
911213	Canton	2/19/2022	17:42:00	Property Damage Only	Not Applicable	Snow	Dark-Lighted	Ice / Frost
911375	Canton	2/18/2022	21:57:00	Property Damage Only	Not Applicable	Clear	Dark-Lighted	Dry
914231	Canton	2/28/2022	07:15:00	Property Damage Only	Front to front	Clear	Daylight	Dry
919349	Canton	3/11/2022	16:16:00	Property Damage Only	Other	Clear	Daylight	Dry
919884	Canton	3/22/2022	10:44:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
920068	Canton	3/4/2022	15:17:00	Property Damage Only	Angle	Clear	Daylight	Dry
921588	Canton	3/30/2022	18:04:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
922109	Canton	3/25/2022	14:48:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
923046	Canton	4/2/2022	14:02:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
923379	Canton	4/1/2022	16:30:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
924020	Canton	4/1/2022	17:58:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Wet
924957	Canton	4/6/2022	07:01:00	Injury of any type (Serious, Minor, Possible)	Angle	Rain	Daylight	Wet
924958	Canton	4/7/2022	10:38:00	Injury of any type (Serious, Minor, Possible)	Angle	Cloudy	Daylight	Dry
925003	Canton	3/30/2022	17:42:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
925861	Canton	4/13/2022	11:01:00	Injury of any type (Serious, Minor, Possible)	Not Applicable	Clear	Daylight	Dry
927837	Canton	4/22/2022	08:13:00	Property Damage Only	Front to front	Clear	Daylight	Dry
930609	Canton	5/2/2022	08:16:00	Property Damage Only	Front to front	Clear	Daylight	Dry
931860	Canton	5/2/2022	18:20:00	Injury of any type (Serious, Minor, Possible)	Front to front	Rain	Daylight	Wet
933118	Canton	5/5/2022	17:09:00	Property Damage Only	Front to front	Clear	Daylight	Dry
936726	Canton	5/22/2022	09:58:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
937884	Canton	5/20/2022	15:22:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
937885	Canton	5/20/2022	15:30:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
937886	Canton	5/20/2022	15:31:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
937887	Canton	5/20/2022	16:06:00	Injury of any type (Serious, Minor, Possible)	Not Applicable	Clear	Daylight	Dry
937888	Canton	5/26/2022	17:01:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
942076	Canton	6/7/2022	06:10:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
943319	Canton	5/27/2022	18:32:00	Property Damage Only	Front to rear	Cloudy	Daylight	Dry
943587	Canton	6/14/2022	14:28:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
944046	Canton	6/17/2022	16:26:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
946103	Canton	6/4/2022	16:51:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
952749	Canton	7/14/2022	07:14:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
953160	Canton	7/17/2022	23:17:00	Property Damage Only	Not Applicable	Clear	Dark-Lighted	Dry
956307	Simsbury	7/23/2022	18:36:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
957026	Canton	7/25/2022	11:23:00	Property Damage Only	Angle	Clear	Daylight	Dry
958075	Canton	7/20/2022	16:10:00	Property Damage Only	Angle	Clear	Daylight	Dry
959276	Canton	7/23/2022	15:06:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
959277	Canton	7/31/2022	10:13:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
960594	Canton	8/5/2022	15:02:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
960973	Canton	8/8/2022	22:08:00	Property Damage Only	Not Applicable	Rain	Dark-Lighted	Wet
963007	Canton	8/13/2022	11:00:00	Property Damage Only	Angle	Clear	Daylight	Dry
963008	Canton	8/18/2022	11:29:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
964031	Canton	8/17/2022	20:20:00	Property Damage Only	Angle	Clear	Dusk	Dry
965025	Canton	8/23/2022	14:20:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Cloudy	Daylight	Dry
966619	Canton	8/26/2022	12:01:00	Property Damage Only	Not Applicable	Clear	Daylight	Dry
966620	Canton	8/26/2022	13:36:00	Property Damage Only	Angle	Clear	Daylight	Dry
968876	Canton	8/16/2022	17:07:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
968877	Canton	8/25/2022	16:38:00	Property Damage Only	Other	Clear	Daylight	Dry
971051	Canton	8/30/2022	17:06:00	Property Damage Only	Angle	Clear	Daylight	Dry
973331	Canton	9/15/2022	12:32:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
973387	Canton	9/13/2022	18:44:00	Property Damage Only	Front to rear	Rain	Dusk	Moving Water
976080	Canton	9/19/2022	15:55:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
977078	Canton	9/27/2022	13:51:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
978384	Canton	9/29/2022	14:21:00	Injury of any type (Serious, Minor, Possible)	Sideswipe, opposite direction	Clear	Daylight	Dry
978635	Canton	10/2/2022	17:07:00	Injury of any type (Serious, Minor, Possible)	Front to front	Clear	Daylight	Dry
981771	Canton	10/6/2022	16:20:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
982363	Canton	8/31/2022	12:05:00	Property Damage Only	Angle	Clear	Daylight	Dry

CrashId	Town Name	Date of Crash	Time of Crash	Crash Severity	Manner of Crash / Collision Impact	Weather Condition	Light Condition	Road Surface Condition
982364	Canton	10/11/2022	08:26:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
985609	Canton	10/6/2022	09:16:00	Property Damage Only	Other	Clear	Daylight	Dry
986305	Canton	10/16/2022	13:28:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
988342	Canton	11/1/2022	07:55:00	Property Damage Only	Front to rear	Clear	Daylight	Wet
992494	Canton	11/9/2022	15:26:00	Property Damage Only	Angle	Clear	Daylight	Dry
993881	Canton	11/12/2022	17:56:00	Property Damage Only	Sideswipe, same direction	Clear	Dark-Not Lighted	Dry
994602	Canton	11/16/2022	06:45:00	Property Damage Only	Sideswipe, same direction	Freezing Rain or Freezing Drizzle	Daylight	Wet
995865	Canton	11/2/2022	16:41:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
998971	Canton	11/22/2022	17:51:00	Property Damage Only	Front to rear	Clear	Dark-Not Lighted	Dry
1000719	Canton	12/4/2022	17:49:00	Property Damage Only	Front to rear	Clear	Dark-Not Lighted	Dry
1001146	Canton	11/22/2022	18:09:00	Property Damage Only	Front to rear	Clear	Dark-Not Lighted	Dry
1001147	Canton	11/22/2022	20:31:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
1001148	Canton	11/27/2022	12:32:00	Property Damage Only	Front to rear	Cloudy	Daylight	Dry
1003082	Canton	12/3/2022	17:00:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
1004631	Canton	12/8/2022	19:12:00	Property Damage Only	Sideswipe, same direction	Clear	Dark-Not Lighted	Dry
1005575	Canton	12/10/2022	16:29:00	Property Damage Only	Front to rear	Clear	Dark-Not Lighted	Dry
1006660	Canton	12/13/2022	13:52:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1007590	Canton	12/20/2022	10:00:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
1007785	Canton	12/20/2022	17:20:00	Property Damage Only	Front to rear	Clear	Dark-Lighted	Dry
1012637	Canton	12/21/2022	20:00:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Dark-Lighted	Dry
1012821	Canton	1/3/2023	14:37:00	Property Damage Only	Angle	Rain	Daylight	Wet
1013024	Canton	1/7/2023	12:13:00	Property Damage Only	Angle	Clear	Daylight	Dry
1013236	Canton	1/4/2023	16:32:00	Property Damage Only	Angle	Clear	Dark-Lighted	Dry
1015441	Canton	1/13/2023	10:52:00	Property Damage Only	Not Applicable	Clear	Daylight	Dry
1016444	Hartford	1/19/2023	02:08:00	Property Damage Only	Angle	Clear	Dark-Not Lighted	Dry
1017674	Canton	1/15/2023	15:30:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
1018107	Canton	1/26/2023	12:34:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1018690	Canton	1/19/2023	21:42:00	Property Damage Only	Sideswipe, same direction	Rain	Dark-Not Lighted	Wet
1021241	Canton	1/31/2023	14:45:00	Property Damage Only	Sideswipe, opposite direction	Clear	Daylight	Dry
1022640	Canton	2/10/2023	19:09:00	Property Damage Only	Other	Clear	Dark-Lighted	Dry
1028216	Canton	3/2/2023	23:20:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Dark-Lighted	Dry
1031477	Canton	3/3/2023	16:10:00	Property Damage Only	Angle	Clear	Daylight	Dry
1034239	Canton	3/5/2023	05:42:00	Property Damage Only	Not Applicable	Clear	Dark-Lighted	Dry
1037158	Canton	3/30/2023	17:51:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1037712	Canton	4/3/2023	15:52:00	Property Damage Only	Unknown	Clear	Daylight	Dry
1039350	Canton	4/1/2023	10:19:00	Property Damage Only	Not Applicable	Rain	Daylight	Wet
1039957	Canton	3/27/2023	15:47:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry
1041684	Canton	4/17/2023	15:13:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1042191	Canton	4/18/2023	13:57:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
1043053	Canton	4/21/2023	10:18:00	Property Damage Only	Angle	Clear	Daylight	Dry
1043925	Canton	4/19/2023	14:44:00	Property Damage Only	Rear to rear	Clear	Daylight	Dry
1044648	Canton	5/1/2023	10:04:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1044760	Canton	4/19/2023	06:59:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
1049367	Canton	5/17/2023	08:18:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1049498	Canton	5/13/2023	08:46:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1049694	Canton	5/14/2023	16:28:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1052118	Canton	5/14/2023	11:01:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1053061	Canton	5/1/2023	17:08:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1053755	Canton	5/24/2023	09:03:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1053967	Canton	5/26/2023	11:35:00	Injury of any type (Serious, Minor, Possible)	Sideswipe, same direction	Clear	Daylight	Dry
1057015	Canton	6/1/2023	10:17:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1057016	Canton	6/6/2023	09:02:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1058431	Canton	6/9/2023	15:25:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1058827	Canton	6/7/2023	09:57:00	Property Damage Only	Front to rear	Clear	Daylight	Dry
1058948	Canton	6/2/2023	12:49:00	Property Damage Only	Sideswipe, same direction	Clear	Daylight	Dry
1063401	Canton	6/22/2023	20:15:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Dusk	Dry
1063722	Canton	6/18/2023	18:04:00	Property Damage Only	Front to rear	Clear	Daylight	Dry

CrashId	Town Name	Date of Crash	Time of Crash	Crash Severity	Manner of Crash / Collision Impact	Weather Condition	Light Condition	Road Surface Condition
1065589	Canton	6/25/2023	11:16:00	Injury of any type (Serious, Minor, Possible)	Front to rear	Clear	Daylight	Dry
1065590	Canton	6/27/2023	10:39:00	Injury of any type (Serious, Minor, Possible)	Angle	Clear	Daylight	Dry

D Traffic Control Signal Plans



PRE-EMPTION SETTINGS

	PRE-EMPT 1	PRE-EMPT 2
PRIORITY	NO	NO
DET. LOCK	YES	YES
DELAY	0	0
ALT. MIN. GRN	5	5
ALT. YELLOW	PARENT	PARENT
ALT. RED	PARENT	PARENT
ALT. PED. CLR.	16	16
HOLD GREEN	15	15
HOLD YELLOW	4.3	3
HOLD RED	1	0.1
HOLD PHASE	2	1+6
EXIT PHASE	2+6	2+6
EXIT CALL	NONE	NONE

PROPERTY OWNER RESPONSIBLE FOR MAJOR COMPONENT REPLACEMENT.

ENERGY BY -DEVELOPER ADDRESS # 314
 MAINT LEVEL 5 SERVICE POLE - CL&P 458
 INTERSECTION # 023-213

UNMETERED SERVICE

OFFICE RECORD

STC # 023-1201-03 SM # N/A
 SIGNAL INSTALLED: 5-13-15
 NEW SIGNAL INSTALLED AND INTERCONNECTED TO EXISTING SIGNAL SYSTEM ALONG ROUTE 44 AS-BUILT PLAN DRAWN:
 REVISION #1
 TIR #N/A SM #100962
 TRAFFIC DES -D. Veronesi ELECT DES -N/A
 SIGNAL REVISED :12/07/2016
 REVISED D4 DETECTOR FROM PRESENCE TO 8" DELAY.

STATE OF CONNECTICUT
 DEPARTMENT OF TRANSPORTATION
 BUREAU OF ENGINEERING & CONSTRUCTION
 DIVISION OF TRAFFIC ENGINEERING

TRAFFIC CONTROL SIGNAL

TOWN OF CANTON
 U.S. ROUTES 44 & 202 (ALBANY TPKE.)
 AT COMMERCIAL DRIVE

DETECTORS

ID	SIZE (WXL)	URNS	MODE	FUNCTION	T I M E	DAYS	COORDINATION TYPE - CLOSED LOOP	PERMIS PERIOD	SYSTEM LOC	TECHNICAL NOTES	
D1	6' X 6'	3	PRESENCE	FLASH	2200-0600	DAILY	CYCLE	SEC	0	023-207	STANDARD OVERLAP SKIP FEATURES APPLY
D2	6' X 6'	4	*	MAX 1	ALL OTHER TIMES	DAILY	SEC	%	01	023-203	PRE-EMPTION TO BE INOPERATIVE DURING FLASH OPERATION.
D2A	6' X 6'	4	*	MAX 2	FUTURE	DAILY	SEC	%	02	023-210	COUNTDOWN ONLY DURING FLASHING PEDESTRIAN CLEARANCE INTERVAL.
D2B	6' X 6'	4	*	PATTERN 1	0630 - 0900	DAILY	SEC	%	03	023-214	INOPERATIVE (RESERVED FOR FUTURE USE)
D2C	6' X 6'	4	*	PATTERN 2	0900 - 1500	M-F SAT-SUN	SEC	%	04	023-211	MANUAL AND INTERVAL ADVANCE TO BE DISCONNECTED DURING PHASE 3 PEDESTRIAN CLEARANCE INTERVAL.
D4	6' X 50' (VIDEO)		8" DELAY PRESENCE	PATTERN 3	ALL OTHER TIMES	DAILY	SEC	%	05	023-209	
D6	6' X 6'	5	*	FREE	NONE	DAILY	SEC	%	06		
D6A	6' X 6'	4	*				SEC	%	07		
D6B	6' X 6'	5	*				SEC	%	08		
D6C	6' X 6'	4	*				SEC	%	09		

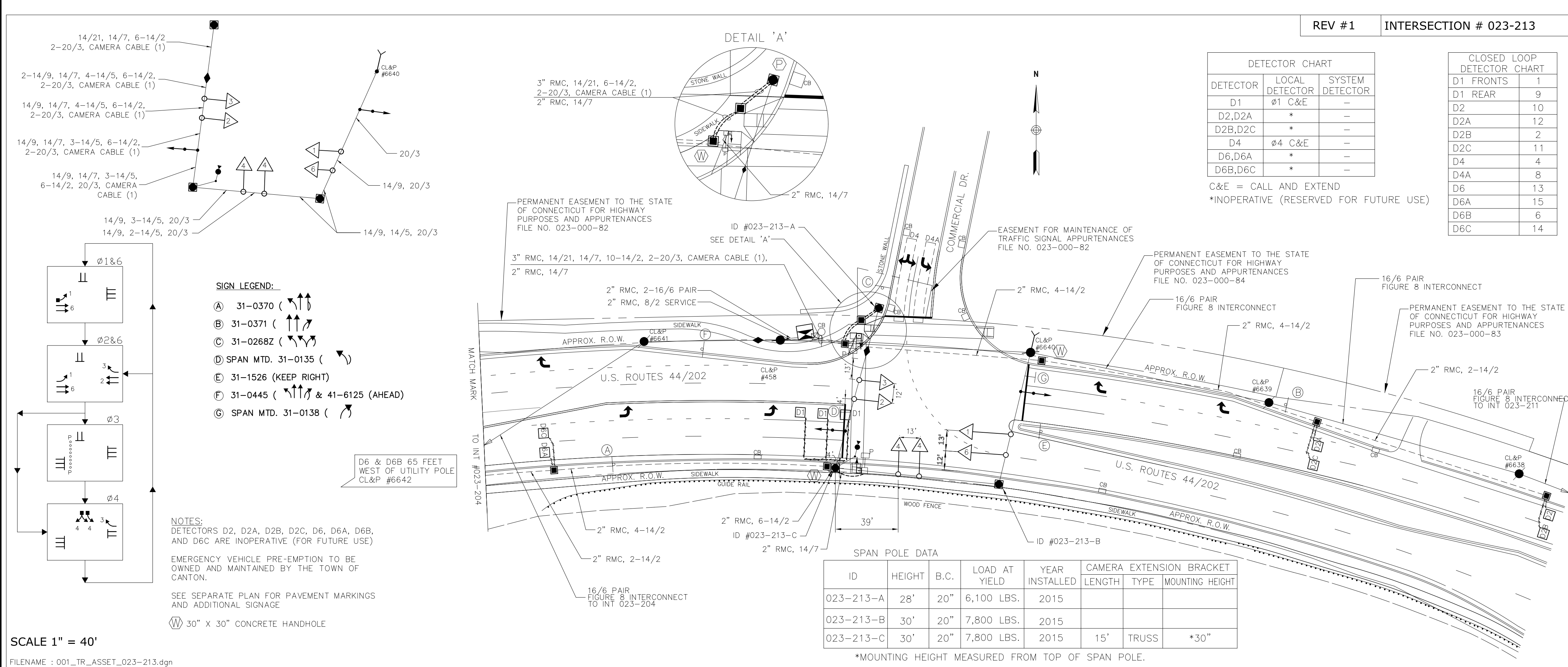
DETECTOR CHART

DETECTOR	LOCAL DETECTOR	SYSTEM DETECTOR
D1	01 C&E	-
D2,D2A	*	-
D2B,D2C	*	-
D4	04 C&E	-
D6,D6A	*	-
D6B,D6C	*	-

C&E = CALL AND EXTEND
 *INOPERATIVE (RESERVED FOR FUTURE USE)

CLOSED LOOP DETECTOR CHART

DETECTOR	FRONTS	REAR
D1	1	9
D2	10	10
D2A	12	2
D2B	11	4
D4	8	13
D6A	15	6
D6B	14	14



CONSTRUCTION NOTES :

REV #1 INTERSECTION # 023-213

DEVELOPER: KONOVER DEVELOPMENT CORPORATION
 135 SOUTH ROAD FARMINGTON, CT 06032

OFFICE OF ENGINEERING

TOWN: CANTON

DRAWING TITLE: TRAFFIC CONTROL SIGNAL PLAN

PROJECT NO. TCS-2A SHEET NO.

REVISIONS

NO.	DATE	INIT.	DESCRIPTION
A	3/18/13	VHB	REVISE SPAN POLE LOC.

DRAWN BY : CB
 DESIGNED BY : CB
 DATE PLOTTED :

STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION

TOWN OF CANTON
 U.S. ROUTES 44 & 202 (ALBANY TPKE.)
 AT COMMERCIAL DRIVE

SCALE 1" = 40'

FILENAME : 001_TR_ASSET_023-213.dgn

LEGEND:

- EXISTING WOOD SPAN POLE
- PROPOSED WOOD SPAN POLE
- EXISTING STEEL SPAN POLE
- PROPOSED STEEL SPAN POLE
- EXISTING UTILITY POLE
- PROPOSED UTILITY POLE
- EXISTING PEDESTAL MOUNTING
- PROPOSED PEDESTAL MOUNTING
- EXISTING SIGNAL FACE
- PROPOSED SIGNAL FACE
- EXISTING TRAFFIC SIGNAL FACE
- PROPOSED TRAFFIC SIGNAL FACE
- EXISTING PEDESTAL MOUNTING
- PROPOSED PEDESTAL MOUNTING
- EXISTING VIDEO DETECTOR
- PROPOSED VIDEO DETECTOR
- EXISTING SONIC DETECTOR
- PROPOSED SONIC DETECTOR
- EXISTING SIDEWALK RAMP
- PROPOSED SIDEWALK RAMP
- EXISTING GUY WIRE
- PROPOSED GUY WIRE
- EXISTING LOOP DETECTOR
- PROPOSED LOOP DETECTOR
- EXISTING MAGNETIC DETECTOR
- PROPOSED MAGNETIC DETECTOR
- EXISTING CABLE CLOSURE
- PROPOSED CABLE CLOSURE

NOTES:
 DETECTORS D2, D2A, D2B, D2C, D6, D6A, D6B, AND D6C ARE INOPERATIVE (FOR FUTURE USE)
 EMERGENCY VEHICLE PRE-EMPTION TO BE OWNED AND MAINTAINED BY THE TOWN OF CANTON.
 SEE SEPARATE PLAN FOR PAVEMENT MARKINGS AND ADDITIONAL SIGNAGE

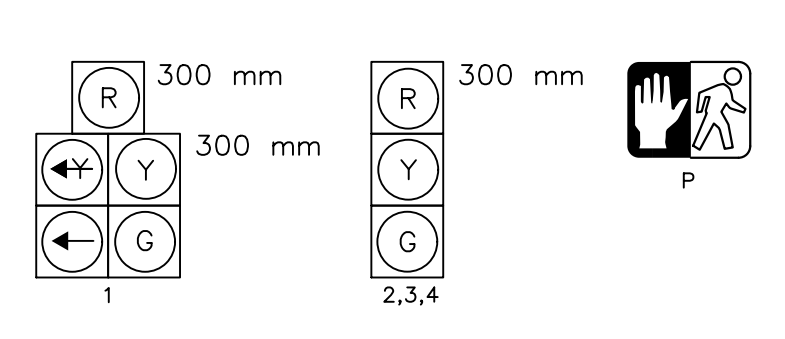
MOVEMENT DIAGRAM																									
NTOR	PHASE 1			PHASE 2 - PRE-EMPT. 1			PHASE 3			PHASE 4			PHASE 5			PHASE 6			PHASE 7			PHASE 8			
NONE	[Diagram]			[Diagram]			[Diagram]			[Diagram]			[Diagram]			[Diagram]			[Diagram]			[Diagram]			
	FLASH	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL
1	Y	←	G	G	G	Y	R	R	R	R	R	R	R												
2	Y	←	G	G	G	Y	R	R	R	R	R	R	R												
3	Y	←	G	G	G	Y	R	R	R	R	R	R	R												
4	R	←	R	R	R	Y	R	R	R	R	R	R	R												
P	OFF				DW				W0																
						←	YIELD PT.																		

TECHNICAL NOTES
 STANDARD OVERLAP SKIP FEATURES APPLY
 PHASE 2 ON TO OMIT PHASE 1.
 UNUSED TIME NOT TO BE REALLOCATED.
 PRE-EMPTION TO BE INOPERATIVE DURING FLASHING OPERATION.

OFFICE RECORD
 REV # 4 TIR # N/A SM # 100471 SIGNAL REVISED: 11/30/2015

INTERVAL #	MIN GRN		MODE	COORDINATION TYPE: CLOSED LOOP	PHASE SPLITS	SEC / %	PERMIS PERIOD
	PHASE 1	PHASE 2					
1	5	15	NON-LOCK	NON-LOCK	LOCK	OFF	OFF
2	7	14					
3	2,0	20					
4	20	20					
5	3,0	4,4					
6	1,0	1,8					
7		4,0					
8		0					
9		3,0					
10		1,0					

SIGNAL FACES



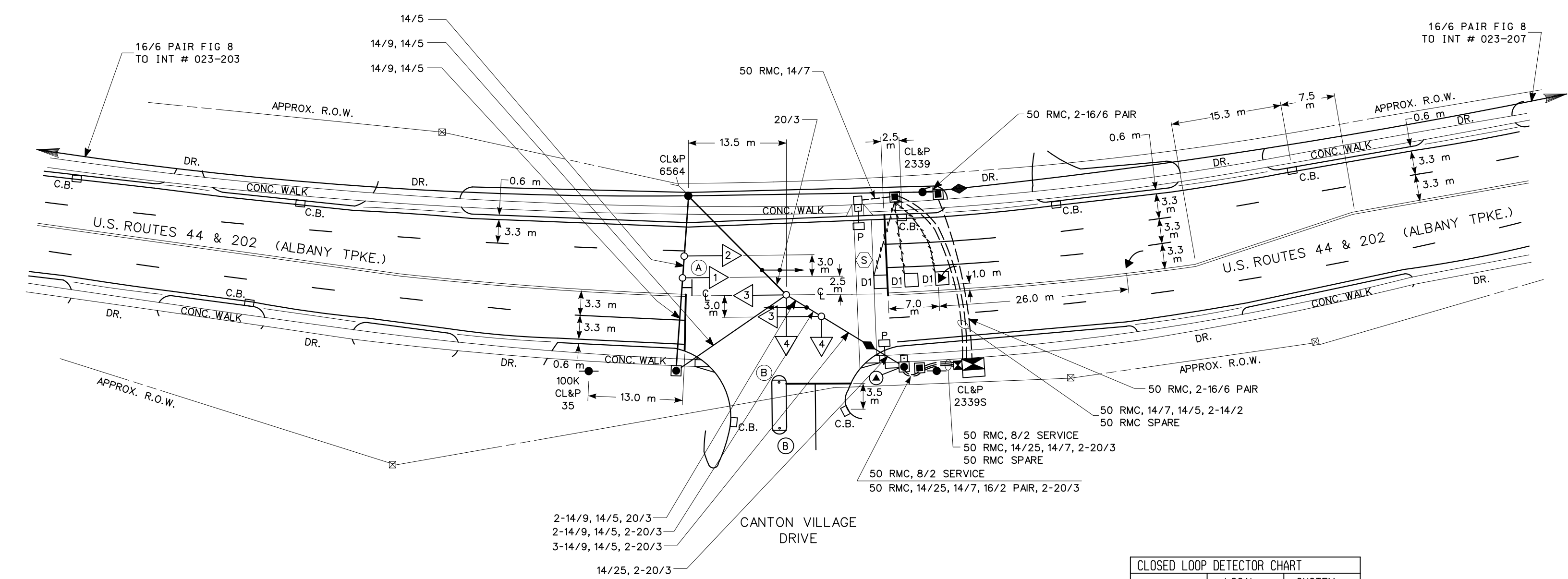
PRE-EMPTION SETTINGS

	PRE-EMPT 1
PRIORITY	NO
DET. LOCK	YES
DELAY	0
ALT. MIN. GRN	5
ALT. YELLOW	PARENT
ALT. RED	PARENT
ALT. PED. CLR.	14
HOLD GREEN	15
HOLD YELLOW	4,4
HOLD RED	1,8
HOLD PHASE	2
EXIT PHASE	4
EXIT CALL	NONE

ALL INDICATIONS HAVE LED LAMPS.

- SIGN LEGEND
- (A) SPAN MTD. 31-0135 (KEEP RIGHT)
 - (B) 31-1526Z (KEEP RIGHT)

NOTES:
 STATE TO MAINTAIN ALL PAVEMENT MARKINGS AND SIGNING ON U.S. ROUTES 44 & 202 AND ALL STOP BARS.
 ALL PAVEMENT MARKINGS AND SIGNING ON CANTON VILLAGE DRIVE TO BE MAINTAINED BY OTHERS.
 BAR TYPE CROSSWALK (0.4 m - 0.4 m X 2.4 m MIN.) STATE MAINTAINED
 EMERGENCY PRE-EMPTION EQUIPMENT TO BE OWNED AND MAINTAINED BY THE TOWN OF CANTON.



CLOSED LOOP DETECTOR CHART

DETECTOR	LOCAL DETECTOR	SYSTEM DETECTOR
D1 REAR	LD1	SD1
D1	LD1	
D4	LD4	SD4



ENGINEER	TRAFFIC	DATE	ELECTRICAL	DATE	REV #4	INTERSECTION # 023-210
					ENERGY BY - DEVELOPER	ADDRESS #
					MAINT LEVEL 5	SERVICE POLE - CL&P #23395
						UNMETERED SERVICE

LEGEND:

R	RED	○	PROPOSED WOOD SPAN POLE
Y	YELLOW	●	EXISTING WOOD SPAN POLE
G	GREEN	◻	PROPOSED STEEL SPAN POLE
←R	RED ARROW	◻	EXISTING STEEL SPAN POLE
←Y	YELLOW ARROW	○	PROPOSED UTILITY POLE
←G	GREEN ARROW	●	EXISTING UTILITY POLE
W/P	WALK/PED. CLR	○	PEDESTAL MOUNTING
D.W.	DON'T WALK	◻	PEDESTRIAN PUSH BUTTON & SIGN
FL.	FLASHING	◻	DIRECTIONAL ARW. FOR PUSH BUTTON

□	TRAFFIC SIGNAL FACE	□	PROPOSED CONTROLLER	□	SIDEWALK RAMP
□	PEDESTRIAN SIGNAL FACE	□	EXISTING CONTROLLER	□	CABLE CLOSURE
---	DET. LEADS IN SAW CUT	□	LOOP DETECTOR	□	WIRELESS SENSOR
---	EXISTING RMC (RIGID METAL CONDUIT)	□	SYSTEM DETECTOR	□	WIRELESS RECEIVER
---	EXISTING RMC (RIGID METAL CONDUIT)	□	MAGNETIC DETECTOR	□	WIRELESS TRANSMITTER
□	AUXILIARY TERMINATION CABINET	□	OPTICAL DETECTOR	□	GUY WIRE
□	AUXILIARY EQUIPMENT CABINET	□	VIDEO DETECTOR	□	PROPOSED HANDHOLE
□	VIDEO DETECTION ZONE	□	AUDIO DETECTOR	□	EXISTING HANDHOLE

STATE OF CONNECTICUT
 DEPARTMENT OF TRANSPORTATION

U.S. ROUTES 44 & 202 (ALBANY TURNPIKE)
 AT CANTON VILLAGE DRIVE

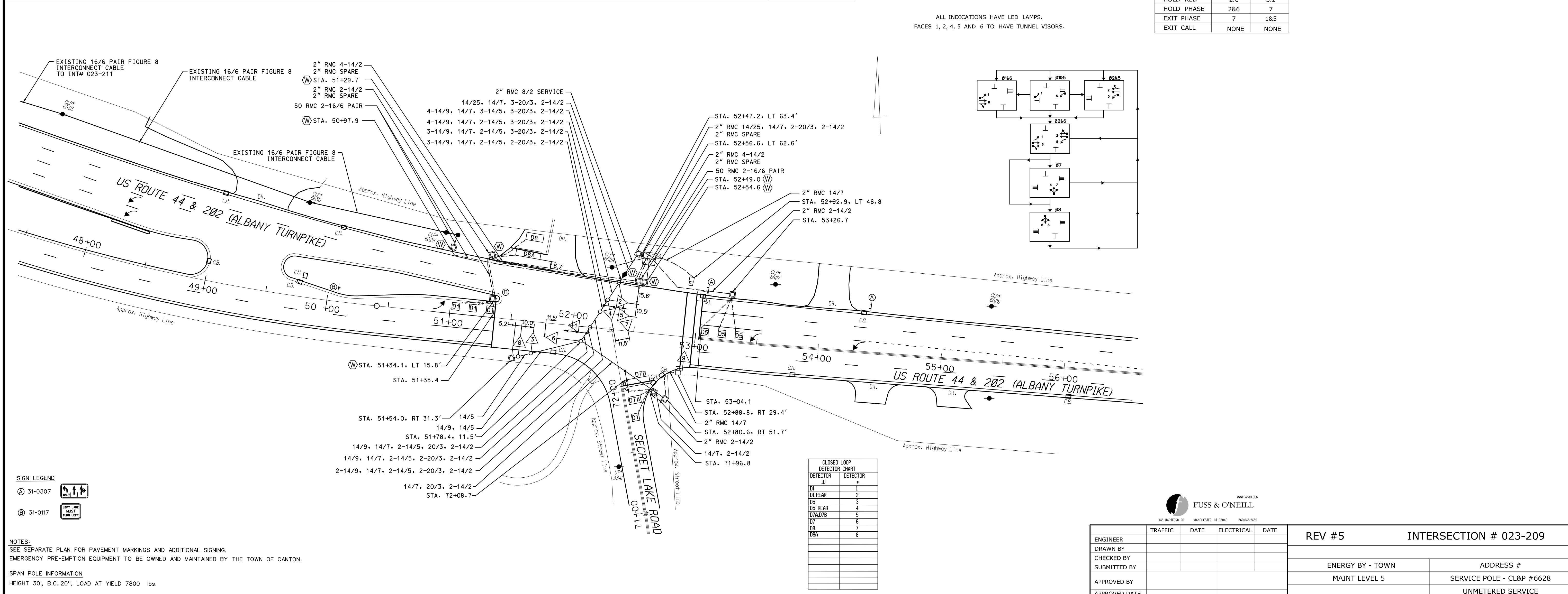
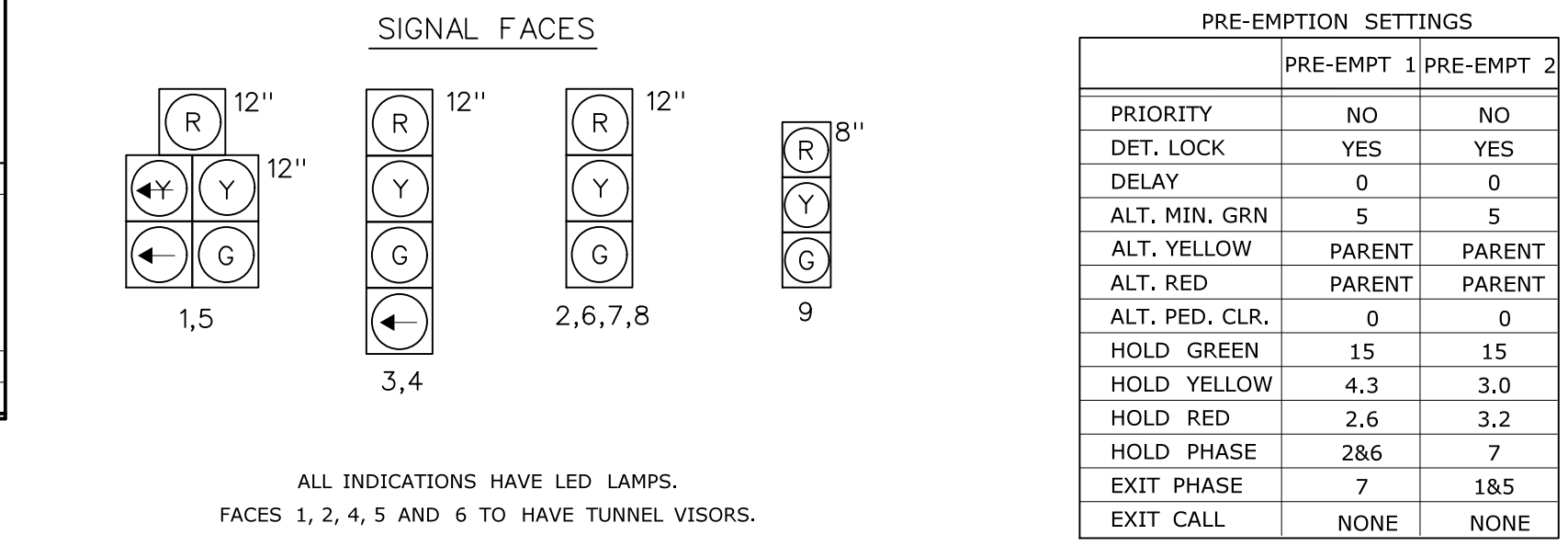
CANTON
 TRAFFIC CONTROL SIGNAL PLAN

PROJECT NO. 174-387
 DRAWING NO.
 SHEET NO.

		MOVEMENT DIAGRAM																																			
		PHASE 1				PHASE 2 - PRE-EMPT. 1				PHASE 3				PHASE 4				PHASE 5				PHASE 6 - PRE-EMPT. 1				PHASE 7 - PRE-EMPT. 2				PHASE 8							
NTOR		NONE				NONE				NONE				NONE				NONE				NONE				NONE				NONE							
F A C E #		FLASH	GRN	CL	CL	GRN	CL	CL	CL	GRN	CL	CL	CL	GRN	CL	CL	CL	GRN	CL	CL	CL	GRN	CL	CL	CL	GRN	CL	CL	CL	GRN	CL	CL	CL	GRN	CL	CL	CL
I N T E R V A L S		MIN GRN	5			15								5				15				9				7											
		WALK																																			
		PED CLR																																			
		VEH EXT	1.5			2.5				1.5				1.5				1.5				1.5				1.5											
		MAX 1	6			44				6				6				6				6				6											
		MAX 2	6			44				6				6				6				6				6											
		YELLOW		3.0			4.4								3.0				4.4								3.0										
		RED			1.0		2.6									1.0			2.6									3.0		3.2							
		ADD INIT																																			
		MAX INIT																																			
		TBR																																			
		TTR																																			
		MIN GAP																																			
		MODE	NON-LOCK				MIN RECALL THIS PHASE				OFF				OFF				NON-LOCK				MIN RECALL THIS PHASE				NON-LOCK				NON-LOCK						
		INT START																																			

TECHNICAL NOTES		OFFICE RECORD	
STANDARD OVERLAP SKIP FEATURES APPLY		REV # 5	TIR # N/A
PED BUTTONS TO CALL PHASE 7		SM # 100471	SIGNAL REVISED: 11/30/2015
PRE-EMPTION TO BE INOPERATIVE DURING FLASH OPERATION.			
PHASE 1 & 5 TO DRIVE FACES 1 & 5 G AND Y ARROWS.			
PHASE 2 & 6 TO DRIVE FACES 1 & 5 G, Y, AND R.			
TIMINGS SHOWN INDICATE FREE OPERATION.			
ACTUAL COORDINATION INFORMATION TO BE DETERMINED BY CLOSED LOOP LOCAL COORDINATION UNIT.			

IDENT	SIZE (WXL)	TURNS	MODE	SYSTEM LOC	COORDINATION TYPE: CLOSED LOOP			CYCLE	OFFSET	
					MASTER	PROGRAM	TIME		DAYS	SEC
D1	6.0' X 6.0'	3	PRESENCE	023-203	FLASH	2200-0600	DAILY			
D5	6.0' X 6.0'	3	PRESENCE	023-210		SEE TECH NOTES				
D7	6.0' X 6.0'	3	PRESENCE	023-204						
D7A	10.0' X 6.0'	3	PRESENCE	023-211						
D7B	20.0' X 6.0'	3	PRESENCE	023-209						
D8	14.0' X 6.0'	3	PRESENCE	023-212						
D8A	20.0' X 6.0'	3	PRESENCE							



NOTES: SEE SEPARATE PLAN FOR PAVEMENT MARKINGS AND ADDITIONAL SIGNING. EMERGENCY PRE-EMPTION EQUIPMENT TO BE OWNED AND MAINTAINED BY THE TOWN OF CANTON.		SPAN POLE INFORMATION HEIGHT 30', B.C. 20", LOAD AT YIELD 7800 lbs.	
LEGEND: R RED Y YELLOW G GREEN ← RED ARROW → GREEN ARROW WALKY PED. CLR D.W. DON'T WALK FL. FLASHING		○ PROPOSED WOOD SPAN POLE ● EXISTING WOOD SPAN POLE □ PROPOSED STEEL SPAN POLE ○ EXISTING STEEL SPAN POLE ○ PROPOSED UTILITY POLE ● EXISTING UTILITY POLE □ PEDESTAL MOUNTING □ PEDESTRIAN PUSH BUTTON & SIGN □ DIRECTIONAL ARW. FOR PUSH BUTTON	
○ PROPOSED WOOD SPAN POLE ● EXISTING WOOD SPAN POLE □ PROPOSED STEEL SPAN POLE ○ EXISTING STEEL SPAN POLE ○ PROPOSED UTILITY POLE ● EXISTING UTILITY POLE □ PEDESTAL MOUNTING □ PEDESTRIAN PUSH BUTTON & SIGN □ DIRECTIONAL ARW. FOR PUSH BUTTON		□ TRAFFIC SIGNAL FACE □ PEDESTRIAN SIGNAL FACE DET. LEADS IN SAW CUT - - - PROPOSED RMC (RIGID METAL CONDUIT) - - - EXISTING RMC (RIGID METAL CONDUIT) □ AUXILIARY TERMINATION CABINET □ AUXILIARY EQUIPMENT CABINET □ VIDEO DETECTION ZONE	
□ PROPOSED CONTROLLER □ EXISTING CONTROLLER □ LOOP DETECTOR □ SD SYSTEM DETECTOR □ MAGNETIC DETECTOR □ OPTICAL DETECTOR □ VIDEO DETECTOR □ AUDIO DETECTOR		□ SIDEWALK RAMP □ CABLE CLOSURE □ WIRELESS SENSOR □ WIRELESS RECEIVER □ WIRELESS TRANSMITTER □ GUY WIRE □ PROPOSED HANDHOLE □ EXISTING HANDHOLE	

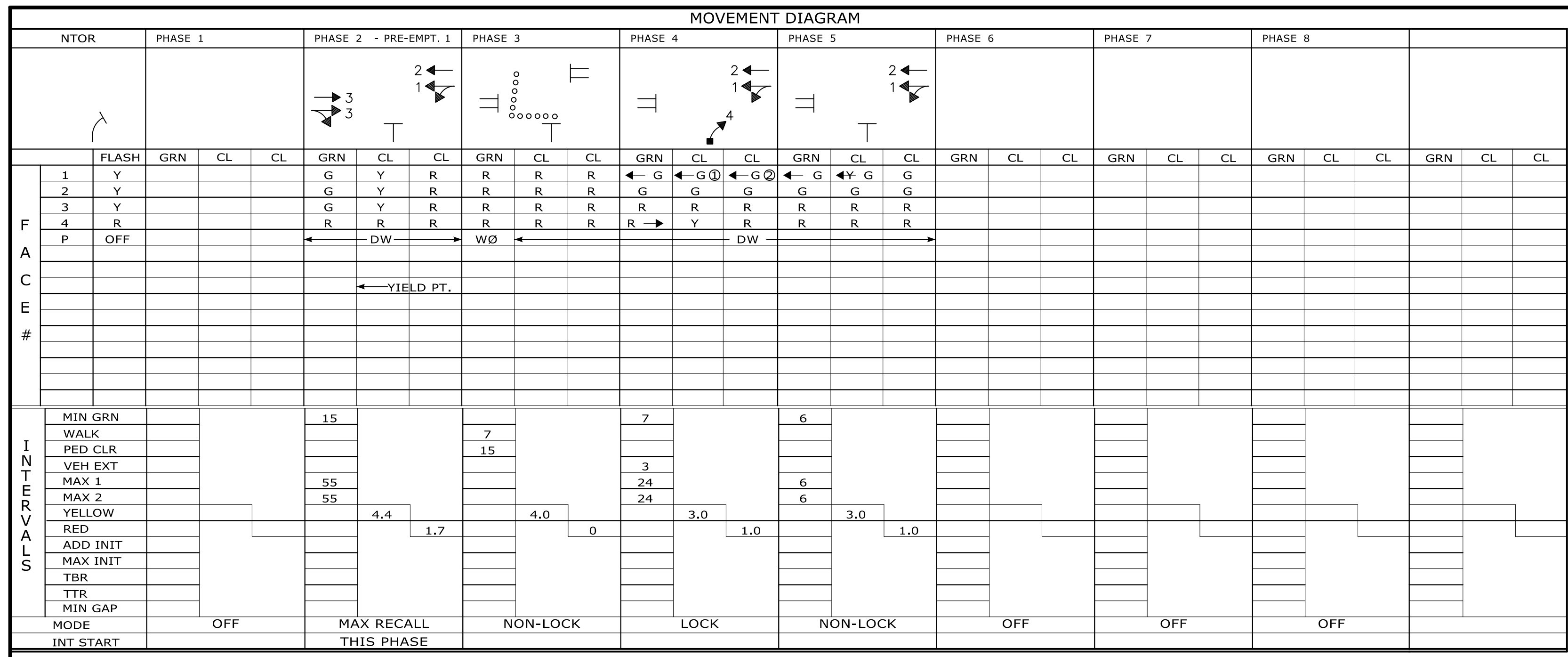
STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

U.S. ROUTES 44 & 202 (ALBANY TPKE.) AT
SECRET LAKE ROAD AND PRIVATE DRIVE

DATE PLOTTED : 2/8/2016 X:\0174-0387\Traffic\FINAL DESIGN\Canton - 23\TR_MSH_174-387_023-209_005.dgn

ENGINEER	TRAFFIC	DATE	ELECTRICAL	DATE	REV #5	INTERSECTION # 023-209
DRAWN BY					ENERGY BY - TOWN	ADDRESS #
CHECKED BY					MAINT LEVEL 5	SERVICE POLE - CL&P #6628
SUBMITTED BY					UNMETERED SERVICE	
APPROVED BY					TOWN:	CANTON
APPROVED DATE					DRAWING TITLE:	TRAFFIC CONTROL SIGNAL PLAN
					PROJECT NO.	174-387
					DRAWING NO.	
					SHEET NO.	

SCALE 1" = 40'

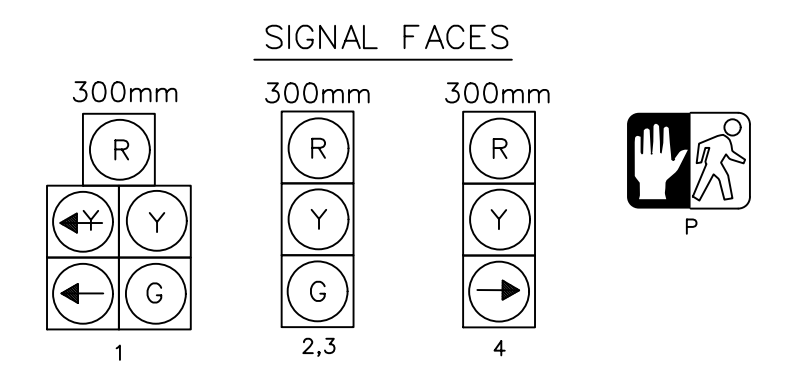


TECHNICAL NOTES

STANDARD OVERLAP SKIP FEATURES APPLY
 PHASE 4 OR PHASE 5 ALWAYS TO PRECEDE PHASE 2.
 PHASE 3 ON TO CALL PHASE 4.
 PHASE 5 ON TO OPERATE DURING PATTERN 5 ONLY.
 PHASE 5 CALLED WITH PHASE 4 ON DURING PATTERN 5.
 PHASE 5 CALLED WITH PHASE 4 ON DURING PATTERN 5.
 (1) TO BE G IF PHASE 2 IS NEXT.
 (2) TO BE G IF PHASE 2 IS NEXT.
 UNUSED TIME TO BE REALLOCATED.
 EMERGENCY PRE-EMPTION TO BE INOPERATIVE DURING FLASHING OPERATION.

OFFICE RECORD		REV #	TIR #	SM #	SIGNAL REVISION
REV # 16	TIR # N/A	SM # 100471	SIGNAL REVISION: 11/30/2015		
REVISED TIMINGS UNDER PROJECT 174-387					
REV. # 17	TIR# N/A	SM#101544	SIGNAL REVISION: 8/27/2018		
UPDATED PAVEMENT MARKINGS AND INSTALLED RIGHT TURN ONLY ARROW ON S.R. 565 (DOWD. AVE.).					

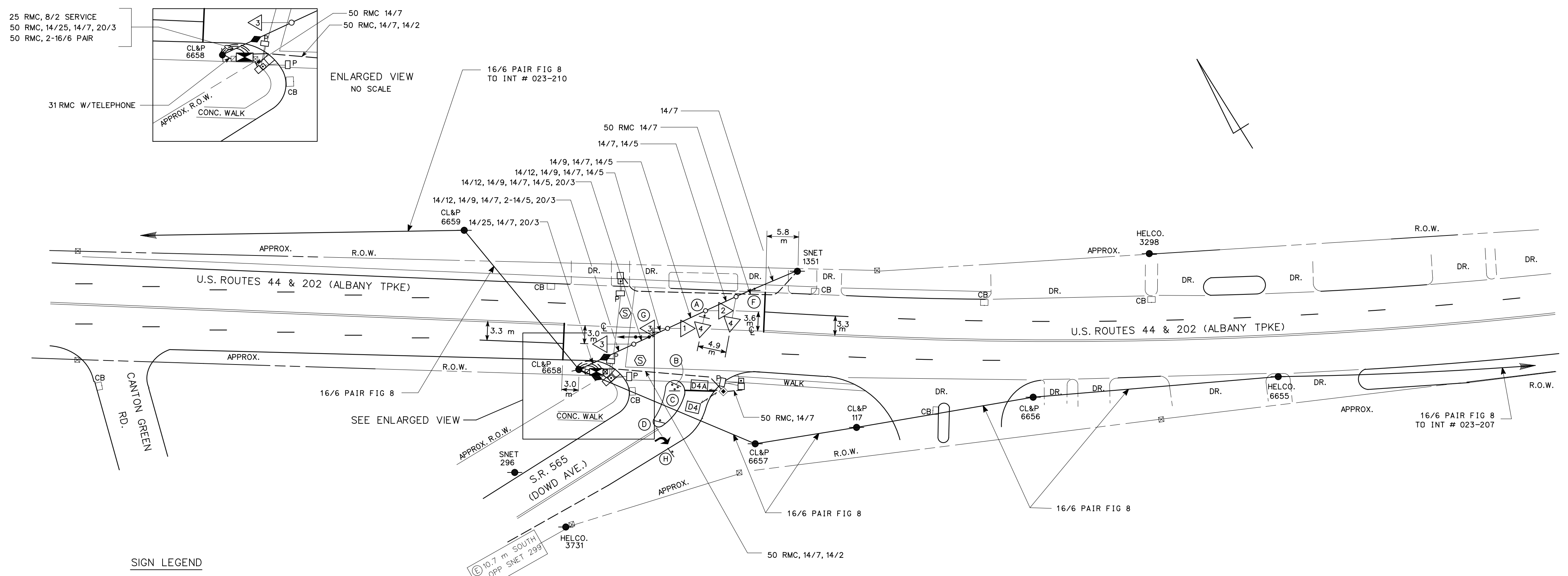
IDENT	SIZE (WXL)	TURNS	MODE	SYSTEM LOC	COORDINATION TYPE: CLOSED LOOP	FUNCTION	PROGRAM	DAYS	CYCLE	OFFSET	PHASE SPLITS								PERMIS PERIOD																																																																								
											SEC	%	O1	O2	O3	O4	O5	O6		O7	O8	O	SEC																																																																				
D4	2.4 m X 1.8 m	3	PRESENCE	023-203	FLASH	2200-0600	DAILY																																																																																				
D4A	3.0 m X 1.8 m	3	PRESENCE	023-210	MAX 1	ALL TIMES																																																																																					
																									023-204	FUTURE																																																																	
																																																023-209	PATTERN 1	0630-0900	M-F	90°	16	18																																					
																																																																							023-211	PATTERN 2	0900-1500	M-F	80°	36	45														
023-212	PATTERN 5	1500-1800	M-F	90°	21	23																																																																																					



PRE-EMPTION SETTINGS

	PRE-EMPT 1
PRIORITY	NO
DET. LOCK	YES
DELAY	0
ALT. MIN. GRN	5
ALT. YELLOW	PARENT
ALT. RED	PARENT
ALT. PED. CLR.	15
HOLD GREEN	15
HOLD YELLOW	4.4
HOLD RED	1.7
HOLD PHASE	2
EXIT PHASE	4
EXIT CALL	NONE

ALL INDICATIONS HAVE LED LAMPS.



- #### SIGN LEGEND
- (A) NIPPLE MTD. 31-1603 (NO LT. TURN)
 - (B) 31-1536 (KEEP RT.)
 - (C) 31-1619 AND 31-1620 (NO. LT. TURN)
 - (D) 31-1526 (KEEP RT.)
 - (E) 41-0836 (SIG AH)
 - (F) SPAN MTD. 31-0823 (NTOR)
 - (G) SPAN MTD. 41-4638 (ONCOMING TRAFFIC HAS EXTENDED GREEN)
 - (H) 31-0104 (RIGHT TURN ONLY)

NOTES:
 STATE TO MAINTAIN ALL PAVEMENT MARKINGS AND SIGNING ON U.S. ROUTE 44 & 202 (ALBANY TPKE.), SR 555 (DOWD AVE.) AND ALL STOP BARS.
 EMERGENCY PRE-EMPTION EQUIPMENT TO BE OWNED AND MAINTAINED BY THE TOWN OF CANTON.
 (S) BAR TYPE CROSSWALKS (0.4 m - 0.4 m x 2.4 m MIN.) - STATE MAINTAINED

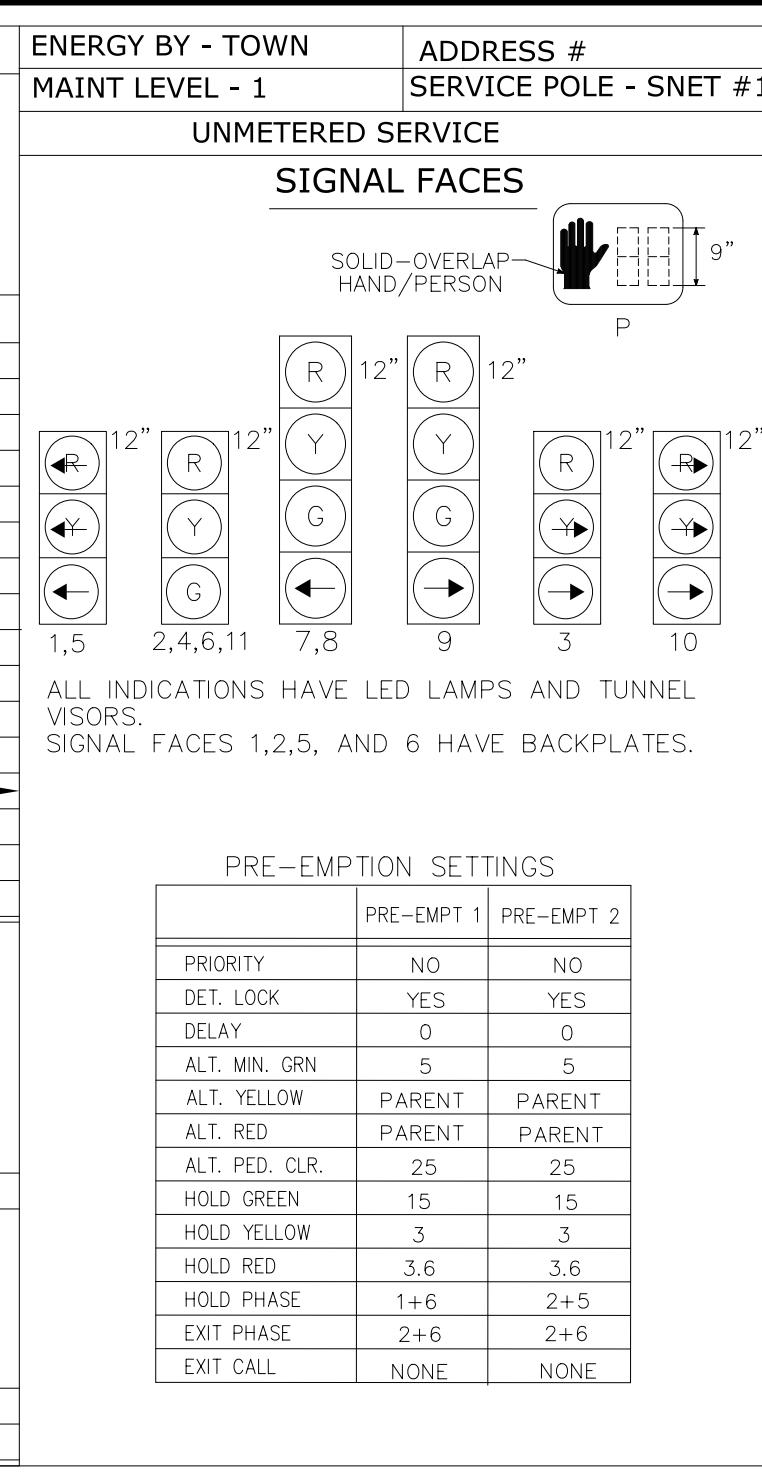
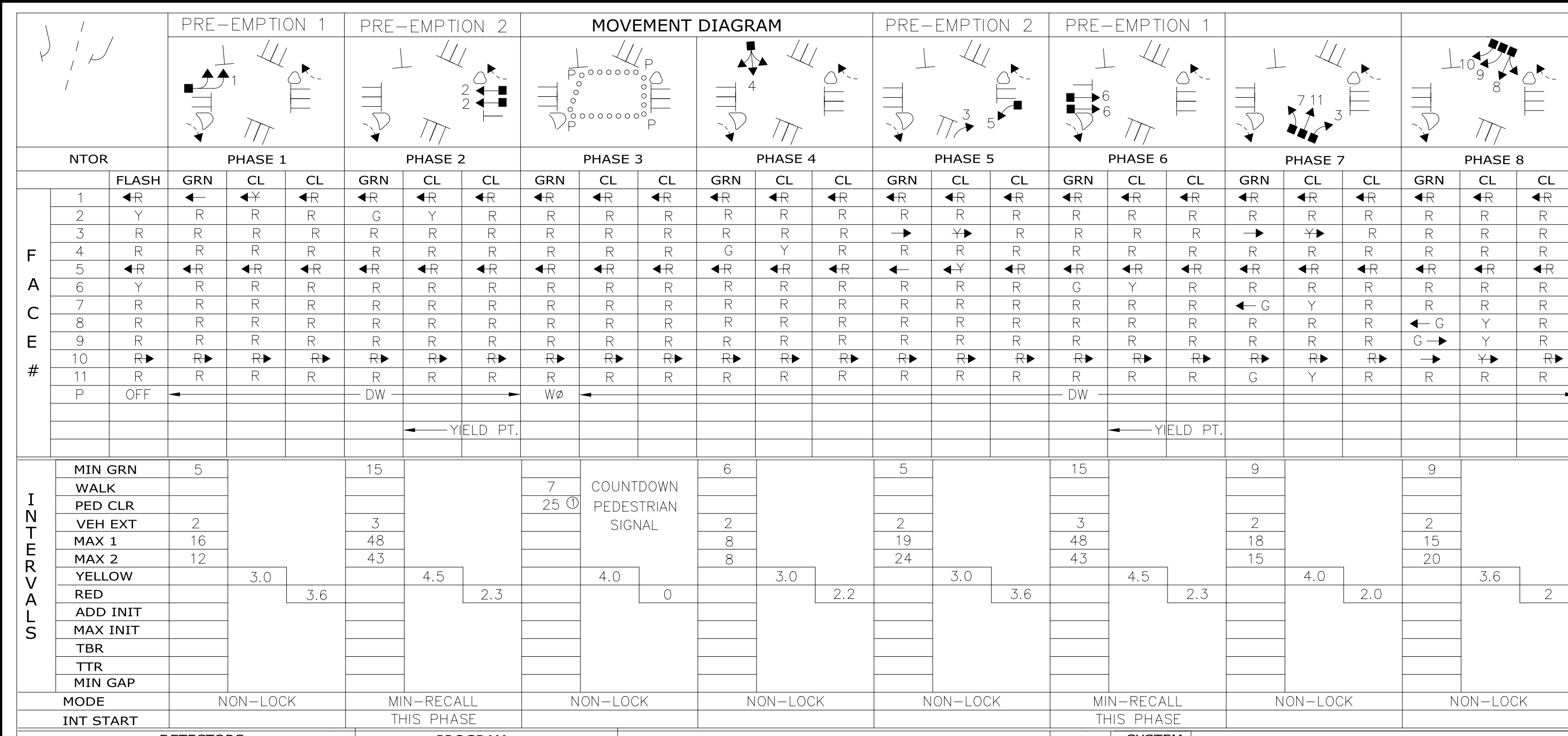
CLOSED LOOP DETECTOR CHART

DETECTOR	LOCAL DETECTOR	SYSTEM DETECTOR
D4	LD1	SD1
D4A	LD2	SD2



ENGINEER	TRAFFIC	DATE	ELECTRICAL	DATE	REV #17	INTERSECTION # 023-207
DRAWN BY						
CHECKED BY						
SUBMITTED BY						
APPROVED BY						
APPROVED DATE						

<table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>NO.</th> <th>DATE</th> <th>REVISION DESCRIPTION</th> </tr> </thead> <tbody> <tr><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td></tr> <tr><td> </td><td> </td><td> </td></tr> </tbody> </table>	NO.	DATE	REVISION DESCRIPTION										<p>LEGEND:</p> <ul style="list-style-type: none"> R RED Y YELLOW G GREEN ←R RED ARROW ←Y YELLOW ARROW ←G GREEN ARROW WØ WALK/ PED. CLR D.W. DON'T WALK FL. FLASHING 	<ul style="list-style-type: none"> ○ PROPOSED WOOD SPAN POLE ● EXISTING WOOD SPAN POLE □ PROPOSED STEEL SPAN POLE ● EXISTING STEEL SPAN POLE ○ PROPOSED UTILITY POLE ● EXISTING UTILITY POLE □ EXISTING MOUNTING □ PEDESTRIAN PUSH BUTTON & SIGN □ DIRECTIONAL ARW. FOR PUSH BUTTON 	<ul style="list-style-type: none"> □ TRAFFIC SIGNAL FACE □ PEDESTRIAN SIGNAL FACE □ DET. LEADS IN SAW CUT □ PROPOSED RMC (RIGID METAL CONDUIT) □ EXISTING RMC (RIGID METAL CONDUIT) □ AUXILIARY TERMINATION CABINET □ AUXILIARY EQUIPMENT CABINET □ VIDEO DETECTION ZONE 	<ul style="list-style-type: none"> □ PROPOSED CONTROLLER □ EXISTING CONTROLLER □ LOOP DETECTOR □ SD SYSTEM DETECTOR □ MAGNETIC DETECTOR □ OPTICAL DETECTOR □ VIDEO DETECTOR □ AUDIO DETECTOR 	<ul style="list-style-type: none"> □ SIDEWALK RAMP □ CABLE CLOSURE □ WIRELESS SENSOR □ WIRELESS RECEIVER □ WIRELESS TRANSMITTER □ GUY WIRE □ PROPOSED HANDHOLE □ EXISTING HANDHOLE 	<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p>	<p>U.S. ROUTES 44 & 202 (ALBANY TURNPIKE) AT S.R. 565 (DOWD AVENUE)</p> <p style="text-align: center;">CANTON</p> <p style="text-align: center;">TRAFFIC CONTROL SIGNAL PLAN</p>	<p>PROJECT NO. 174-387</p> <p>DRAWING NO.</p> <p>SHEET NO.</p>
NO.	DATE	REVISION DESCRIPTION																		



ENERGY BY - TOWN
MAINT LEVEL - 1

ADDRESS #
SERVICE POLE - SNET #1676

INTERSECTION # 023-204

UNMETERED SERVICE

OFFICE RECORD

REVISION #18

STC # 023-1201-02 SM # N/A

SIGNAL REVISED: 11/7/2012

WIDENED LAWTON ROAD, ADDED EXCLUSIVE PEDESTRIAN PHASE, REVISED SIDE STREET PHASING & TIMINGS

AS-BUILT PLAN DRAWN

REVISION #19

TIR # N/A SM # N/A

TRAFFIC DESIGN: GRP ELECT. DESIGN: N/A

SIGNAL REVISED: 7/28/2016

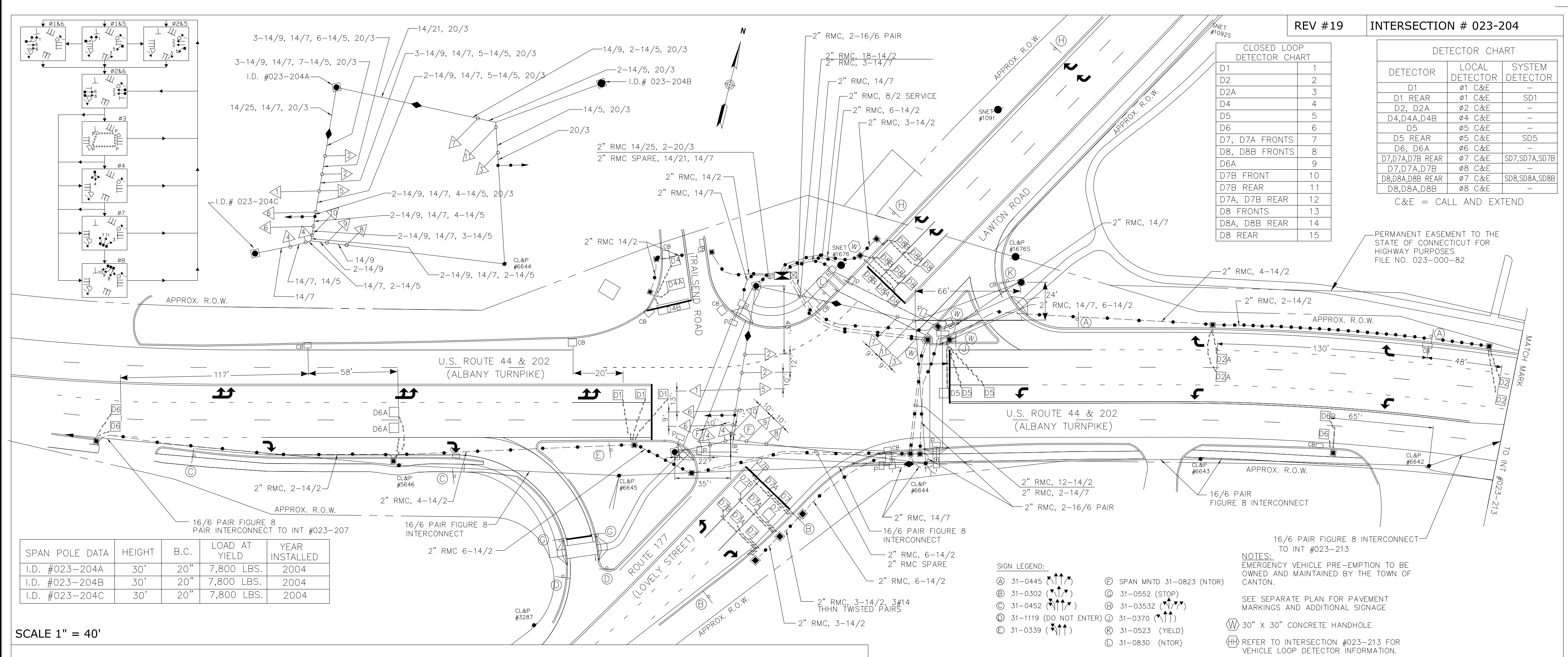
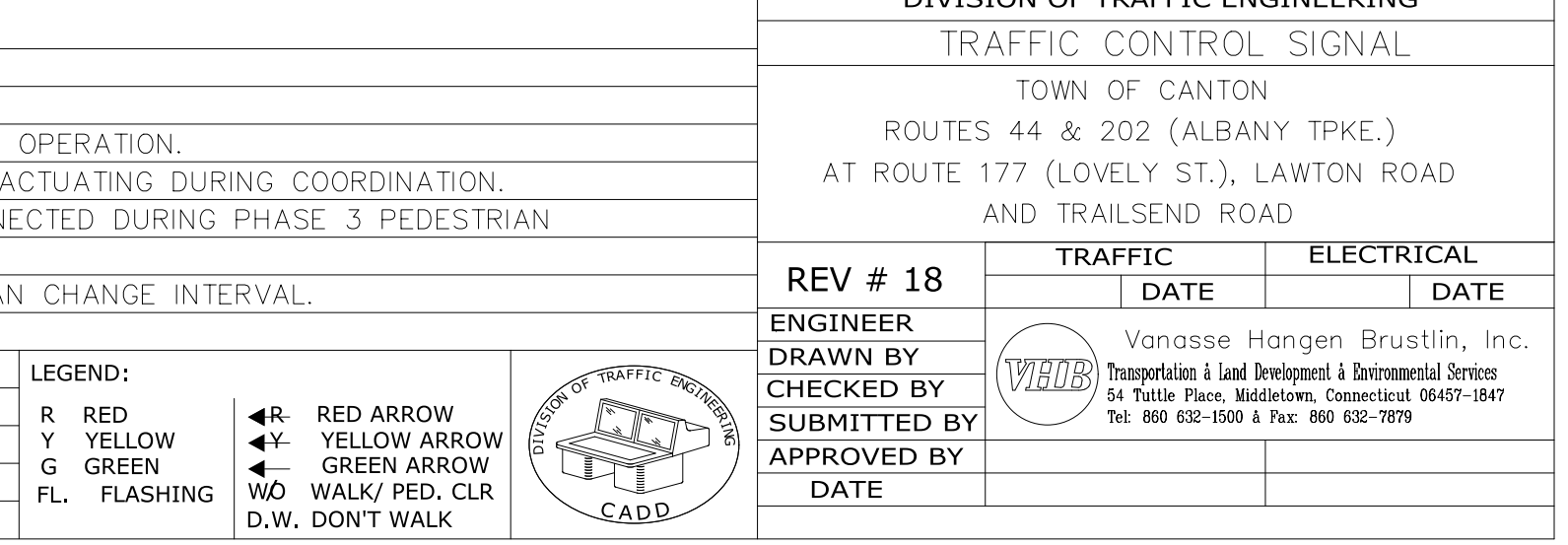
REVISED FACE 3 RED ARROW TO RED BALL

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING & CONSTRUCTION
DIVISION OF TRAFFIC ENGINEERING

TRAFFIC CONTROL SIGNAL

TOWN OF CANTON
ROUTES 44 & 202 (ALBANY TPKE.)
AT ROUTE 177 (LOVELY ST.), LAWTON ROAD
AND TRAILSEND ROAD

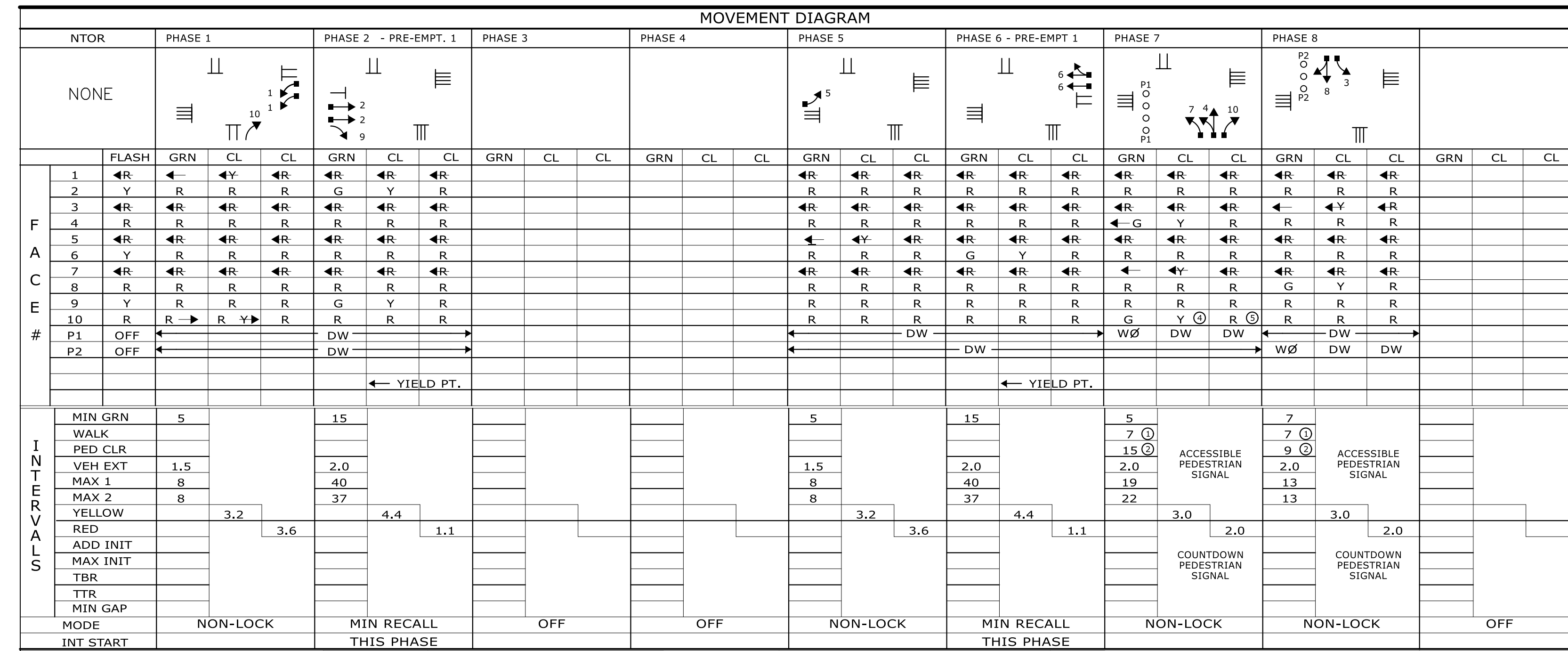
IDENT	SIZE (WXL)	MODE	FUNCTION	T I M E	DAYS	COORDINATION TYPE - CLOSED LOOP	PERMITS PERIOD	SYSTEM LOC MASTER	TECHNICAL NOTES
D1	6' X 6'	3 PRESENCE	FLASH	NONE				023-207	STANDARD OVERLAP SKIP FEATURES APPLY
D1 REAR	6' X 6'	4 PRESENCE						023-203	PRE-EMPTION TO BE INOPERATIVE DURING FLASH OPERATION.
D2A	6' X 6'	4 PRESENCE	MAX 1	ALL OTHER TIMES	DAILY			023-210	DETECTORS D2, D2A, D6, AND D6A TO BE NON-ACTUATING DURING COORDINATION.
D4	11' X 6'	3 PRESENCE	MAX 2	1500 - 1900	M-F			023-204	MANUAL AND INTERVAL ADVANCE TO BE DISCONNECTED DURING PHASE 3 PEDESTRIAN CHANGE INTERVAL.
D4A	20' X 6'	3 PRESENCE						023-213	
D4B	6' X 6'	3 PRESENCE						023-211	COUNTDOWN ONLY DURING FLASHING PEDESTRIAN CHANGE INTERVAL.
D5	6' X 6'	3 PRESENCE	FREE	ALL OTHER TIMES	DAILY			023-209	
D6	6' X 6'	4 PRESENCE							
D6A	6' X 6'	4 PRESENCE							
D7	6' X 6'	3 PRESENCE							
D7A	6' X 6'	3 PRESENCE							
D7B	6' X 6'	3 PRESENCE							
D8	6' X 6'	3 PRESENCE							
D8A	6' X 6'	3 PRESENCE							
D8B	6' X 6'	3 PRESENCE							



DRAWN BY :
DESIGNED BY :
DATE PLOTTED : Jul 11, 2012

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

TOWN OF CANTON
ROUTES 44 & 202 (ALBANY TPKE.)
AT ROUTE 177 (LOVELY ST.), LAWTON ROAD
AND TRAILSEND ROAD

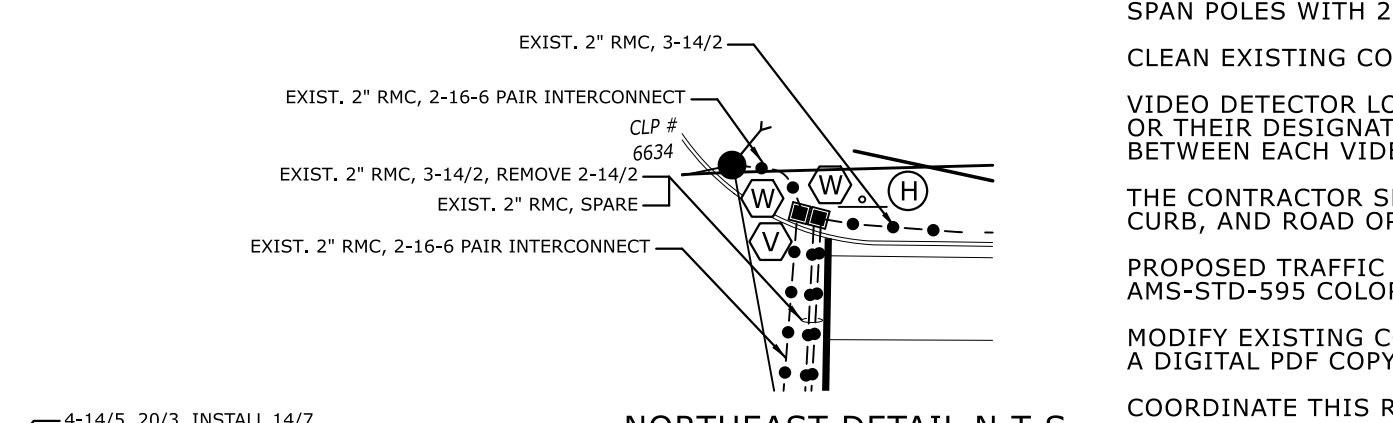
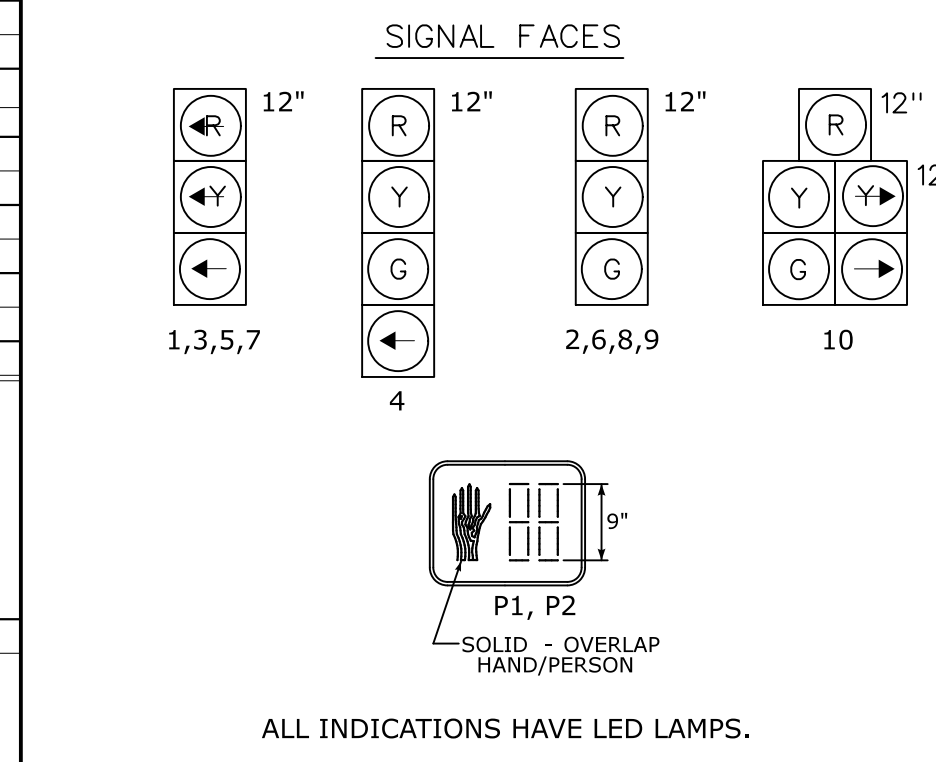


TECHNICAL NOTES
 STANDARD OVERLAP SKIP FEATURES APPLY
 PRE-EMPTION TO BE INOPERATIVE DURING FLASH OPERATION.
 ARTERY DETECTORS TO BE NON-ACTUATING DURING COORDINATION.
 ① PERCUSSIVE TONE ONLY DURING PEDESTRIAN WALK INTERVAL.
 ② COUNTDOWN ONLY DURING FLASHING PEDESTRIAN CHANGE INTERVAL.
 ③ CONCURRENT PED PHASE ONLY COMES IN WHEN ACTUATED BY PUSHBUTTON.
 ④ TO BE Y → IF PHASE 1 IS NEXT.
 ⑤ TO BE R → IF PHASE 1 IS NEXT.
 TIMINGS SHOWN REFLECT FREE OPERATION.
 ACTUAL TIMINGS TO BE DETERMINED BY THE CLOSED LOOP LOCAL COORDINATION UNIT.

REV #	TR # N/A	SM # N/A	SIGNAL REVISED:
NEW	REVISOR TO INCORPORATE FOURTH LEG AT INTERSECTION.		
NEW	SIGNAL HEADS 3, 5, 7, 8, & 9; NEW CROSSWALK ACROSS WESTERN APPROACH OF ALBANY TURNPIKE, PEDESTRIAN EQUIPMENT WITH APS PUSH BUTTONS, AND VIDEO DETECTION INSTALLED UNDER ENCROACHMENT PERMIT.		

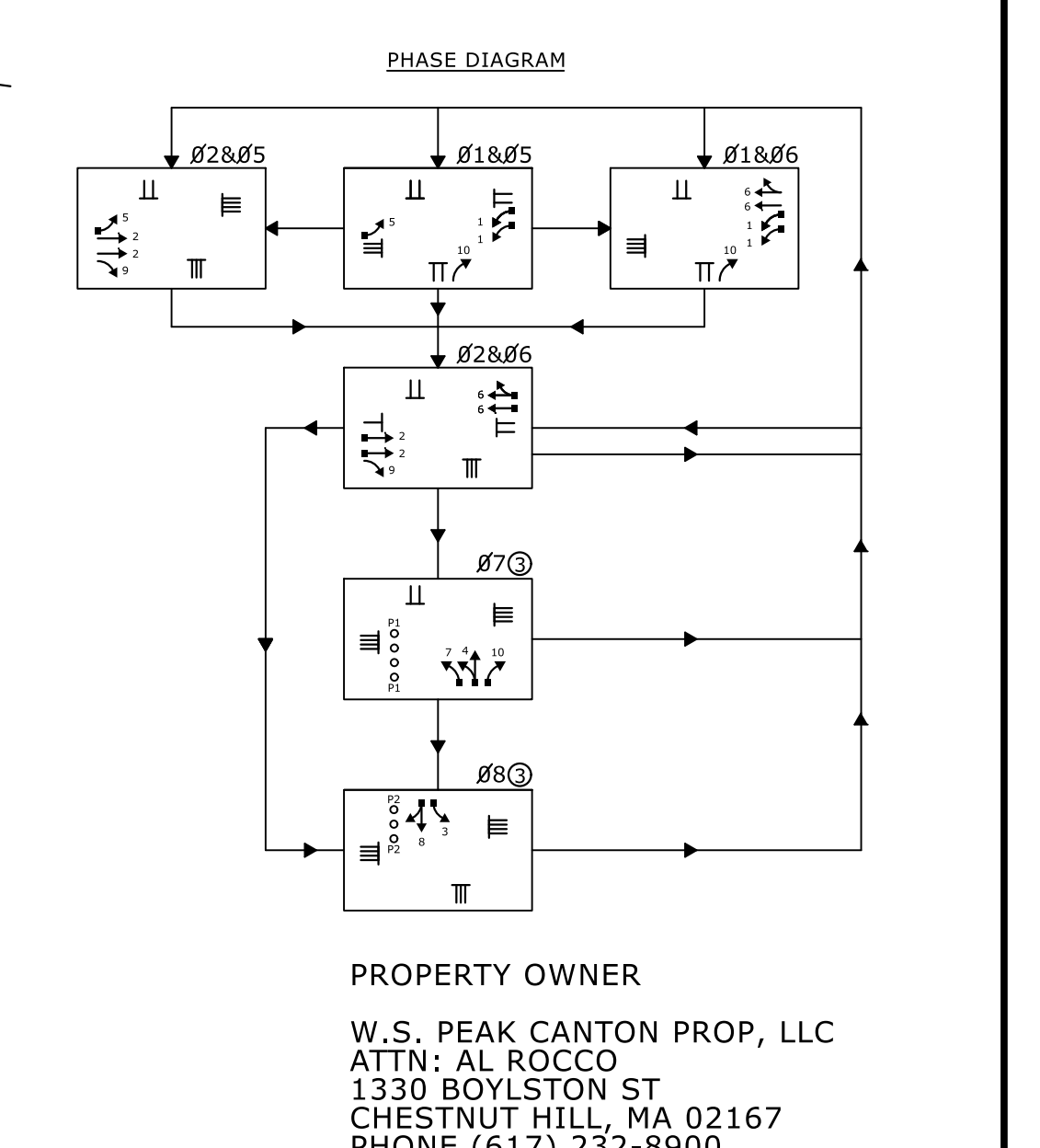
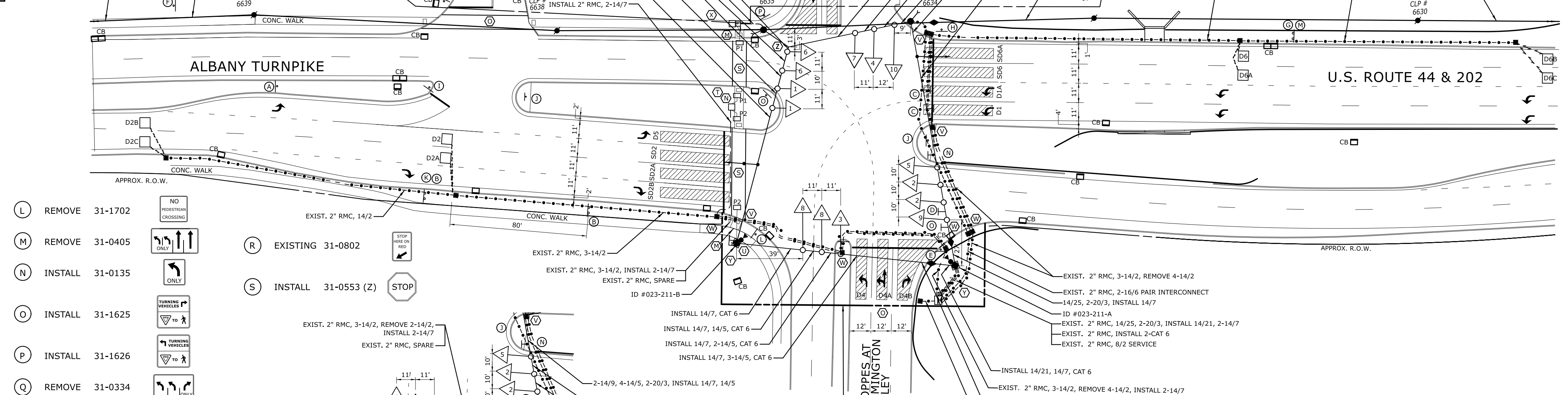
FLASH	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL	GRN	CL	CL
1	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←
2	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←
3	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←
4	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←
5	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←
6	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←
7	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←
8	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←
9	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←
10	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←
P1	OFF	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←
P2	OFF	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←	←
MIN GRN		5		15									5		15				5		7			
WALK													7		15				7		9			
PED CLR													15		15				15		2.0			
VEH EXT		1.5		2.0									1.5		2.0				2.0		2.0			
MAX 1		8		40									40		40				40		13			
MAX 2		8		37									8		37				8		13			
YELLOW		3.2		4.4									3.2		4.4				3.0		3.0			
RED		3.6		1.1									3.6		1.1				2.0		2.0			
ADD INIT																								
MAX INIT																								
TBR																								
TTR																								
MIN GAP																								
MODE		NON-LOCK		MIN RECALL THIS PHASE		OFF		OFF		NON-LOCK		MIN RECALL THIS PHASE		NON-LOCK		NON-LOCK		OFF		NON-LOCK		OFF		
INT START																								

CONSTRUCTION NOTES:
 ALL TRAFFIC SIGNAL EQUIPMENT IS EXISTING, EXCEPT AS NOTED.
 ALL MATERIAL AND CONSTRUCTION METHODS SHALL CONFORM TO THE FOLLOWING CURRENT D.O.T. DOCUMENTS WHICH CAN BE ACCESSED ON THE D.O.T. WEBSITE:
 * STANDARD SPECIFICATIONS FOR ROADS, BRIDGES AND INCIDENTAL CONSTRUCTION,
 * SUPPLEMENTAL SPECIFICATIONS TO THE STANDARD SPECIFICATIONS,
 * SPECIAL PROVISIONS TO THE STANDARD SPECIFICATIONS,
 * STANDARD INSTALLATION AND GUIDE DETAIL SHEETS.
 COORDINATE WITH UTILITY COMPANY REPRESENTATIVES LISTED IN THE SPECIAL PROVISION, 1.07 - LEGAL RELATIONS AND RESPONSIBILITIES.
 STAKE ALL R.O.W. PRIOR TO EXCAVATION.
 ANY PROPOSED REVISIONS TO THE LOCATION OF THE APPURTENANCES SHOWN ON THE PLAN MUST BE SUBMITTED FOR REVIEW AND APPROVAL BY THE DIVISION OF TRAFFIC ENGINEERING PRIOR TO INSTALLATION.
 THE LOCATION OF TRAFFIC SIGNAL FOUNDATIONS WHEN IN OR ADJACENT TO SIDEWALKS SHALL BE VERIFIED PRIOR TO INSTALLATION TO PROVIDE A FREE PATH OF NOT LESS THAN 4 FEET. IF A MINIMUM 4 FOOT FREE PATH IS UNAVAILABLE NOTIFY THE ENGINEER AND CONTACT THE DIVISIONS OF TRAFFIC ENGINEERING.
 SIGNAL HEADS 3, 5, 7, 8, & 9 ARE NEW. ALL OTHERS ARE EXISTING.
 INSTALL PEDESTAL FOUNDATION ADJACENT TO LANDING AREAS AND WALK AT BACK EDGE.
 ALL PEDESTRIAN PUSH-BUTTONS MUST MEET A.D.A. STANDARDS AND SHALL BE APS TYPE.
 (M) CONTRACTOR TO INSTALL RAMP TYPE 13 AS SHOWN.
 (N) CONTRACTOR TO INSTALL PEDESTRIAN REFUGE TYPE 22 AS SHOWN.
 (O) CONTRACTOR TO ERADICATE CONFLICTING AND INSTALL PAVEMENT MARKINGS AS SHOWN.
 (T) INSTALL PEDESTRIAN SIGNAL HEADS ON PEDESTAL AS SHOWN.
 (U) CONTRACTOR TO INSTALL ONE WAY SPAN POLE MOUNTED PEDESTRIAN SIGNAL HEAD AND APS PUSH BUTTON AS SHOWN.
 (V) CONTRACTOR TO VERIFY AND COORDINATE ANY IMPACTS TO EXISTING TRAFFIC SIGNAL EQUIPMENT WITH ENGINEER PRIOR TO CONDUCTING WORK.
 (X) INSTALL PEDESTRIAN SIGNAL HEAD AND APS PUSH BUTTON ON PEDESTAL AS SHOWN.
 (Y) VIDEO DETECTION BY USE OF 360 DEGREE CAMERAS MOUNTED ON SPAN POLES #023-211-A AND #023-211-B.
 (Z) INSTALL RISER ON CLP #6635.
 SPAN POLES WITH 2 SPAN ATTACHMENTS TO HAVE 2 SPAN CLAMPS.
 CLEAN EXISTING CONDUITS AND HANDHOLES THAT ARE TO BE USED.
 VIDEO DETECTOR LOCATIONS ARE FOR ILLUSTRATION ONLY, EXACT LOCATIONS SHALL BE DETERMINED BY MANUFACTURER OR THEIR DESIGNATED REPRESENTATIVE, ALL VIDEO CABLES ARE TO BE INSTALLED BY THE CONTRACTOR CONTINUOUS BETWEEN EACH VIDEO DETECTION CAMERA TO THE CONTROLLER.
 THE CONTRACTOR SHALL OBTAIN ALL NECESSARY STATE AND TOWN PERMITS, INCLUDING BUT NOT LIMITED TO: SIDEWALK, CURB, AND ROAD OPENING.
 PROPOSED TRAFFIC SIGNAL HEADS AND PEDESTRIAN SIGNAL HEADS SHALL HAVE A DARK GREEN FINISH AMS-STD-595 COLOR NO. 14056, TO MATCH EXISTING EQUIPMENT.
 MODIFY EXISTING CONTROLLER TO ACCOMMODATE CHANGES. SUPPLY 5 COPIES OF REVISED CABINET WIRING DIAGRAM AND A DIGITAL PDF COPY TO DOT.TRAFFICELECTRICAL@CT.GOV.
 COORDINATE THIS REVISION WITH THE TOWN OF CANTON AT LEAST 14 DAYS PRIOR TO REVISION.
 REMOVE ALL ABANDONED TRAFFIC SIGNAL EQUIPMENT PER SPECIAL PROVISION.
 COORDINATE THIS REVISION WITH CONNECTICUT D.O.T. SIGNAL LAB, CONTACT DOT.SIGNALLAB@CT.GOV AT LEAST 14 DAYS PRIOR TO REVISION.
 ESTIMATED LOAD ON UTILITY POLES: CLP#6634: 393 LBS., CLP#6635: 383 LBS.



SIGN LEGEND

(A) EXISTING 31-0120	(L) REMOVE 31-1702	(R) EXISTING 31-0802
(B) INSTALL 31-0445	(M) REMOVE 31-0405	(S) INSTALL 31-0553 (Z)
(C) EXISTING 31-0135 SPAN MTD.	(N) INSTALL 31-0135	
(D) INSTALL 31-0138	(O) INSTALL 31-1625	
(E) INSTALL 31-0331	(P) INSTALL 31-1626	
(F) EXISTING 31-0371	(Q) REMOVE 31-0334	
(G) INSTALL 31-0467		
(H) EXISTING 31-1702		
(I) EXISTING 31-0553		
(J) EXISTING 31-1526		
(K) REMOVE 31-0371		



NOTES:
 TOWN OF CANTON TO MAINTAIN ALL SIGNS AND PAVEMENT MARKINGS ON ROUTE 44 AND ALL STOP BARS.
 ALL OTHER SIGNS AND PAVEMENT MARKINGS TO BE MAINTAINED BY OTHERS.
 (D) BAR TYPE CROSSWALK - OWNER MAINTAINED
 (S) BAR TYPE CROSSWALK - STATE MAINTAINED
 (W) 30" X 30" HANDHOLE
 (Y) VIDEO DETECTION BY USE OF 360 DEGREE CAMERAS MOUNTED ON SPAN POLES #023-211-A AND #023-211-B.
 EXISTING EMERGENCY VEHICLE PRE-EMPTION EQUIPMENT TO BE OWNED AND MAINTAINED BY THE TOWN OF CANTON.

LEGEND:

<ul style="list-style-type: none"> RED: RED Y: YELLOW G: GREEN ←: RED ARROW ↔: YELLOW ARROW →: GREEN ARROW WD: WALK/ PED. CLR D.W: DON'T WALK FL: FLASHING 	<ul style="list-style-type: none"> ○: PROPOSED WOOD SPAN POLE ●: EXISTING WOOD SPAN POLE ◻: PROPOSED STEEL SPAN POLE ◐: EXISTING STEEL SPAN POLE ○: PROPOSED UTILITY POLE ●: EXISTING UTILITY POLE ◻: PEDESTAL MOUNTING ◐: PEDESTRIAN PUSH BUTTON & SIGN ◑: DIRECTIONAL ARW. FOR PUSH BUTTON 	<ul style="list-style-type: none"> ◁: TRAFFIC SIGNAL FACE ◒: PEDESTRIAN SIGNAL FACE ◓: DET. LEADS IN SAW CUT —: PROPOSED RMC (RIGID METAL CONDUIT) ---: EXISTING RMC (RIGID METAL CONDUIT) —: AUXILIARY TERMINATION CABINET —: AUXILIARY EQUIPMENT CABINET ◻: VIDEO DETECTION ZONE ◑: VIDEO DETECTION ZONE 	<ul style="list-style-type: none"> ◁: PROPOSED CONTROLLER ◒: EXISTING CONTROLLER ◓: LOOP DETECTOR ◔: SD SYSTEM DETECTOR ◕: MAGNETIC DETECTOR ◖: OPTICAL DETECTOR ◗: VIDEO DETECTOR ◘: AUDIO DETECTOR ◙: SIDEWALK RAMP ◚: CABLE CLOSURE ◛: WIRELESS SENSOR ◜: WIRELESS RECEIVER ◝: WIRELESS TRANSMITTER ◞: GUY WIRE ◟: PROPOSED HANDHOLE ◠: EXISTING HANDHOLE
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SPAN POLE INFORMATION

ID #	POLE LENGTH	BOLT CIRCLE	CAMERA BRACKET LENGTH	VERTICAL RISER FOR 360 CAMERA	YEAR INSTALLED
023-211-A	30'	20"	N/A	3'	2004
023-211-B	30'	20"	N/A	3'	2004

ENGINEER	TRAFFIC	DATE	ELECTRICAL	DATE
DRAWN BY				
CHECKED BY				
SUBMITTED BY				
APPROVED BY				
APPROVED DATE				

SLR
 99 REALTY DRIVE
 CHeshire, CT 06024
 860.737.7170
 slrconsulting.com

REV # INTERSECTION # 023-211

ENERGY BY - PROPERTY OWNER	91 ALBANY TURNPIKE
W.S. PEAK CANTON PROP, LLC	SERVICE POLE - CLP #6634
MAINT LEVEL 3	UNMETERED SERVICE

TOWN: CANTON

DRAWING TITLE: TRAFFIC CONTROL SIGNAL PLAN

PROJECT NO.: --

DRAWING NO.:

SHEET NO.:

NO.	DATE	REVISION DESCRIPTION

DOCUMENT ALL LOOP DETECTOR VALUES BOTH CALCULATED AND MEASURED.

DEFINITIONS:

LOOP: #14 AWG WIRE IN SAWCUT, TERMINATED IN HANDHOLE, IMSA SPEC 51-7.
 LEAD-IN: 14/2 SHIELDED TWISTED PAIR CABLE FROM HANDHOLE TO CONTROLLER, IMSA SPEC 50-2.
 LOOP CIRCUIT: LOOP SAWCUT WIRE SPLICED TO 14/2 LEAD-IN CABLE.
 AMPLIFIER: ELECTRONIC DEVICE CONNECTED TO LOOP CIRCUIT. SENSES CHANGE IN RESONANT FREQUENCY AND CREATES AN OUTPUT TO THE CONTROLLER.
 MEGOHMMETER: INSTRUMENT SPECIFICALLY DESIGNED TO TEST THE INSULATION RESISTANCE OF A CIRCUIT. COMMON MANUFACTURERS: AMEC®, AMPROBE®, FLUKE®, MEGGER®.

1: RESISTANCE:

1a: INSULATION RESISTANCE: PERFORM A 600 VOLT (MINIMUM) MEGOHMMETER TEST ON LOOP CIRCUIT. THE LOOP AMPLIFIER MUST BE DISCONNECTED FROM THE LOOP CIRCUIT OR THE LOOP AMPLIFIER WILL BE DAMAGED. THE RESISTANCE OF THE LOOP WIRE TO GROUND MUST BE GREATER THAN 100 MEG OHMS.

1b: WIRE RESISTANCE: MEASURE THE DC RESISTANCE OF THE LOOP CIRCUIT. THE LOOP CIRCUIT MUST BE DISCONNECTED FROM THE AMPLIFIER. USING AN OHMMETER CONNECTED ACROSS THE LOOP CIRCUIT, MEASURE THE DC RESISTANCE OF THE CONDUCTORS. THE RESISTANCE SHOULD BE LESS THAN 4 OHMS.

NOTE: ALL TESTS SHALL BE DONE AT THE CONTROLLER ASSEMBLY (CA), HOWEVER IT IS RECOMMENDED TO PERFORM A PRELIMINARY MEGOHMMETER TEST AT THE HANDHOLE PRIOR TO SEALING THE SAWCUT AND SPLICING TO THE LEAD-IN. IF A DEFECTIVE LOOP WIRE IS FOUND, IT MAY BE EASILY REPLACED.

2: LOOP CIRCUIT INDUCTANCE:

2a: CALCULATE INDUCTANCE OF LOOP (L_{LOOP}) AND LEAD-IN CABLE (L_{14/2}).

LOOP INDUCTANCE (ENGLISH)	LOOP INDUCTANCE (METRIC)
$L_{LOOP} = (P/4) (N^2 + N)$	$L_{LOOP} = (3.28P/4) (N^2 + N)$
LEAD-IN INDUCTANCE	LEAD-IN INDUCTANCE
$L_{14/2} = (0.24 \mu h/FT) (D)$	$L_{14/2} = (0.78 \mu h/m) (D)$

WHERE:
 L_{LOOP} = INDUCTANCE OF INDIVIDUAL LOOP SEGMENTS IN MICROHENRIES (μh).
 L_{14/2} = INDUCTANCE OF LEAD-IN CABLE.
 P = PERIMETER OF INDIVIDUAL LOOP SEGMENT, IN FEET OR METERS.
 N = NUMBER OF TURNS.
 D = LENGTH OF LEAD-IN CABLE FROM SPLICE IN HANDHOLE TO CONTROLLER, IN FEET OR METERS.
 $L_T = L_1 + L_2 + L_3$ etc.,
 (TOTAL INDUCTANCE OF SEGMENTED LOOP SPLICED IN SERIES.)
 $L_T = 1 / [(1/L_1) + (1/L_2) + (1/L_3) + \text{etc.}]$,
 (TOTAL INDUCTANCE OF SEGMENTED LOOP SPLICED IN PARALLEL.)

WHERE:
 L_T = TOTAL INDUCTANCE OF THE SEGMENTED ARRANGEMENT.
 L₁, L₂, L₃ = INDUCTANCE OF INDIVIDUAL LOOP SEGMENTS.

EXAMPLE: (IN ENGLISH)

6' x 6', 4 TURNS, APPROXIMATELY 300' FROM THE CONTROLLER

$L_{LOOP} = (24/4) (4^2 + 4)$	$L_{14/2} = (0.24 \mu h/FT) (300)$
$L_{LOOP} = (6) (20)$	$L_{14/2} = (0.24) (300)$
$L_{LOOP} = 120 \mu h$	$L_{14/2} = 72 \mu h$

2b: MEASURE INDUCTANCE OF LOOP AND LEAD-IN AT CONTROLLER. USE INSTRUMENT DESIGNED TO MEASURE LOOP CIRCUIT INDUCTANCE.

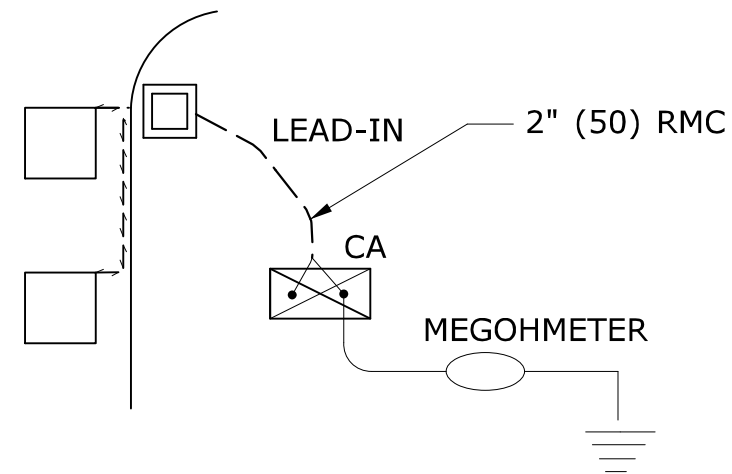
3: POWER INTERRUPTION:

AFTER THE AMPLIFIER HAS TUNED AND IS OPERATING, DISCONNECT POWER BY REMOVING FUSE OR HARNESS CONNECTOR. RETURN POWER TO THE AMPLIFIER AND CONFIRM IT RE-TUNES AUTOMATICALLY WITHOUT ANY MANUAL ADJUSTMENTS.

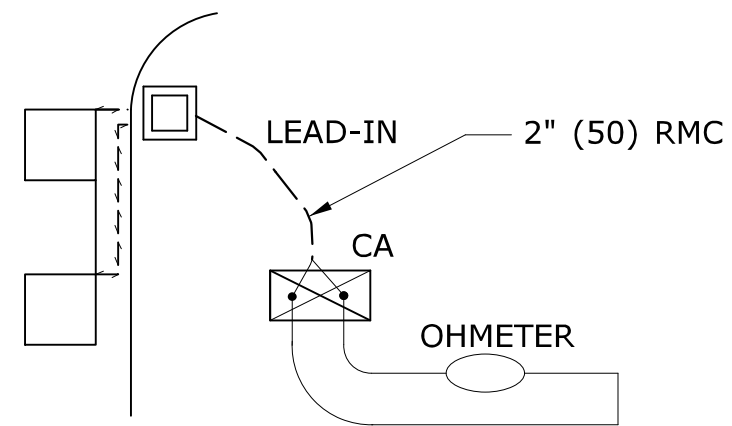
INDUCTIVE LOOP TEST PROCEDURE

PIN	COLOR	FUNCTION
A	WHITE	110 VAC Neutral
B	BROWN	Output Relay Common (moving contact)
C	BLACK	110 VAC (Fused)
D	RED	Loop
E	ORANGE	Loop
F	YELLOW	Output Relay Contact (Closes with moving contact when detecting vehicle)
G	BLUE	Output Relay Contact (Opens with moving contact when detecting vehicle)
H	GREEN	Chassis Ground
J	GREY	110 VAC Delay/Extend Override
Shell		Ground (shall be connected to pin H in the connector)

DETECTOR AMPLIFIER PIN DESIGNATION



TEST 1a



TEST 1b

LOOP NUMBER	RESISTANCE OHMS		INDUCTANCE MICROHENRIES (μh)		AMPLIFIER POWER INTERRUPTION PASS/FAIL (3)
	TO GROUND (1a)	LOOP WIRE (1b)	CALCULATED (2a)	MEASURED (2b)	
D1 FRONT					
D1 REAR					
D2A					
D2B					
D4A FRONT					
D4B REAR					
D5					
D6A					
D6B					

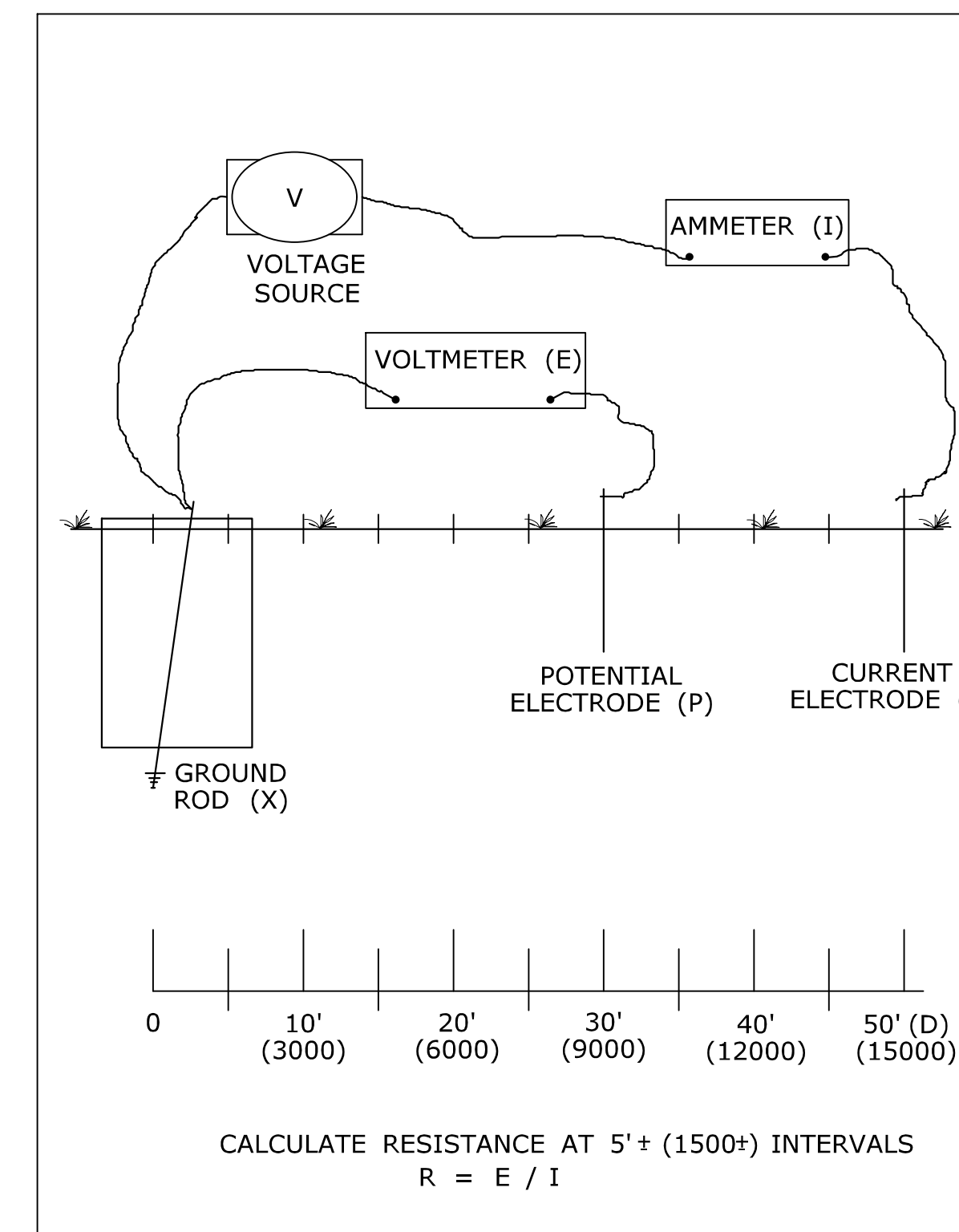
LOOP CIRCUIT TEST DATA (EXAMPLE)

TEST PROCEDURE:

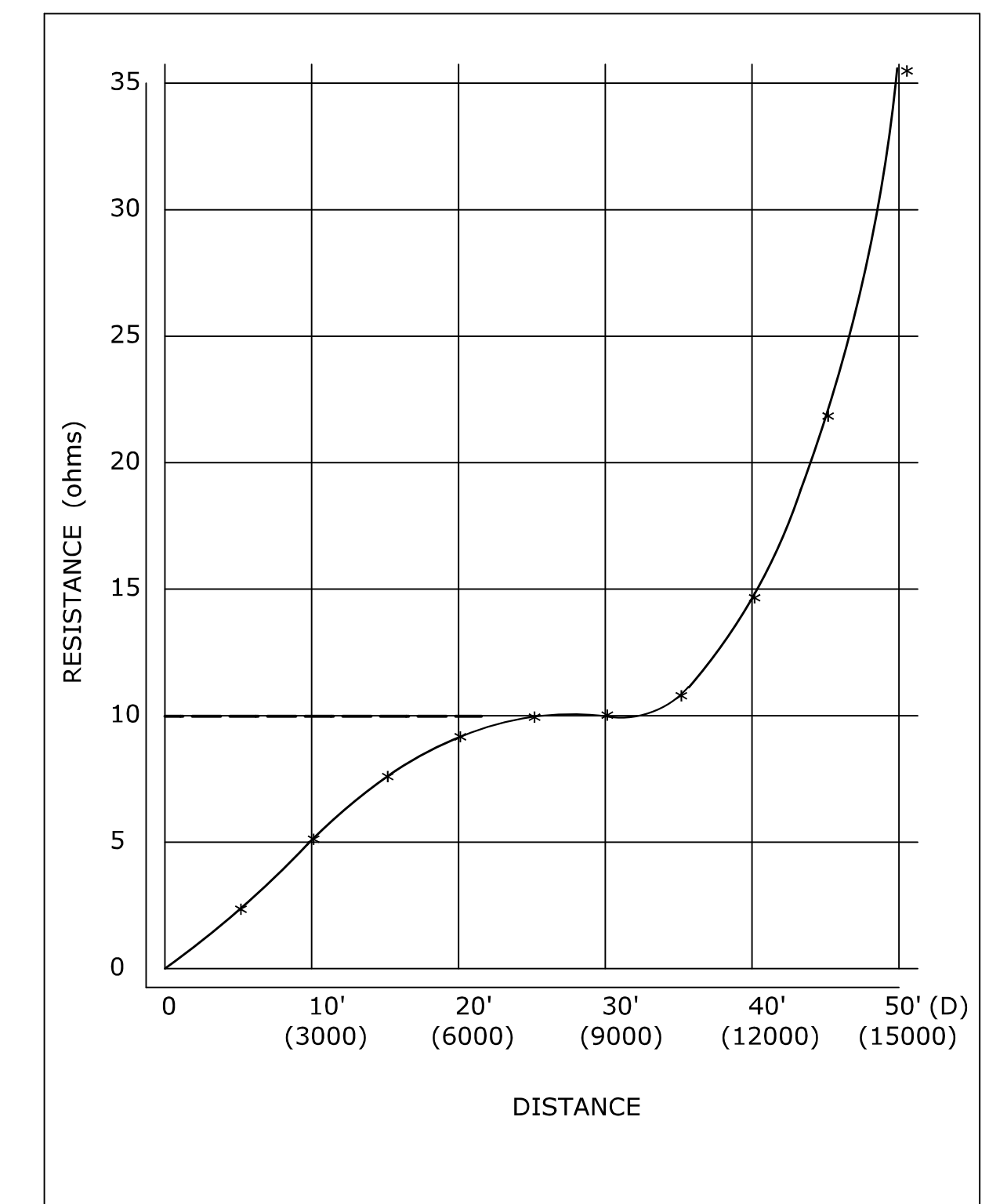
- INSERT ELECTRODE (C) A DISTANCE (D) FROM THE FOUNDATION. RECOMMEND A MINIMUM 50'.
- CONNECT A VOLTAGE SOURCE AND AMMETER BETWEEN THE FOUNDATION GROUND ROD (X) AND C.
- MEASURE THE CURRENT FLOW (I) BETWEEN X AND C.
- INSERT POTENTIAL ELECTRODE (P) AT 5' (1500) INTERVALS IN A STRAIGHT LINE TO ELECTRODE C.
- MEASURE VOLTAGE (E) AT EACH LOCATION OF P.
- CALCULATE RESISTANCE (R) AT EACH LOCATION OF P USING THE FORMULA $R = E/I$.
- PLOT THE VALUES ON A RxD GROUND RESISTANCE CHART.
- THE ACTUAL GROUND RESISTANCE IS WHERE THE PLOTTED CURVE IS RELATIVELY FLAT, USUALLY AT 62%± OF D.
- SEE EXAMPLE CHART: CURVE FLATTENS OUT AT 10 OHMS, APPROXIMATELY 30' (9000) FROM FOUNDATION.
- IF GROUND RESISTANCE IS GREATER THAN 10 OHMS, PERFORM CORRECTIVE ACTION AND RE-TEST.

SUGGESTED CORRECTIVE ACTION:

- A. INSTALL ADDITIONAL 10' (3000) GROUND ROD(S). REFER TO NESC SECTION 09, RULE 94.B.2. DRIVE ADDITIONAL GROUND RODS NO CLOSER TO FOUNDATION THAN 6' (1800). IF MORE THAN ONE IS NEEDED, SPACE MINIMUM 6' (1800) APART. BONDS TO ADDITIONAL GROUND ROD(S) SHALL BE MADE BY A CLAMP DESIGN FOR DIRECT BURIAL OR BY EXOTHERMIC WELDING TECHNIQUE. TOP OF ADDITIONAL GROUND ROD(S) SHALL BE 6" (150) BELOW GRADE.
- B. IN AREAS OF SHALLOW BEDROCK, INSTALL A GROUND GRID OR ARRAY CONSISTING OF BURIED WIRE, RODS, STRIPS OR PLATES. REFER TO NESC SECTION 09, RULE 94.B.3. REFER TO NEC SECTION 250. MINIMUM DEPTH OF 18" (450). GRID CONNECTIONS AND BONDS ON GROUND GRID SHALL BE MADE BY CLAMPS DESIGNED FOR DIRECT BURIAL OR BY EXOTHERMIC WELDING TECHNIQUE.



3 POINT GROUND RESISTANCE TEST CIRCUIT



GROUND RESISTANCE CHART (EXAMPLE)

NOTES:

1. WHEN REQUESTED BY THE ENGINEER, MEASURE RESISTANCE-TO-GROUND OF GROUND ROD AT TRAFFIC CONTROL FOUNDATIONS. SEE FALL-OF-POTENTIAL METHOD. IF LESS THAN 10 ohms, INSTALL SUPPLEMENTAL ELECTRODES AS REQUIRED. NEC ARTICLE 250.
2. DURING THE TEST, THE GROUND ROD SHOULD NOT BE BONDED TO ANY RMC IN THE FOUNDATION.
3. THE VOLTAGE SOURCE, VOLTMETER, AMMETER, ELECTRODES P AND C, AND CONNECTING CABLES ARE AVAILABLE AS A SPECIALIZED TEST INSTRUMENT.
4. REFER TO NATIONAL ELECTRICAL SAFETY CODE (NESC) SECTION 09, GROUNDING METHODS FOR ELECTRIC SUPPLY AND COMMUNICATIONS FACILITIES.
5. REFER TO NATIONAL ELECTRICAL CODE (NEC) CHAPTER 2, ARTICLE 250, GROUNDING.

3 POINT FALL-OF-POTENTIAL GROUND RESISTANCE TEST

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

□	INDUCTIVE LOOP DETECTOR
---	SAW CUT
—	RIGID METAL CONDUIT
□	HANDHOLE

REV.	DATE	REVISION DESCRIPTION
2	1-2014	REVISED GROUND RESISTANCE NOTES.
1	4-2012	MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 1/7/2014

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

File name: CTDOT_TRAFFIC_STD.DGN Model: TR-1000_01

SUBMITTED BY: NAME/DATE/TIME:
Tracy L. Fogarty Tracy L. Fogarty 2014.01.07 16:11:26-05'00'

APPROVED BY: NAME/DATE/TIME:
Charles S. Harlow Charles S. Harlow 2014.01.08 09:02:11-05'00'

CTDOT
STANDARD SHEET

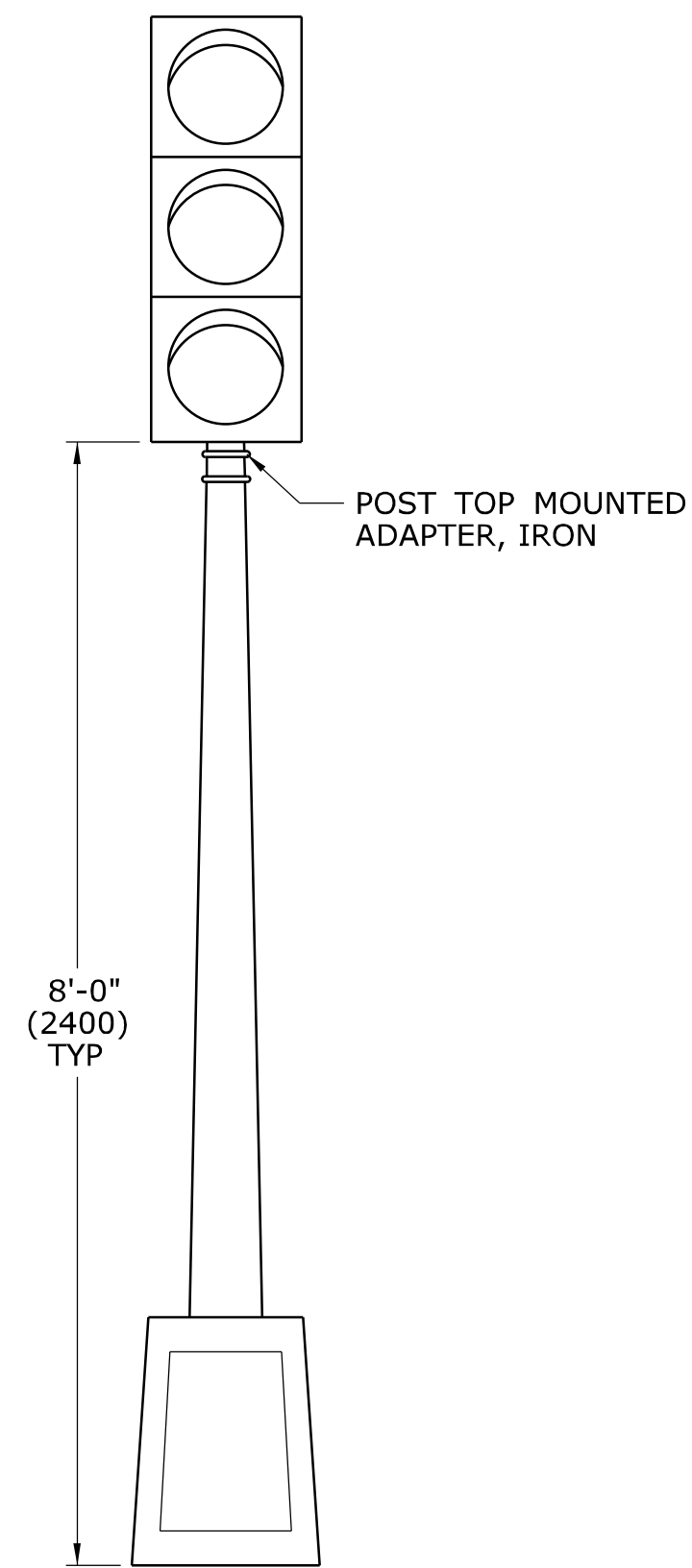
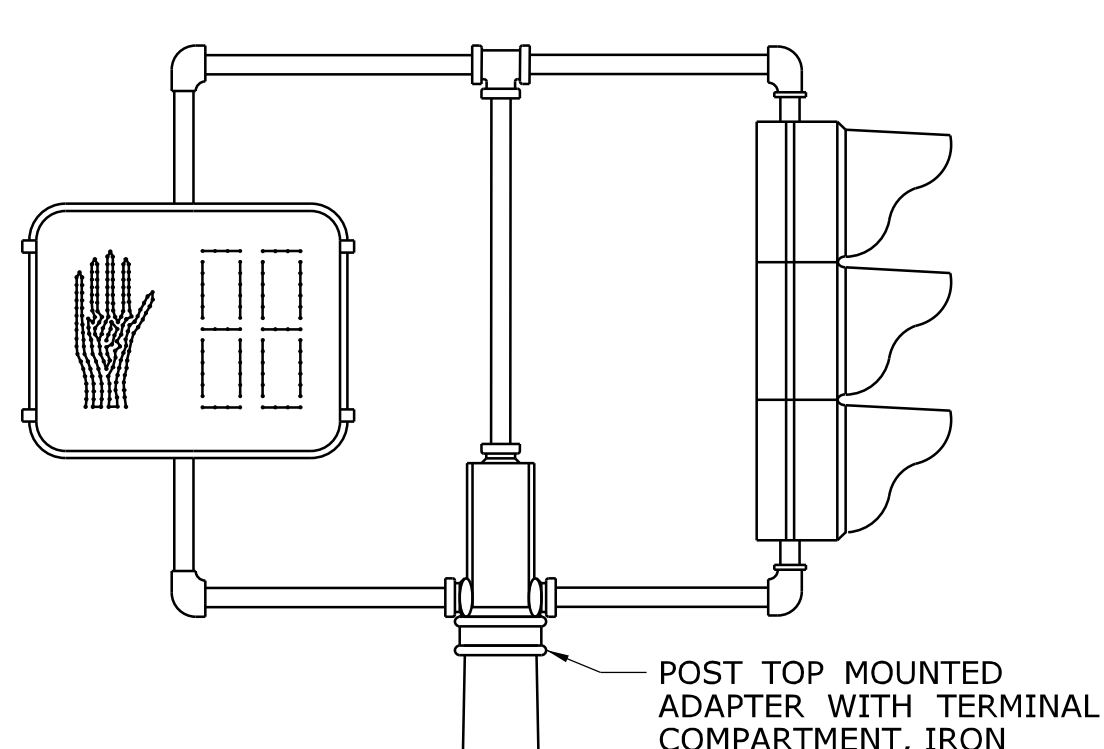
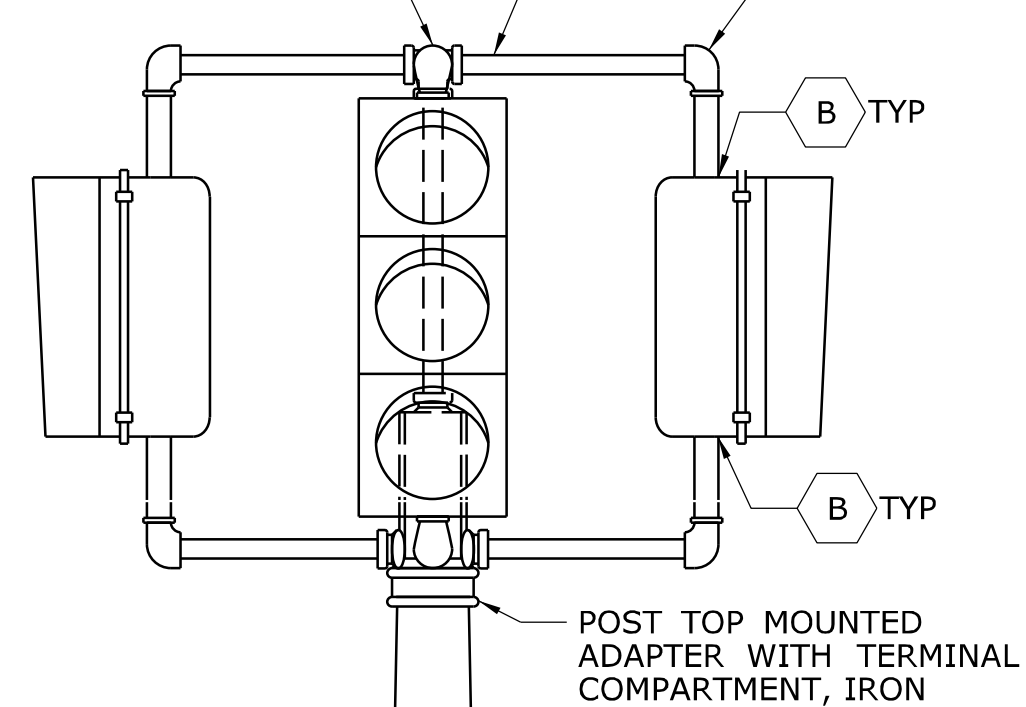
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
GENERAL CLAUSES (TEST PROCEDURES)

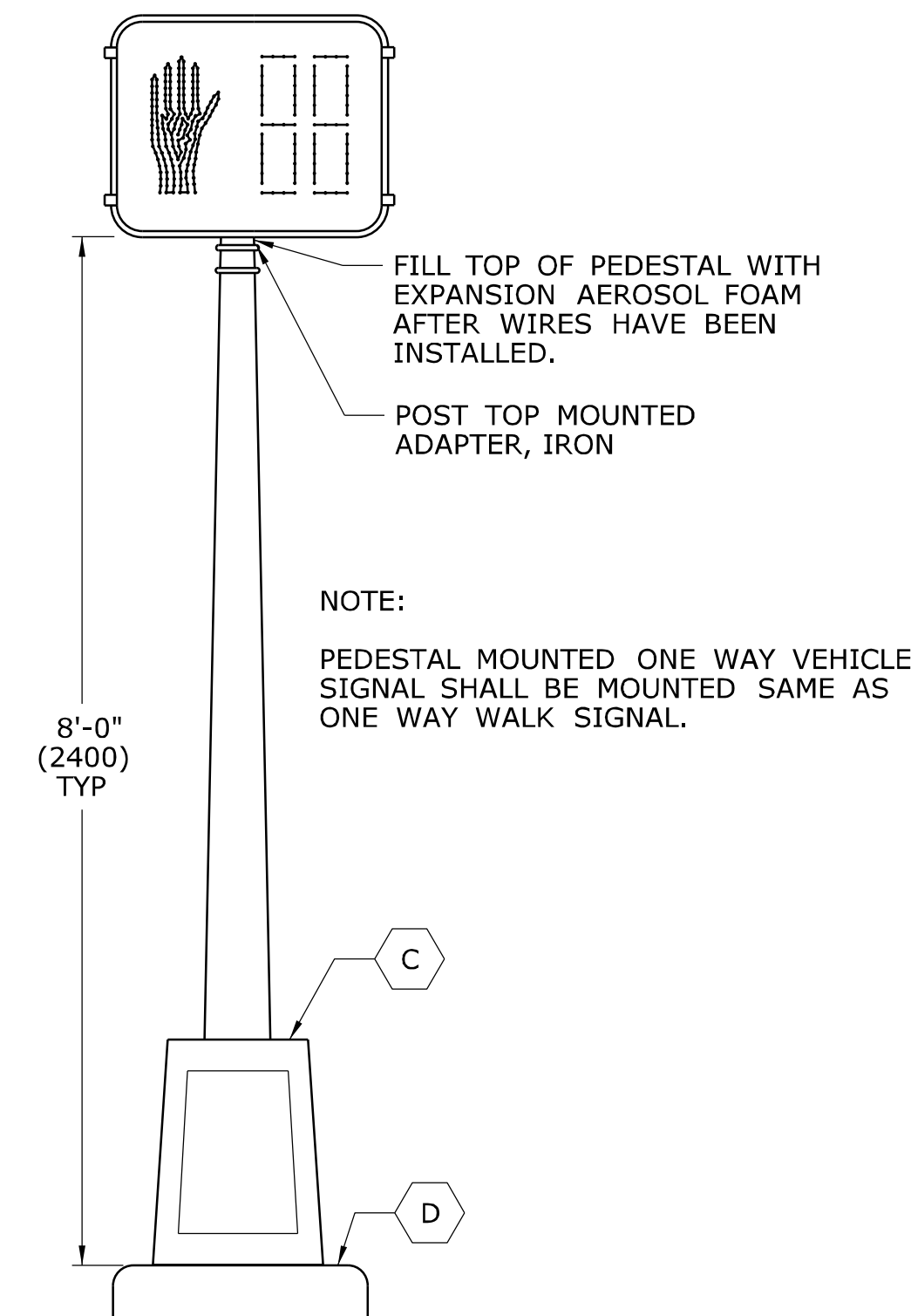
STANDARD SHEET NO.:
TR-1000_01

1 1/2" (38) SSIDE
OUTLET TEE, IRON, TYP

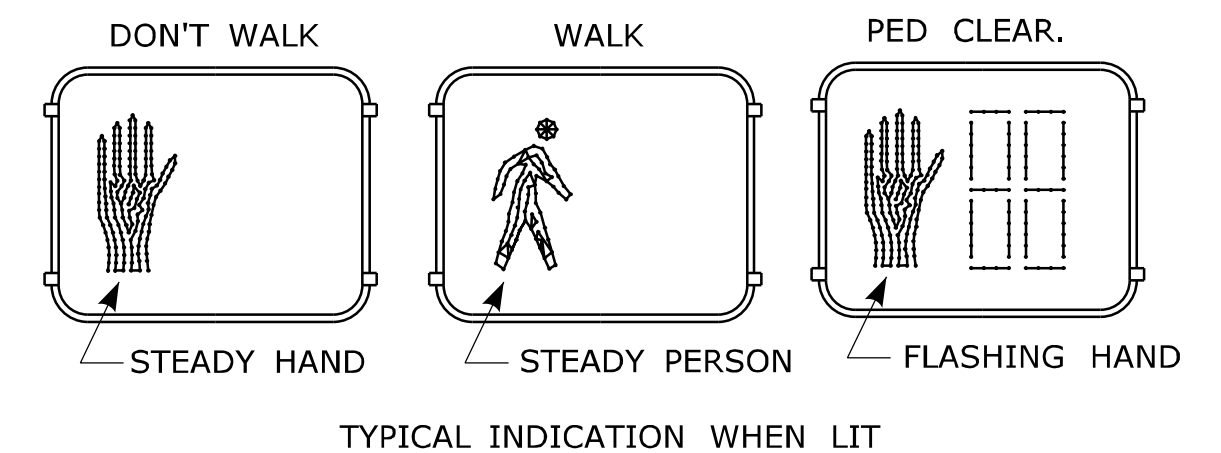
1 1/2" (38) NIPPLE, STEEL, TYP



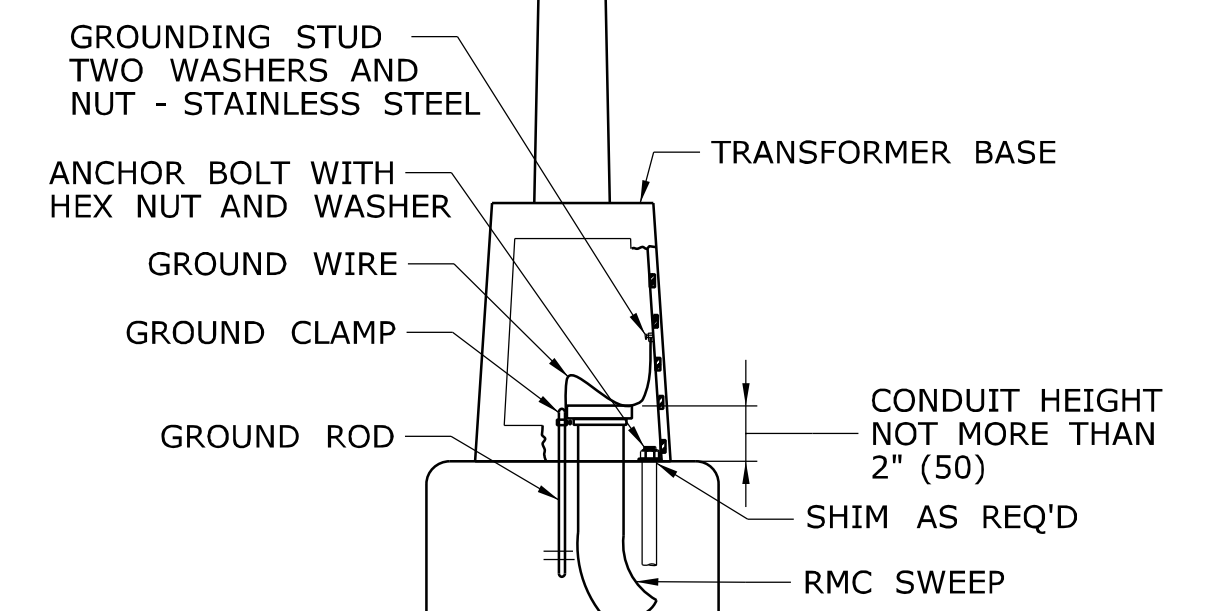
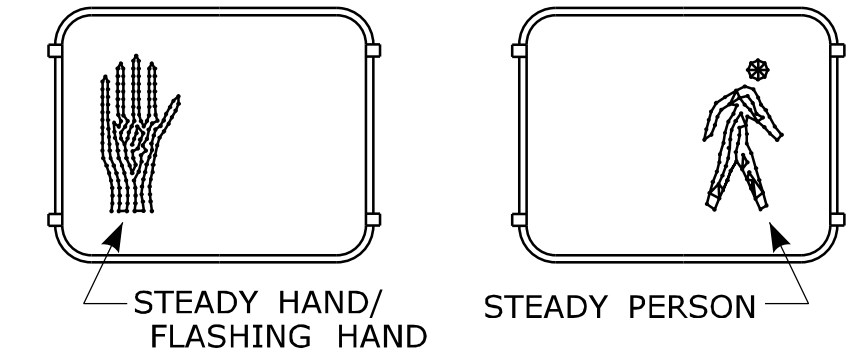
**ONE WAY TRAFFIC SIGNAL
PEDESTAL MOUNTED**



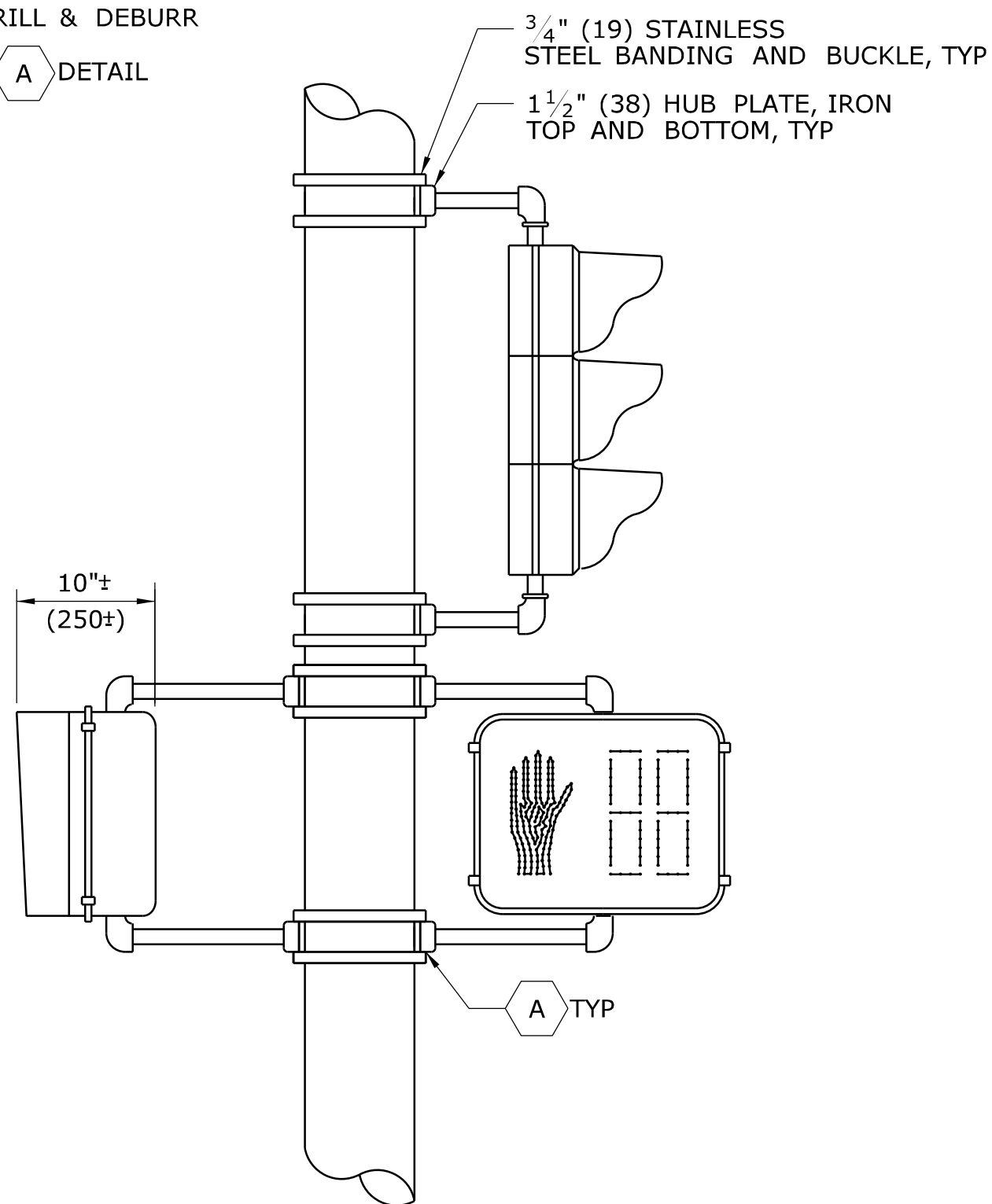
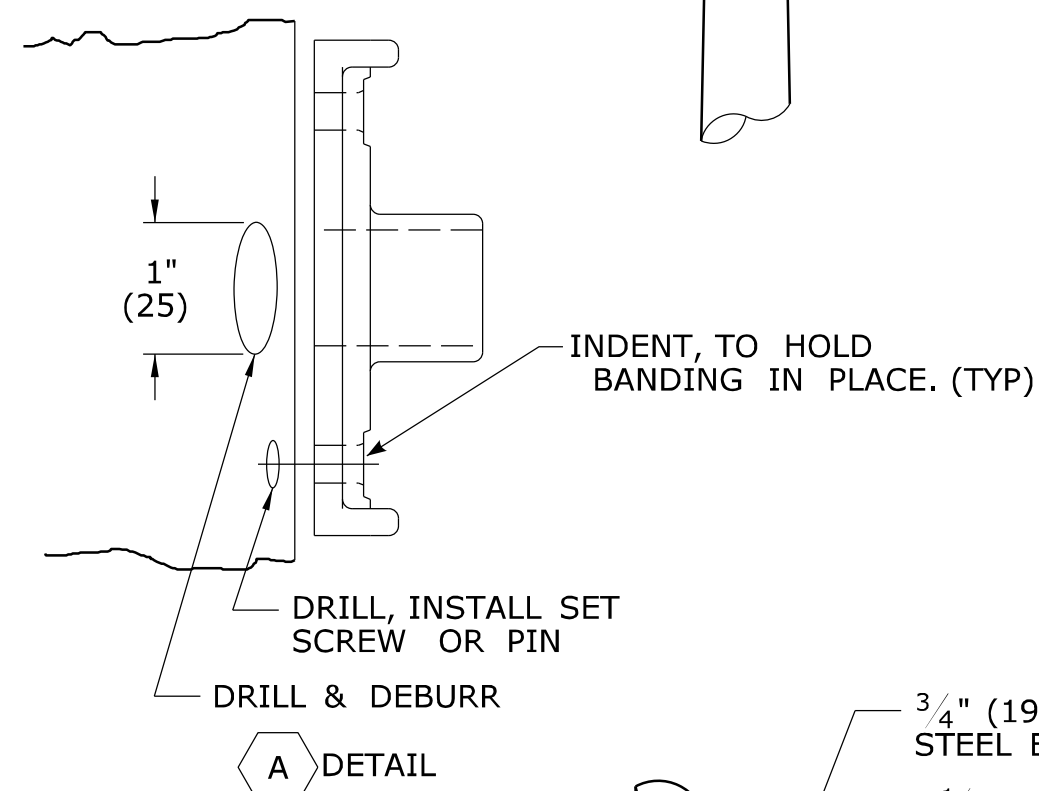
**ONE WAY WALK SIGNAL
PEDESTAL MOUNTED**



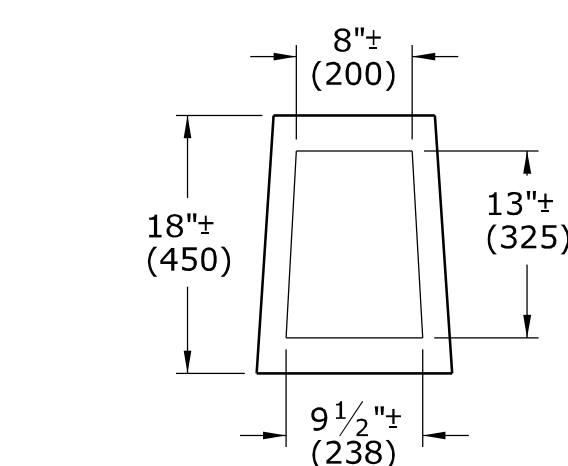
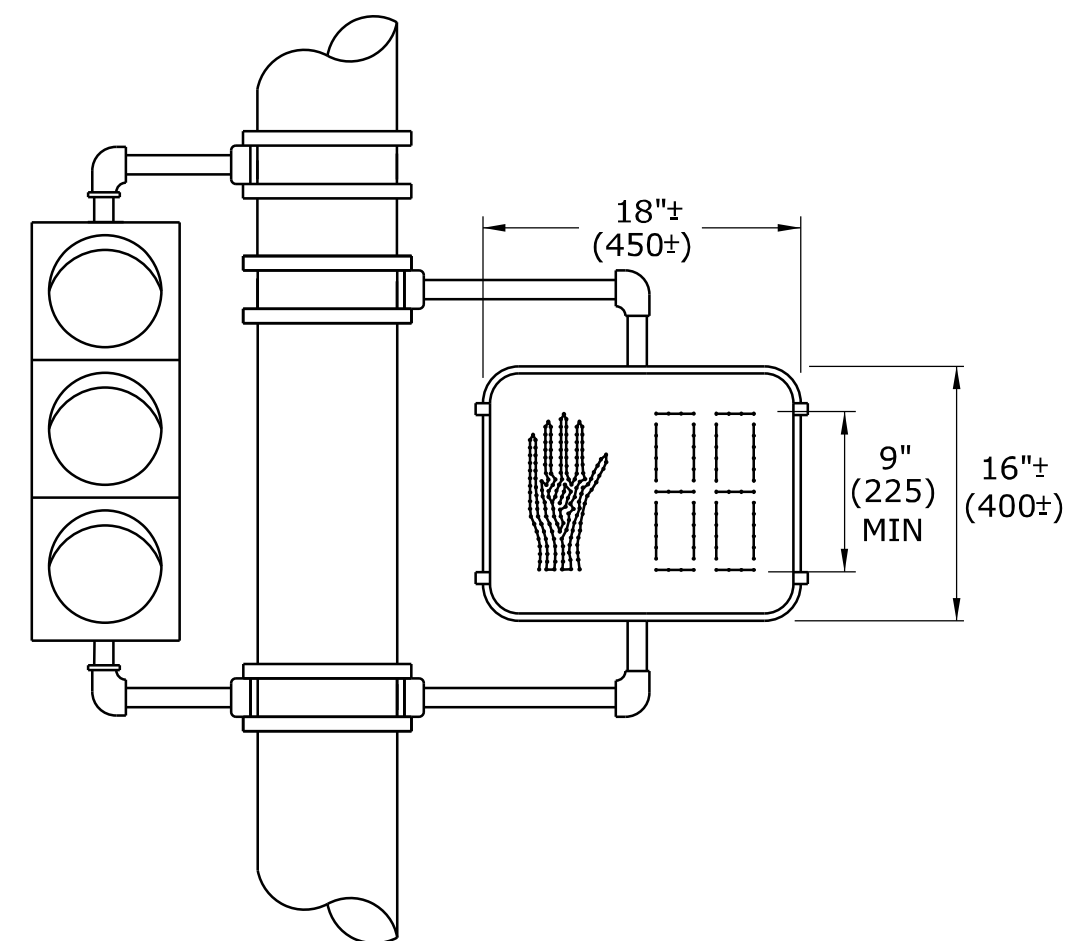
NON-COUNTDOWN DISPLAY, ONLY WHEN SHOWN ON PLAN.
DON'T WALK/PED CLEAR. WALK



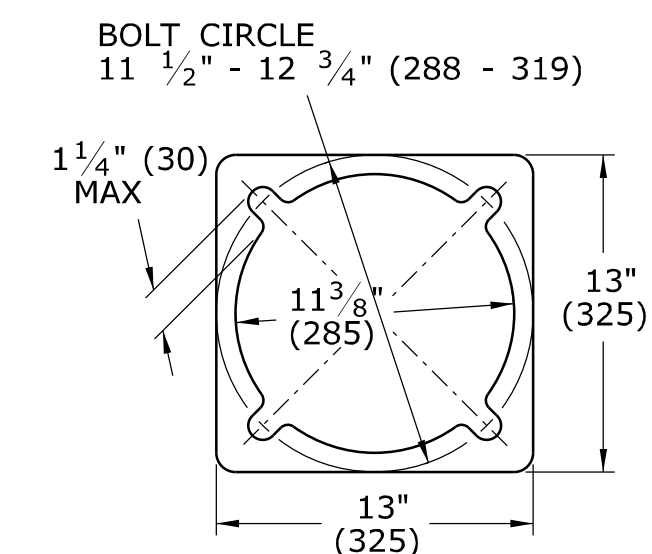
**ALUMINUM PEDESTAL
INSTALLATION DETAIL**



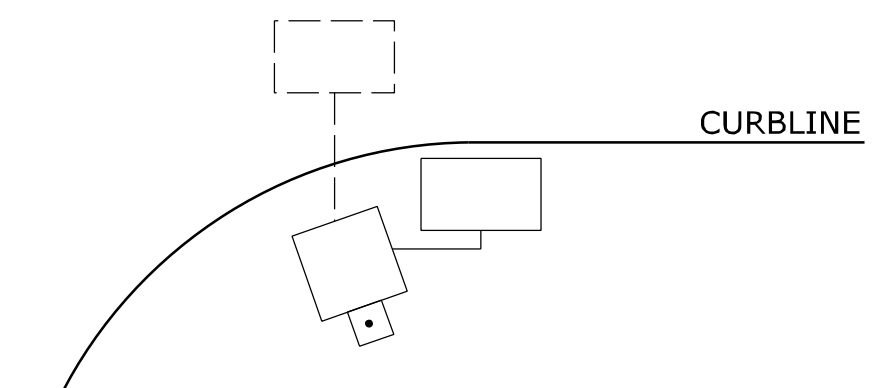
**ONE WAY TRAFFIC SIGNAL
POLE MOUNTED**



**ALUMINUM PEDESTAL
DOOR OPENING DETAIL**



PEDESTAL BASE PLAN



WHEN PEDESTALS OR SPAN POLES ARE INSTALLED CLOSE TO THE CURB,
SIDE MOUNT PEDESTRIAN OR TRAFFIC SIGNALS TO AVOID VISOR DAMAGE
FROM TURNING VEHICLES.

NOTES:

- A SECURE LOWER HUB PLATE WITH STAINLESS STEEL SET SCREW OR PIN PRIOR TO BANDING TO PREVENT MOVEMENT. INSTALL CABLE THROUGH BOTTOM OF HUB PLATE.
- B REFER TO CTDOT TRAFFIC STANDARD SHEET, TR-1105.01, TRAFFIC SIGNALS & CABLE ASSIGNMENTS.
- C IF THREADED, MIN 1" (25) THREADED INTO BASE, SECURED WITH STAINLESS STEEL SET SCREWS.
- D BASE DESIGNED AS BREAK-AWAY.

INCANDESCENT WALK SIGNAL LAMPS ARE 67 WATTS, RATED AT 8000 HOURS LAMP LIFE.
LED WALK SIGNAL LAMPS ARE MAXIMUM 15 WATTS, WARRANTED AT 5 YEAR LIFE.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:		□ PEDESTRIAN SIGNAL
□	STEEL SPAN POLE, MAST ARM ASSEMBLY SHAFT	□ PEDESTAL MOUNTED, TRAFFIC & PEDESTRIAN SIGNALS
□	ALUMINUM PEDESTAL	□ POLE MOUNTED, TRAFFIC & PEDESTRIAN SIGNALS
□	TRAFFIC SIGNAL	

2	4-2012	MINOR REVISIONS.
1	1-2010	INCLUDED COUNTDOWN PEDESTRIAN SIGNALS.
REV.	DATE	REVISION DESCRIPTION

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- UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE



Filename: CTDOT_TRAFFIC_STD.dgn Model: TR-1102_01

SUBMITTED BY: NAME/DATE/TIME:
Tracy L. Fogarty Tracy L. Fogarty
2012.05.01 12:55:27-04'00'

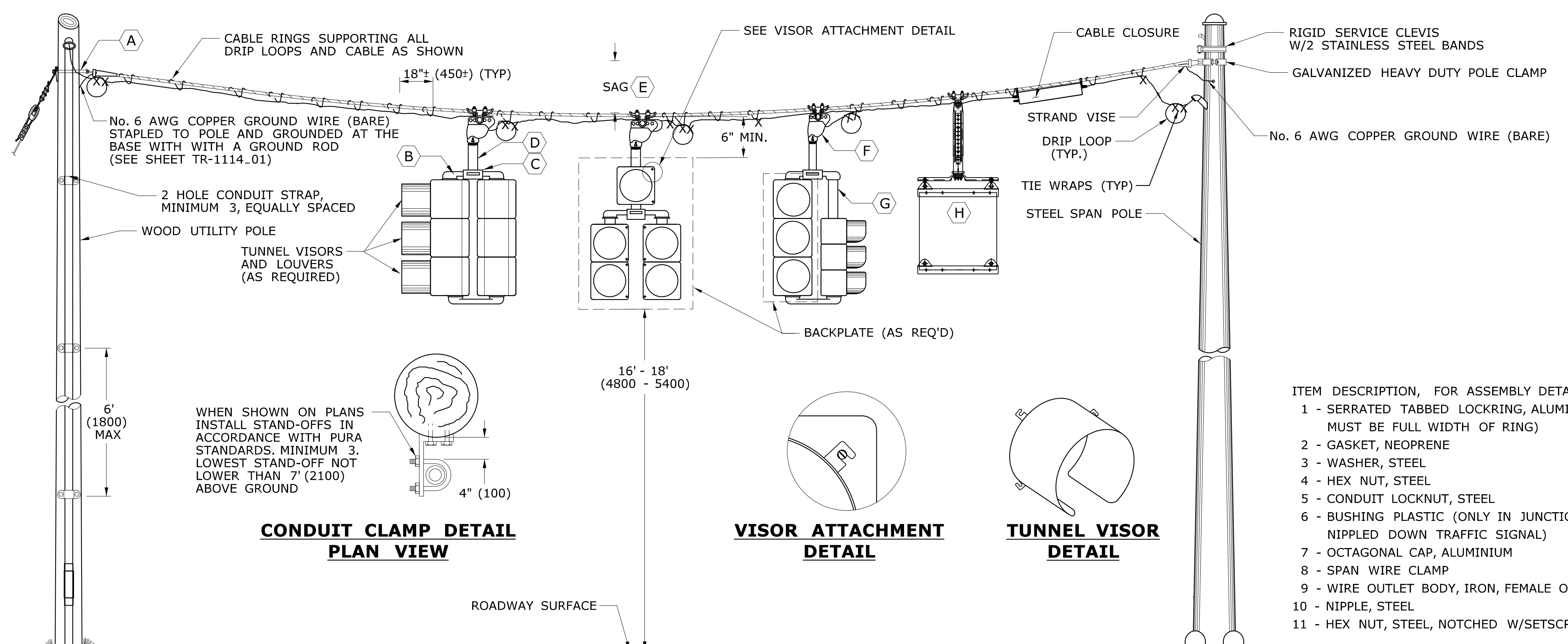
APPROVED BY: NAME/DATE/TIME:
Timothy M. Wilson Timothy M. Wilson
2012.05.09 10:24:58-04'00'

**CTDOT
STANDARD SHEET**

OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
PEDESTALS, PEDESTRIAN SIGNALS

STANDARD SHEET NO.:
TR-1102_01



TRAFFIC SIGNAL CABLE COLOR ASSIGNMENTS					
SIGNAL ASSEMBLY & CABLE USED	SIGNAL FUNCTION	ARTERY 1	ARTERY 2	SIDE STREET 1	SIDE STREET 2
2 - WAY 9 CONDUCTOR	RED	RED		BLACK	
	YELLOW	ORANGE		WHITE \ BLACK	
	GREEN	GREEN		BLUE	
	SPARE	GREEN\BLACK		RED \ BLACK	
	NEUTRAL	WHITE			
3 - WAY 12 CONDUCTOR	RED	RED	RED \ BLACK	BLACK	
	YELLOW	ORANGE	ORANGE \ BLACK	WHITE \ BLACK	
	GREEN	GREEN	GREEN \ BLACK	BLUE	
	SPARE	BLUE\BLACK	BLACK \ WHITE		
	NEUTRAL	WHITE			
4 - WAY 15 CONDUCTOR	RED	RED	RED \ BLACK	BLACK	RED \ WHITE
	YELLOW	ORANGE	ORANGE \ BLACK	WHITE \ BLACK	BLACK \ WHITE
	GREEN	GREEN	GREEN \ BLACK	BLUE	GREEN \ WHITE
	SPARE	BLUE\BLACK		BLUE \ WHITE	
	NEUTRAL	WHITE			

PEDESTRIAN SIGNAL CABLE COLOR ASSIGNMENTS		
SIGNAL ASSEMBLY & CABLE USED	SIGNAL FUNCTION	WIRE COLOR
WALK SIGNAL W/ PUSHBUTTON 7 CONDUCTOR	DON'T WALK	RED
	WALK	GREEN
	NEUTRAL FOR WALK SIGNAL	WHITE
	PEDESTRIAN PUSHBUTTON	BLACK
	NEUTRAL FOR PUSHBUTTON	ORANGE
WALK SIGNAL W/ PUSHBUTTON 7 CONDUCTOR	SPARE CONDUCTOR	WHITE \ BLACK
	SPARE CONDUCTOR *	BLUE \ BLACK
	RED	RED
	YELLOW	ORANGE
	GREEN	GREEN
NEUTRAL FOR TRAFFIC SIGNAL PEDESTRIAN PUSHBUTTON NEUTRAL FOR PUSHBUTTON SPARE CONDUCTOR *	WHITE	WHITE
	BLACK	BLACK
	WHITE \ BLACK	WHITE \ BLACK
	BLUE \ BLACK	BLUE \ BLACK
	BLUE \ BLACK	BLUE \ BLACK

- ITEM DESCRIPTION, FOR ASSEMBLY DETAILS
- 1 - SERRATED TABBED LOCKRING, ALUMINUM (TAB MUST BE FULL WIDTH OF RING)
 - 2 - GASKET, NEOPRENE
 - 3 - WASHER, STEEL
 - 4 - HEX NUT, STEEL
 - 5 - CONDUIT LOCKNUT, STEEL
 - 6 - BUSHING PLASTIC (ONLY IN JUNCTION BOX OR NIPPLED DOWN TRAFFIC SIGNAL)
 - 7 - OCTAGONAL CAP, ALUMINIUM
 - 8 - SPAN WIRE CLAMP
 - 9 - WIRE OUTLET BODY, IRON, FEMALE ONLY
 - 10 - NIPPLE, STEEL
 - 11 - HEX NUT, STEEL, NOTCHED W/SETSCREWS

* IF 14/7 FEEDS MORE THAN ONE BUTTON, SPLIT THE BUTTONS AND USE BLUE WITH BLACK TRACER FOR THE ADDITIONAL BUTTON.

TABLE NOTES:

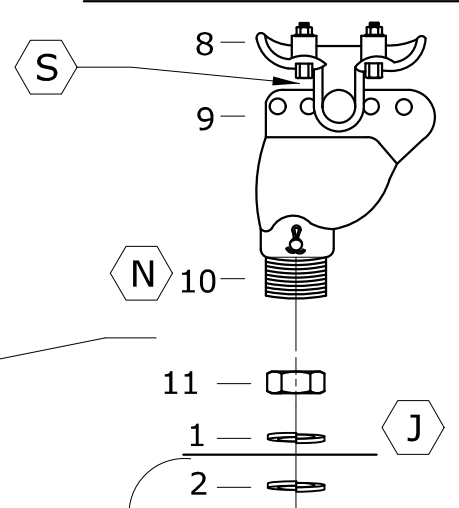
1. INSTALL SEPARATE CABLE BETWEEN CLOSURE AND EACH TRAFFIC SIGNAL ASSEMBLY. WIRE EACH TRAFFIC SIGNAL SECTION SEPARATELY BACK TO CABLE CLOSURE. JUMPERS BETWEEN TERMINALS ARE NOT ALLOWED EXCEPT ON NEUTRAL CONDUCTORS.
2. WIRE ALL SIGNALS, SAME DIRECTION FROM CONTROLLER, SEPARATELY WITH CONDUCTORS IN 21 CONDUCTOR CABLE, EVEN IF INDICATIONS ARE IDENTICAL.
3. CABLES THAT FEED PEDESTRIAN INDICATIONS, PUSH BUTTONS, AND DETECTORS BYPASS CABLE CLOSURE.
4. REFER TO STANDARD SHEET TR-1113.01 FOR CABLE CLOSURE - TYPE A.

NOTES:

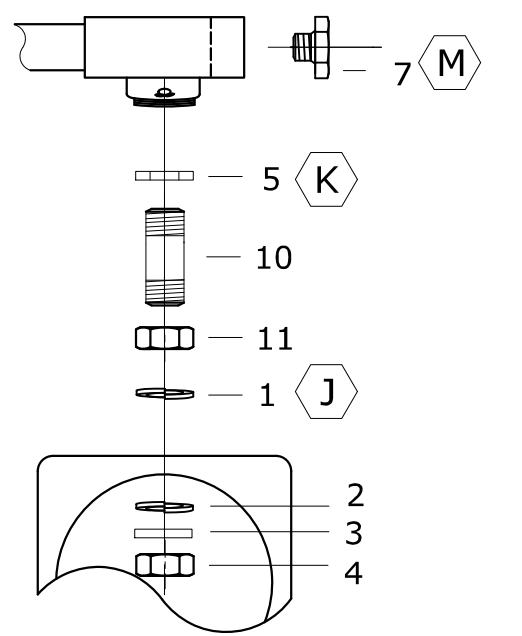
- SERVICE CONDUCTORS: THW, THWN OR XHHW. INDIVIDUAL WIRES MAY BE USED IN LIEU OF MULTI-CONDUCTOR CABLE.
- ALL WORK ON UTILITY POLES MUST COMPLY WITH CURRENT PURA REGULATIONS AND NESC RULES.
- A ATTACH SPAN AT LEAST 12" (300) BELOW LOWEST POWER COMPANY ATTACHMENT, AND AT LEAST 40" (1000) ABOVE HIGHEST COMMUNICATIONS ATTACHMENT, UNLESS OTHERWISE DIRECTED ON PLANS.
 - B ELBOW OR "T" FITTING MUST HAVE NOTCH FOR SERRATED TABBED LOCKRING.
 - C TOP BRACKET CENTER HUB SHALL BE MIN 4" (100) ROUND AND 3" (75) DEEP OR EQUAL VOLUME. SERRATION CAST IN HUB OR TABBED OR SERRATED LOCKRING, TOP OPENING NOT THREADED.
 - D NIPPLE LENGTH DEPENDS ON SPAN HEIGHT.
 - E SAG OF SPAN TO BE 5%± LENGTH, UNLESS OTHERWISE ALLOWED BY ENGINEER.
 - F FACE ALL ENTRANCE FITTINGS TOWARD CABLE CLOSURE.
 - G INSTALL EXTENSION NIPPLE ON TOP OF SIGNAL HOUSING SO BOTTOM OF ALL SIGNALS ARE EVEN.
 - H REFER TO TR-GS-01 "SIGN FACE SHEET ALUMINUM, R-SERIES SIGNS TYPICAL DETAILS", AND TO TR-1114.01 FOR SIGN HANGER ASSEMBLY. MAXIMUM SIGN SIZE 36" X 36" (900 X 900). ALL STAINLESS STEEL HARDWARE. SECURE LOUVERS TO TUNNEL VISORS WITH 3 STAINLESS STEEL SCREWS.

TRAFFIC SIGNALS SPAN WIRE SUSPENDED

SPAN WIRE HANGER ASSEMBLY DETAIL

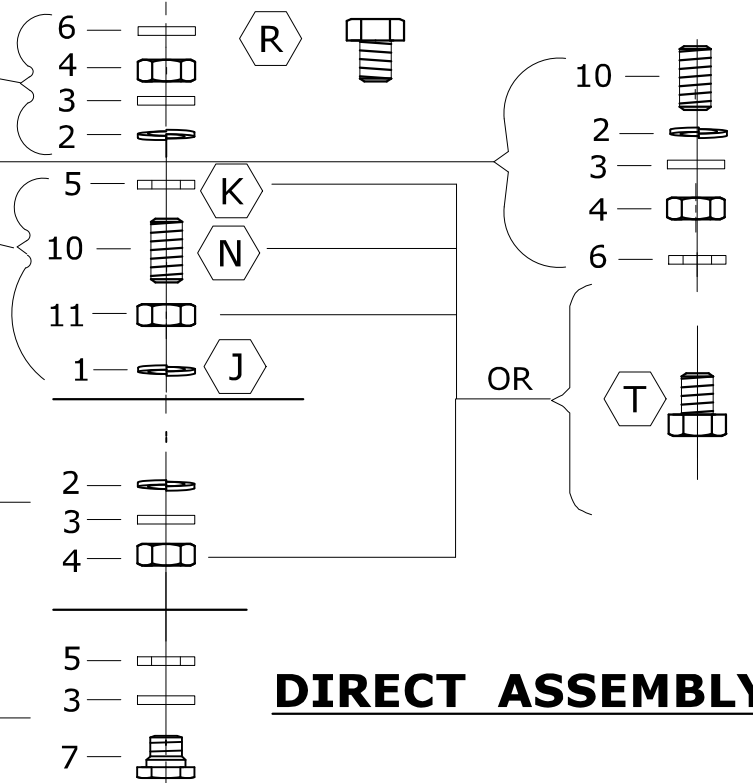


TWO WAY, THREE WAY & FOUR WAY NIPPLE DOWN ASSEMBLY DETAIL

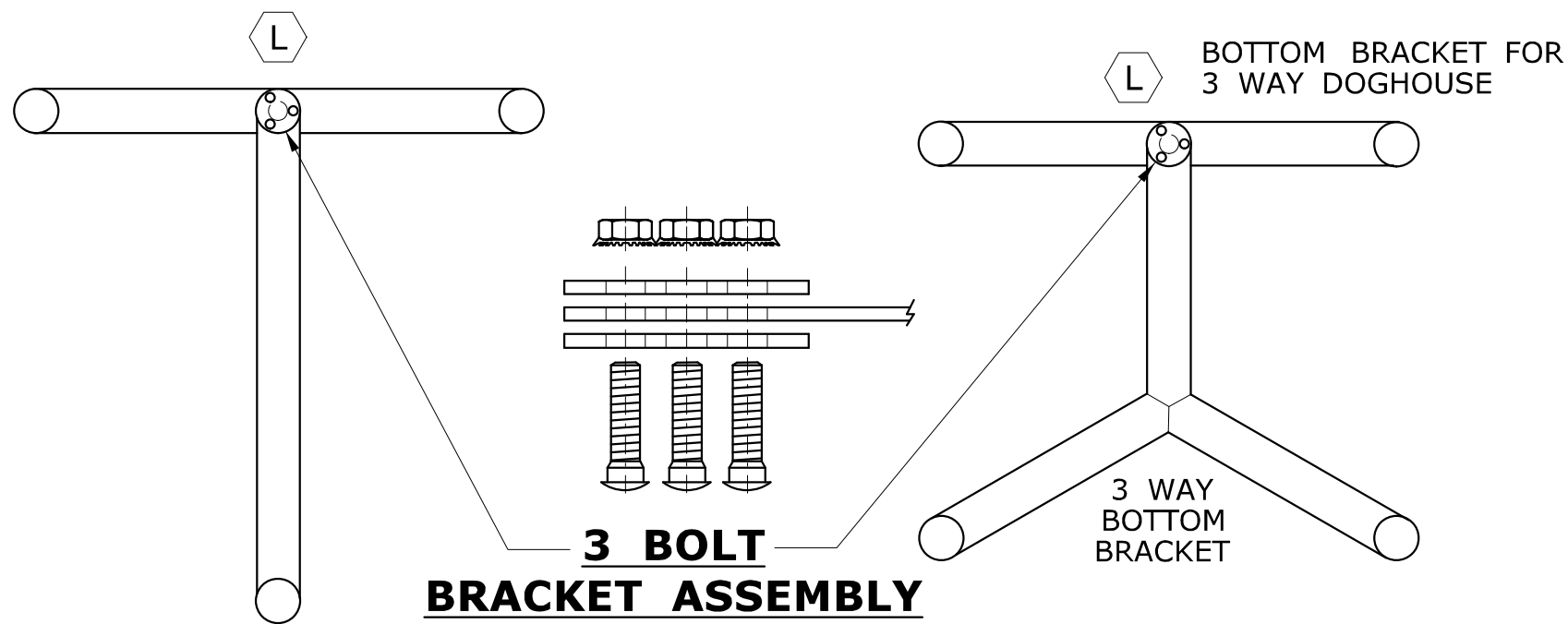
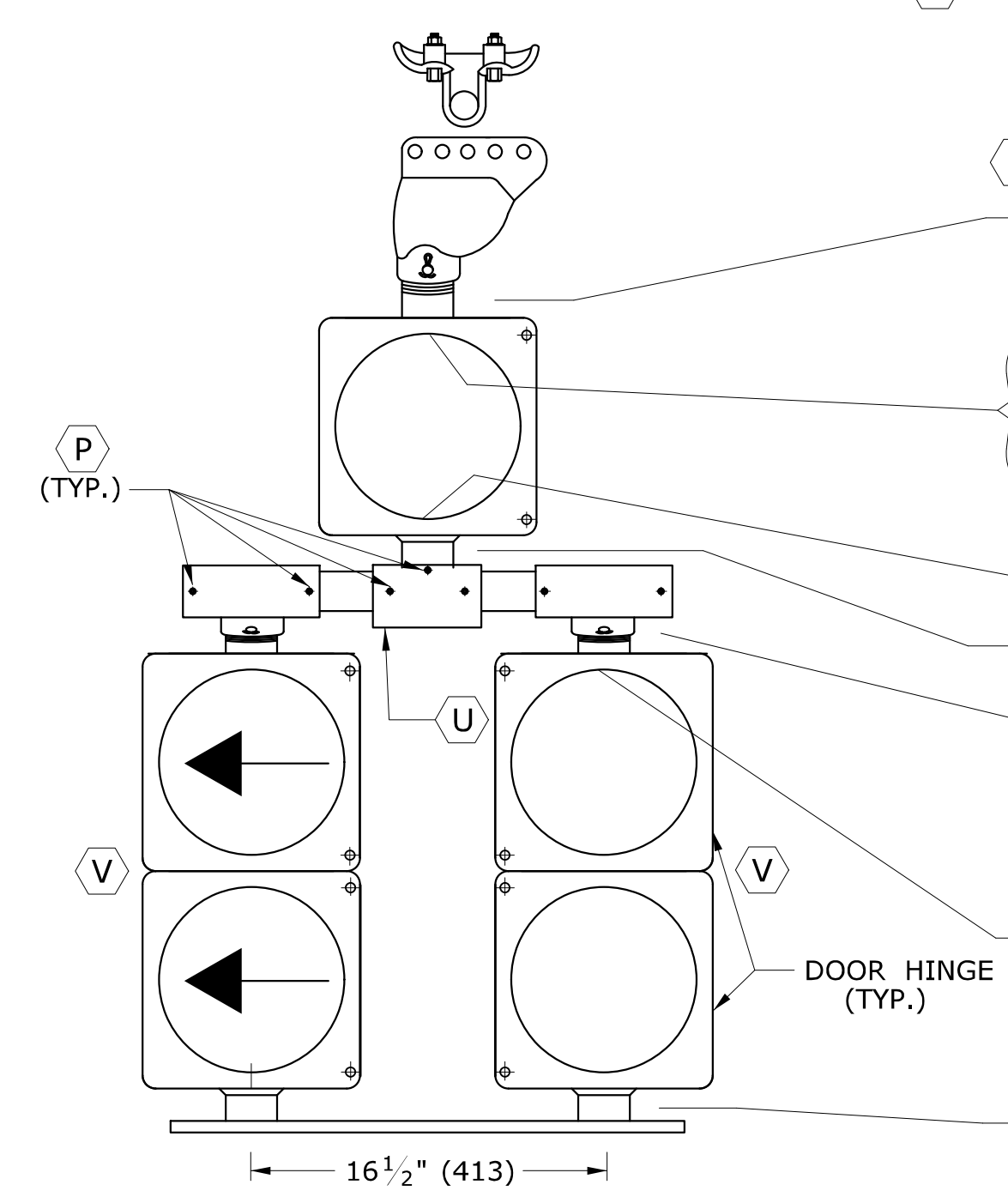


- NOTES: FOR ASSEMBLY DETAILS
- J APPLY SILICONE CAULK BETWEEN OR AROUND SERRATED LOCKRING AND HOUSING.
 - K OPTIONAL USE IF NIPPLE THREADS TOO FAR INTO ELBOW.
 - L DRILL HOLE IN CENTER OF 2 WAY BOTTOM BRACKET - INSTALL 3 BOLT BRACKET (SEE DETAIL).
 - M DO NOT INSERT ORNAMENTAL CAP PAST DOTTED LINE.
 - N ALL THREAD.
 - P SETSCREW (SQUARE OR ALLEN) ON ALL FITTINGS.
 - R CHASE NIPPLE CAN BE SUBSTITUTED FOR THE COMBINATION OF ITEMS 6, 5 AND 10.
 - S INSTALL STAINLESS STEEL WASHER ON INSIDE OF COTTER PIN. COTTER PIN AND WASHER SHALL BE ON SIDE OF HANGER AWAY FROM SIGNAL CABLES.
 - T CHASE NIPPLE CAN BE SUBSTITUTED FOR COMBINATION 4, 5, 10 AND 11.
 - U CENTER HUB SAME AS (C) EXCEPT TOP OPENING MAY BE THREADED.
 - V DOOR HINGE ON OUTSIDE OF SIDE BY SIDE ASSEMBLY.

UPPER CENTER SUPPORT DETAIL



DIRECT ASSEMBLY DETAIL



BOTTOM "T" BRACKET FOR 2 WAY DOGHOUSE

REV.	DATE	REVISION DESCRIPTION
4	1-2018	REVISED GROUNDING NOTE FOR SPAN AND OTHER MINOR REVISIONS.
3	3-2015	REMOVED STRAIN INSULATOR.
2	5-2013	MINOR REVISIONS.
1	4-2012	MINOR REVISIONS.

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NOT TO SCALE

STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION

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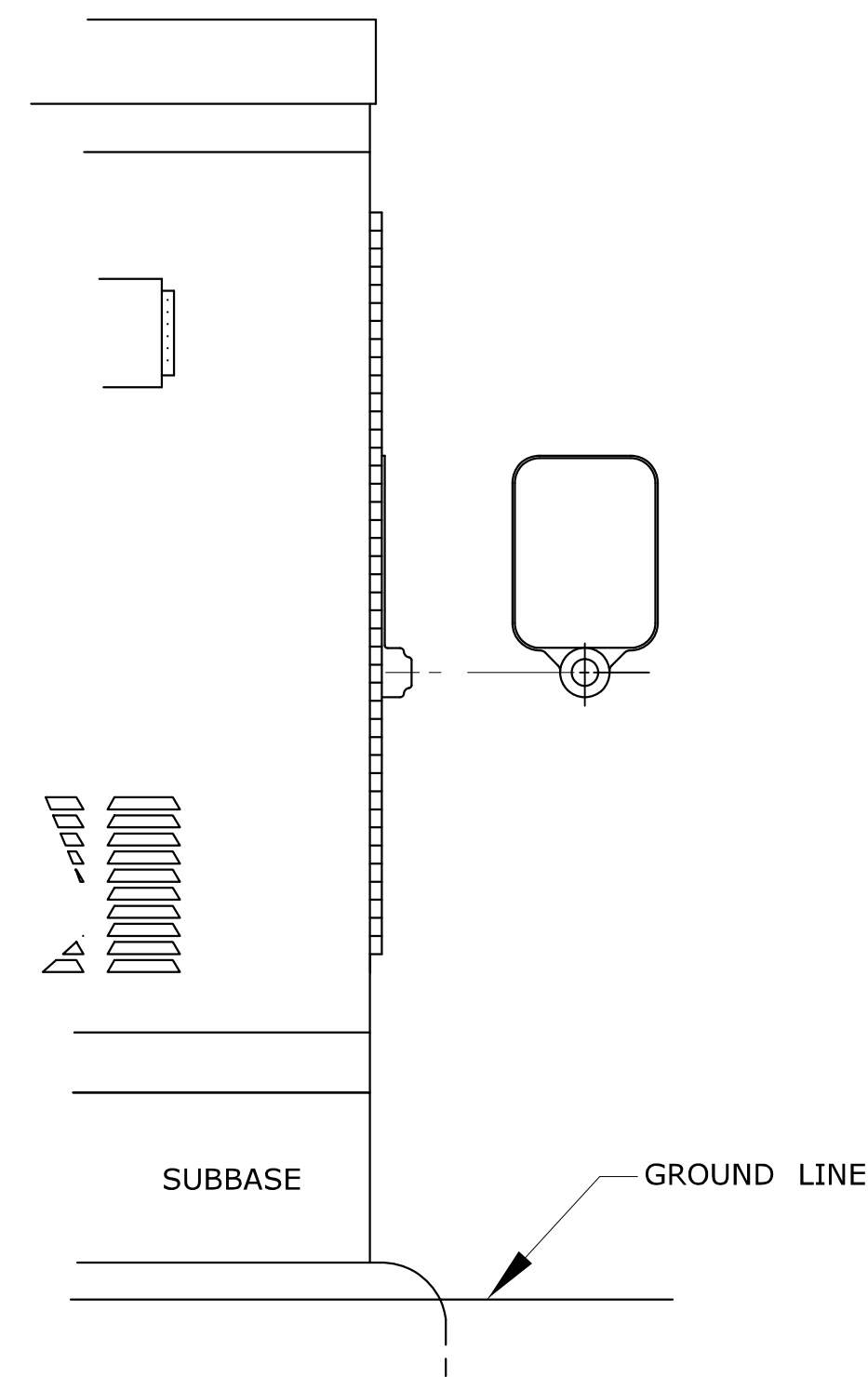
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APPROVED BY: NAME/DATE/TIME: Mark F. Carlino, P.E. 2018.08.21 07:46:03-04'00'

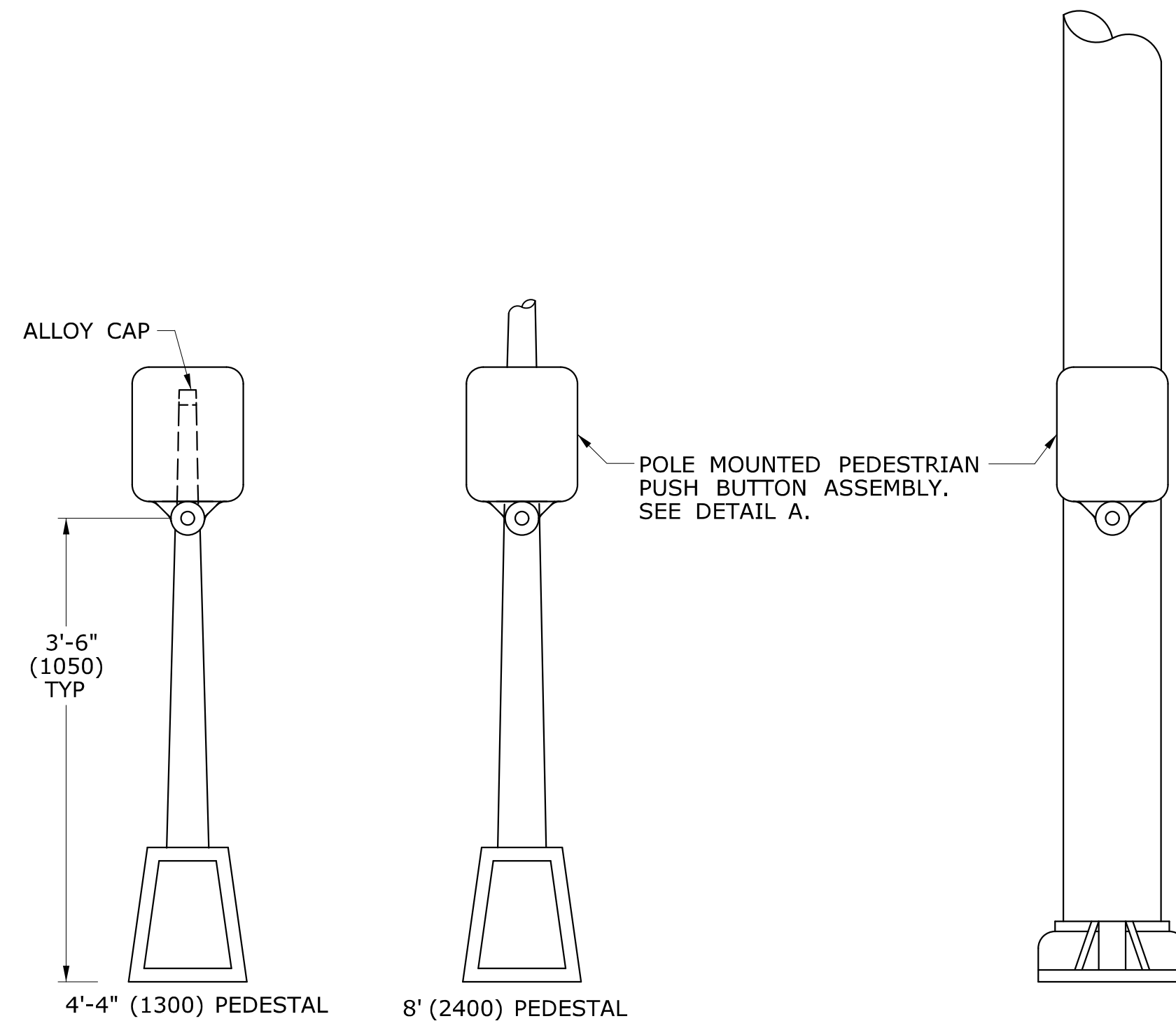
CTDOT STANDARD SHEET OFFICE OF ENGINEERING

STANDARD SHEET TITLE: TRAFFIC SIGNALS & CABLE ASSIGNMENTS

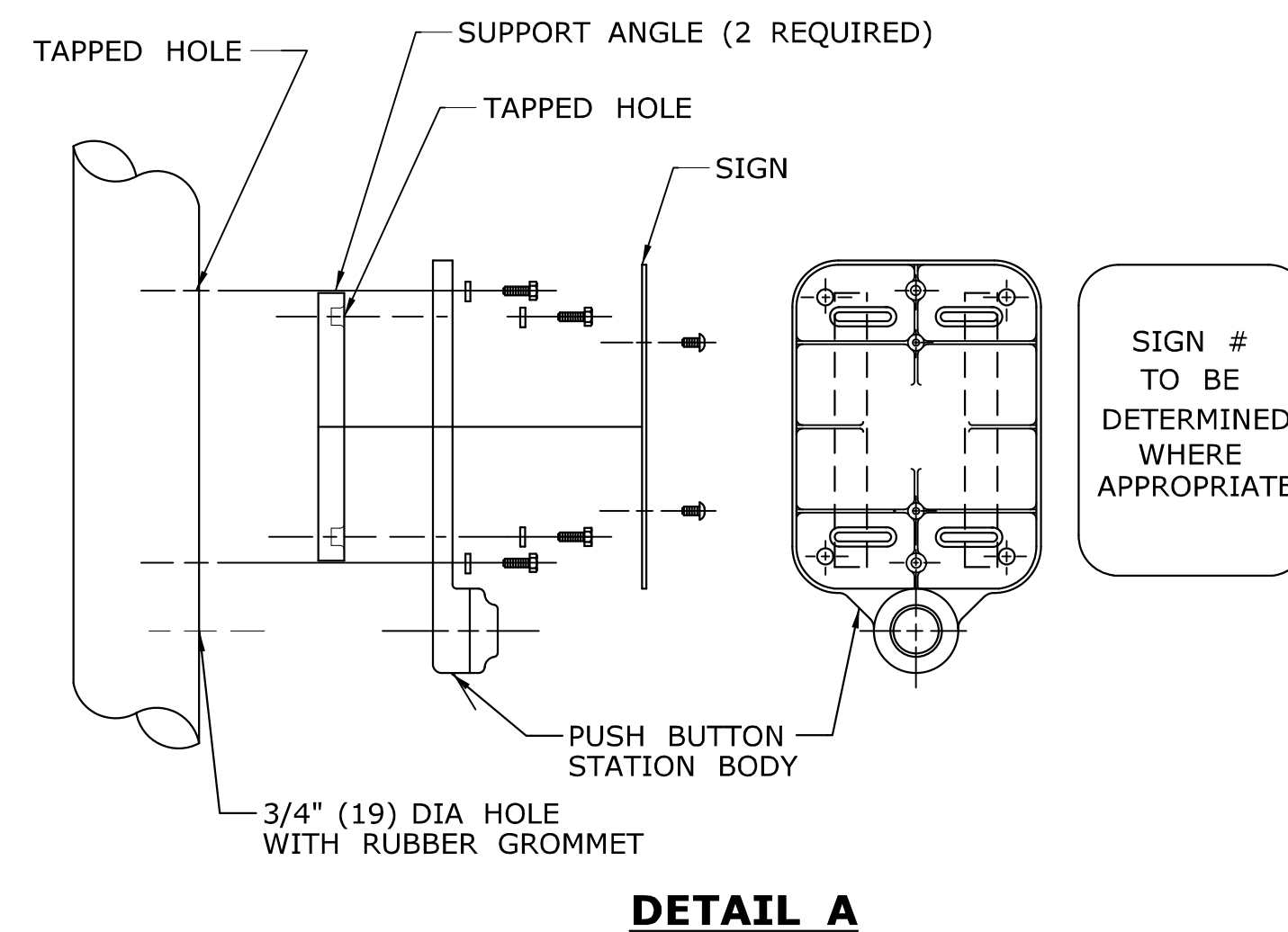
STANDARD SHEET NO.: TR-1105_01



SURFACE MOUNTED



PEDESTAL MOUNTED



DETAIL A



SIGN # 31-0833
USE APPROPRIATE LEFT OR RIGHT ARROW

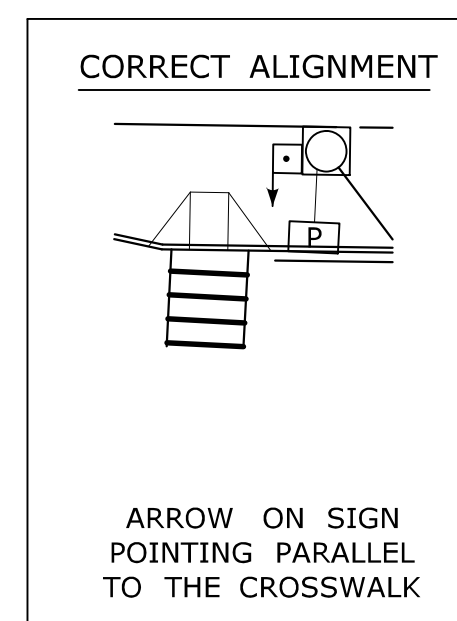


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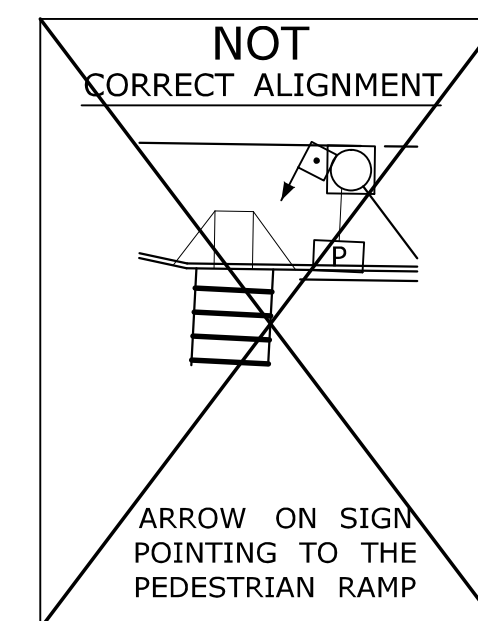
FOR CROSSING WITH SIDE STREET GREEN

GENERAL NOTES:

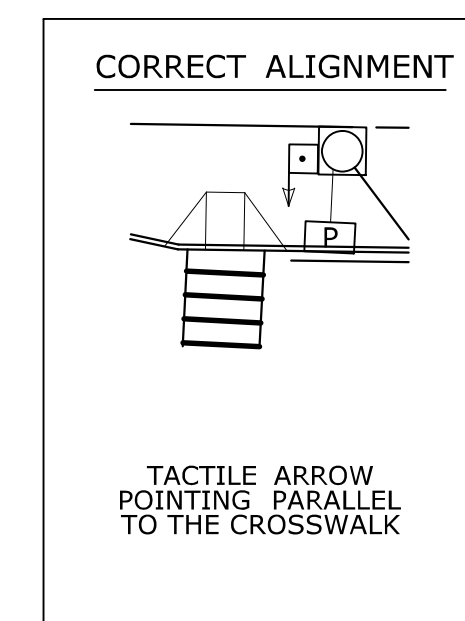
- 3'-6" (1050) FROM FINISHED GRADE SUCH AS SIDEWALK TO CENTER OF PUSH BUTTON.
- PUSH BUTTON INSTALLATIONS SHALL CONFORM TO THE REQUIREMENTS OF THE AMERICANS WITH DISABILITIES ACT (ADA) STANDARDS FOR ACCESSIBLE DESIGN, CURRENT EDITION GOVERNS.
- 4'-4" (1300) PEDESTAL TO INCLUDE ALLOY CAP SECURED WITH STAINLESS STEEL SET SCREW.



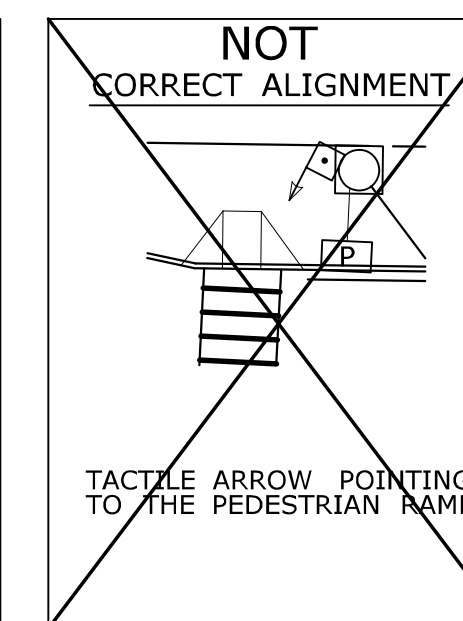
PEDESTRIAN PUSH BUTTON ALIGNMENT



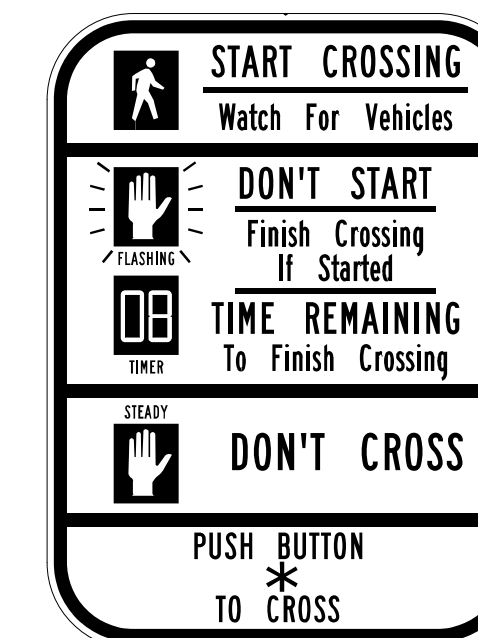
NOT CORRECT ALIGNMENT



ACCESSIBLE PEDESTRIAN SIGNAL AND DETECTOR



NOT CORRECT ALIGNMENT



*USE APPROPRIATE ARROW UNLESS OTHERWISE NOTED ON PLAN.

FOR NEW PUSHBUTTON HOUSING, USE 9" x 15" SIGN NO. 31-0856.

FOR EXISTING PUSHBUTTON HOUSING, WITH 9" x 12" SIZE, USE SIGN NO. 31-0845.

EXAMPLE ALIGNMENTS FOR EXCLUSIVE PEDESTRIAN PHASE

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

	PEDESTRIAN PUSH BUTTON
	PEDESTRIAN PUSH BUTTON, PEDESTAL MOUNTED
	PEDESTRIAN PUSH BUTTON, POLE MOUNTED

REV.	DATE	REVISION DESCRIPTION
3	8-2018	UPDATED PEDESTRIAN SIGN LEGENDS AND NOTES.
2	4-2014	ADDED PEDESTRIAN EXAMPLE ALIGNMENTS
1	4-2012	MINOR REVISIONS & UPDATED SIGN #31-0845.

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Plotted Date: 8/9/2018

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NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

File name: CTDOT_TRAFFIC_STD_2018-01-25.dgn Model: TR-1107_01

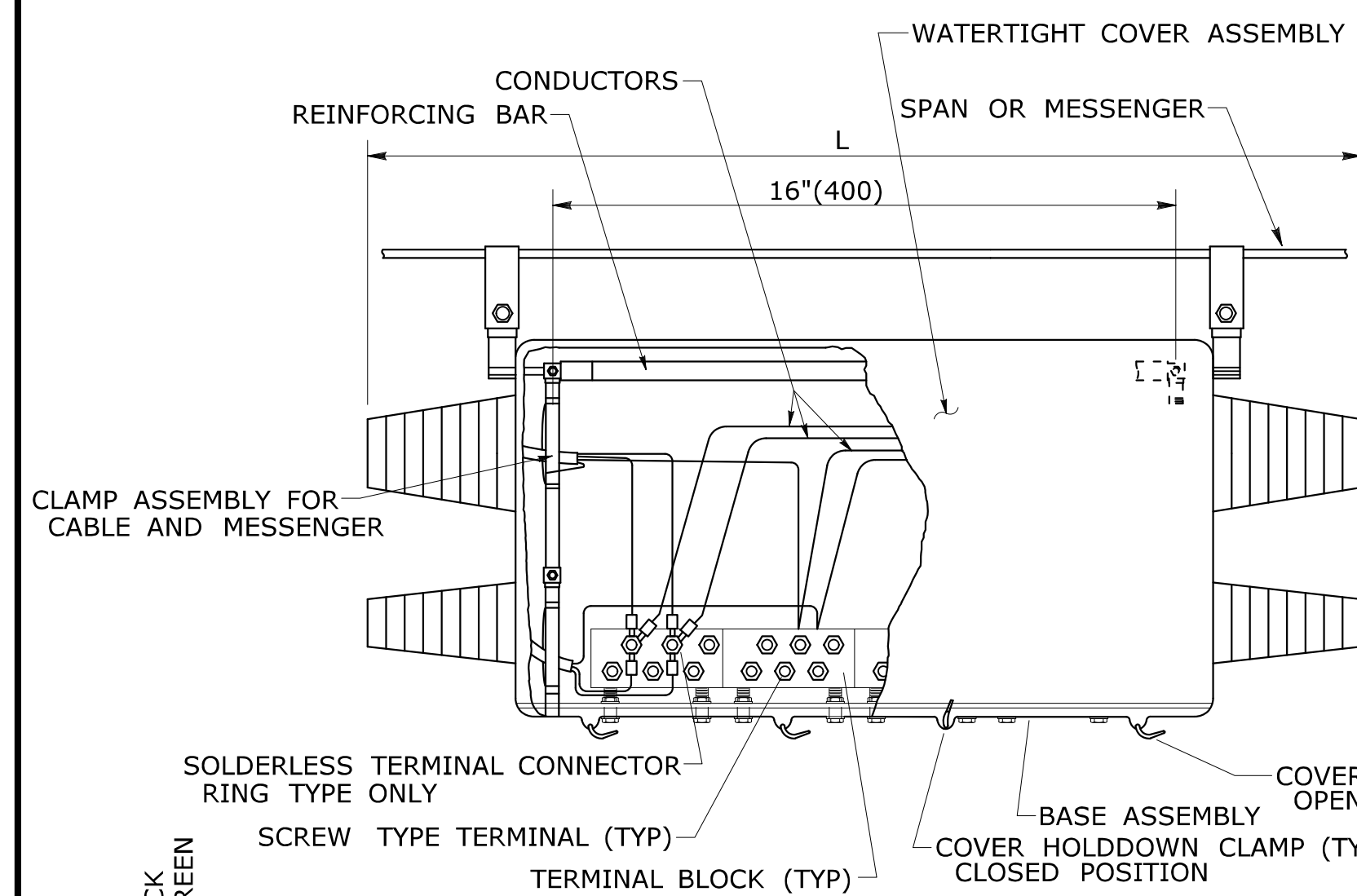
SUBMITTED BY: NAME/DATE/TIME:
Tracy L. Fogarty Tracy L. Fogarty, P.E. 2018.08.16 12:13:35-04'00'

APPROVED BY: NAME/DATE/TIME:
Mark F. Carlino, P.E. 2018.08.21 07:46:57-04'00'

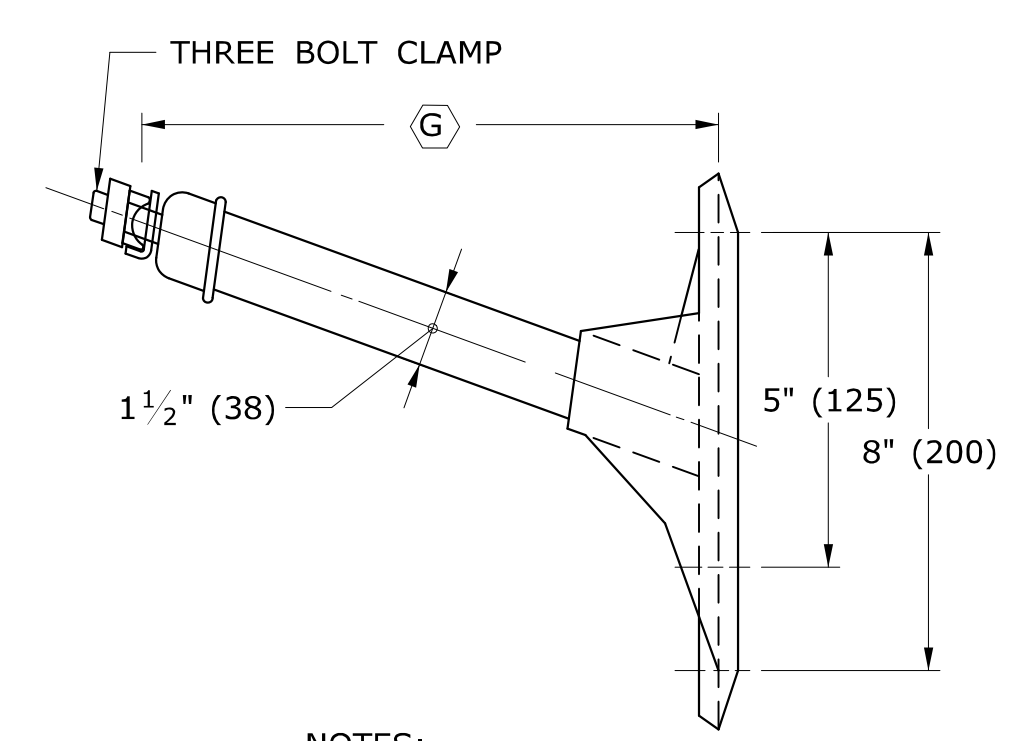
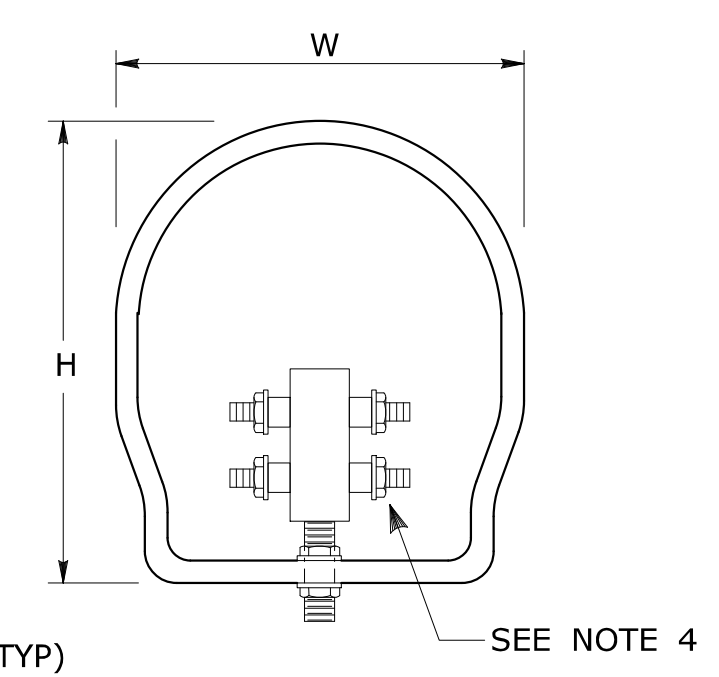
CTDOT
STANDARD SHEET
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
PEDESTRIAN PUSH BUTTONS

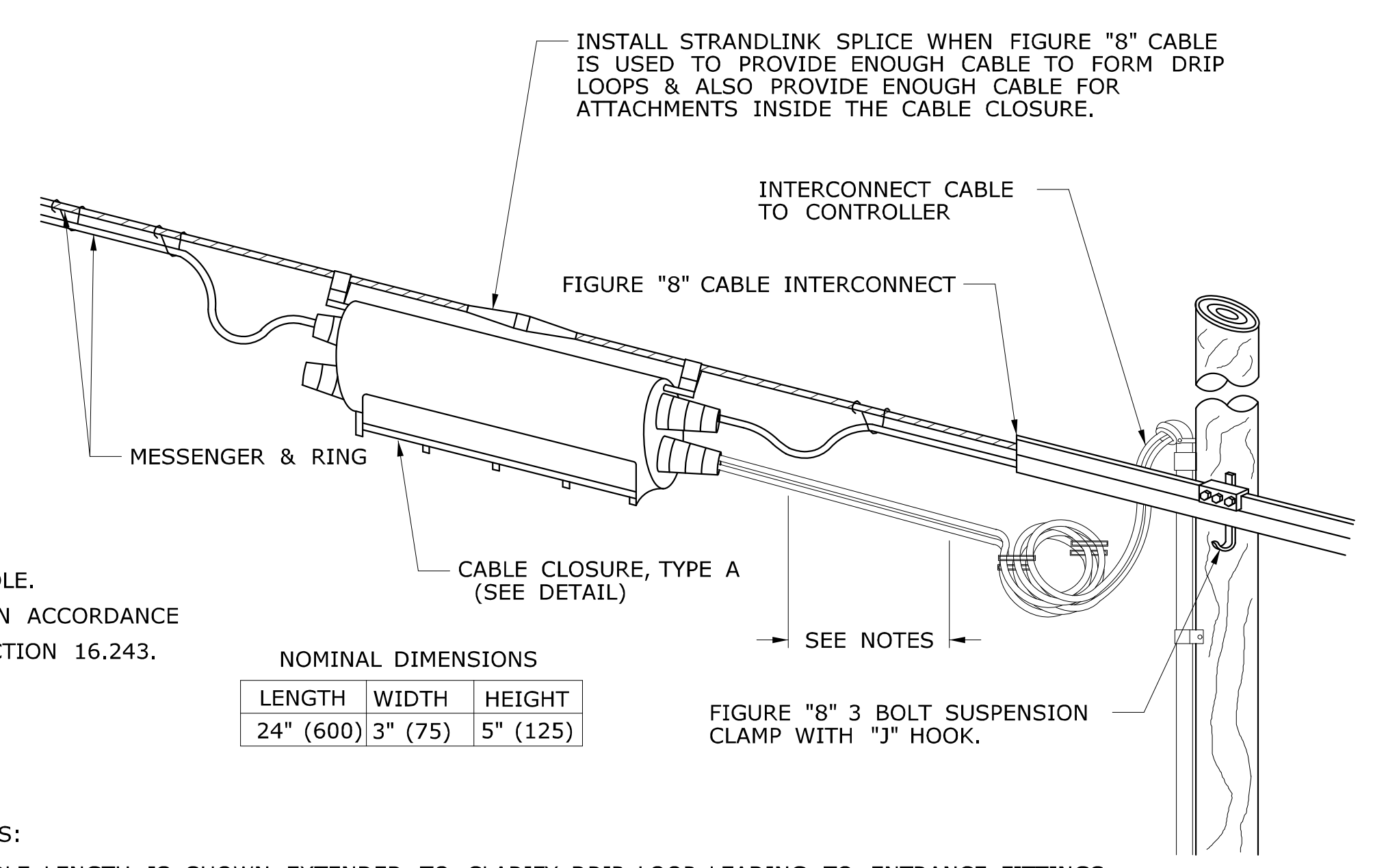
STANDARD SHEET NO.:
TR-1107_01



	LENGTH	WIDTH	HEIGHT
MIN	28" (700)	3" (75)	5" (125)
MAX	33" (840)	6" (150)	8" (200)



NOTES:
 5/8" (16) THROUGH BOLT IN TOP HOLE.
 1/2" (13) X 4" (100) LAG BOLT IN BOTTOM HOLE.
 (G) LENGTH REQUIRED TO PROVIDE CLEARANCE IN ACCORDANCE WITH PURA CONSTRUCTION STANDARD SECTION 16.243.



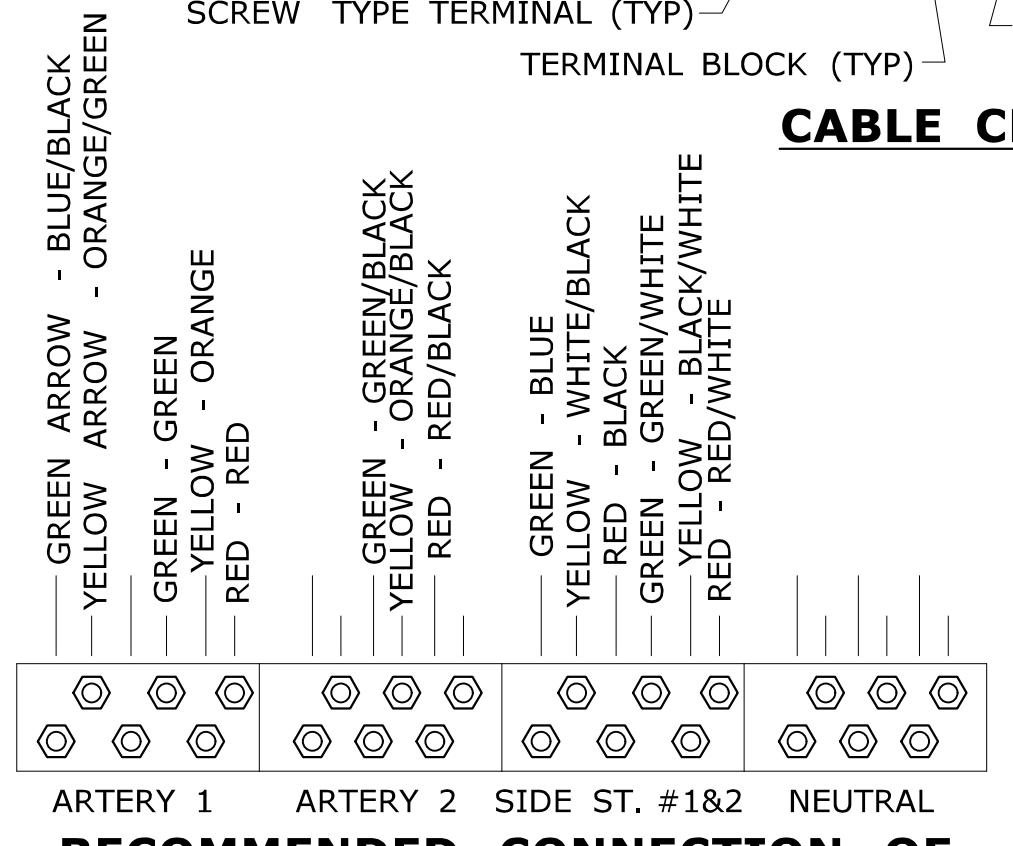
NOMINAL DIMENSIONS

LENGTH	WIDTH	HEIGHT
24" (600)	3" (75)	5" (125)

NOTES:
 CABLE LENGTH IS SHOWN EXTENDED TO CLARIFY DRIP LOOP LEADING TO ENTRANCE FITTINGS. WHEN CABLE IS TOO LARGE TO FORM DRIP LOOPS, INSTALL DIRECTLY INTO CLOSURE WITHOUT DRIP LOOPS.

CABLE CLOSURE FOR TRAFFIC SIGNALS

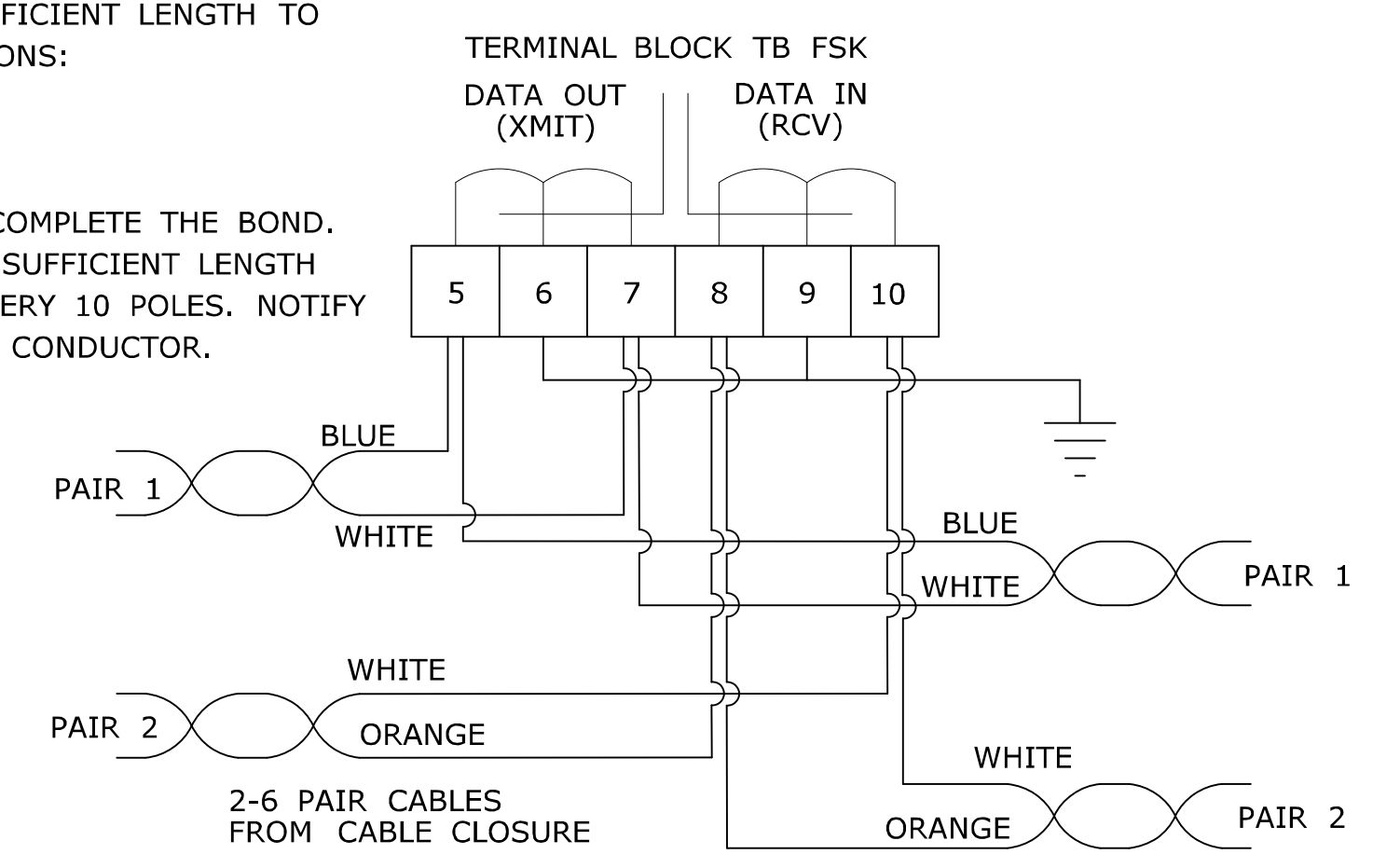
- NOTES:
- PROVIDE A MINIMUM OF FOUR TERMINAL BLOCKS WITH SEPARATE SCREW TYPE TERMINALS, SIX ON EACH SIDE.
 - INSTALL SEPARATE CABLE BETWEEN CLOSURE & EACH TRAFFIC SIGNAL ASSEMBLY. WIRE EACH TRAFFIC SIGNAL SECTION SEPARATELY BACK TO CABLE CLOSURE. JUMPERS BETWEEN TERMINALS ARE NOT ALLOWED EXCEPT ON NEUTRAL CONDUCTORS.
 - WIRE ALL SIGNALS, SAME DIRECTION FROM CONTROLLER, SEPARATELY WITH CONDUCTORS IN 21 OR 25 CONDUCTOR CABLE, EVEN IF INDICATIONS ARE IDENTICAL.
 - PREVENT CONNECTORS, TERMINAL POSTS AND CONDUCTORS FROM CONTACT WITH CLOSURE COVER AND SECURELY WRAP WITH ELECTRICAL TAPE OR RUBBER MASTIC TAPE.
 - CABLES THAT FEED PEDESTRIAN INDICATIONS, PUSH BUTTONS, AND DETECTORS BYPASS CABLE CLOSURE.
 - INSTALL TRAFFIC SIGNAL CABLE CLOSURE ON THE SPAN +5' (+1.5 m) FROM CURBLINE.



RECOMMENDED CONNECTION OF 21 CONDUCTOR CABLE IN CLOSURE

OVERHEAD INTERCONNECT INSTALLATION

- BONDING AND GROUNDING REQUIREMENTS FOR COMMUNICATION CABLES
- PLACE BOND ON STRAND USING #6 AWG LEAVE COIL OF SUFFICIENT LENGTH TO REACH THE NEXT STRAND AT ALL OF THE FOLLOWING LOCATIONS:
 - FIRST POLE
 - LAST POLE
 - JUNCTION POLE
 CONTACT THE UTILITY THAT OWNS THE NEXT STRAND TO COMPLETE THE BOND.
 - LEAVE COIL #6 AWG WIRE ATTACHED TO CABLE STRAND OF SUFFICIENT LENGTH TO REACH VERTICAL GROUNDING CONDUCTOR AT LEAST EVERY 10 POLES. NOTIFY ELECTRIC COMPANY TO MAKE ATTACHMENT TO GROUNDING CONDUCTOR.



NOTES:
 CONNECT SHIELDS TO GROUND ONLY AT EVERY OTHER CABINET, LEAVE SHIELD OPEN AT ALTERNATE CABINETS. TAG 6 PAIR CABLES WITH INTERSECTION DESTINATION. SPARE PAIRS TO BE FOLDED BACK AND NEATLY TIED ADJACENT TO TERMINAL BLOCK.

TYPICAL COMMUNICATION CABLE CONNECTION IN CONTROLLER CABINET

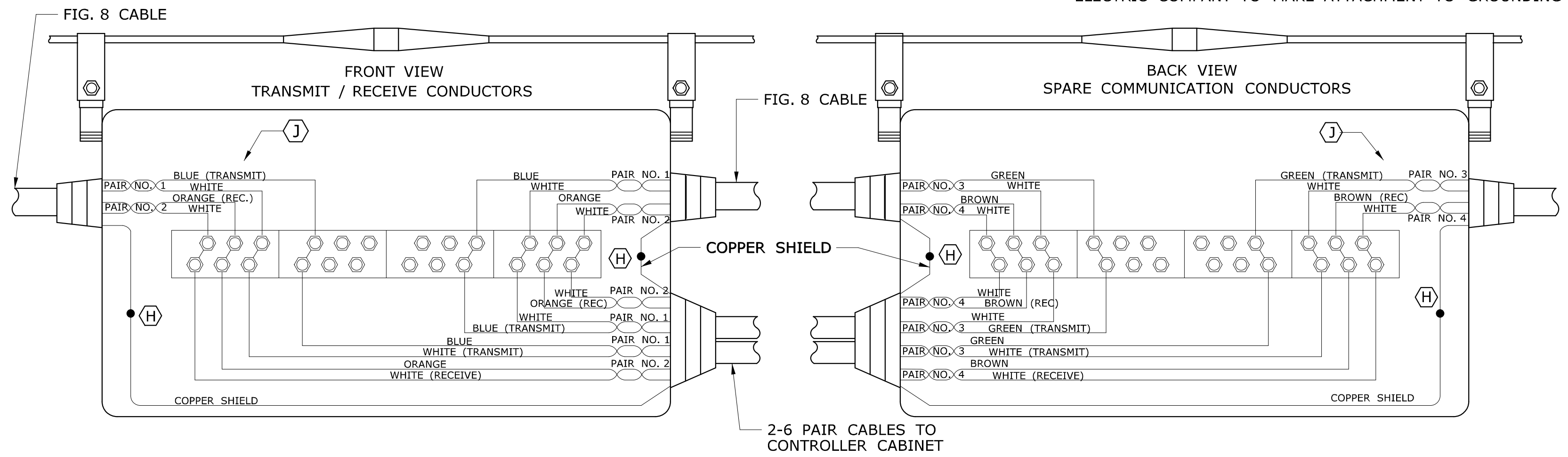
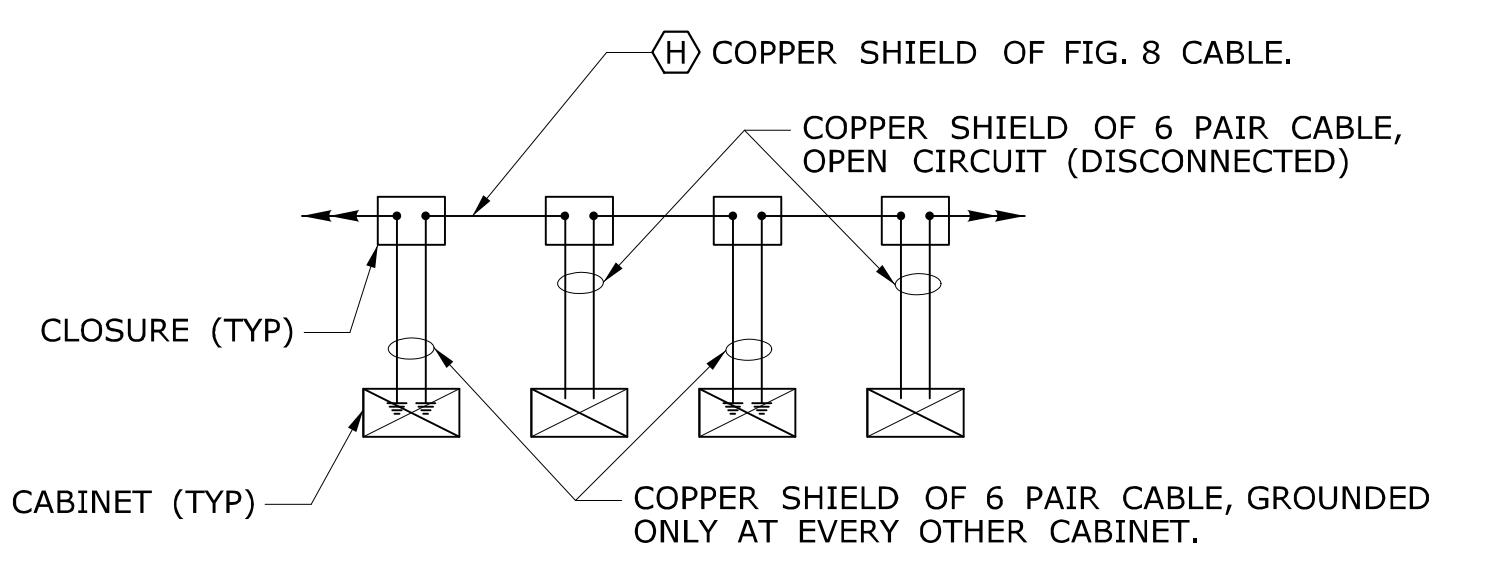
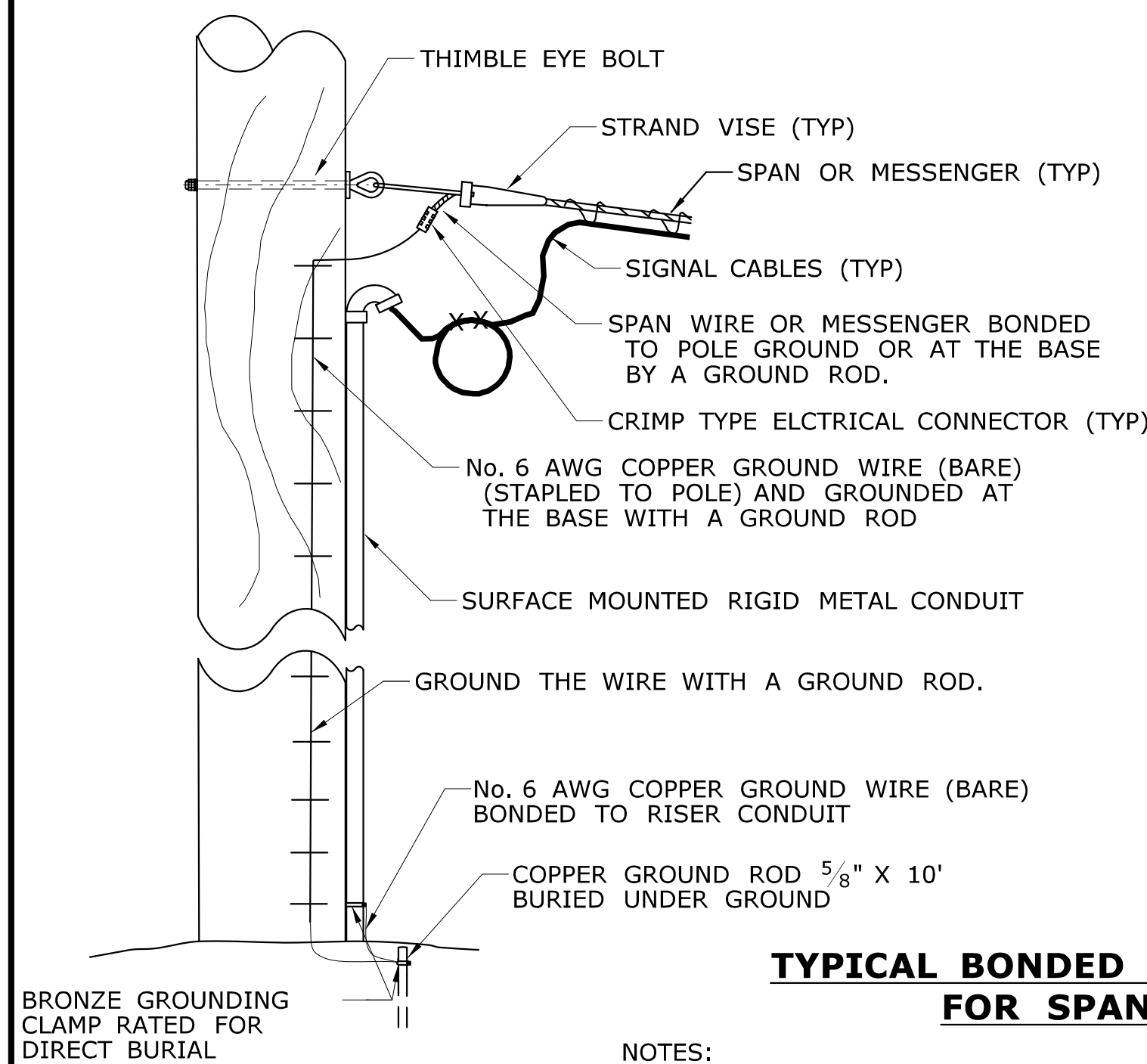


FIG. 8 CABLE				6 PAIR, CABLE			
PAIR #	DOT	SPARE	PAIR #	MUNICIPAL SPARES	PAIR #	DOT	SPARE
3	GREEN	- WHITE	5	SLATE - WHITE	5	SLATE	- WHITE
4	BROWN	- WHITE	6	BLUE - RED	6	BLUE	- RED

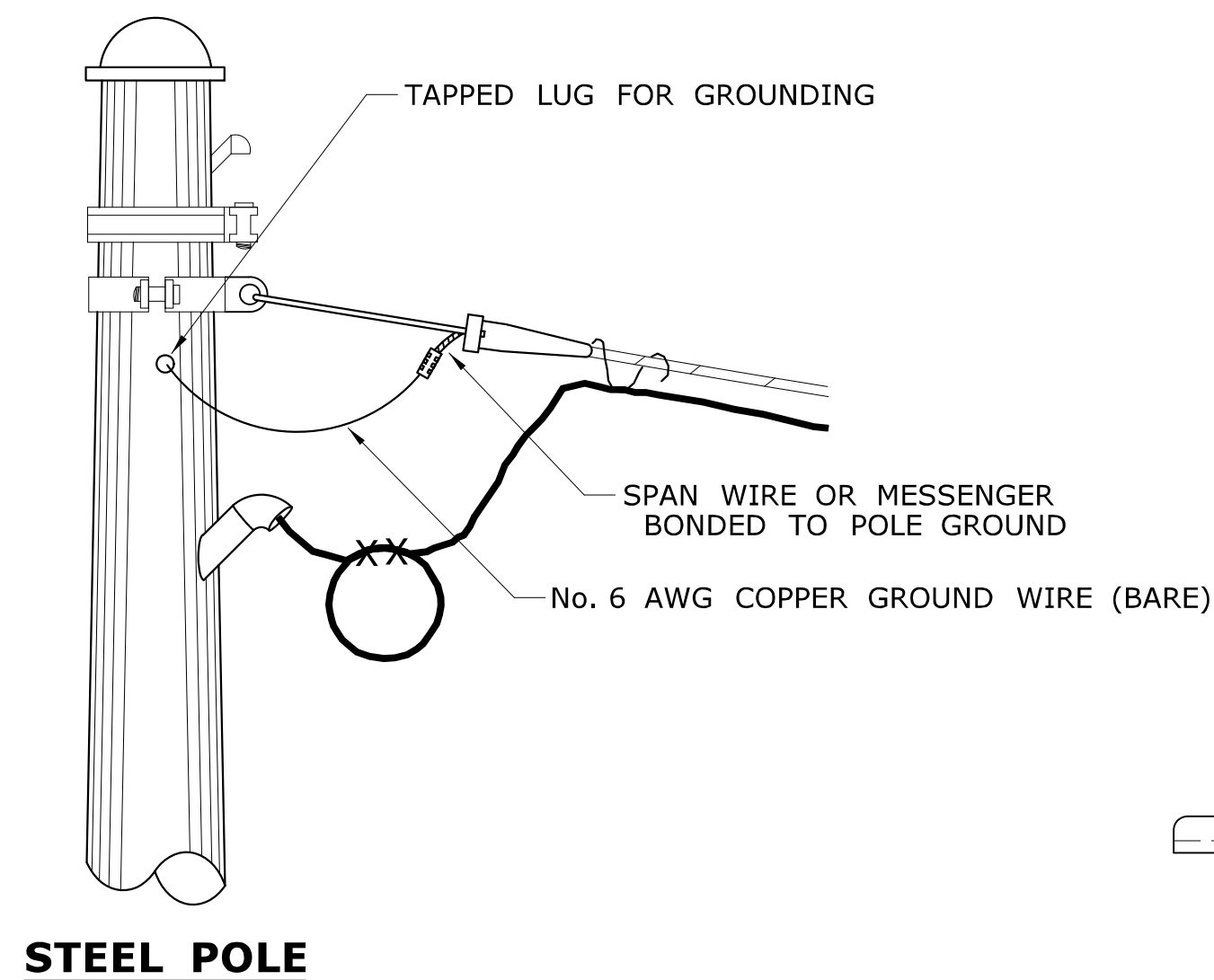
NOTES:
 SPARE PAIRS TO BE FOLDED BACK AND NEATLY TIED. GROUP MUNICIPAL SPARES TOGETHER, SEPARATE FROM THOSE RESERVED FOR D.O.T. MUNICIPAL SPARES ARE NOT TO BE CUT. DO NOT BOND THE CABLE SHEATH TO THE SUPPORT STRAND.
 (H) CONNECT ONLY TO SHIELD OF CORRESPONDING 6 PAIR CABLE.
 (J) WHEN CONNECTING TO AN EXISTING SYSTEM, VERIFY COLOR CODE OF TRANSMIT AND RECEIVE CONDUCTORS.

TYPICAL COMMUNICATION CABLE CONNECTION IN CABLE CLOSURE, TYPE A

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:
 ○ PROPOSED UTILITY POLE
 ● EXISTING UTILITY POLE
 — POLE ANCHOR & GUY
 — CABLE CLOSURE



WOOD UTILITY POLE

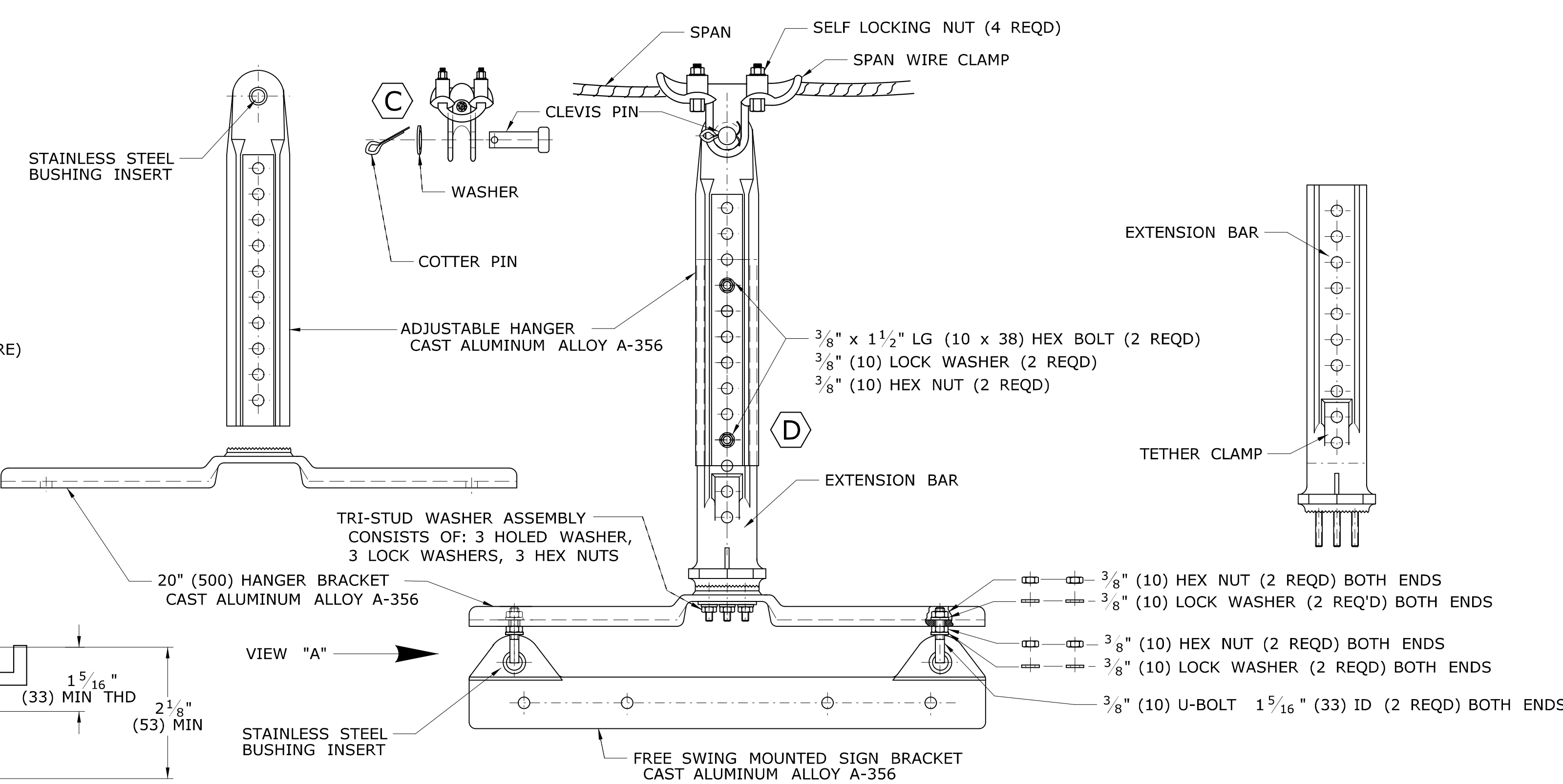


STEEL POLE

TYPICAL BONDED ATTACHMENT DETAIL FOR SPAN/MESSENGER

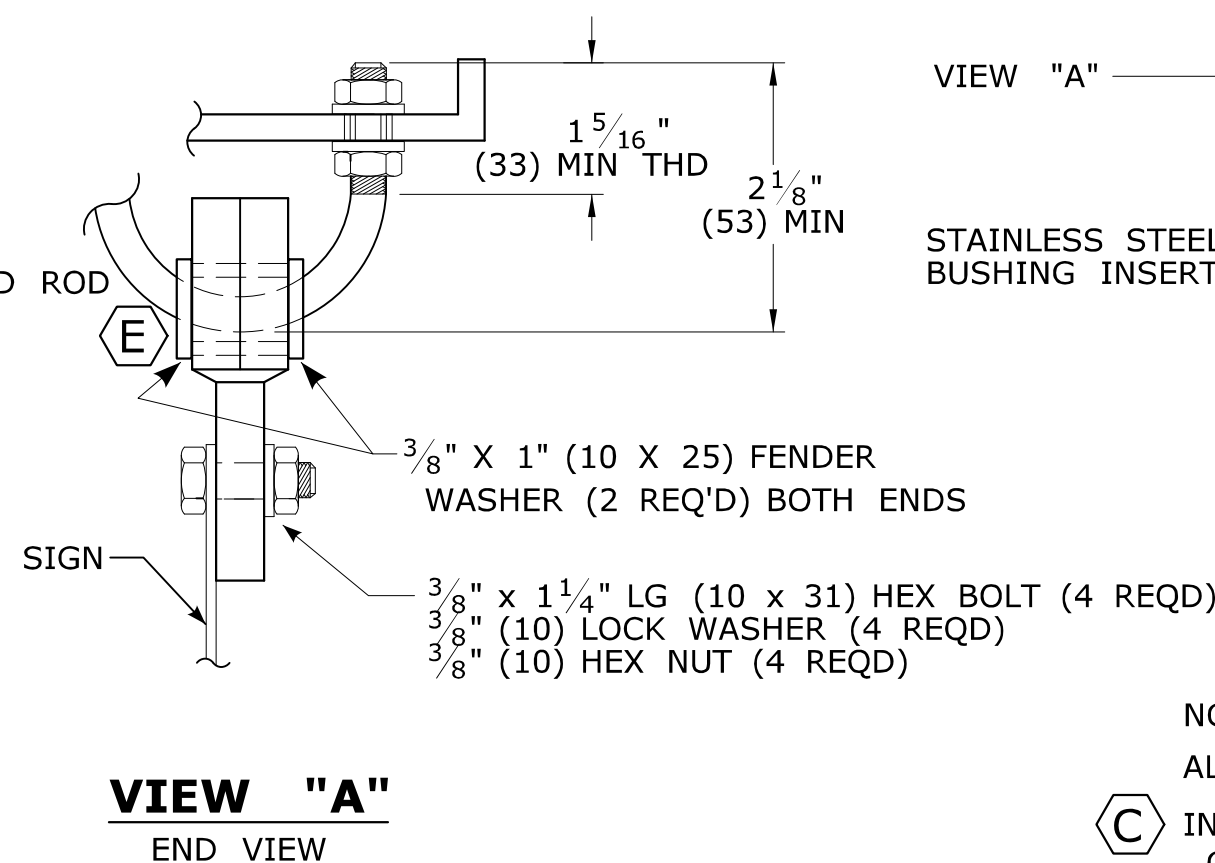
NOTES:
NOTE THAT EXISTING COPPER GROUND WIRE MAY BE USED IF AVAILABLE. ALL ATTACHMENTS REQUIRE BONDING.

THE WOOD UTILITY POLE DETAIL APPLIES TO INSTALLATIONS OF GROUND RODS IN SOIL, SIDEWALK, PAVEMENT. THE TOP SURFACE SHALL BE REPLACED IN KIND AFTER THE GROUND ROD INSTALLATION. WEHN ROCK BOTTOM IS ENCOUNTERED, REFER TO NEC 250.53G FOR INSTALLATION REQUIREMENTS.

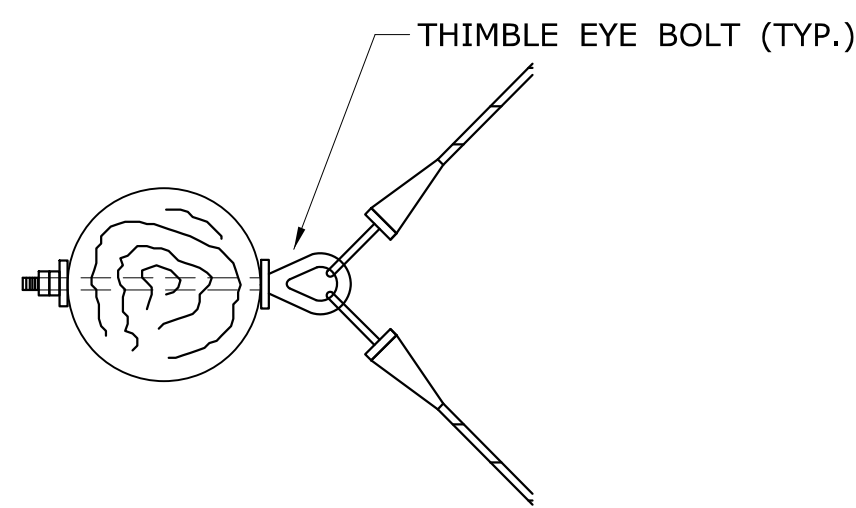


FREE SWINGING SPAN MOUNTED SIGN HANGER ASSEMBLY

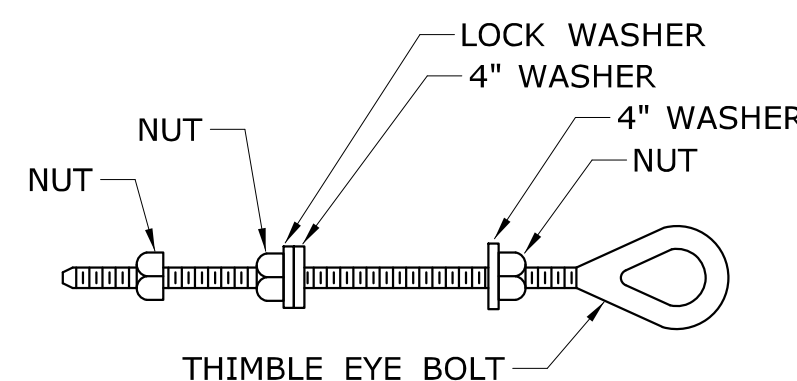
NOTES:
ALL STAINLESS STEEL HARDWARE: BOLTS, NUTS, WASHERS, CLEVIS PIN, COTTER PIN, ETC.
C INSTALL CLEVIS PIN SO THAT THE WASHER AND COTTER PIN ARE ON SIDE OF HANGER AWAY FROM SIGNAL CABLES.
D OVERLAP MAXIMUM NUMBER OF HOLES SEAT SNUGLY ON EXTENSION BAR. LEAVE BOTTOM HOLE OPEN FOR TETHER CLAMP.
E INSTALL FENDER WASHER ON U-BOLT, ON BOTH ENDS, ON BOTH SIDES OF SIGN BRACKET.



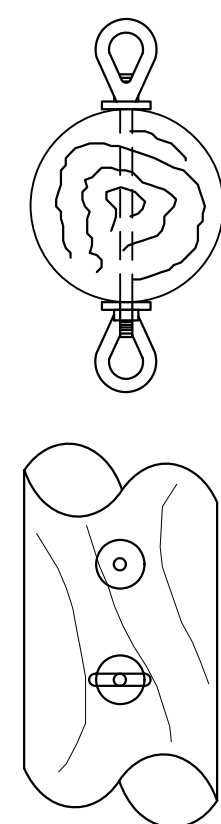
VIEW "A" END VIEW



TYPICAL SINGLE ATTACHMENT WITH DOUBLE PULL-OFF

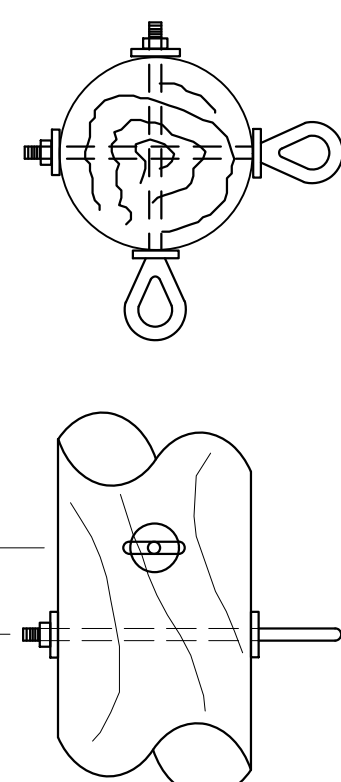


THIMBLE EYE BOLT SUPPLIED BY THE CONTRACTOR.

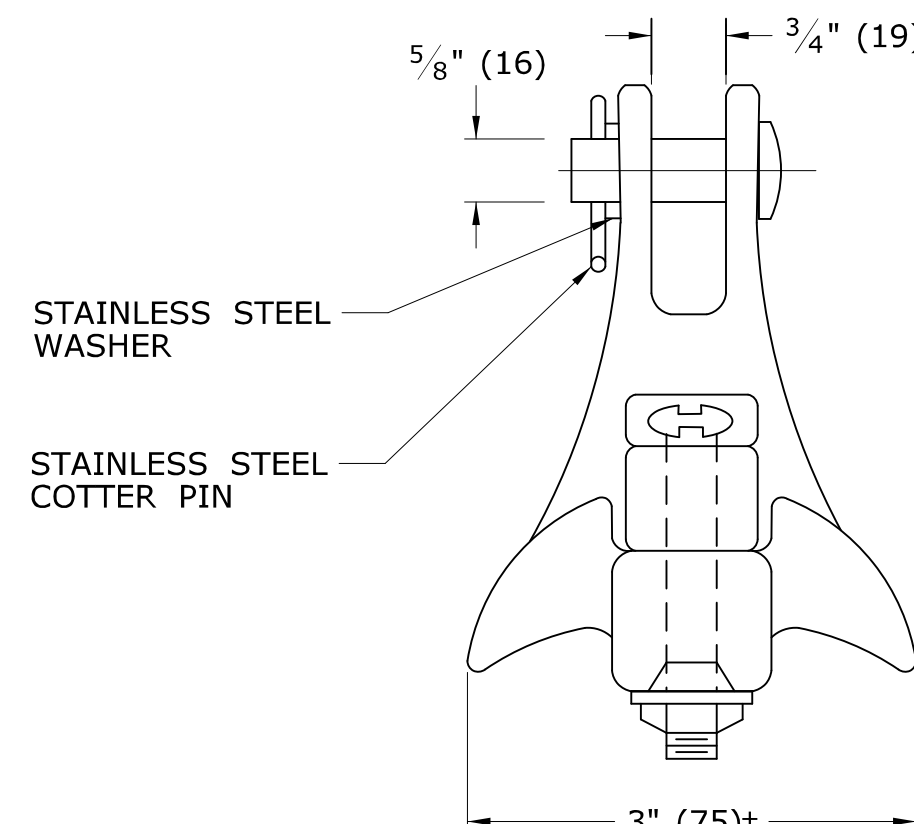


SAME PLANE

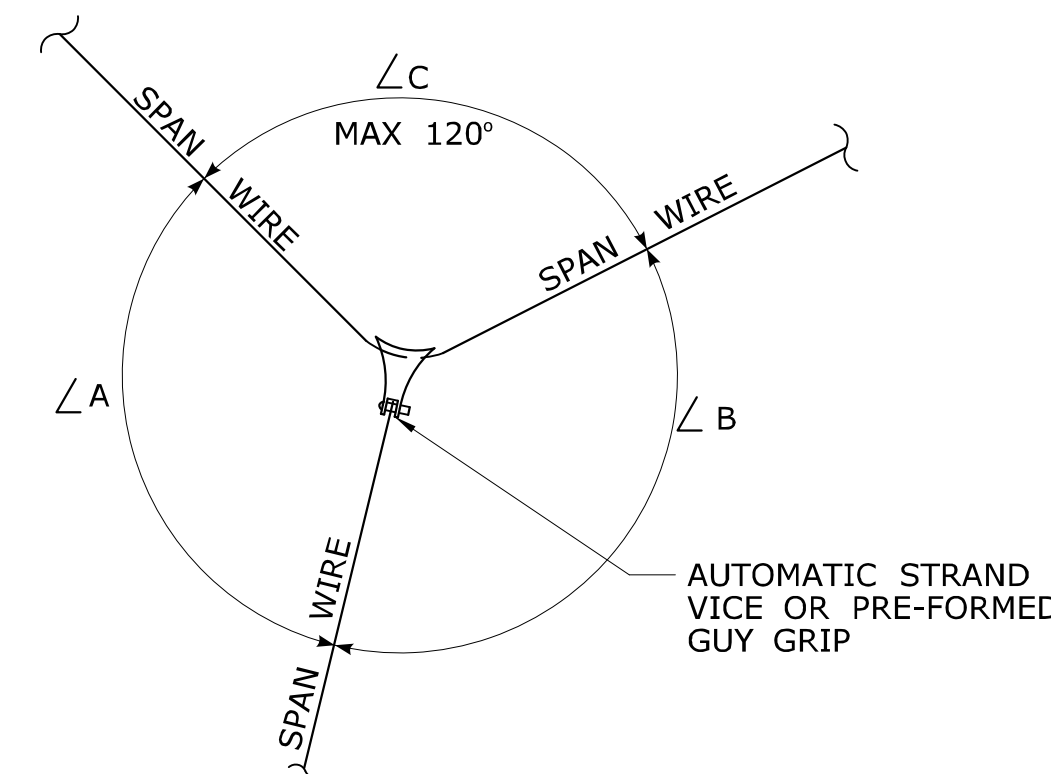
TYPICAL DOUBLE ATTACHMENT DETAIL ON WOOD POLE



PERPENDICULAR



TANGENT "Y" SPAN CLAMP DETAIL



CLAMP ORIENTATION DETAIL

NOTES:
MINIMUM 11,000 LBS. (10.7 kN) BREAKING STRENGTH.
CLAMP SMALLEST ANGLE OF Y CONNECTION.
 $\angle A \approx \angle B$ SO PULL-OFF BISECTS $\angle C$.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:
SPAN MOUNTED SIGN

NOTE:
METHOD OF ATTACHMENT TO BE DETERMINED BY POLE CUSTODIAN.

REV.	DATE	REVISION DESCRIPTION
4	1-2018	REVISED WOOD POLE GROUNDING DETAILS, ADDED GROUND ROD
3	3-2015	REVISED SPAN WIRE TO WOOD POLE DETAILS, ADDED BONDING ATTACHMENT DETAILS, REMOVED ANCHOR & SIGN HANGER DETAILS.
2	4-2014	ADDED SIGNAL "Y" SPAN ATTACHMENT DETAIL.
1	4-2012	INCLUDED SIGN HANGER BOLTS, REMOVED AUDIBLE PED. SIGNAL & RENUMBERED STANDARD SHEET (FROM TR-1107_02).

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

DIMENSIONS ARE IN ENGLISH ("") & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED: - OVER 1" TO NEAREST 5 mm - UNDER 1" TO NEAREST 1 mm.

NOT TO SCALE

STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION

Plotted Date: 8/10/2018

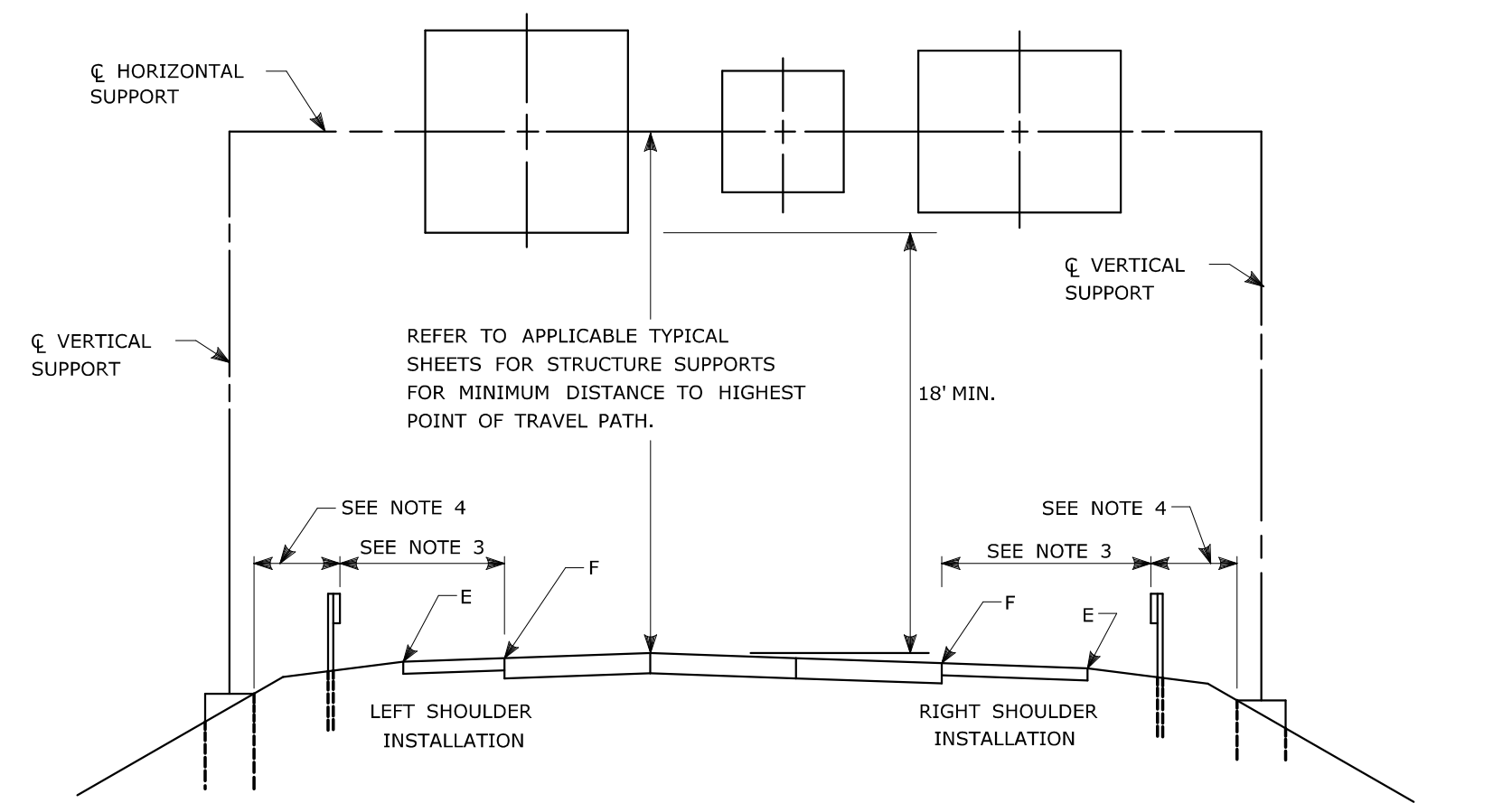
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APPROVED BY: Mark F. Carlino, P.E. 2018.08.21 07:47:24-04'00'

CTDOT STANDARD SHEET OFFICE OF ENGINEERING

STANDARD SHEET TITLE: **BONDING AND UTILITY POLE ATTACHMENT DETAILS, SIGN HANGER, "Y" CLAMP DETAIL**

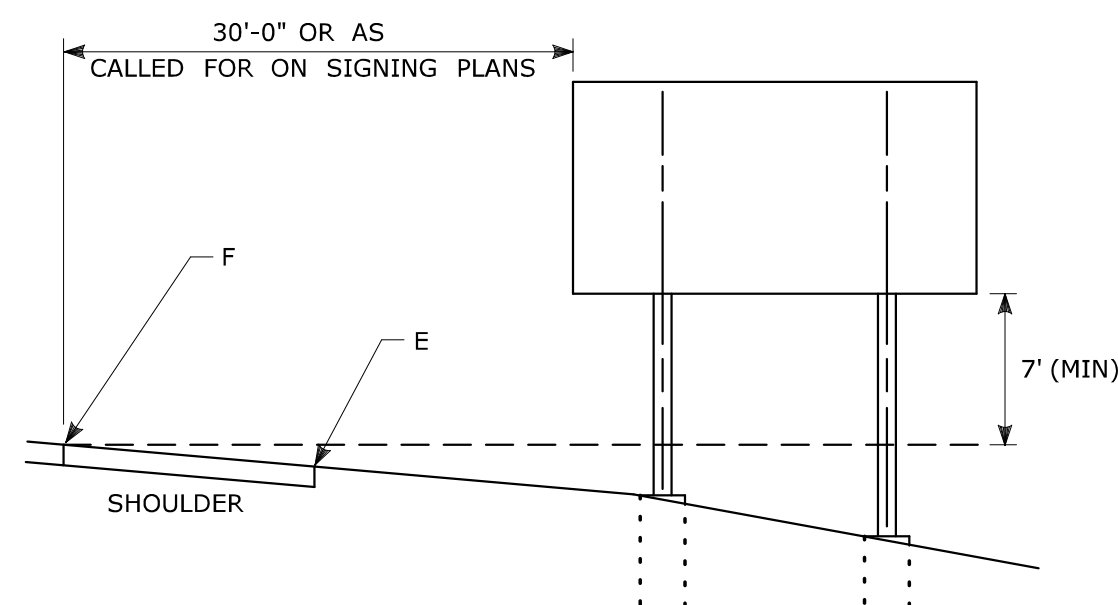
STANDARD SHEET NO.: TR-1114_01



TYPICAL PLACEMENT OF OVERHEAD SIGNS ON SIGN SUPPORTS

NOTES:

- 1) FOR PLACEMENT OF CANTILEVER SIGN SUPPORT USE APPLICABLE PORTION OF ABOVE DETAIL.
- 2) BARRIER SYSTEMS MAY BE REQUIRED FOR BOTH SIDES OF SUPPORTS IN MEDIANS.
- 3) IMPACT PROTECTION SHALL BE PROVIDED FOR THE SIGN SUPPORTS LOCATED WITHIN CLEAR ZONE.
- 4) SIGN SUPPORT FOUNDATIONS SHALL BE LOCATED OUTSIDE OF BARRIER SYSTEMS DEFLECTION AREA.
- 5) ALL SIGNS ARE TO BE LEVEL, REGARDLESS OF CAMBER IN SUPPORT.



TYPICAL PLACEMENT OF SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS

NOTES:

- 1) MIN. VERTICAL CLEARANCE ABOVE SIDEWALKS SHALL BE 7'.
- 2) WHERE GUIDE RAIL IS USED, THE OFFSET TO THE NEAR EDGE OF SIGN FACE SHALL BE AS SHOWN ELSEWHERE IN THE CONTRACT PLANS.
- 3) ON INTERSECTING ROADS AT RAMP TERMINI, THE OFFSET TO THE NEAR EDGE OF OF SIGN FACE SHALL BE 6' MIN. FROM POINT "E".
- 4) IF 30'-0" MIN. CANNOT BE MET, PLEASE CONTACT THE ENGINEER.

FOR MAXIMUM EFFECTIVENESS, POSITION SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS AS FOLLOWS:

ON A TANGENT SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH THE TRAFFIC LANE WHICH THE SIGN SERVES. SIGNS LOCATED 30 FT OR MORE FROM THE EDGE OF THE ROAD SHALL BE TURNED APPROXIMATELY 3° TOWARD THE ROAD.

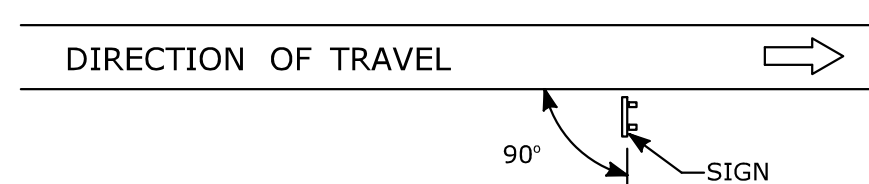


DIAGRAM "A"

ON A HORIZONTAL CURVE SECTION, POSITION THE SIGN SO THE VERTICAL AXIS IS PLUMB AND THE HORIZONTAL AXIS IS AT AN ANGLE OF 90° WITH A STRAIGHT LINE BETWEEN THE SIGN AND THE POINT AT WHICH THE SIGN SHALL BE READ.

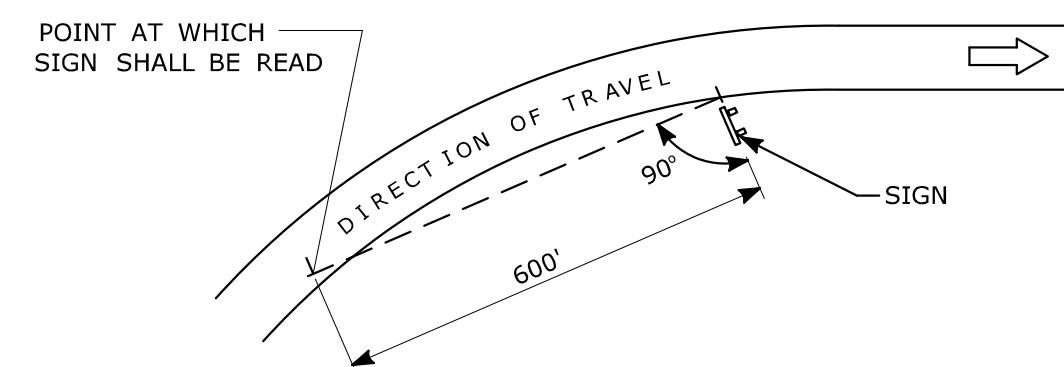
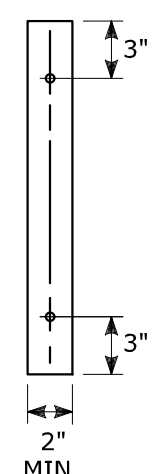


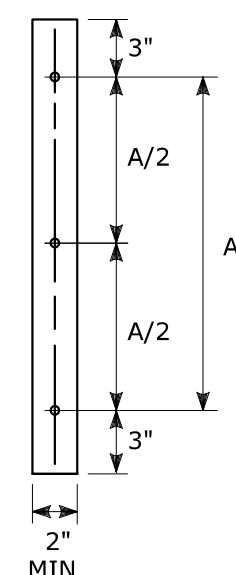
DIAGRAM "B"

SIGN ORIENTATION DETAILS FOR SIDE MOUNTED SIGNS ON STRUCTURAL STEEL BREAKAWAY SIGN SUPPORTS

RETROREFLECTIVE STRIPS 48" LONG OR LESS:



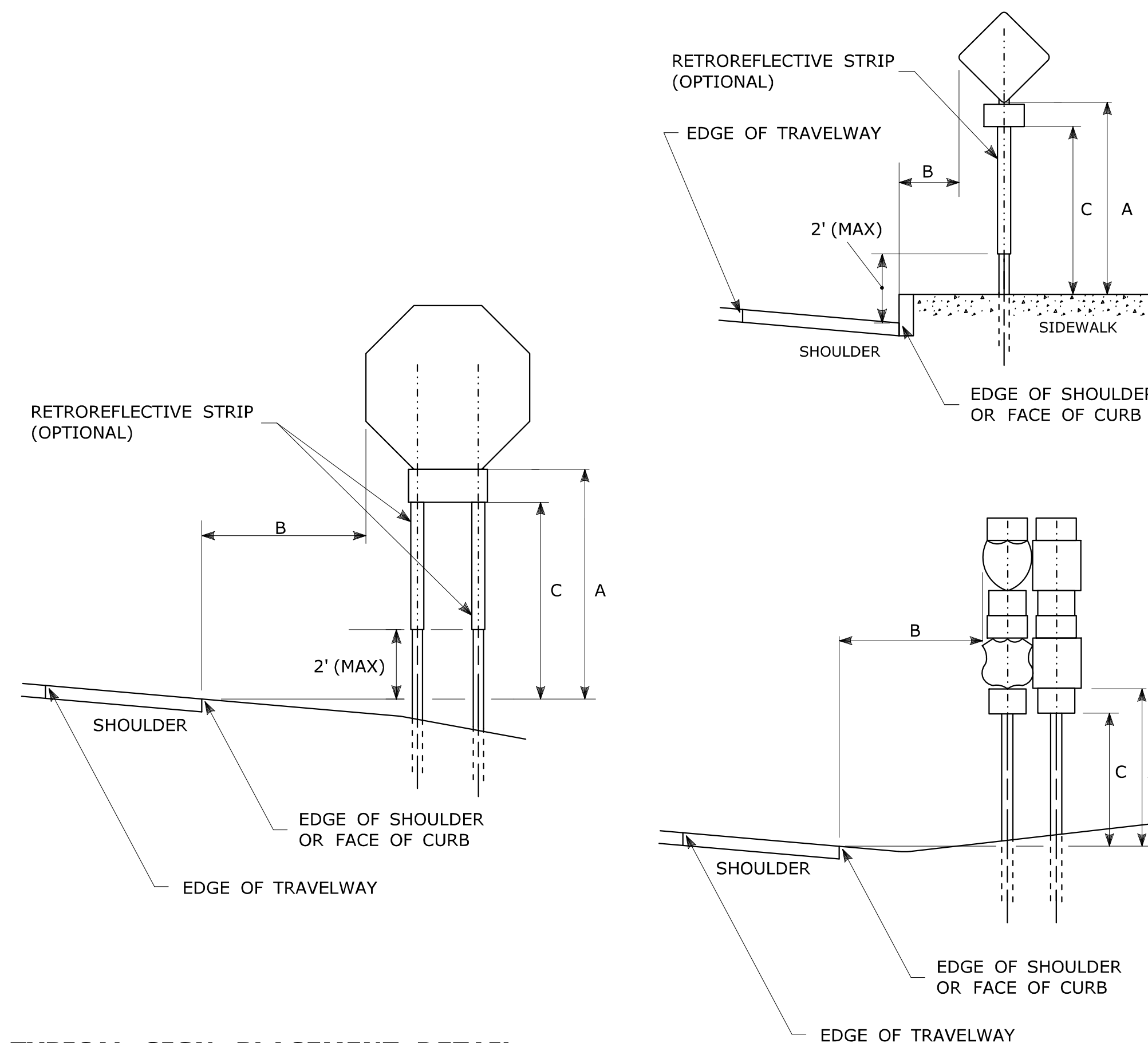
RETROREFLECTIVE STRIPS OVER 48" LONG:



RETROREFLECTIVE STRIP DETAIL

NOTES:

- RETROREFLECTIVE STRIPS WHICH ARE 48 IN LONG OR LESS SHALL BE ATTACHED USING 2 BOLTS AND RETROREFLECTIVE STRIPS OVER 48 IN LONG SHALL BE ATTACHED USING 3 BOLTS AS SHOWN ON THE DETAILS ABOVE. REFER TO STANDARD SHEET No. TR-1208.02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR MOUNTING DETAILS.
- RETROREFLECTIVE STRIP COLOR SHALL MATCH THE BACKGROUND COLOR OF THE SIGN, EXCEPT THAT THE COLOR OF THE STRIP FOR "YIELD" AND "DO NOT ENTER" SIGNS SHALL BE RED.



TYPICAL SIGN PLACEMENT DETAIL

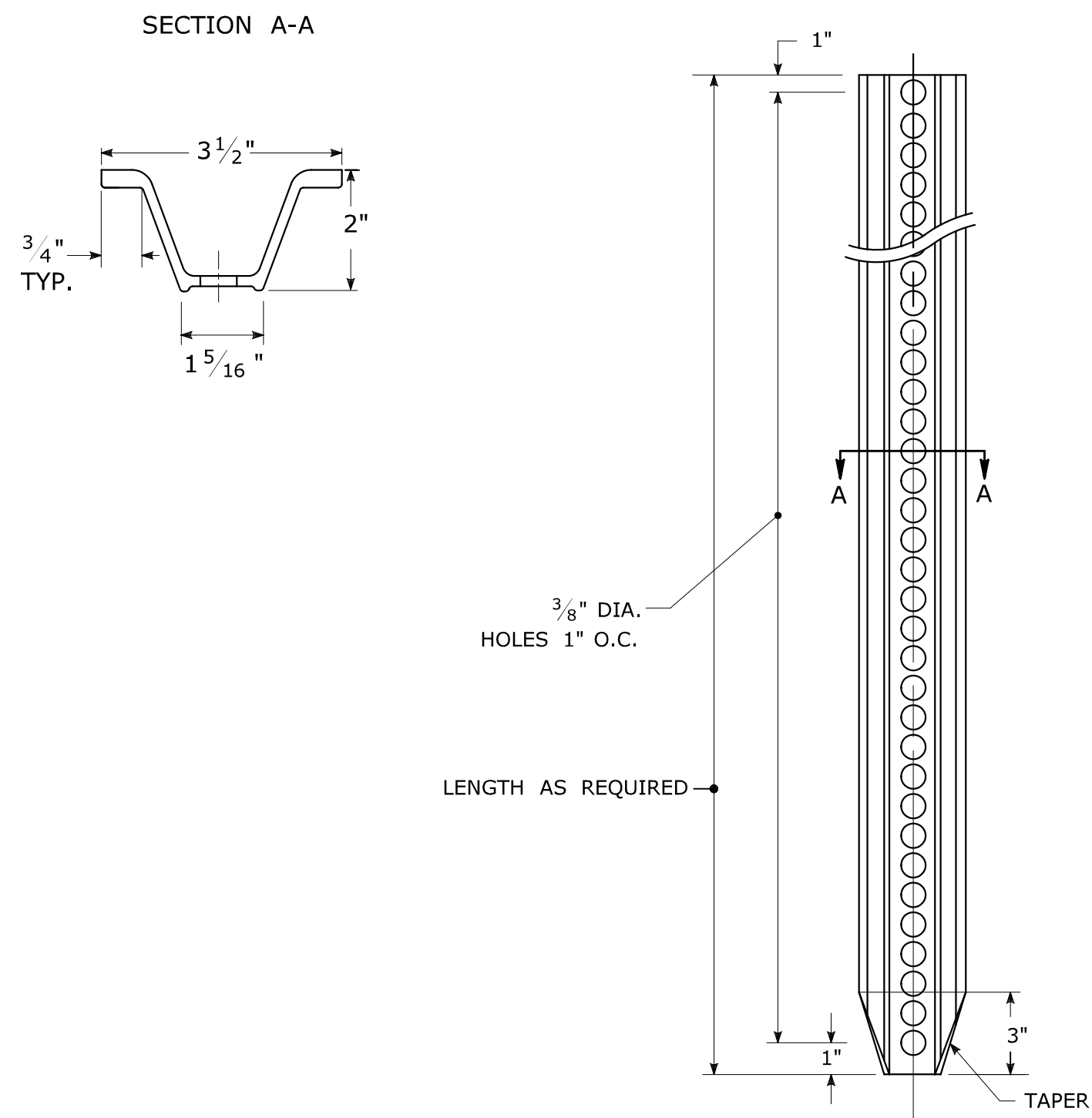
NOTES:

- ALL SIGNS AND SHIELDS ON DIRECTIONAL ASSEMBLIES SHALL ABUT VERTICALLY. REFER TO STANDARD SHEET No. TR-1208.02 "METAL SIGN POSTS AND SIGN MOUNTING DETAILS" FOR SIGN POSTS AND SIGN MOUNTING.
- IF A RETROREFLECTIVE STRIP IS USED ON SIGN SUPPORT, IT SHALL BE PLACED FOR THE FULL LENGTH OF THE SUPPORT FROM THE BOTTOM OF THE SIGN TO WITHIN 2 FT ABOVE THE EDGE OF THE ROADWAY. PARKING SIGNS TYPICALLY USE 45° MOUNTING BRACKET.

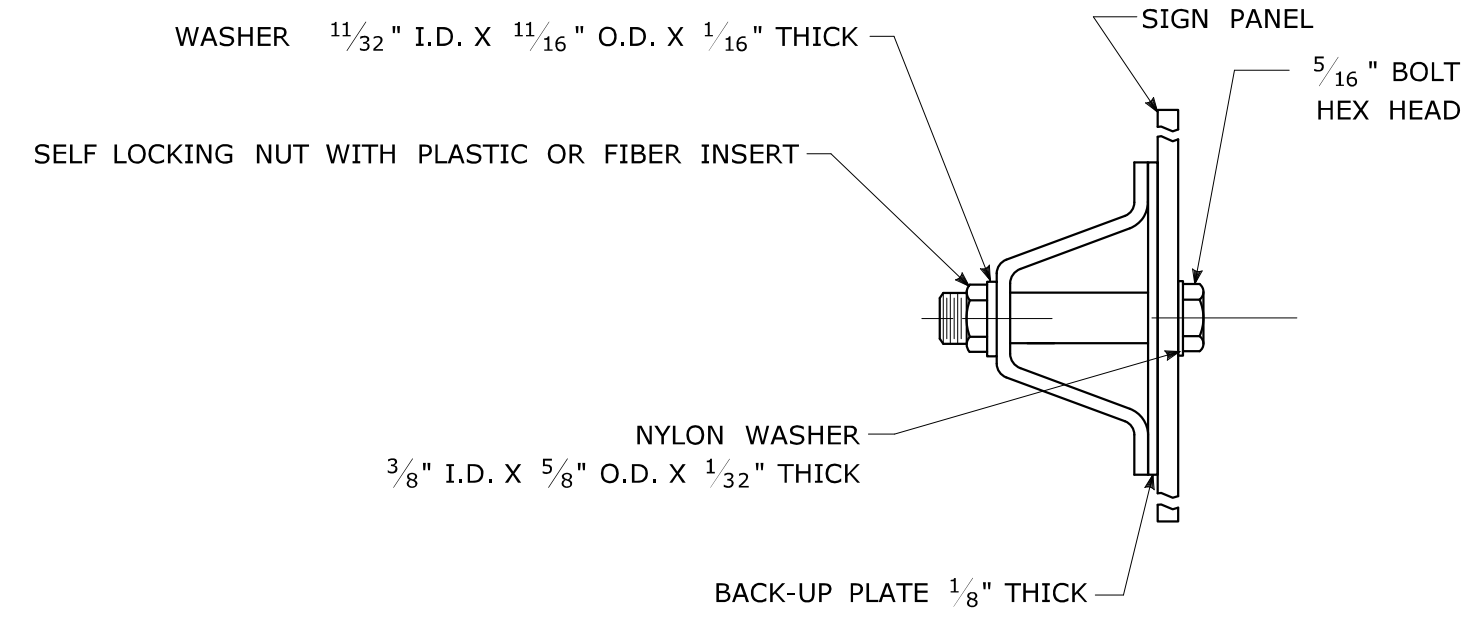
DIM."A" MIN SIGN HEIGHT	DIM."B" MIN LATERAL OFFSET (1)	DIM."C" MIN PLAQUE HEIGHT (1)	ASSEMBLY LOCATION
7' (2)	6' 12' (3)	5'	SIGNS ON FREEWAYS AND EXPRESSWAYS EXCEPT CHEVRON ALIGNMENT SIGNS, ONE-DIRECTION LARGE ARROW SIGNS, DO NOT ENTER SIGNS, AND WRONG WAY SIGNS
5'	2'	4'	• SIGNS IN RURAL AREAS • DO NOT ENTER AND WRONG WAY SIGNS ALONG EXIT RAMPS • DO NOT ENTER AND WRONG WAY SIGNS ON LIMITED ACCESS HIGHWAYS
5'	2'	N/A	• CHEVRON ALIGNMENT SIGNS LOCATED ON FREEWAYS, EXPRESSWAYS, RAMPS, AND IN RURAL AREAS • ONE-DIRECTION LARGE ARROW SIGNS LOCATED ON FREEWAYS, EXPRESSWAYS, RAMPS, AND IN RURAL AREAS
4'	6' 12' (3)	N/A	INCIDENT MANAGEMENT SIGNS AND MILE POST MARKER ASSEMBLIES LOCATED ON FREEWAYS AND EXPRESSWAYS
4'	2'	4'	CENTRAL ISLANDS OF ROUNDABOUTS
7'	2' (4)	6'	BUSINESS & RESIDENTIAL AREAS WHERE PARKING OR OTHER OBSTRUCTIONS LIMIT VISIBILITY
7'	2' (4)	7'	SIDEWALKS (5)

- (1) OR AS DIRECTED BY THE ENGINEER
- (2) 8 FT MINIMUM HEIGHT REQUIRED IF A SUPPLEMENTAL PLAQUE IS SUBMOUNTED BELOW THE MAJOR SIGN.
- (3) 6 FT FROM EDGE OF SHOULDER, WHEN SHOULDER IS OVER 6 FT WIDE
12 FT FROM EDGE OF TRAVELWAY, WHEN SHOULDER IS LESS THAN 6 FT WIDE.
- (4) A LATERAL OFFSET OF AT LEAST 1 FT FROM THE FACE OF THE CURB MAY BE USED WHERE SIDEWALK WIDTH IS LIMITED OR WHERE EXISTING UTILITY POLES ARE CLOSE TO THE CURB.
- (5) A CLEAR PATH OF NOT LESS THAN 4 FT SHALL BE PROVIDED IN SIDEWALK AREAS.

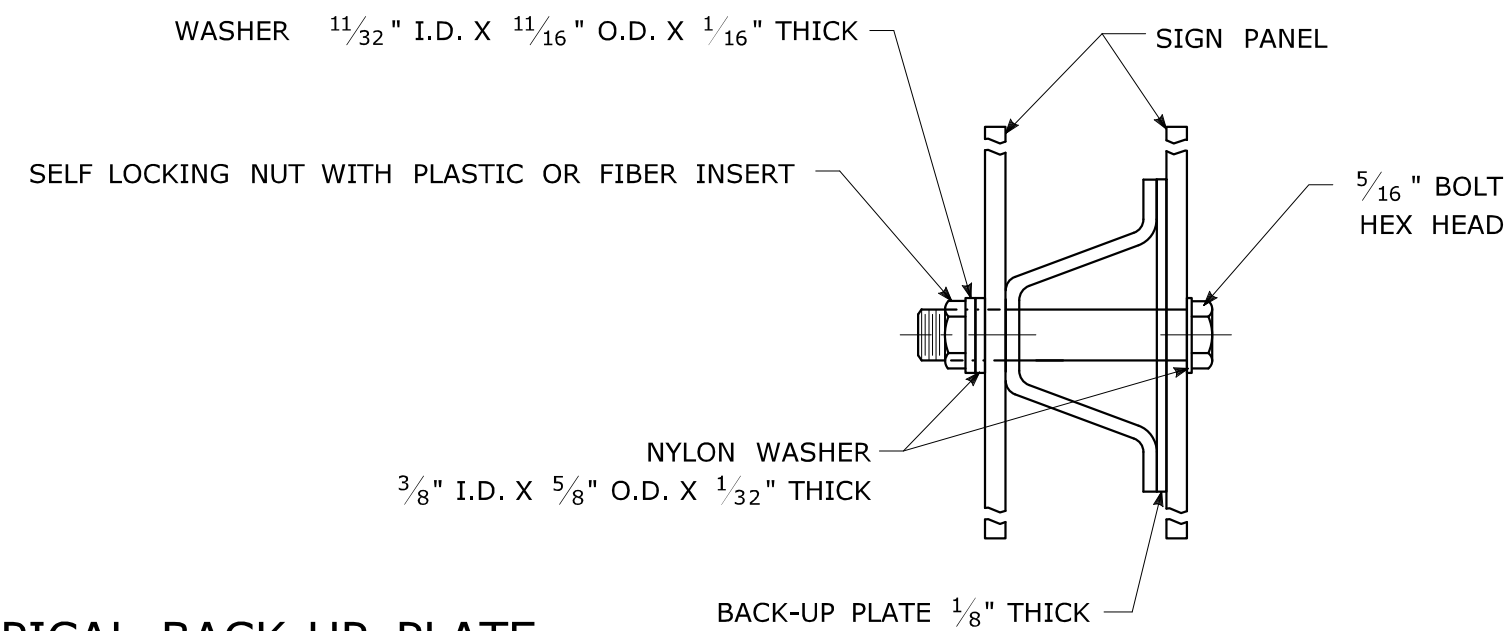
TYPICAL METAL SIGN POSTS



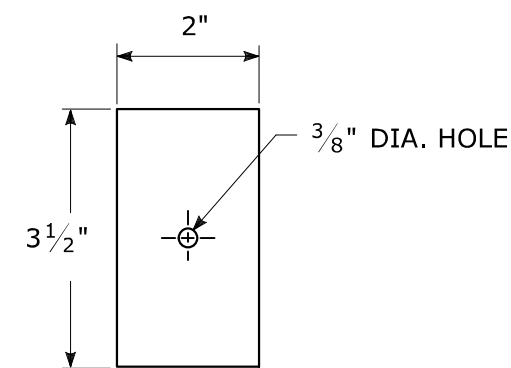
TYPICAL SIGN PANEL ATTACHMENT



TYPICAL BACK TO BACK SIGN PANEL ATTACHMENT



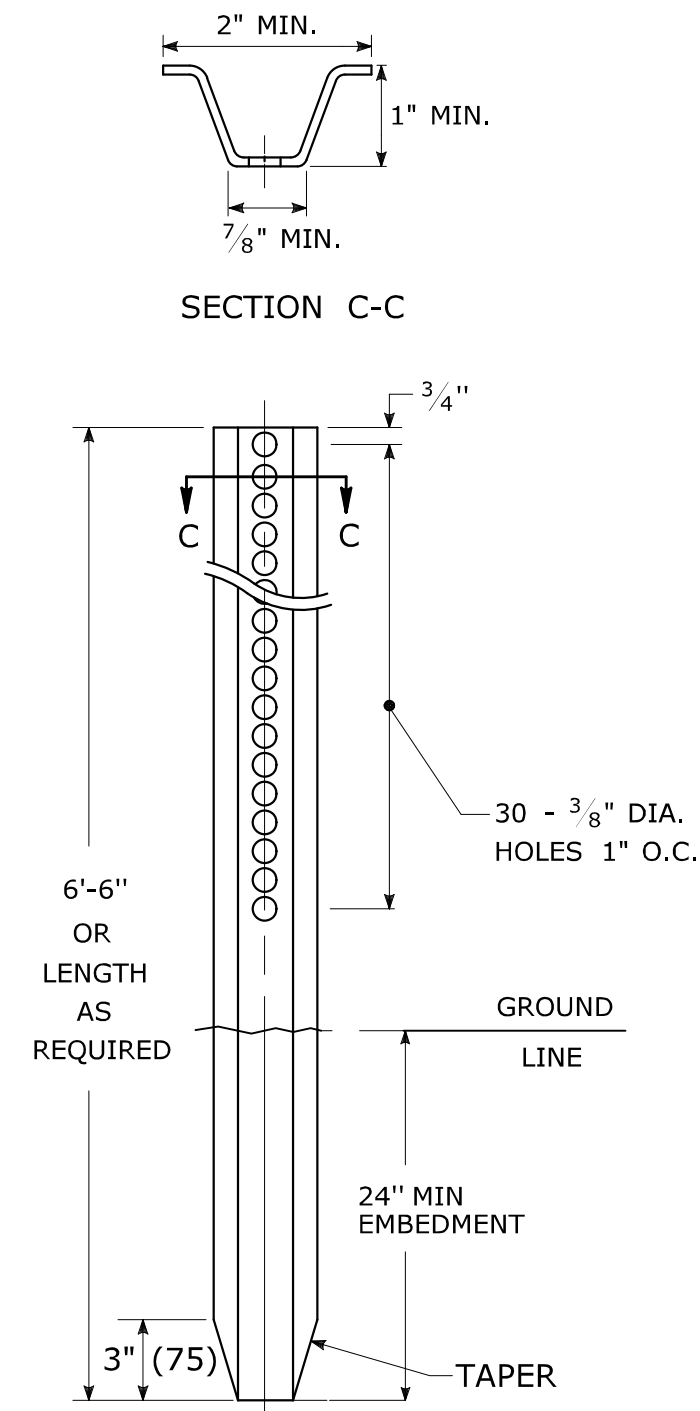
TYPICAL BACK-UP PLATE



BOLTS - STAINLESS STEEL CONFORMING TO ASTM F593, ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316),
 SELF LOCKING NUTS - STAINLESS STEEL CONFORMING TO ASTM F594, ALLOY GROUP 1 OR 2 (ALLOY TYPES 304 OR 316),
 WASHERS - STAINLESS STEEL CONFORMING TO ASTM A240, (ALLOY TYPES 304 OR 316).

METAL DELINEATOR POST

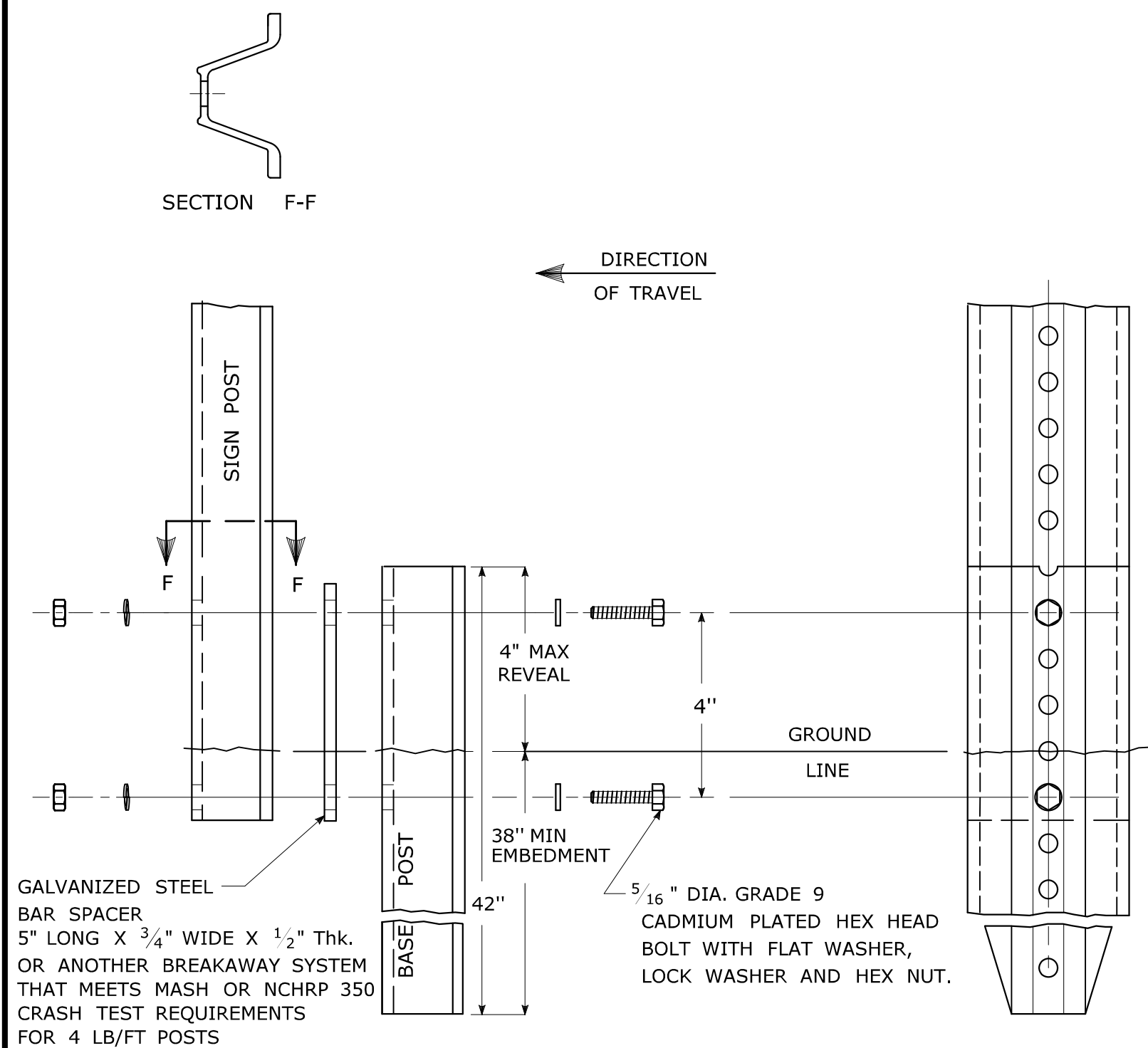
WT./FT. = 1.12 LBS./FT. MIN.



GENERAL NOTES:

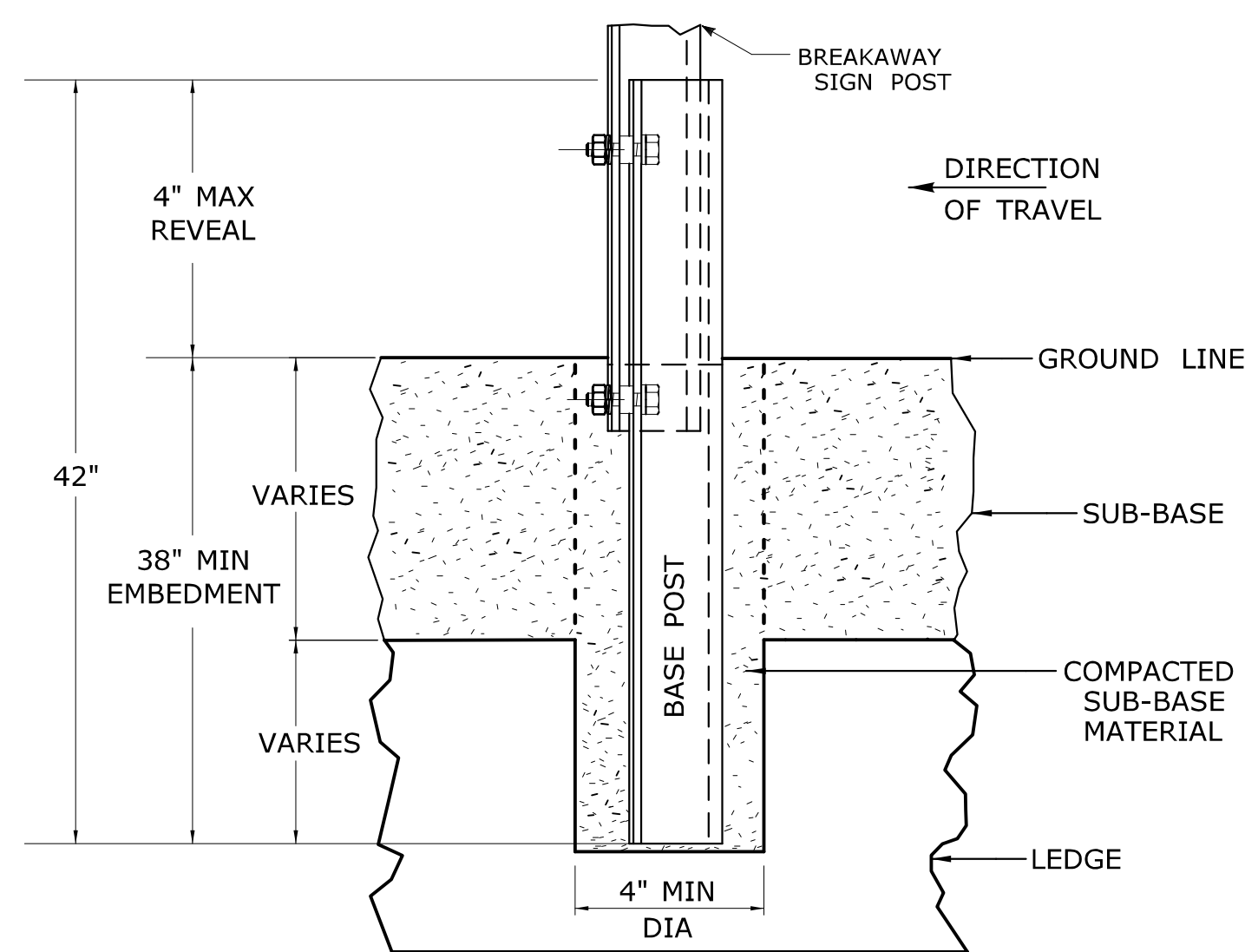
1. STEEL FOR DELINEATOR POSTS SHALL BE ASTM A36 STEEL. STEEL FOR ALL OTHER POSTS SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A 499 GRADE 80 AND TO THE CHEMICAL REQUIREMENTS OF ASTM A1 CARBON STEEL TEE RAIL HAVING NOMINAL WEIGHT (MASS) OF 91 LBS. OR GREATER PER LINEAR YARD.
2. AFTER FABRICATION, ALL STEEL POSTS, STRAPS AND PLATES SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A123.
3. WASHERS FOR BREAKAWAY INSTALLATIONS SHALL MEET ASTM F436, TYPE 1.
4. SPACER BAR FOR BREAKAWAY INSTALLATION SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A36.
5. ALL BOLTS, NUTS, AND WASHERS FOR BREAKAWAY INSTALLATIONS SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A153.
6. ALL SIGN POSTS SHALL HAVE BREAKAWAY FEATURES THAT MEET AASHTO REQUIREMENTS CONTAINED IN THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS." THE BREAKAWAY FEATURES SHALL BE STRUCTURALLY ADEQUATE TO CARRY THE SIGNS SHOWN IN THE PLANS AT 60 MPH WIND LOADINGS. INSTALLATIONS SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
7. SIGN POSTS SHALL BE 4 LBS./FT.

BREAKAWAY INSTALLATION FOR 4 LBS./FT. POSTS

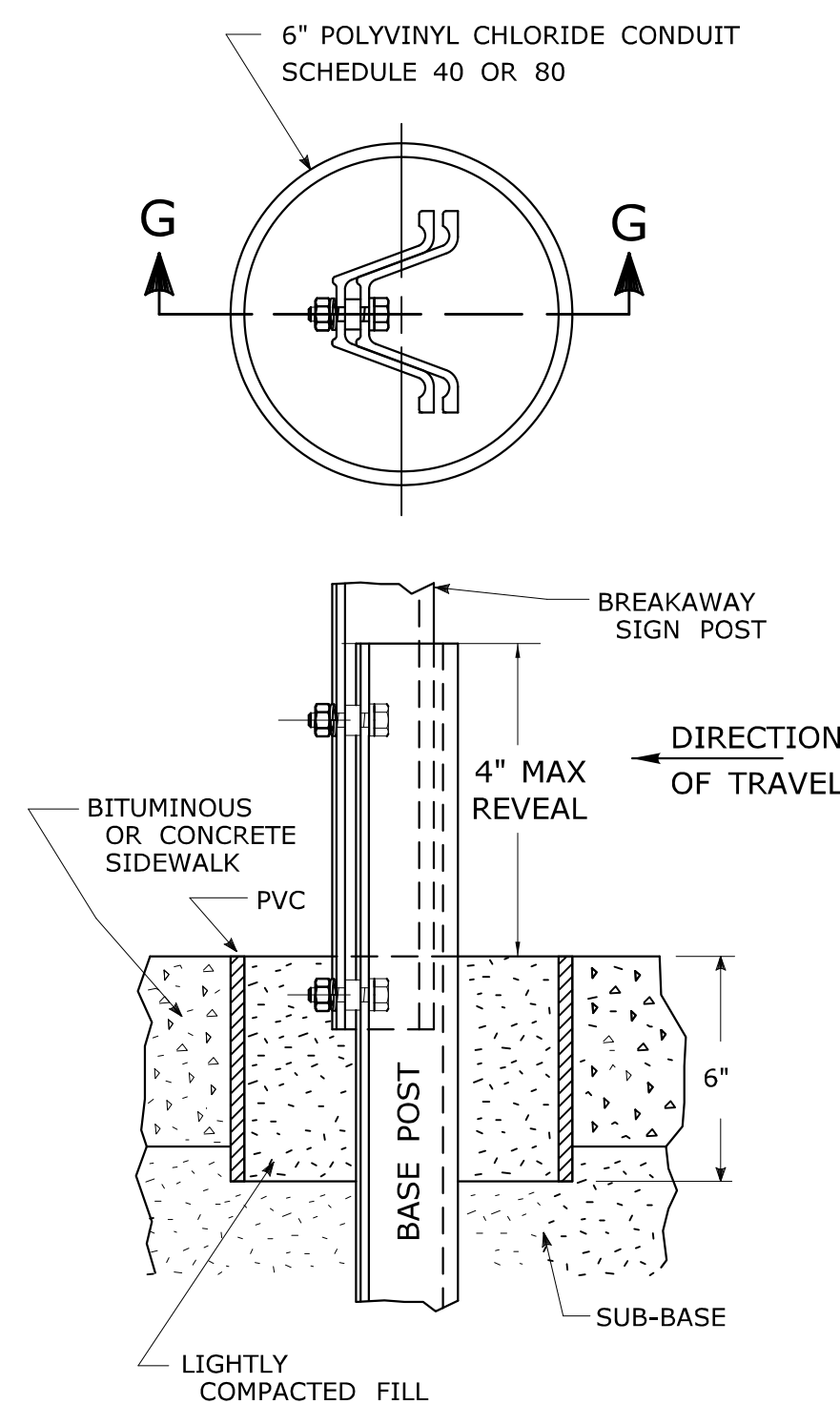


TYPICAL SIGN POST INSTALLATION IN LEDGE

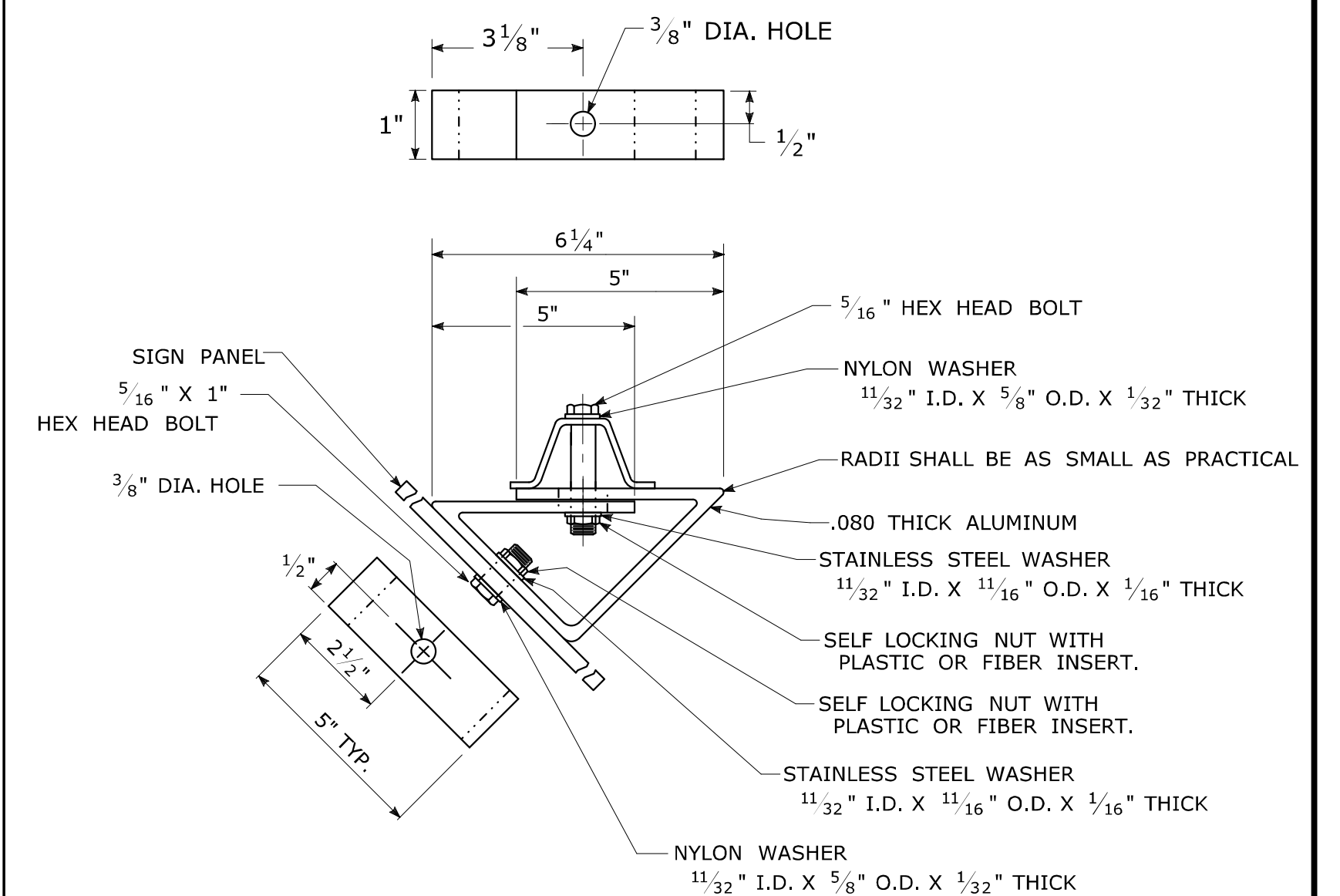
LEDGE SHALL BE REMOVED TO DRIVE THE BASE POST TO A DEPTH OF 38".
 HOLE SHALL BE FILLED WITH SUB-BASE MATERIAL AND COMPACTED WITH A TAMPING BAR, OR TECHNIQUE APPROVED BY THE ENGINEER, PRIOR TO BASE POST INSTALLATION.



TYPICAL SLEEVE FOR PAVED AREAS



45° MOUNTING BRACKET FOR INSTALLATION OF PARKING SIGNS



REV.	DATE	REVISION DESCRIPTION
2	6-2017	SIGN POST REVISIONS.
1	2-2011	MINOR REVISIONS.

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Plotted Date: 6/6/2017

NOT TO SCALE

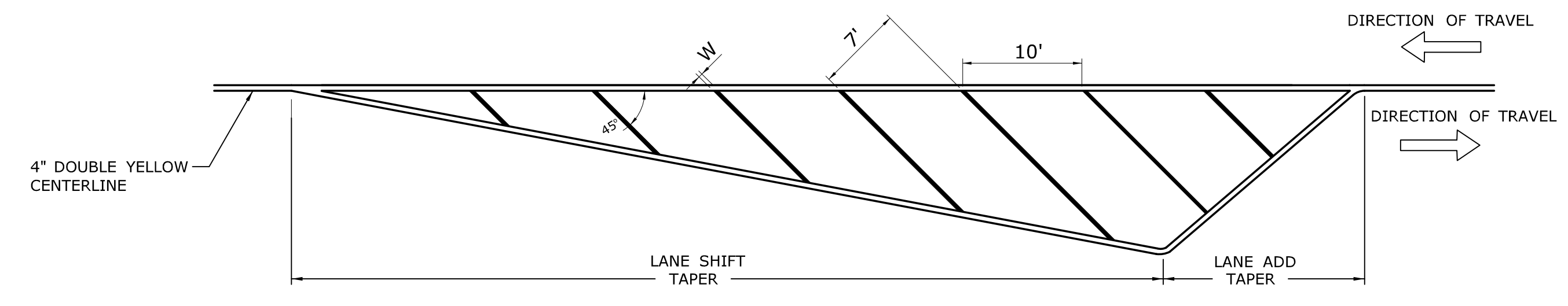
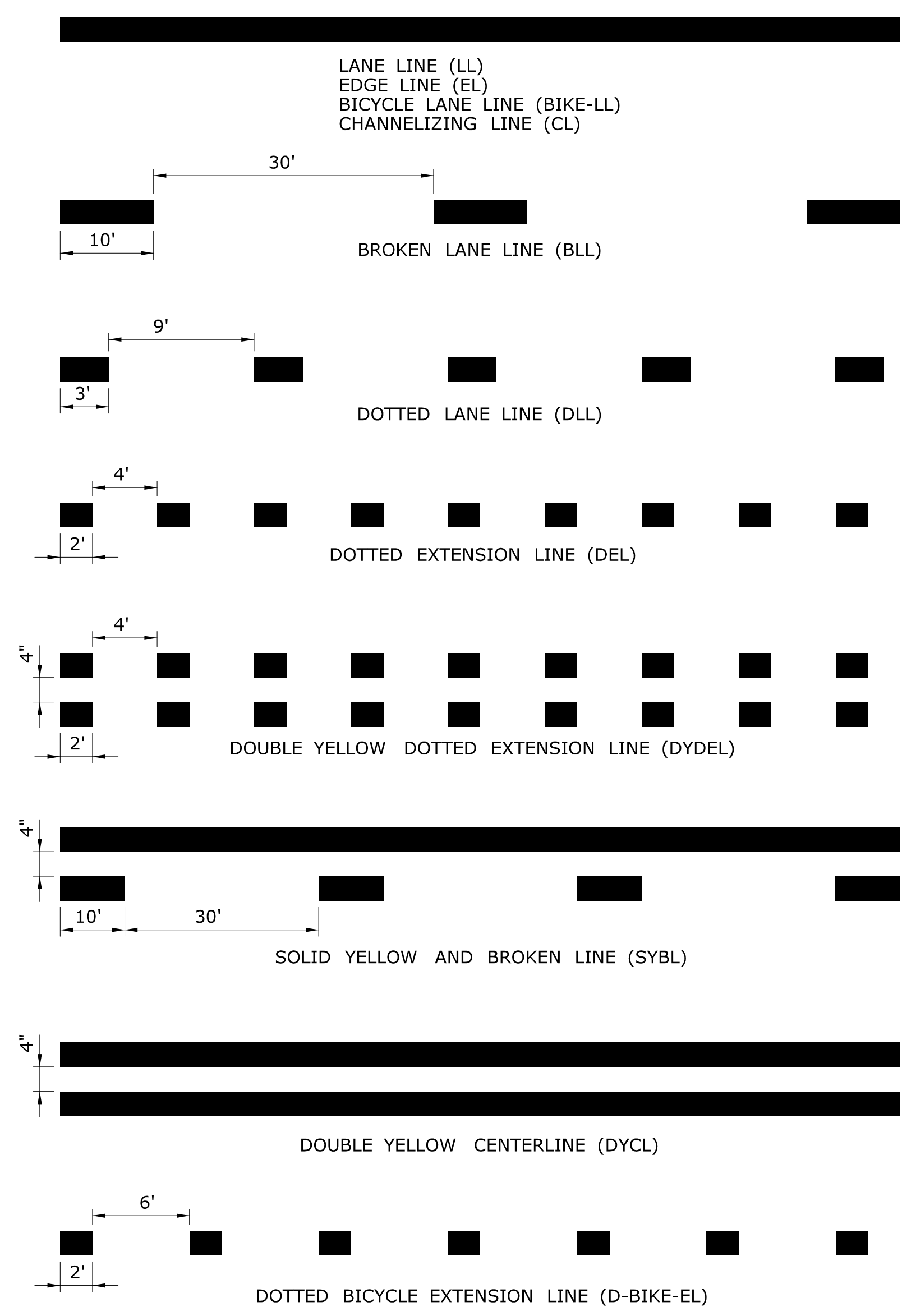


Filename: TR-1208_02_May_2017_Revision.dgn Model: TR-1208_02

SUBMITTED BY:	NAME/DATE/TIME:
Mark F. Makuch	Mark F. Makuch, P.E. 2017.06.07 07:30:30-04'00'
Mary E. Baker	Mary E. Baker, P.E. 2017.06.13 15:28:14-04'00'
Gregory M. Dorosh	Gregory M. Dorosh, P.E. 2017.06.15 09:27:29-04'00'

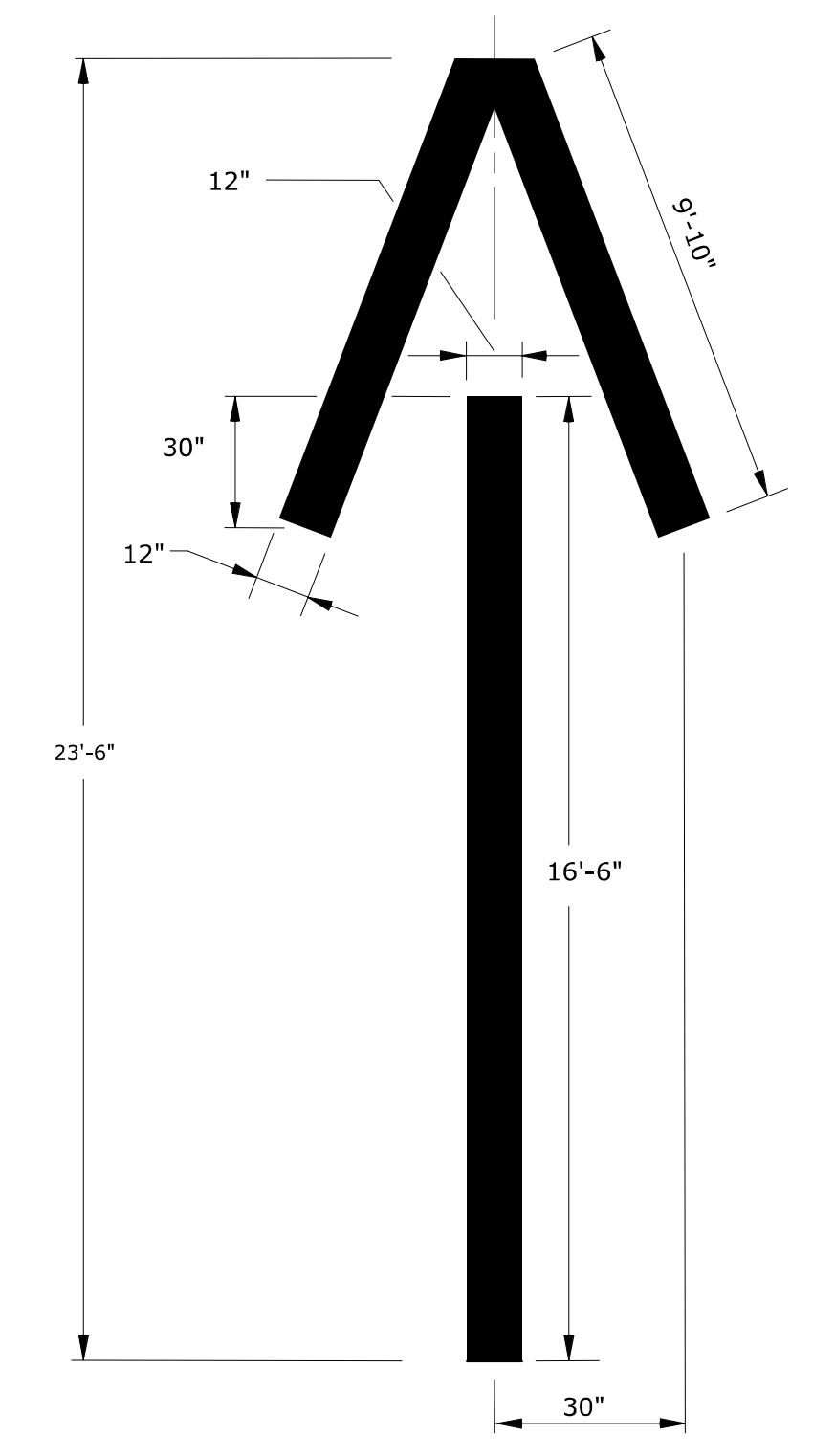
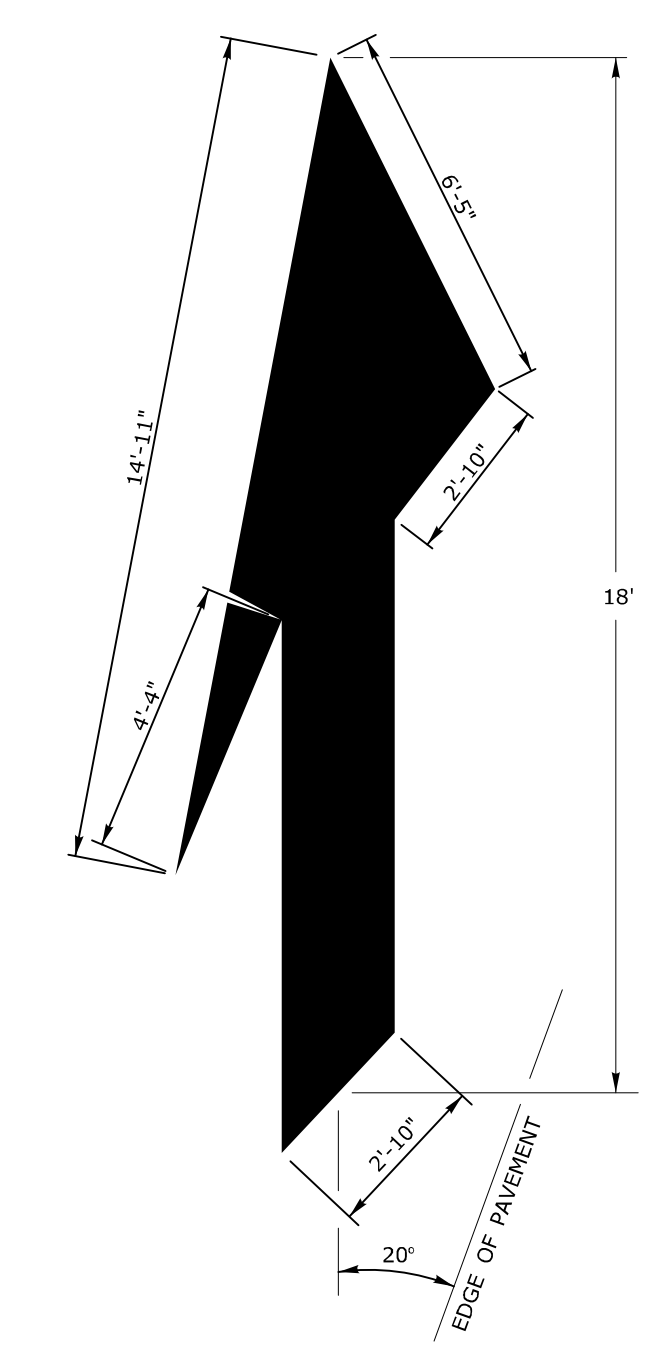
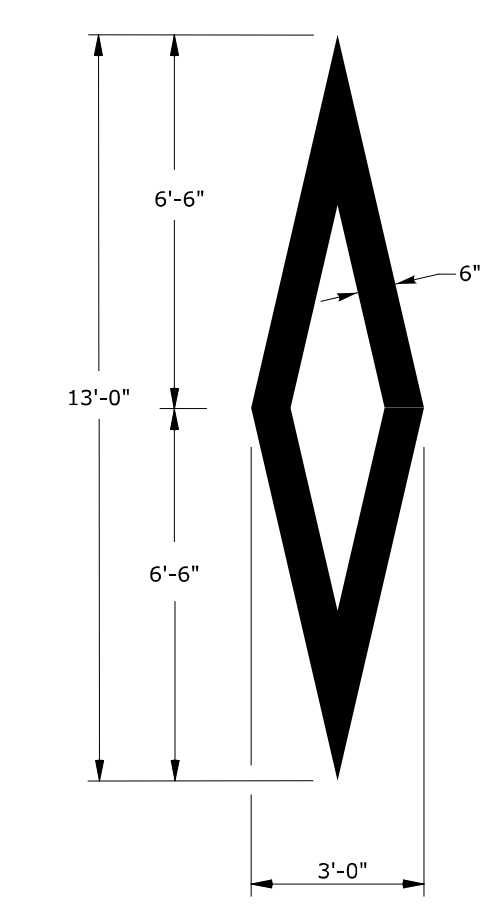
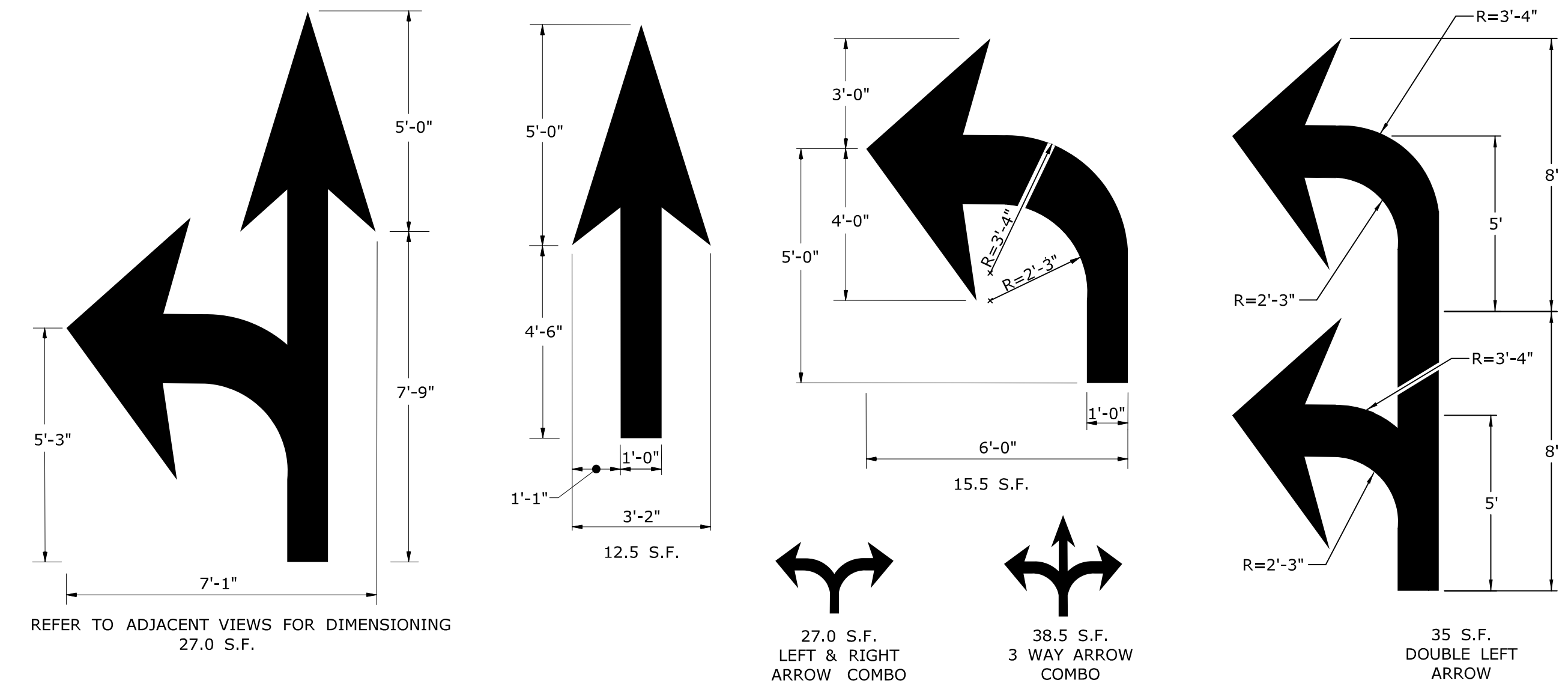
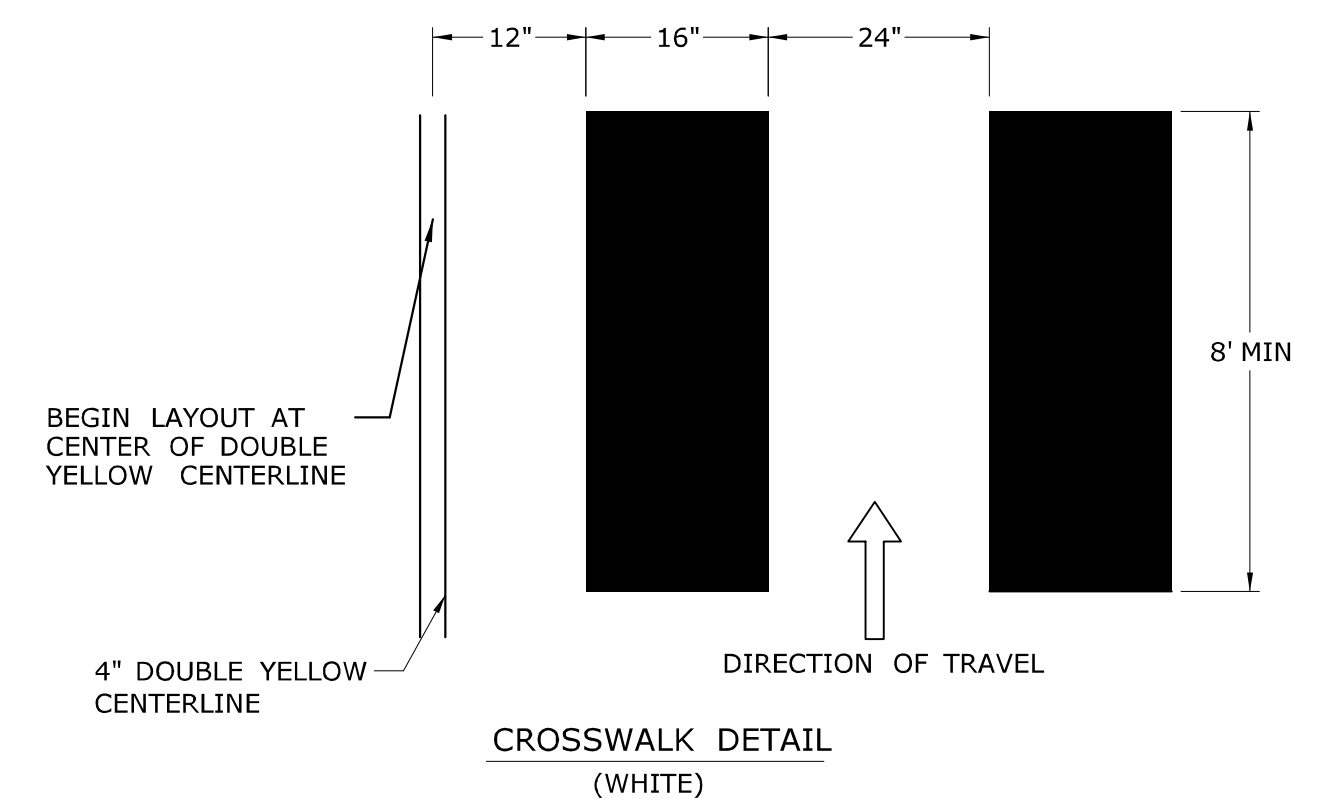
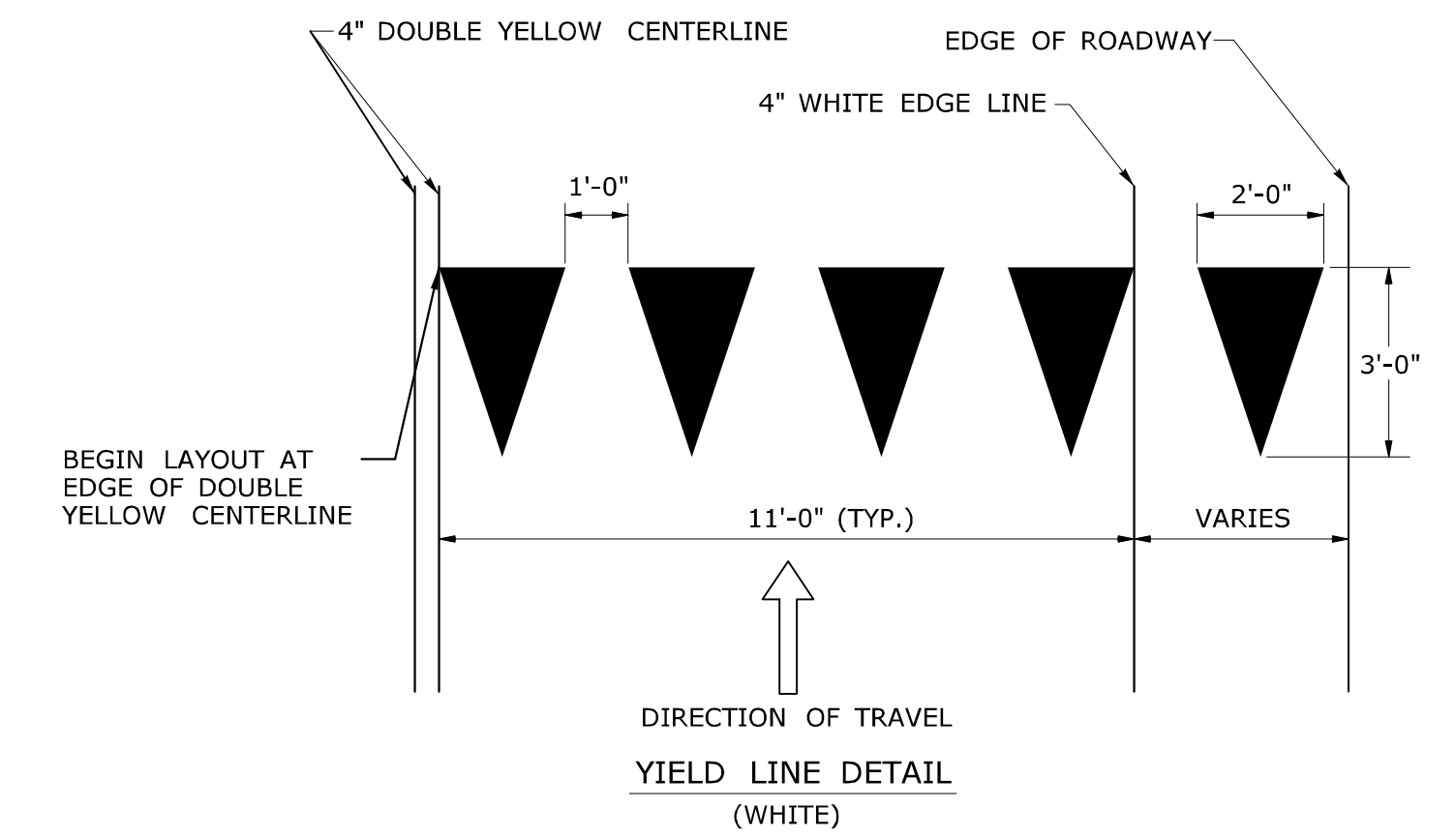
CTDOT
 STANDARD SHEET
 OFFICE OF ENGINEERING

STANDARD SHEET TITLE:	GUIDE SHEET NO.:
METAL SIGN POSTS AND SIGN MOUNTING DETAILS	TR-1208_02



CROSS HATCHED ISLAND DETAIL
(YELLOW)

W IS TO BE 6" WHEN POSTED SPEED ≤ 45 MPH
W IS TO BE 12" WHEN POSTED SPEED > 45 MPH
CROSS HATCHED ISLANDS ARE TO BE INSTALLED WHERE CALLED FOR ON THE PLANS



- NOTES :
1. AREA OF PAVEMENT MARKINGS AS INDICATED IS APPROXIMATE.
 2. RIGHT TURN PAVEMENT MARKING ARROWS ARE MIRROR IMAGE OF LEFT TURN PAVEMENT MARKING ARROWS.

REV.	DATE	REVISION DESCRIPTION
1	8-2018	REMOVED ROUNDABOUT MARKINGS.

Plotted Date: 8/10/2018

NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: TR-1210_04.dgn Model: CT_Civil_2D_Sheet

SUBMITTED BY:	NAME/DATE/TIME:
<i>Mark Makuch</i>	Mark F. Makuch, P.E. 2018.08.17 09:07:44-04'00'
APPROVED BY:	NAME/DATE/TIME:
<i>Mark F. Carlino</i>	Mark F. Carlino, P.E. 2018.08.21 07:48:45-04'00'

CTDOT
STANDARD SHEET
OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
PAVEMENT MARKING LINES AND SYMBOLS

STANDARD SHEET NO.:
TR-1210_04

E5 - SERIES				G20 - SERIES				M4 - SERIES				R1 - SERIES				R9 & R11 - SERIES				W1 - SERIES				W3 - SERIES																																																																																																																											
<p>COPY & BORDER - WHITE BACKGROUND - GREEN</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>16.0</td> <td>48</td> <td>51-6147</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	51-6147	2	<p>END ROAD WORK</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>8.0</td> <td>48X24</td> <td>80-9612</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	8.0	48X24	80-9612	2	<p>DETOUR</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>2.0</td> <td>24X12</td> <td>80-9707</td> <td>1</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.0	24X12	80-9707	1	<p>STOP</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>5.19</td> <td>30</td> <td>31-0552</td> <td>1</td> </tr> <tr> <td>13.30</td> <td>48</td> <td>31-0557</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	5.19	30	31-0552	1	13.30	48	31-0557	2	<p>COPY & BORDER - BLACK BACKGROUND - WHITE</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>3.75</td> <td>30X18</td> <td>80-9076</td> <td>1</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	3.75	30X18	80-9076	1	<p>VARIABLE MILEAGE COPY & BORDER - BLACK BACKGROUND - WHITE</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>12.5</td> <td>60X30</td> <td>80-9077</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	12.5	60X30	80-9077	2	<p>(L) (R)</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>9.0</td> <td>36</td> <td>80-9432L</td> <td>1</td> </tr> <tr> <td>9.0</td> <td>36</td> <td>80-9431R</td> <td>1</td> </tr> <tr> <td>16.0</td> <td>48</td> <td>80-9452L</td> <td>2</td> </tr> <tr> <td>16.0</td> <td>48</td> <td>80-9451R</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9432L	1	9.0	36	80-9431R	1	16.0	48	80-9452L	2	16.0	48	80-9451R	2	<p>(L) (R)</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>16.0</td> <td>48</td> <td>80-9433L</td> <td>2</td> </tr> <tr> <td>16.0</td> <td>48</td> <td>80-9435R</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	80-9433L	2	16.0	48	80-9435R	2	<p>(L) (R)</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>25.0</td> <td>60</td> <td>80-9483L</td> <td>2</td> </tr> <tr> <td>25.0</td> <td>60</td> <td>80-9485R</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	25.0	60	80-9483L	2	25.0	60	80-9485R	2	<p>OCTAGON - RED W/ WHITE BORDER ARROW & BORDER - BLACK BACKGROUND - FLUORESCENT ORANGE</p> <table border="1"> <thead> <tr> <th>AREA (SQ. FT)</th> <th>SIZE (INCHES)</th> <th>CONN. D.O.T. #</th> <th>POSTS</th> </tr> </thead> <tbody> <tr> <td>9.0</td> <td>36</td> <td>80-9050</td> <td>1</td> </tr> <tr> <td>16.0</td> <td>48</td> <td>80-9051</td> <td>2</td> </tr> </tbody> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9050	1	16.0	48	80-9051	2
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9.0	36	80-9431R	1																																																																																																																																																
16.0	48	80-9452L	2																																																																																																																																																
16.0	48	80-9451R	2																																																																																																																																																
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6	8-2018	REVISED POST REQUIREMENTS AND SHEETING TYPE.	THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.	<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p>	SUBMITTED BY: <i>Mark Makuch</i> NAME/DATE/TIME: Mark F. Makuch, P.E. 2018.08.17 09:11:08-04'00'	CTDOT STANDARD SHEET OFFICE OF ENGINEERING	STANDARD SHEET TITLE: SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS	STANDARD SHEET NO.: TR-1220_01
5	8-2015	UPDATED PER MUTCD AND FORM 816 JAN 2015 REVISION.			APPROVED BY: <i>Mark F. Carino</i> NAME/DATE/TIME: Mark F. Carino, P.E. 2018.08.21 07:49:34-04'00'			
4	6-2012	REVISED NOTE #1 TO REFERENCE "O.S.T.A."						
3	4-2012	REVISED NEW SIGNAL SIGN(S) TO CONFORM TO 2009 MUTCD.						
2	2-2011	MINOR REVISIONS.						
1	3-2010	REMOVED OBSOLETE SIGNS (50-5925, 50-5935).						
REV.	DATE	REVISION DESCRIPTION	Plotted Date: 8/10/2018	NOT TO SCALE	Filename: TR-1220_01_1.2018.dgn Model: TR-1220_01			

NOTES:

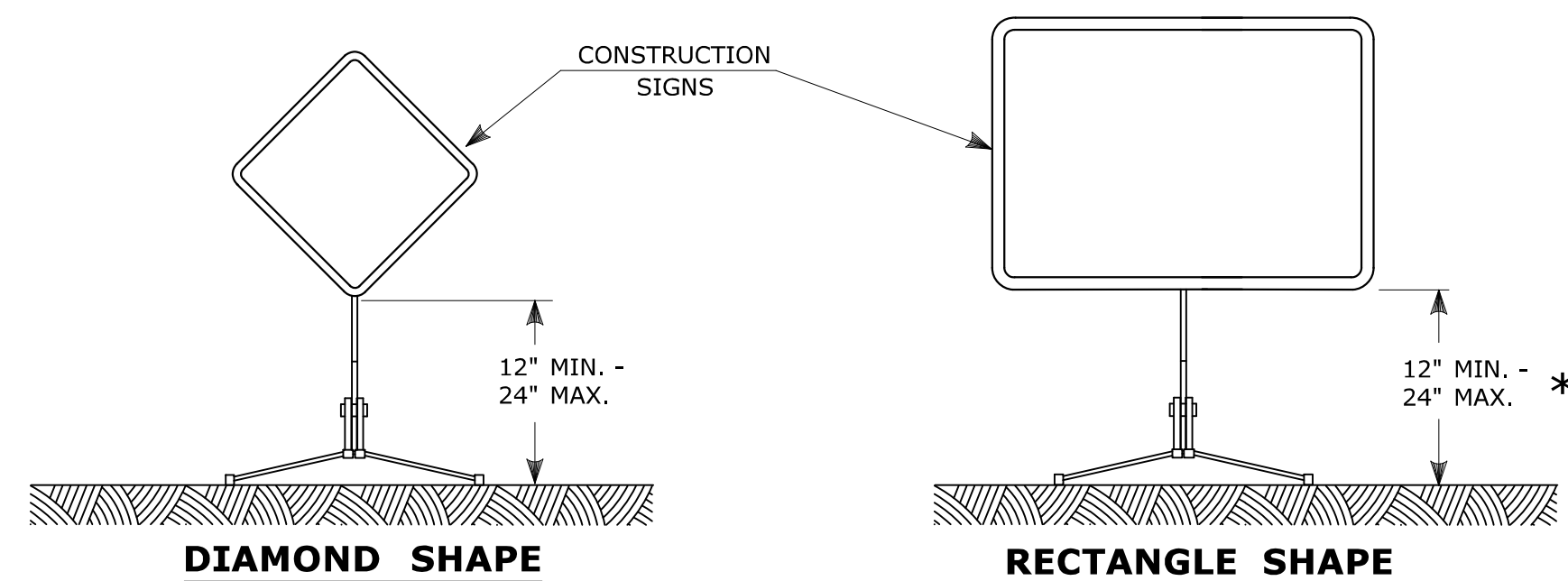
- R1-SERIES SIGN THE LEGEND "O.S.T.A." SHALL APPEAR.
- POSTS - SEE STANDARD SHEET TR-1208_02 - "METAL SIGN POSTS AND SIGN MOUNTING DETAILS".
- POSTS SHALL BE 4 LBS./FT.
- ALL POSTS NOTED ARE FOR LONG TERM INSTALLATION. SEE STANDARD SHEET TR-1208_02.
- FOR TEMPORARY SUPPORTS SEE STANDARD SHEET TR-1220_02 - "CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES".
- FOR SPECIFIC SIGN DESIGN, CONTACT CONN. D.O.T., DIVISION OF TRAFFIC ENGINEERING. FOR BOLT HOLE PATTERN REFER TO FHWA PUBLICATION "STANDARD HIGHWAY SIGNS". SIGNS OF DIFFERENT DIMENSIONS TO BE ERCTED ON THE SAME POSTS, OR SPAN/MAST ARM MOUNTED, MAY REQUIRE SPECIAL BOLT HOLE PATTERNS.
- ALL CONSTRUCTION SIGNS TO BE PAID FOR UNDER THE CONSTRUCTION SIGNS ITEM IN THE CONTRACT.
- MATERIALS & COLORS SHALL CONFORM TO STATE SPECIFICATIONS.

MATERIALS:

SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES. ALUMINUM THICKNESS FOR POST MOUNTED SIGNS SHALL BE .100" EXCEPT SIGN #s. 80-1605, 80-9914, 80-9815, 80-9728, 80-9519, & 51-6147 (L OR R) WHICH SHALL BE .125". PLYWOOD THICKNESS FOR POST MOUNTED SIGNS SHALL BE 1/2" EXTERIOR GRADE A-C OR BETTER. SIGN BLANKS SHALL HAVE ONE COAT OF PRIMER PAINT PRIOR TO APPLICATION OF RETROREFLECTIVE SHEETING & COPY.

COLORS:

BACKGROUND - FLUORESCENT ORANGE - EXCEPT AS NOTED.
 LEGEND - BLACK - EXCEPT AS NOTED.
 ALL SIGNS WITH FLUORESCENT ORANGE BACKGROUND TO USE TYPE VIII RETROREFLECTIVE SHEETING.
 ALL OTHER SIGNS TO USE TYPE IX RETROREFLECTIVE SHEETING.

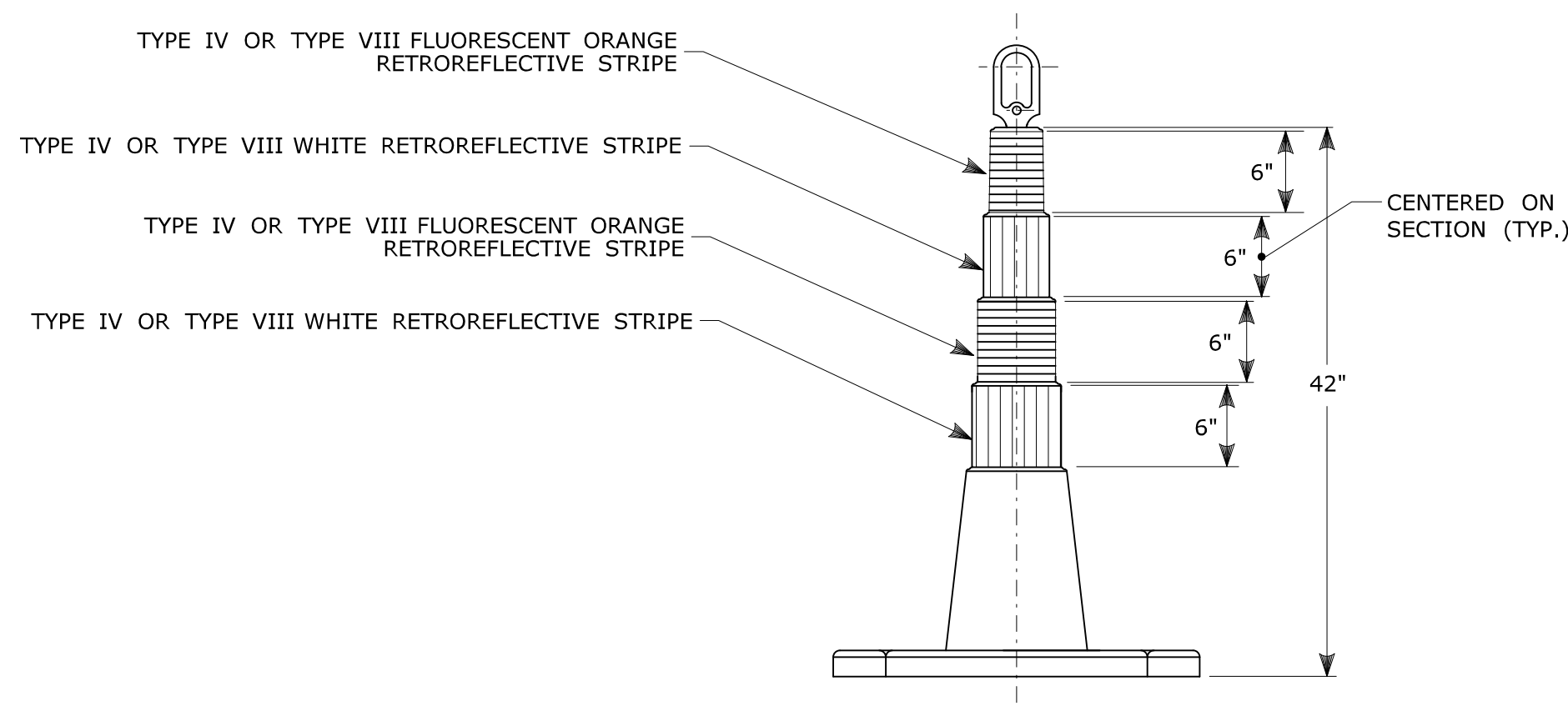


PORTABLE CONSTRUCTION SIGNS

NOTES FOR PORTABLE SIGN SUPPORTS:

- SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- MOUNTING HEIGHT OF SIGNS SHALL BE A MINIMUM OF 12" AND A MAXIMUM OF 24". SIGNS SHALL BE MOUNTED HIGHER AS NEEDED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY SUPPORT DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- PORTABLE SIGN SUPPORTS SHALL BE STABILIZED IN A MANNER THAT WILL NOT AFFECT THEIR COMPLIANCE WITH NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 2 DEVICES.
- PORTABLE CONSTRUCTION SIGN SUPPORTS SHOULD NOT BE USED FOR DURATION OF MORE THAN 3 DAYS EXCEPT FOR R9-8 THROUGH R9-11a SERIES, R11 SERIES, W1-6 THROUGH W1-8 SERIES, M4-10, AND E5-1. SEE STANDARD SHEET TR-1220.01 - "SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS" FOR SIGN DETAILS.

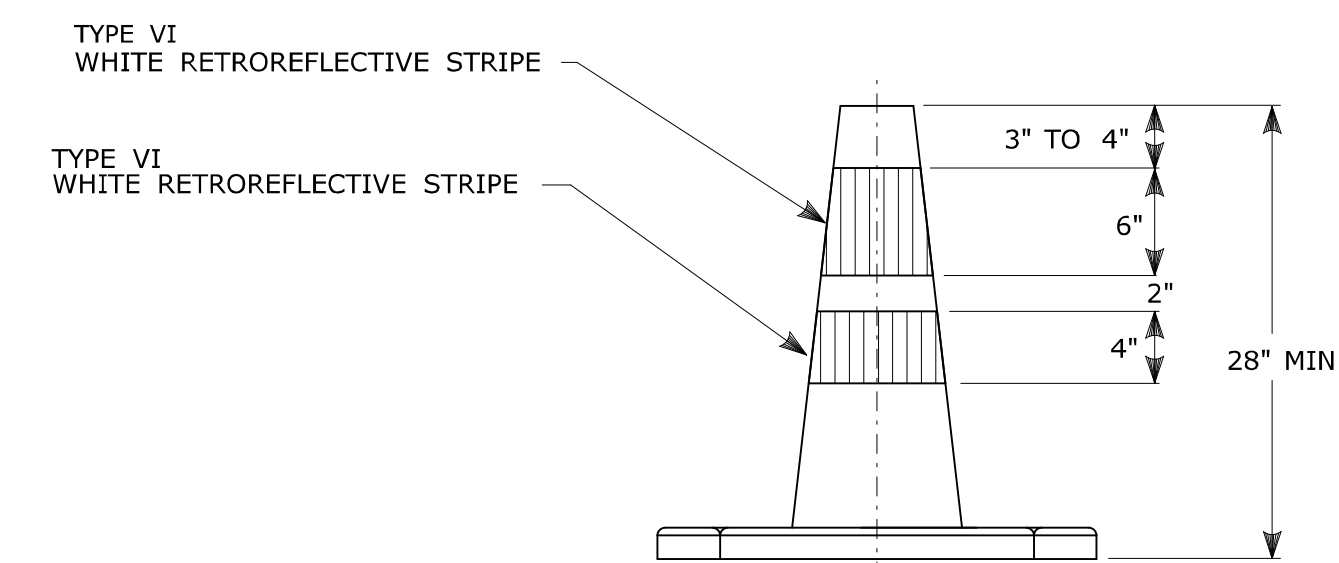
* FOR E5-1 (EXIT SIGNS) USE MIN 48".



42" TRAFFIC CONE

NOTES:

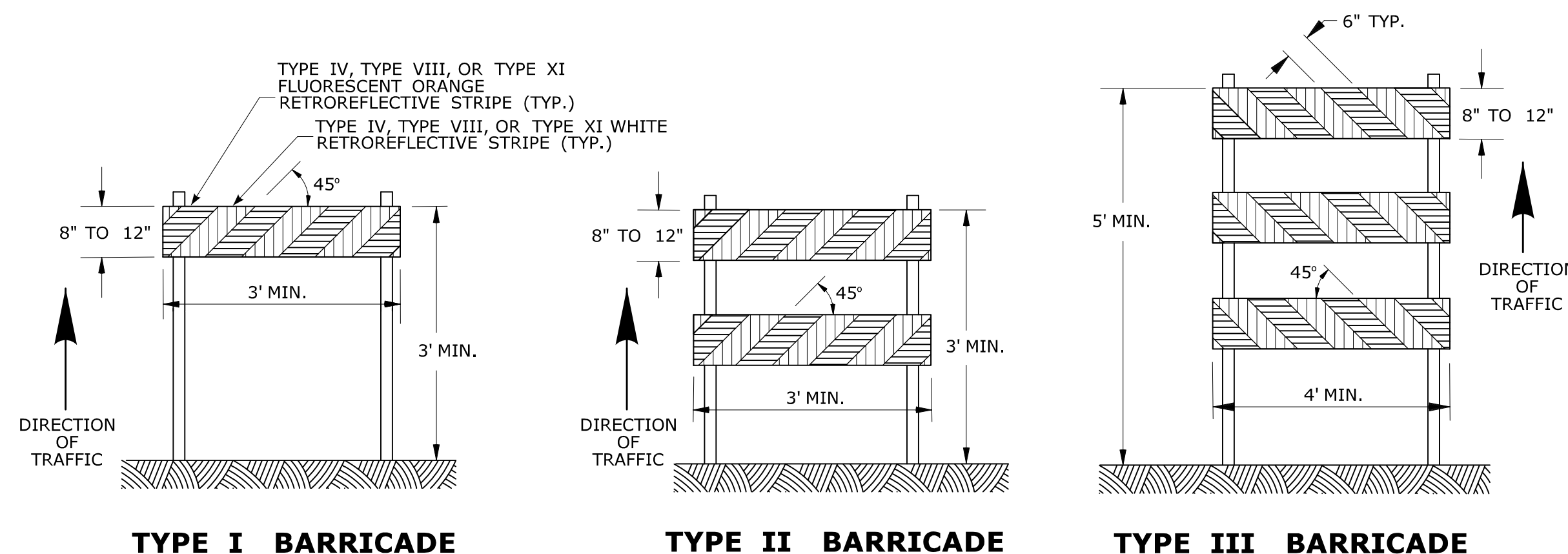
- TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
- IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



TRAFFIC CONE

NOTES:

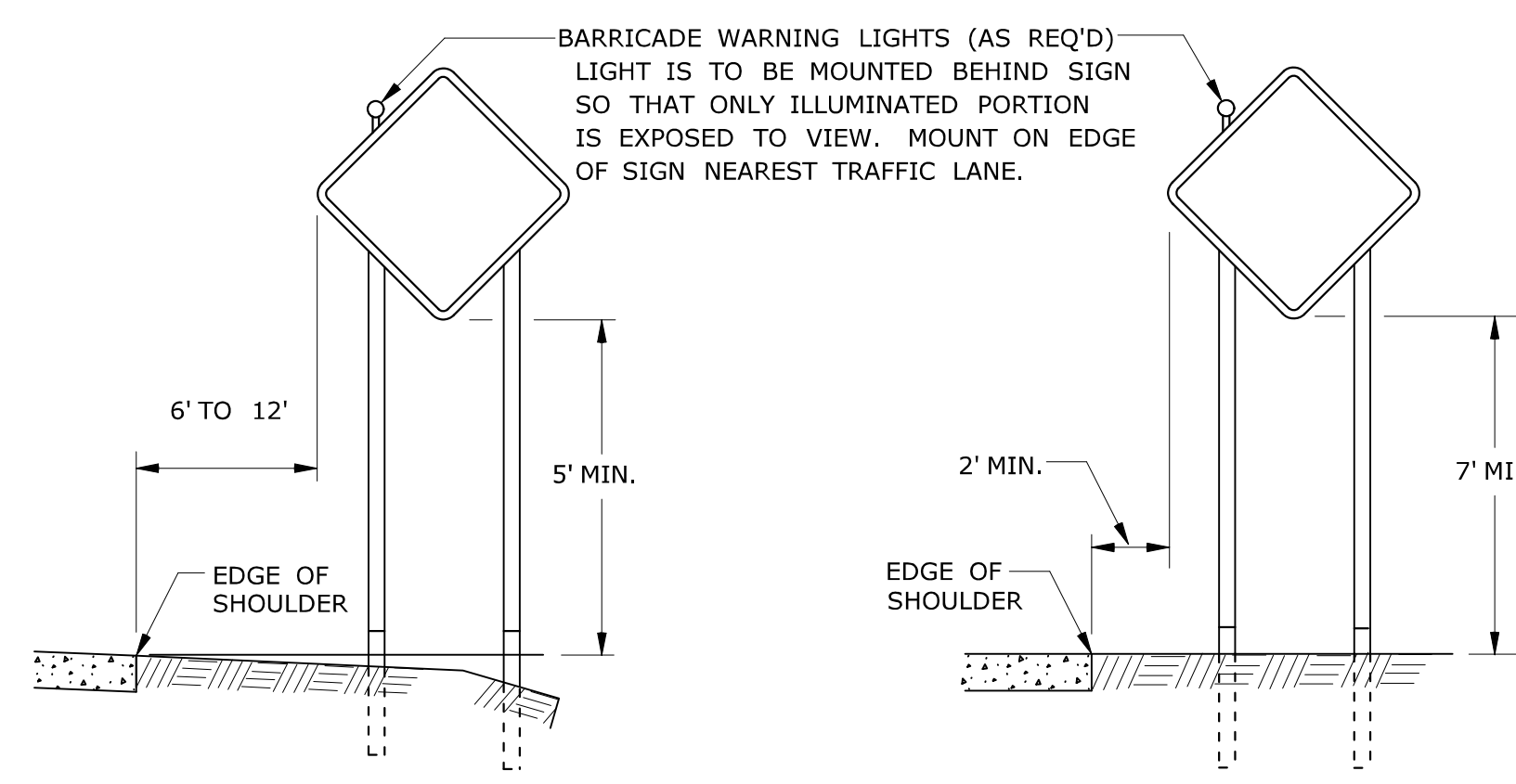
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- IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- TRAFFIC CONES NOT USED AT NIGHT MAY UTILIZE TYPE III SHEETING.
- THE SECTIONS OF CONES NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.



CONSTRUCTION BARRICADES

NOTES:

- CONSTRUCTION BARRICADES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH AND THE LATEST EDITION OF THE MUTCD.
- MARKINGS FOR BARRICADE RAILS SHALL BE ALTERNATE FLUORESCENT ORANGE AND WHITE STRIPES SLOPING DOWNWARD IN THE DIRECTION TRAFFIC IS TO PASS. 6" WIDE STRIPES SHALL BE USED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS. THE SIDES OF BARRICADES FACING TRAFFIC SHALL HAVE RETROREFLECTIVE RAIL FACES.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY BARRICADE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- CORNERS OF BARRICADE RAILS SHALL BE ROUNDED.
- SIGNS MAY ONLY BE INSTALLED ON TYPE III BARRICADES AND SHALL BE PLACED SO AS TO COVER NO MORE THAN ONE BARRICADE RAIL.



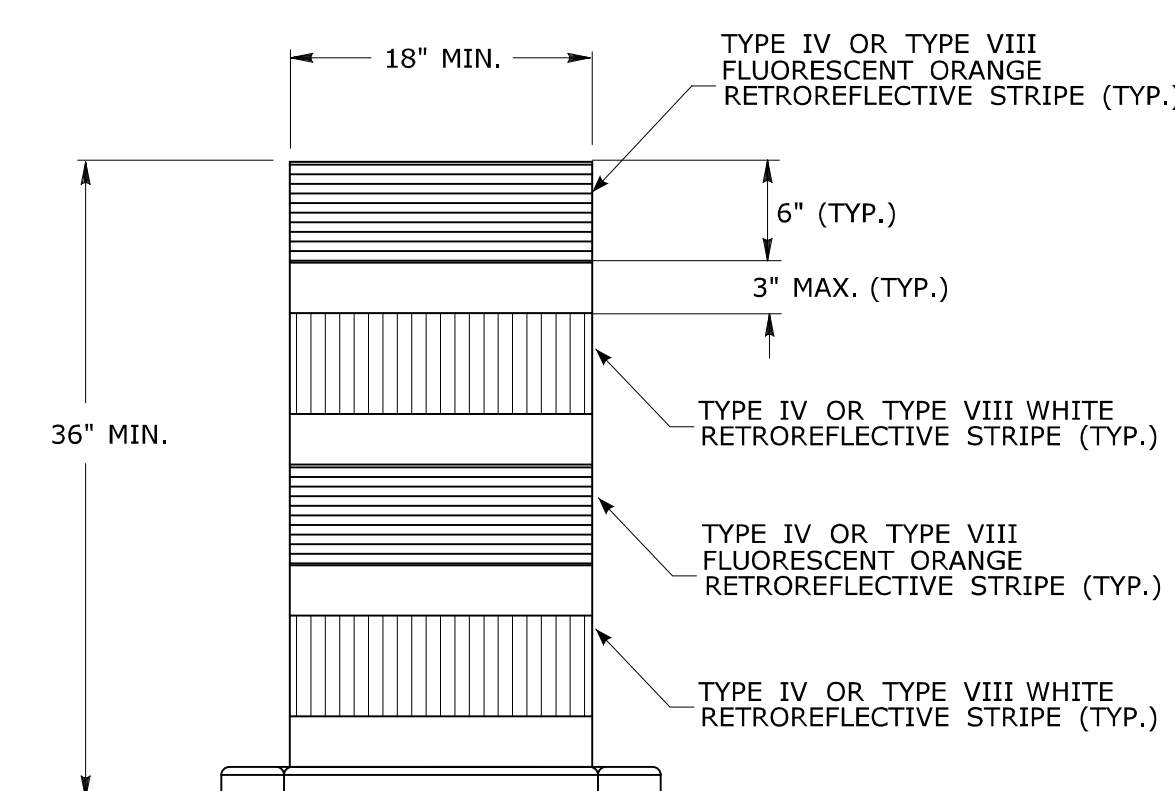
RURAL AREA

URBAN AREA

**PLACEMENT OF CONSTRUCTION SIGNS
TYPICAL LONG TERM INSTALLATION**

NOTES:

- SUPPORTS SHALL BE METAL SIGN POSTS AND HAVE BREAK-AWAY FEATURES.
REFER TO STANDARD SHEETS:
TR-1208.01 - "SIGN PLACEMENT AND RETROREFLECTIVE STRIP DETAILS."
TR-1208.02 - "METAL SIGN POSTS AND SIGN MOUNTING DETAILS."

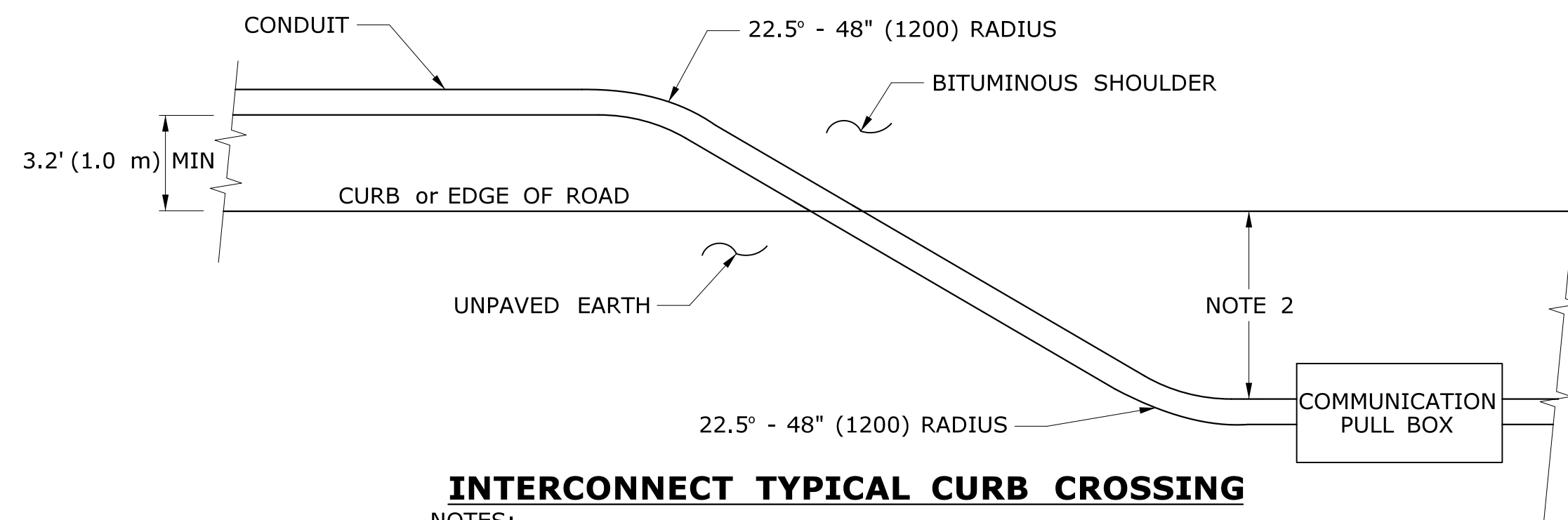


**TRAFFIC DRUM
FRONT VIEW**

NOTES:

- TRAFFIC DRUM SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) OR THE AASHTO MASH FOR CATEGORY 1 DEVICES AND THE LATEST EDITION OF THE MUTCD.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY DRUM DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF FLUORESCENT ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- THE SECTIONS OF DRUMS NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.

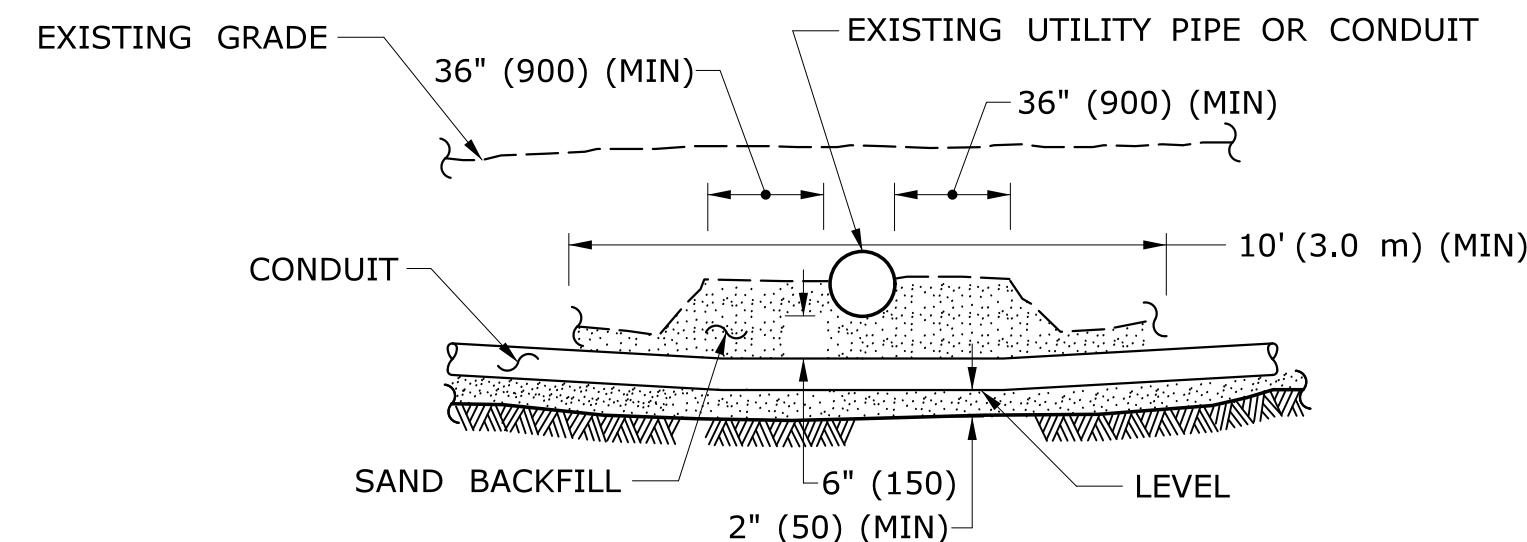
<p>THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.</p>		<p>NOT TO SCALE</p>	<p>STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION</p>	<p>SUBMITTED BY: <i>Mark Makuch</i> NAME/DATE/TIME: Mark F. Makuch, P.E. 2018.08.17 09:12:43-04'00'</p>	<p>CTDOT STANDARD SHEET</p>	<p>STANDARD SHEET TITLE: CONSTRUCTION SIGN SUPPORTS AND CHANNELIZING DEVICES</p>	<p>STANDARD SHEET NO.: TR-1220_02</p>
<p>3 8-2018 UPDATED SHEETING TYPE AND COLOR.</p> <p>2 8-2015 UPDATED PER MUTCD AND FORM 816 JAN 2015 REVISION.</p> <p>1 2-2011 MINOR REVISIONS.</p>	<p>APPROVED BY: <i>Mark F. Carino</i> NAME/DATE/TIME: Mark F. Carino, P.E. 2018.08.21 07:49:51-04'00'</p>			<p>OFFICE OF ENGINEERING</p>			
<p>REV. DATE REVISION DESCRIPTION</p>	<p>Plotted Date: 8/10/2018</p>		<p>Filename: TR-1220_02_3_2018.dgn Model: TR-1220_02</p>				



INTERCONNECT TYPICAL CURB CROSSING

NOTES:

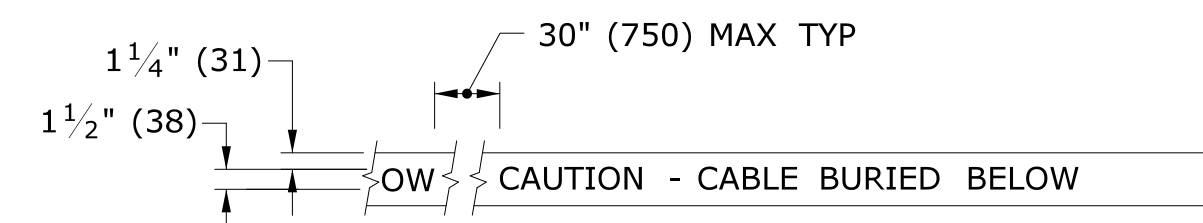
1. RESTORE AREAS DISTURBED BY TRENCH TO ORIGINAL CONDITION.
2. INSTALL PULL BOX A MINIMUM OF 10' (3.0 m) FROM CURB UNLESS OTHERWISE SHOWN ON PLANS OR DIRECTED BY ENGINEER.



CROSSING UNDER EXISTING UTILITY

NOTES:

1. WHEN ENCOUNTERED AT APPROXIMATELY THE SAME DEPTH, CROSS BENEATH.
2. PROTECT & SUPPORT EXPOSED EXISTING UTILITY.



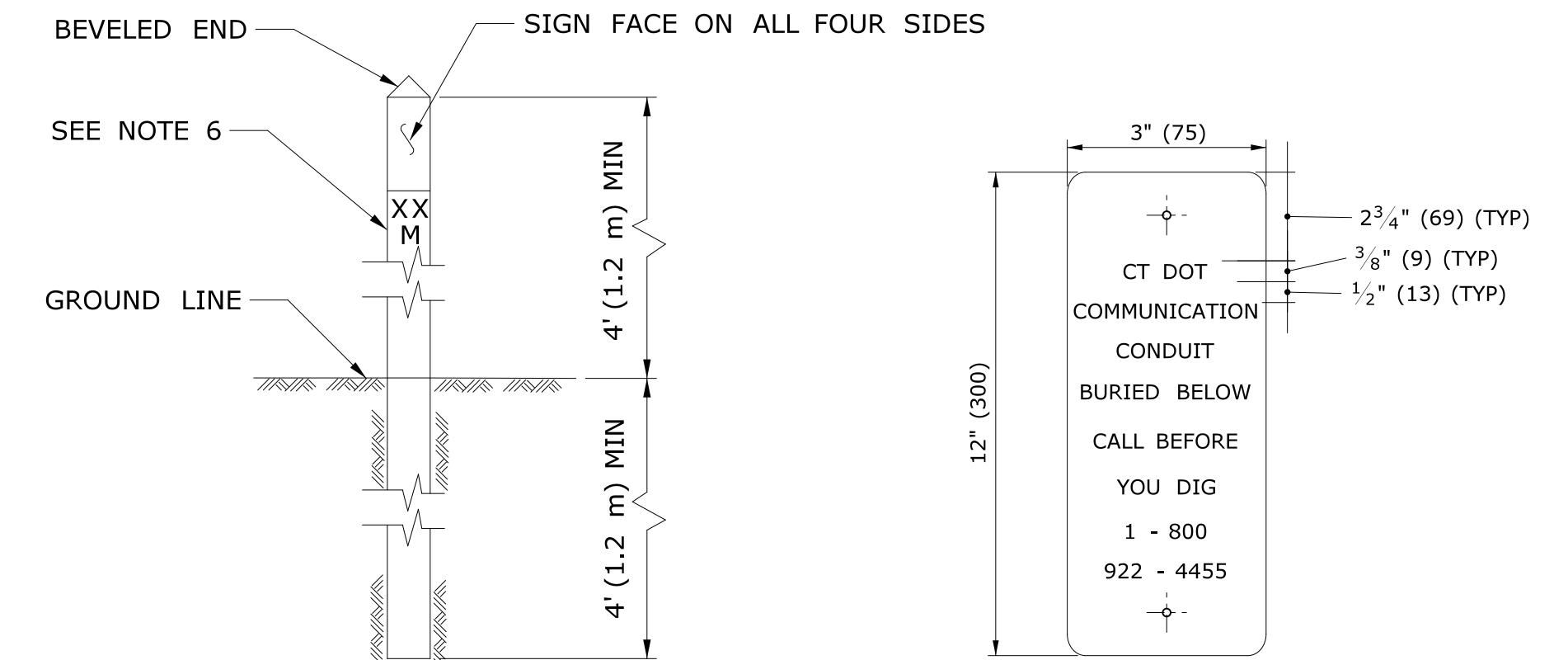
DETECTABLE WARNING TAPE

NOTE:

STANDARD SPECIFICATIONS, ARTICLE: 1.05.15

1. TAPE COLORS:

- COMMUNICATION - ORANGE BACKGROUND / BLACK LEGEND
- POWER - RED BACKGROUND / BLACK LEGEND



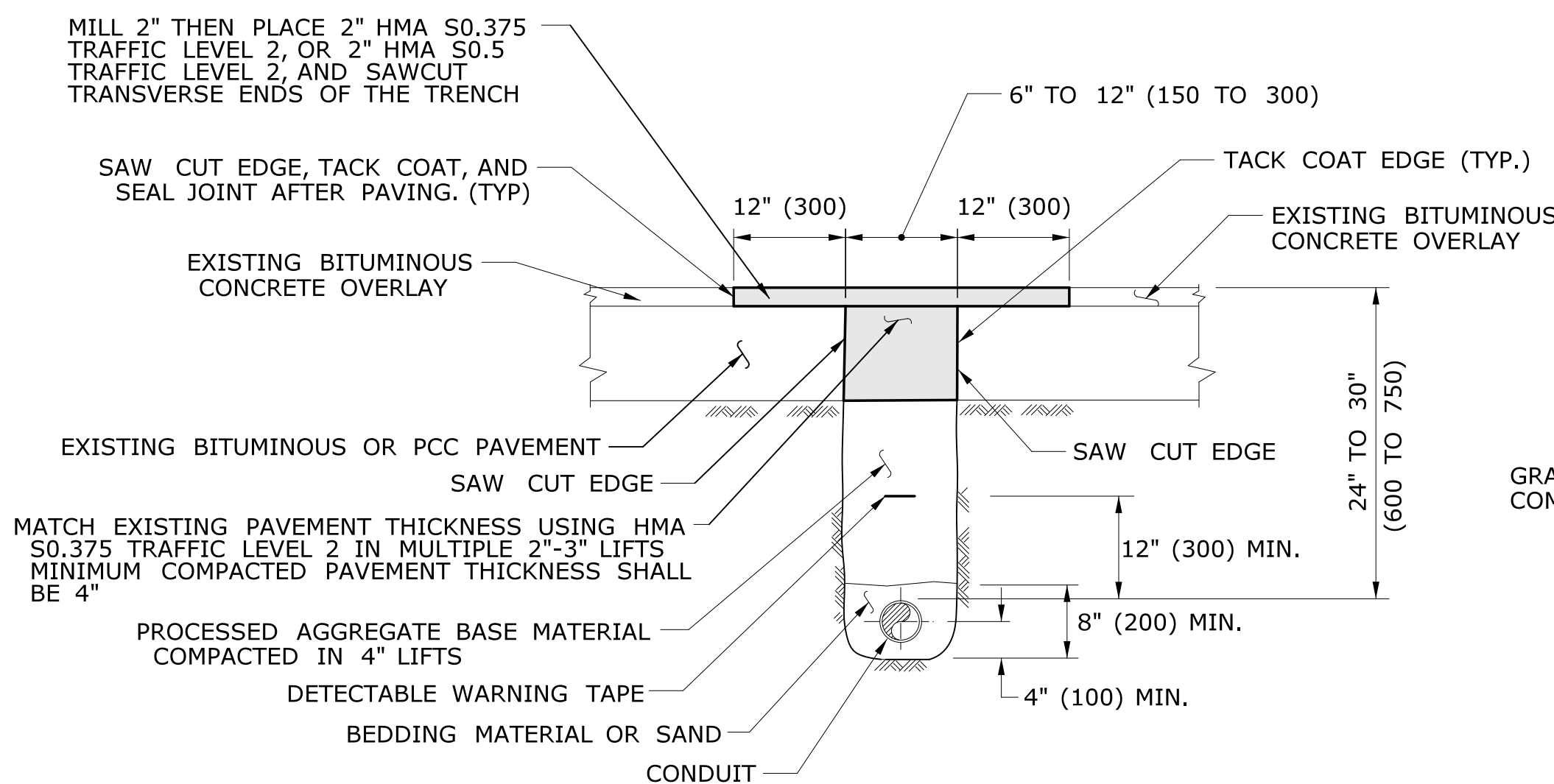
INTERCONNECT CONDUIT IDENTIFICATION POST

NOTES:

1. 4" x 4" (100 x 100) NOMINAL, PRESSURE TREATED WOOD POST.
2. ATTACH SIGN TO POST WITH 1/4" x 1 1/4" (6 x 31) STAINLESS STEEL LAG SCREW WITH NYLON WASHER ON FACE OF SIGN.
3. SIGN COLORS: BACKGROUND - ORANGE (RETROREFLECTIVE) LEGEND - BLACK (OPAQUE).
4. INSTALL POST APPROX 24" (600) FROM RMC IN VICINITY OF EACH PULL BOX.
5. INSTALL POSTS BETWEEN PULL BOXES, APPROX 10' (3.0 m) OFF CURB. SPACE POSTS 1500± (460 m±) APART.
6. PERMANENTLY ATTACH STAINLESS STEEL NUMBERS INDICATING DISTANCE TO TRENCH IN FEET (METERS) CONTAINING COMMUNICATION CABLE. ATTACH NUMBERS TO SIDE OF POST FACING CONDUIT. INCLUDE "M" SUFFIX IF METERS.

SIGN FACE DETAIL

SIGN # 41-4669

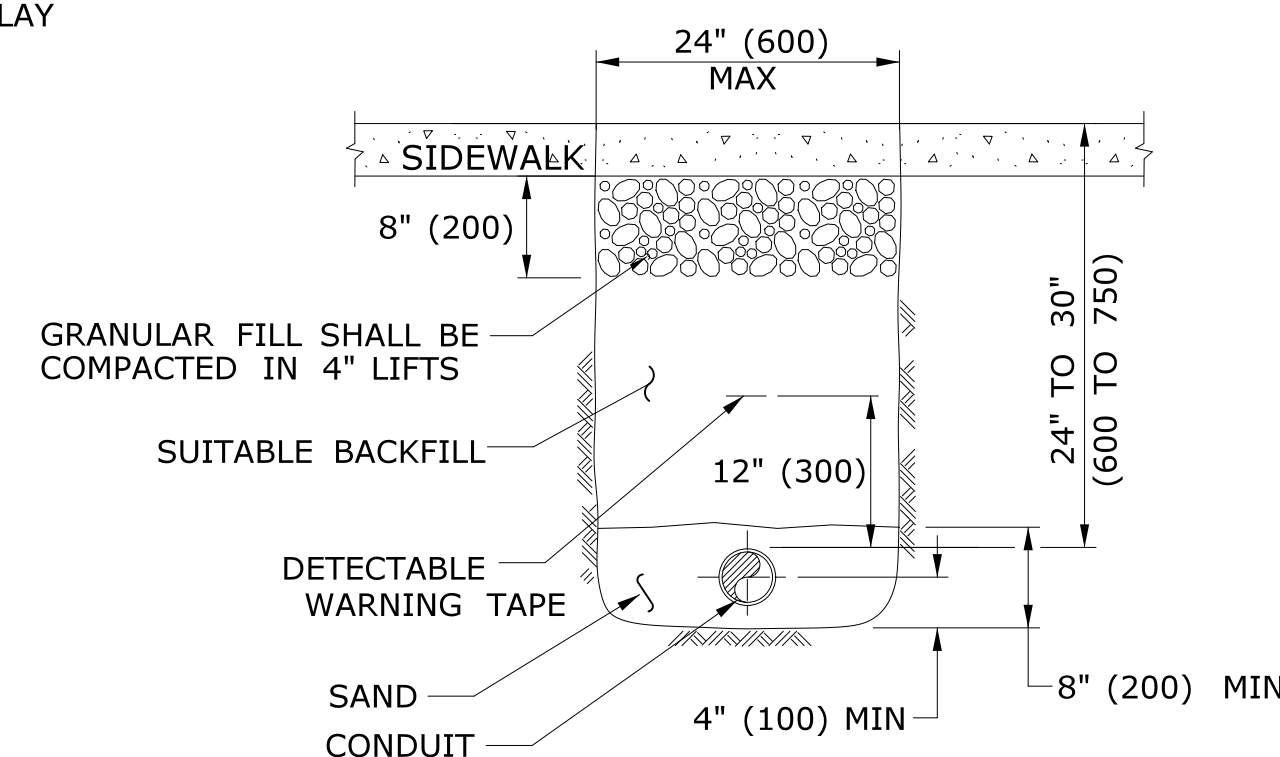


PAVEMENT - BITUMINOUS CONCRETE OR OVERLAYED PORTLAND CEMENT CONCRETE

NOTES:

STANDARD SPECIFICATION, ARTICLE 3.04 AND STANDARD SPECIFICATION, SECTION 4.06.03

1. TOTAL HOT MIX ASPHALT (HMA) THICKNESS TO MATCH EXISTING BITUMINOUS CONCRETE AND PORTLAND CEMENT CONCRETE (PCC) THICKNESS.
2. WHEN ALLOWED BY ENGINEER, USE CONTROLLED LOW STRENGTH MATERIAL (CLSM) AS BEDDING MATERIAL. TOP OF CLSM AT LEAST 20" (500) BELOW SURFACE.

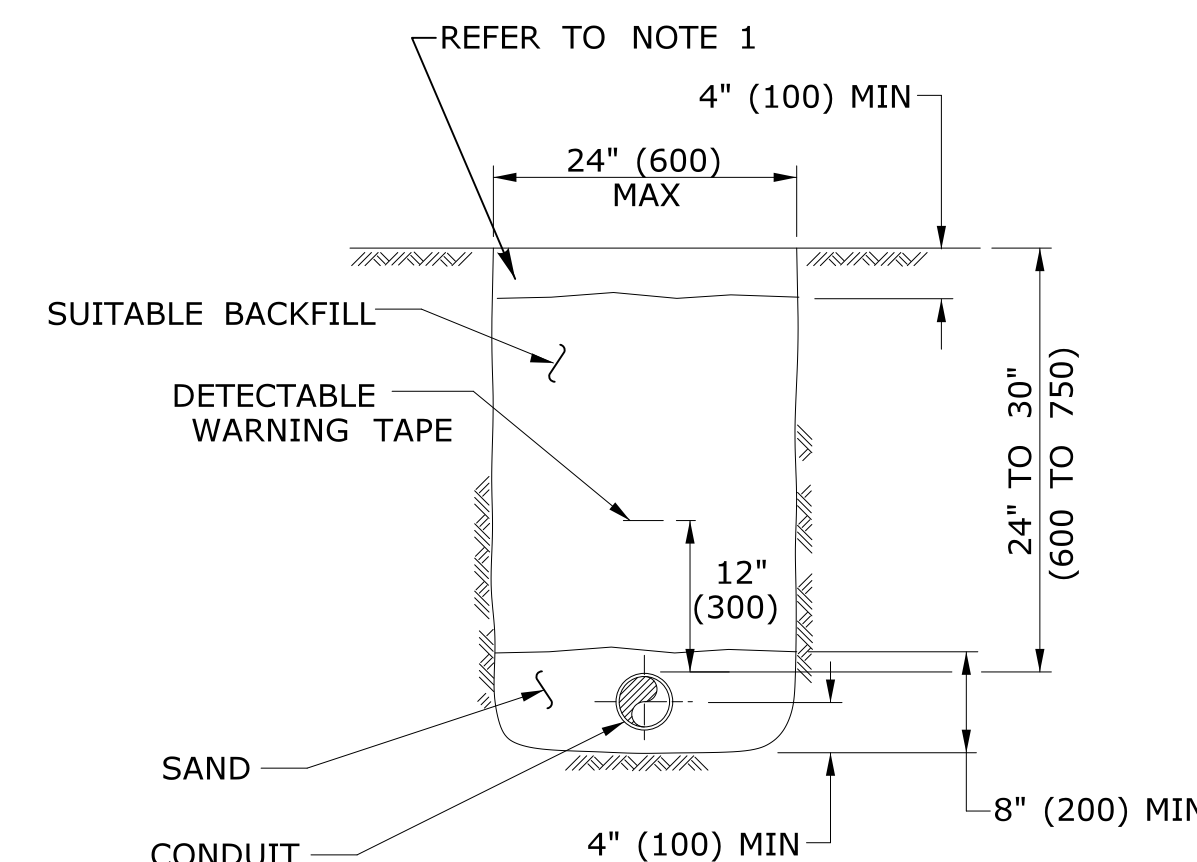


SIDEWALK

NOTES:

STANDARD SPECIFICATIONS, ARTICLE: 9.21 & 9.22

1. WHERE CONCRETE SIDEWALK DAMAGED OR CUT, REPLACE THE ENTIRE SECTION BETWEEN JOINTS. REPLACEMENT SIDEWALK IS PAID FOR AT THE CONTRACT UNIT PRICE FOR "CONCRETE SIDEWALK".



EARTH

NOTES:

STANDARD SPECIFICATIONS, ARTICLE: 9.50

1. IN MOWED AREAS: PLACE TOPSOIL, FERTILIZER, SEED, & MULCH.

GENERAL NOTES:

1. TOP OF CONDUIT NO LESS THAN 24" (600) DEEP.
2. COMPACT BACKFILL IN ≤ 6" (150) LIFTS. HAND COMPACTION NOT PERMITTED.

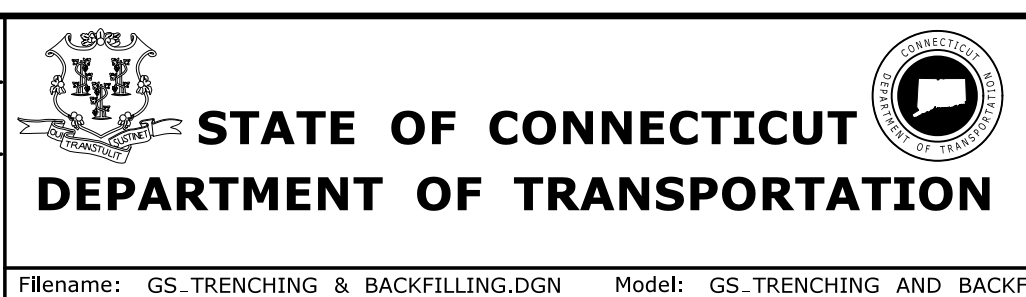
LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:
--- RMC (RIGID METAL CONDUIT)

REV.	DATE	REVISION DESCRIPTION
3	7-2021	SECTION 4.06.03 IS NOW REFERENCED IN THE STANDARD SPECS.
2	4-2019	REVISED FILL & OVERLAY REQUIREMENTS, & MINOR REVISIONS.
1	4-2012	REVISED BITUMINOUS CONCRETE TO HMA, & MINOR REVISIONS.

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

Plotted Date: 10/27/2022

DESIGNER/DRAFTER:
CHECKED BY:
NO SCALE

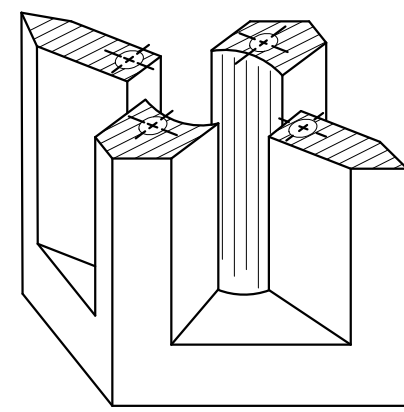


SIGNATURE/
BLOCK:
OFFICE OF ENGINEERING
APPROVED BY: DATE:

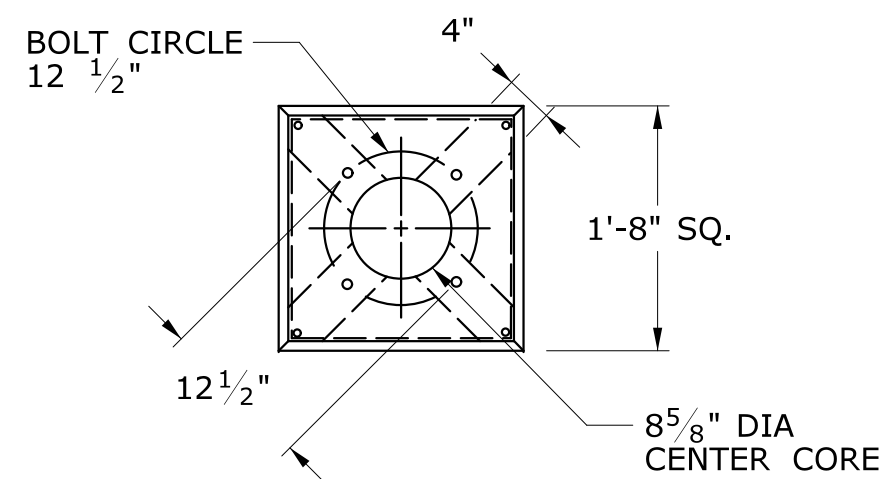
PROJECT TITLE:
**U.S. ROUTES 44 & 202
(ALBANY TURNPIKE) AT THE SHOPPES
AT FARMINGTON VALLEY**

TOWN:
CANTON
DRAWING TITLE:
**TRENCHING & BACKFILLING,
ELECTRICAL CONDUIT**

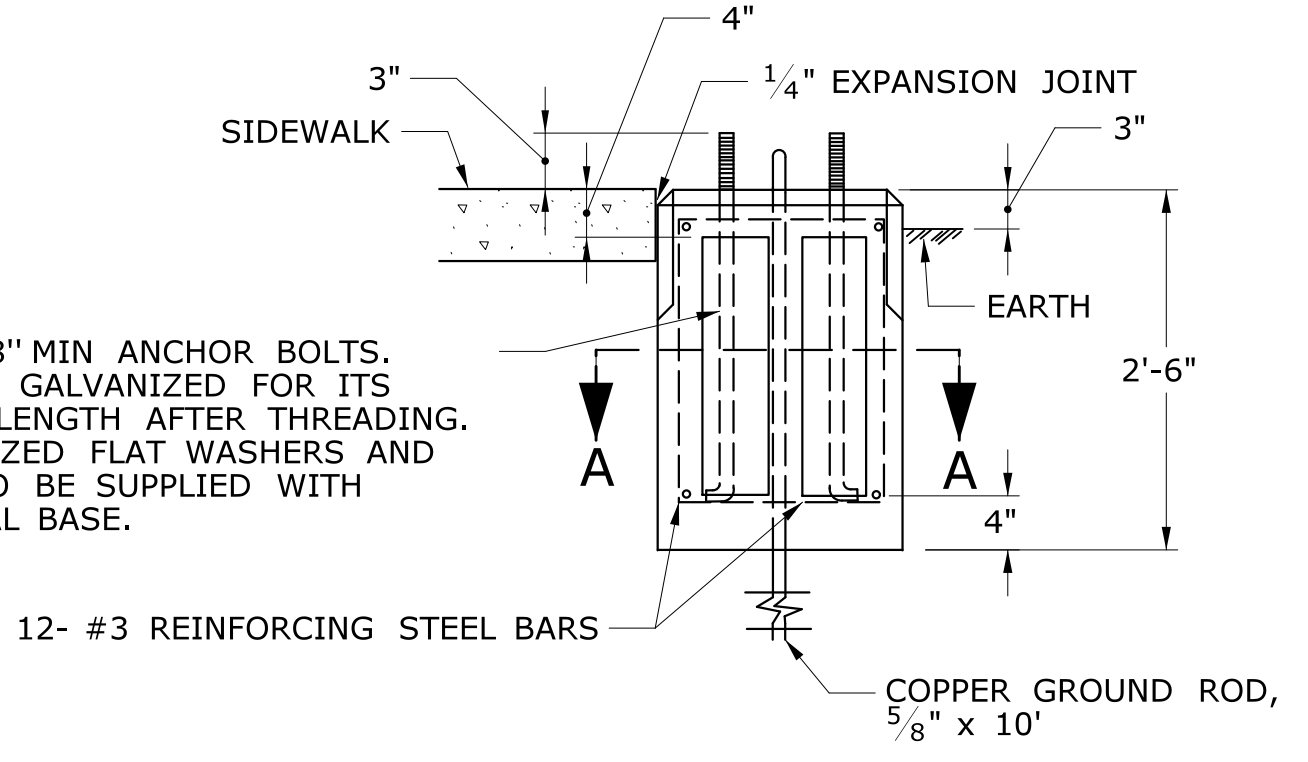
PROJECT NO.
141.17337.00001
DRAWING NO.
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SHEET NO.
GS-1001_01



PICTORIAL SECTION A-A

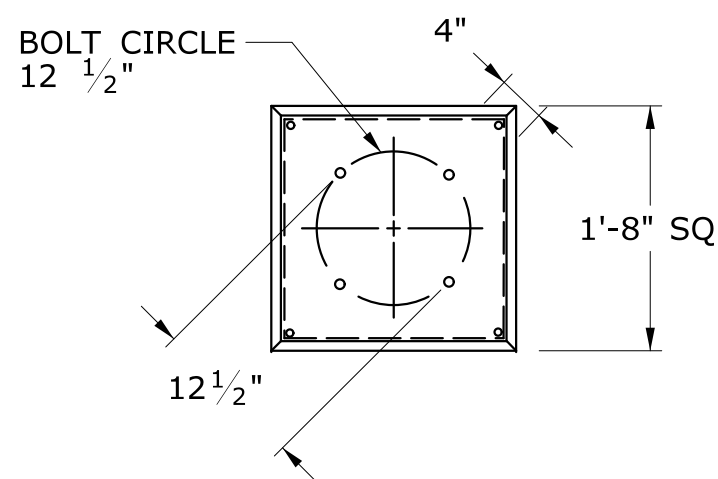


3/4" x 18" MIN ANCHOR BOLTS. HOT DIP GALVANIZED FOR ITS ENTIRE LENGTH AFTER THREADING. GALVANIZED FLAT WASHERS AND NUTS TO BE SUPPLIED WITH PEDESTAL BASE.

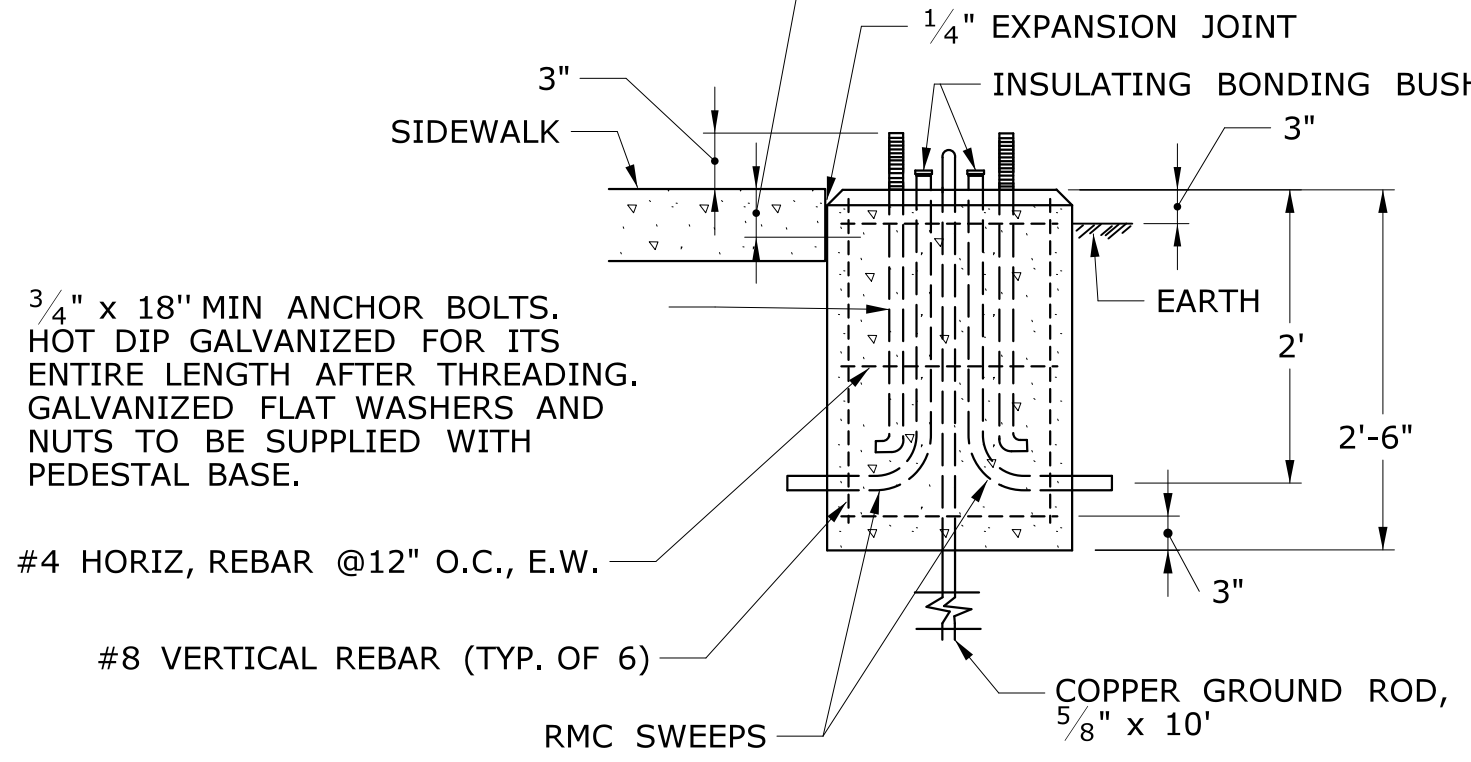


TRAFFIC CONTROL FOUNDATION PEDESTAL - TYPE I - PRECAST

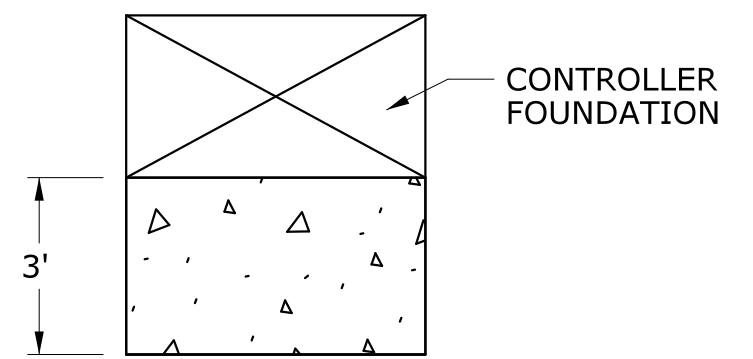
NOTES:
PLACE NO. 6 CRUSHED STONE IN CENTER OPENING AFTER CONDUITS AND GROUND ROD HAVE BEEN INSTALLED.



3/4" x 18" MIN ANCHOR BOLTS. HOT DIP GALVANIZED FOR ITS ENTIRE LENGTH AFTER THREADING. GALVANIZED FLAT WASHERS AND NUTS TO BE SUPPLIED WITH PEDESTAL BASE.

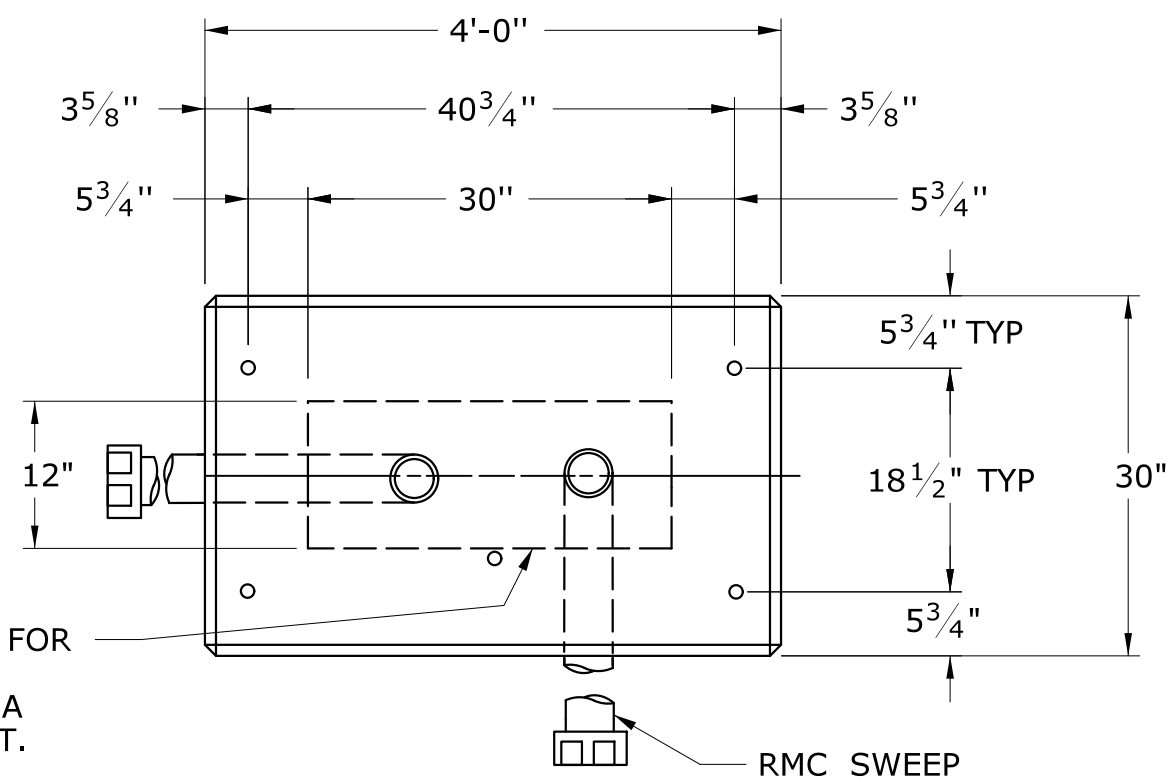


TRAFFIC CONTROL FOUNDATION PEDESTAL - TYPE I - CAST IN PLACE

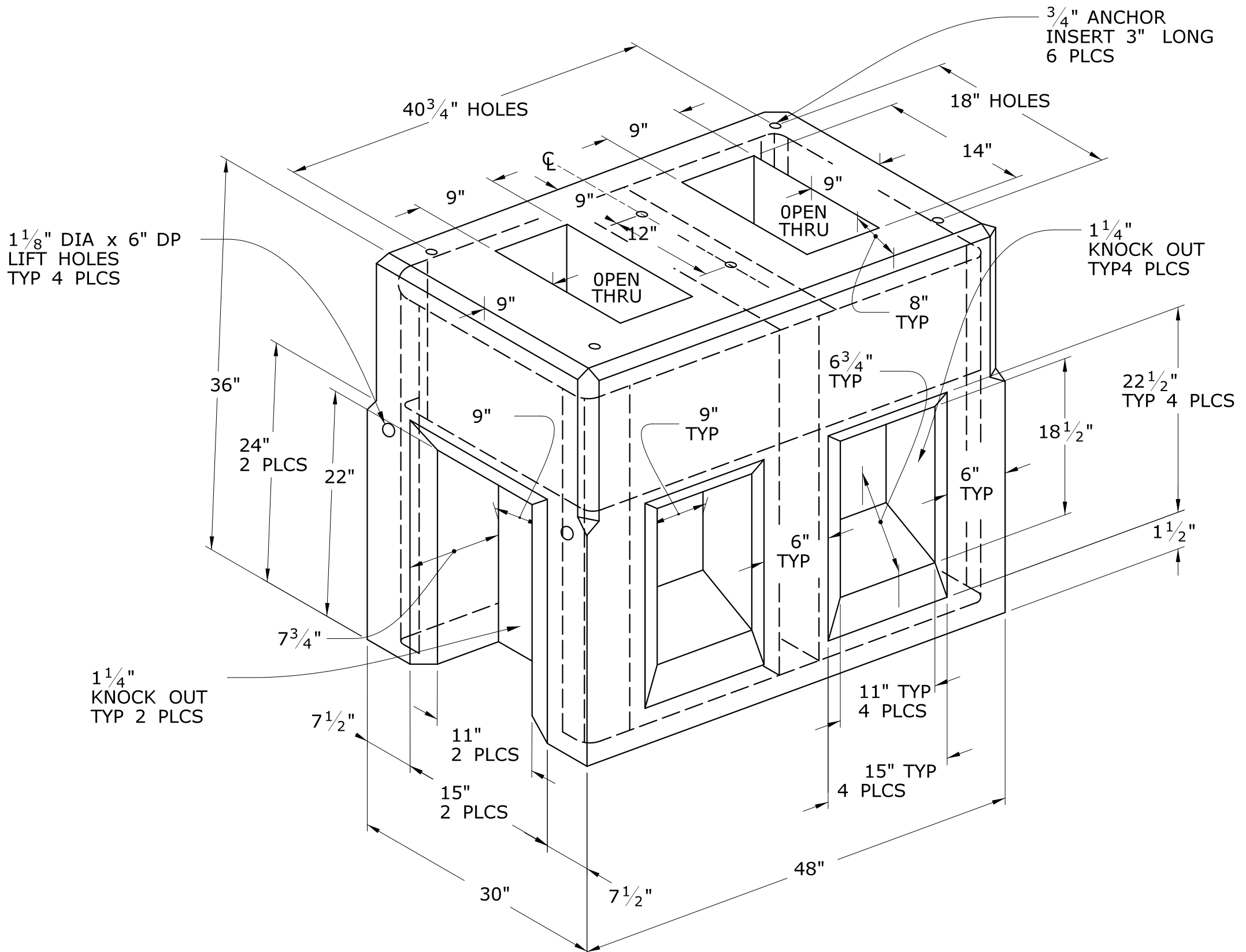


INSTALL PRECAST OR CAST IN PLACE CONCRETE SIDEWALK ON CABINET DOOR SIDE OF CONTROLLER FOUNDATION. PITCH SIDEWALK 1/4" PER FOOT AWAY FROM THE CONTROLLER FOUNDATION. REFER TO HIGHWAY STANDARD SHEET HW-921.01 FOR SIDEWALK CONSTRUCTION.

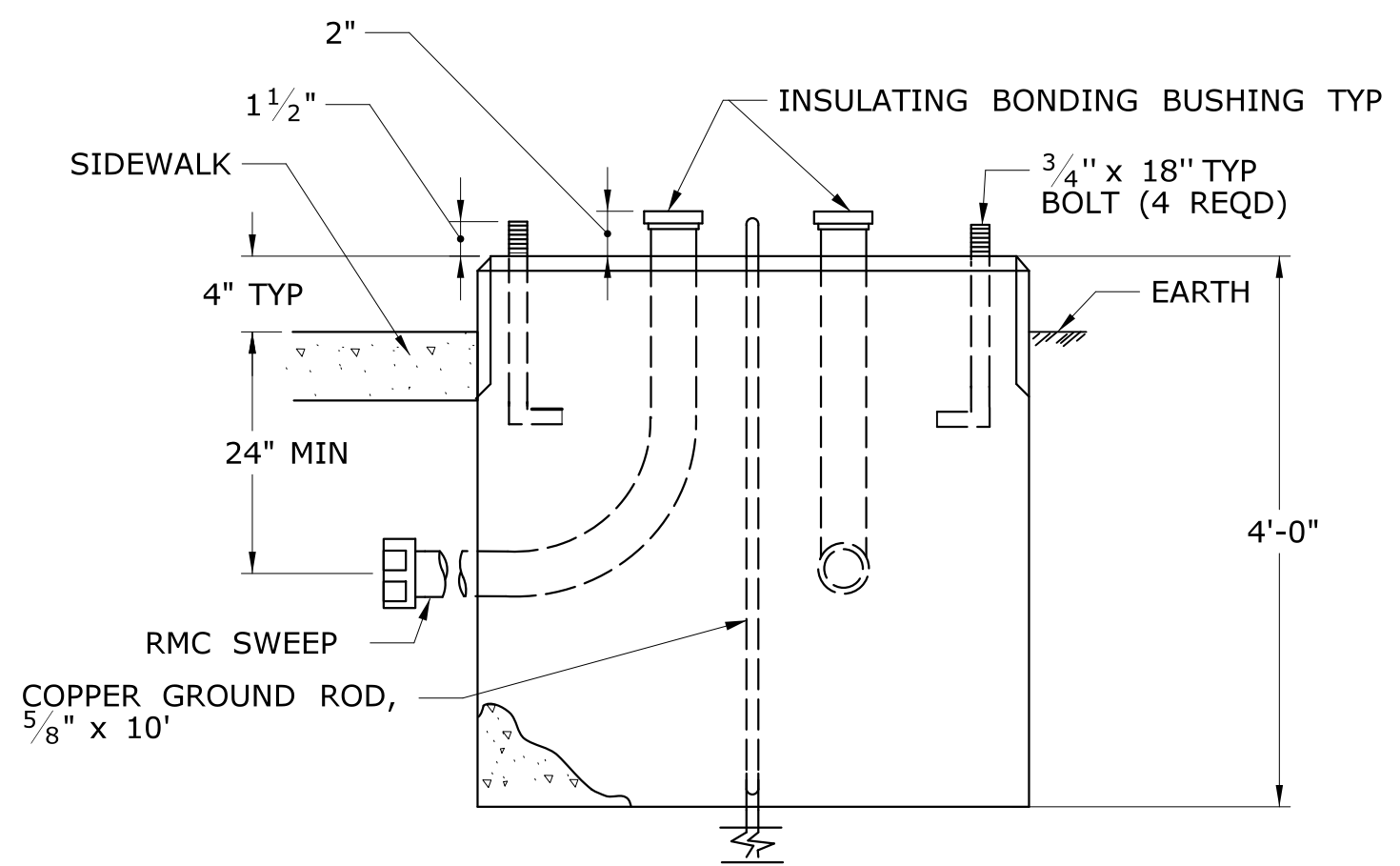
TYPICAL CONCRETE SIDEWALK AT CONTROLLER FOUNDATION



AREA OF LIMITATION FOR CONDUIT SWEEPS. SEPARATE CONDUITS A MINIMUM OF 2" APART.



TRAFFIC CONTROL FOUNDATION CONTROLLER - TYPE IV - PRECAST



TRAFFIC CONTROL FOUNDATION CONTROLLER - TYPE IV - CAST IN PLACE

NOTES:
INSTALL FOUNDATION ON 6" OF COMPACTED GRAVEL IN ACCORDANCE WITH SECTION 2.14. LEVEL FOUNDATION WITH A PROJECTION OF 4" ABOVE FINISHED GRADE.
INSTALL COPPER GROUND ROD: 5/8" x 10'.
PLACE NO. 6 CRUSHED STONE IN THE CENTER OPENINGS AFTER THE CONDUITS AND GROUND ROD HAVE BEEN INSTALLED. THE OPENINGS SHALL BE CAPPED WITH A 2" GROUT LEVEL WITH THE TOP OF THE FOUNDATION AND NEATLY FINISHED. THE GROUT SHALL CONFORM WITH THE REQUIREMENTS OF ARTICLE M.3.01-12.
CONCRETE: CLASS PCC04461 CONFORMING TO ARTICLE M.03.01.
#4 REBAR 2" MIN COVER AROUND ALL OPENINGS, 3-#4 REBARS IN EACH CORNER.
CONDUITS SHALL NOT PROJECT MORE THAN 2" ABOVE FOUNDATION.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:	
	PROPOSED CONTROLLER
	EXISTING CONTROLLER
	PROPOSED STEEL SPAN POLE
	EXISTING STEEL SPAN POLE

1	7-2021	REVISED MINIMUM CONCRETE CLASS.	SHEET NO.	Plotted Date: 10/27/2022
REV.	DATE	REVISION DESCRIPTION		

DESIGNER/DRAFTER:
CHECKED BY:
NOT TO SCALE

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE CONDITIONS OF ACTUAL QUANTITIES OF WORK WHICH WILL BE REQUIRED.

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Filename: ...VGS100201\GS-1002_01.DGN

SIGNATURE/BLOCK:
OFFICE OF ENGINEERING

APPROVED BY:

PROJECT TITLE:
U.S. ROUTES 44 & 202 (ALBANY TURNPIKE) AT THE SHOPPES AT FARMINGTON VALLEY

TOWN:
CANTON

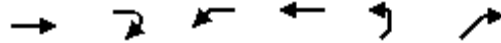
DRAWING TITLE:
TRAFFIC CONTROL FOUNDATIONS

PROJECT NO. 141.17337.00001
DRAWING NO. --
SHEET NO. GS-1002_01

E Intersection Capacity Analysis Worksheets

Queues
1: Canton Village Shopping Mall & Route 44

Existing AM
01/29/2024

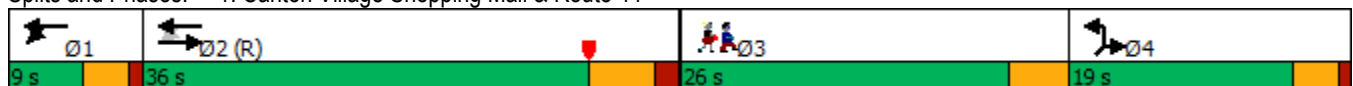


Lane Group	EBT	EBR	WBL	WBT	NEL	NER	Ø3
Lane Configurations	↑↑		↙	↑↑	↙	↗	
Traffic Volume (vph)	900	30	40	470	20	50	
Future Volume (vph)	900	30	40	470	20	50	
Peak Hour Factor	0.93	0.93	0.97	0.97	0.79	0.79	
Heavy Vehicles (%)	3%	3%	6%	6%	6%	6%	
Shared Lane Traffic (%)							
Lane Group Flow (vph)	1000	0	41	485	25	63	
Turn Type	NA		D.P+P	NA	Prot	Prot	
Protected Phases	2		1	12	4	4	3
Permitted Phases			2				
Detector Phase	2		1	12	4	4	
Switch Phase							
Minimum Initial (s)	15.0		5.0		8.0	8.0	5.0
Minimum Split (s)	21.2		9.0		12.0	12.0	26.0
Total Split (s)	36.0		9.0		19.0	19.0	26.0
Total Split (%)	40.0%		10.0%		21.1%	21.1%	29%
Yellow Time (s)	4.4		3.0		3.0	3.0	4.0
All-Red Time (s)	1.8		1.0		1.0	1.0	0.0
Lost Time Adjust (s)	0.0		0.0		0.0	0.0	
Total Lost Time (s)	6.2		4.0		4.0	4.0	
Lead/Lag	Lag		Lead		Lag	Lag	Lead
Lead-Lag Optimize?	Yes		Yes		Yes	Yes	Yes
Recall Mode	C-Max		None		None	None	None
v/c Ratio	0.42		0.09	0.17	0.16	0.31	
Control Delay	6.6		1.7	1.3	40.4	15.0	
Queue Delay	0.0		0.0	0.0	0.0	0.0	
Total Delay	6.6		1.7	1.3	40.4	15.0	
Queue Length 50th (ft)	115		3	22	13	0	
Queue Length 95th (ft)	160		6	27	33	28	
Internal Link Dist (ft)	340			1313	474		
Turn Bay Length (ft)			160				
Base Capacity (vph)	2396		434	2823	301	322	
Starvation Cap Reductn	0		0	0	0	0	
Spillback Cap Reductn	0		0	0	0	0	
Storage Cap Reductn	0		0	0	0	0	
Reduced v/c Ratio	0.42		0.09	0.17	0.08	0.20	

Intersection Summary

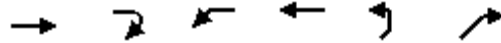
Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 76 (84%), Referenced to phase 2:EBWB, Start of Yellow
 Natural Cycle: 80
 Control Type: Actuated-Coordinated

Splits and Phases: 1: Canton Village Shopping Mall & Route 44



HCM Signalized Intersection Capacity Analysis
1: Canton Village Shopping Mall & Route 44

Existing AM
01/29/2024



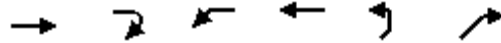
Movement	EBT	EBR	WBL	WBT	NEL	NER
Lane Configurations	↑↑		↵	↑↑	↵	↵
Traffic Volume (vph)	900	30	40	470	20	50
Future Volume (vph)	900	30	40	470	20	50
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	11	12	10	11	14	14
Grade (%)	0%			0%	1%	
Total Lost time (s)	6.2		4.0	4.0	4.0	4.0
Lane Util. Factor	0.95		1.00	0.95	1.00	1.00
Frt	1.00		1.00	1.00	1.00	0.85
Flt Protected	1.00		0.95	1.00	0.95	1.00
Satd. Flow (prot)	3372		1589	3292	1807	1617
Flt Permitted	1.00		0.26	1.00	0.95	1.00
Satd. Flow (perm)	3372		442	3292	1807	1617
Peak-hour factor, PHF	0.93	0.93	0.97	0.97	0.79	0.79
Adj. Flow (vph)	968	32	41	485	25	63
RTOR Reduction (vph)	1	0	0	0	0	59
Lane Group Flow (vph)	999	0	41	485	25	4
Heavy Vehicles (%)	3%	3%	6%	6%	6%	6%
Turn Type	NA		D.P+P	NA	Prot	Prot
Protected Phases	2		1	1 2	4	4
Permitted Phases			2			
Actuated Green, G (s)	63.2		69.4	73.4	6.4	6.4
Effective Green, g (s)	63.2		69.4	73.4	6.4	6.4
Actuated g/C Ratio	0.70		0.77	0.82	0.07	0.07
Clearance Time (s)	6.2		4.0		4.0	4.0
Vehicle Extension (s)	0.2		1.0		2.0	2.0
Lane Grp Cap (vph)	2367		419	2684	128	114
v/s Ratio Prot	c0.30		0.01	c0.15	c0.01	0.00
v/s Ratio Perm			0.07			
v/c Ratio	0.42		0.10	0.18	0.20	0.04
Uniform Delay, d1	5.7		2.6	1.8	39.4	38.9
Progression Factor	1.00		0.80	0.82	1.00	1.00
Incremental Delay, d2	0.6		0.0	0.0	0.3	0.1
Delay (s)	6.2		2.1	1.5	39.6	39.0
Level of Service	A		A	A	D	D
Approach Delay (s)	6.2			1.5	39.2	
Approach LOS	A			A	D	

Intersection Summary

HCM 2000 Control Delay	6.5	HCM 2000 Level of Service	A
HCM 2000 Volume to Capacity ratio	0.41		
Actuated Cycle Length (s)	90.0	Sum of lost time (s)	18.2
Intersection Capacity Utilization	46.6%	ICU Level of Service	A
Analysis Period (min)	15		
c Critical Lane Group			

Queues
2: Dowd Ave & Route 44

Existing AM
01/29/2024

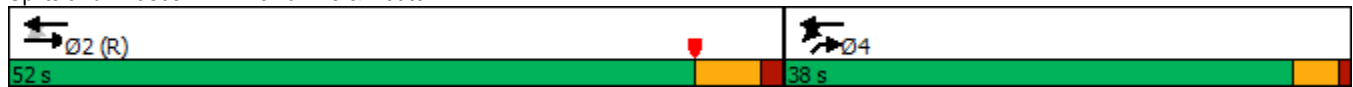


Lane Group	EBT	EBR	WBL	WBT	NEL	NER
Lane Configurations	↑↑			↑↑		↑
Traffic Volume (vph)	950	0	220	510	0	420
Future Volume (vph)	950	0	220	510	0	420
Peak Hour Factor	0.91	0.91	0.93	0.93	0.82	0.82
Heavy Vehicles (%)	3%	3%	5%	5%	3%	3%
Shared Lane Traffic (%)						
Lane Group Flow (vph)	1044	0	0	785	0	512
Turn Type	NA		D.P+P	NA		Over
Protected Phases	2		4	2 4		4
Permitted Phases			2			
Detector Phase	2		4	2 4		4
Switch Phase						
Minimum Initial (s)	15.0		7.0			7.0
Minimum Split (s)	21.1		11.0			11.0
Total Split (s)	52.0		38.0			38.0
Total Split (%)	57.8%		42.2%			42.2%
Yellow Time (s)	4.4		3.0			3.0
All-Red Time (s)	1.7		1.0			1.0
Lost Time Adjust (s)	0.0					0.0
Total Lost Time (s)	6.1					4.0
Lead/Lag						
Lead-Lag Optimize?						
Recall Mode	C-Max		None			None
v/c Ratio	0.59			0.39		0.89
Control Delay	11.5			1.2		46.3
Queue Delay	0.0			0.0		0.0
Total Delay	11.5			1.2		46.3
Queue Length 50th (ft)	210			0		263
Queue Length 95th (ft)	280			1		#376
Internal Link Dist (ft)	1313			1683	869	
Turn Bay Length (ft)						
Base Capacity (vph)	1771			2068		596
Starvation Cap Reductn	0			0		0
Spillback Cap Reductn	0			0		0
Storage Cap Reductn	0			0		0
Reduced v/c Ratio	0.59			0.38		0.86

Intersection Summary

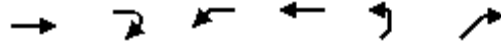
Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 16 (18%), Referenced to phase 2:EBWB, Start of Yellow
 Natural Cycle: 55
 Control Type: Actuated-Coordinated
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.

Splits and Phases: 2: Dowd Ave & Route 44



HCM Signalized Intersection Capacity Analysis
2: Dowd Ave & Route 44

Existing AM
01/29/2024

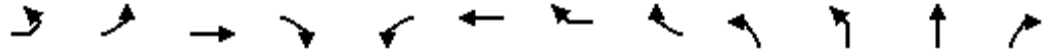


Movement	EBT	EBR	WBL	WBT	NEL	NER
Lane Configurations	↑↑			↑↑		↑
Traffic Volume (vph)	950	0	220	510	0	420
Future Volume (vph)	950	0	220	510	0	420
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	11	12	12	11	12	12
Grade (%)	0%			0%	2%	
Total Lost time (s)	6.1			6.1		4.0
Lane Util. Factor	0.95			0.95		1.00
Frt	1.00			1.00		0.86
Flt Protected	1.00			0.99		1.00
Satd. Flow (prot)	3388			3274		1580
Flt Permitted	1.00			0.52		1.00
Satd. Flow (perm)	3388			1735		1580
Peak-hour factor, PHF	0.91	0.91	0.93	0.93	0.82	0.82
Adj. Flow (vph)	1044	0	237	548	0	512
RTOR Reduction (vph)	0	0	0	0	0	0
Lane Group Flow (vph)	1044	0	0	785	0	512
Heavy Vehicles (%)	3%	3%	5%	5%	3%	3%
Turn Type	NA		D.P+P	NA		Over
Protected Phases	2		4	2 4		4
Permitted Phases			2			
Actuated Green, G (s)	47.1			79.9		32.8
Effective Green, g (s)	47.1			79.9		32.8
Actuated g/C Ratio	0.52			0.89		0.36
Clearance Time (s)	6.1					4.0
Vehicle Extension (s)	0.2					3.0
Lane Grp Cap (vph)	1773			2101		575
v/s Ratio Prot	c0.31			0.14		c0.32
v/s Ratio Perm				0.20		
v/c Ratio	0.59			0.37		0.89
Uniform Delay, d1	14.8			0.8		26.9
Progression Factor	0.66			1.00		1.00
Incremental Delay, d2	1.3			0.1		15.9
Delay (s)	11.1			1.0		42.8
Level of Service	B			A		D
Approach Delay (s)	11.1			1.0	42.8	
Approach LOS	B			A	D	

Intersection Summary			
HCM 2000 Control Delay	14.6	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.71		
Actuated Cycle Length (s)	90.0	Sum of lost time (s)	10.1
Intersection Capacity Utilization	60.7%	ICU Level of Service	B
Analysis Period (min)	15		
c Critical Lane Group			

Queues
3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr

Existing AM
01/29/2024



Lane Group	EBL2	EBL	EBT	EBR	WBL	WBT	WBR	WBR2	NBL2	NBL	NBT	NBR
Lane Configurations												
Traffic Volume (vph)	10	180	1090	90	90	550	10	20	130	0	90	160
Future Volume (vph)	10	180	1090	90	90	550	10	20	130	0	90	160
Peak Hour Factor	0.98	0.98	0.98	0.98	0.88	0.88	0.88	0.88	0.89	0.89	0.89	0.89
Heavy Vehicles (%)	3%	3%	3%	3%	6%	6%	6%	6%	2%	2%	2%	2%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	194	1112	92	102	636	0	23	0	146	101	180
Turn Type	Prot	Prot	NA	Free	Prot	NA		Free	Split	Split	NA	pm+ov
Protected Phases	1	1	6		5	2			7	7	7	5
Permitted Phases				Free				Free				7
Detector Phase	1	1	6		5	2			7	7	7	5
Switch Phase												
Minimum Initial (s)	5.0	5.0	15.0		5.0	15.0			9.0	9.0	9.0	5.0
Minimum Split (s)	11.6	11.6	21.8		11.6	21.8			15.0	15.0	15.0	11.6
Total Split (s)	22.6	22.6	54.8		25.6	57.8			24.0	24.0	24.0	25.6
Total Split (%)	13.0%	13.0%	31.5%		14.7%	33.2%			13.8%	13.8%	13.8%	14.7%
Yellow Time (s)	3.0	3.0	4.5		3.0	4.5			4.0	4.0	4.0	3.0
All-Red Time (s)	3.6	3.6	2.3		3.6	2.3			2.0	2.0	2.0	3.6
Lost Time Adjust (s)		0.0	0.0		0.0	0.0			0.0	0.0	0.0	0.0
Total Lost Time (s)		6.6	6.8		6.6	6.8			6.0	6.0	6.0	6.6
Lead/Lag	Lead	Lead	Lag		Lead	Lag			Lead	Lead	Lead	Lead
Lead-Lag Optimize?	Yes	Yes	Yes		Yes	Yes			Yes	Yes	Yes	Yes
Recall Mode	None	None	Min		None	Min			None	None	None	None
v/c Ratio		0.96	0.85	0.06	0.65	0.56		0.02		0.71	0.42	0.41
Control Delay		110.5	45.6	0.1	75.9	36.3		0.0		74.0	59.1	9.7
Queue Delay		0.0	0.0	0.0	0.0	0.0		0.0		0.0	0.0	0.0
Total Delay		110.5	45.6	0.1	75.9	36.3		0.0		74.0	59.1	9.7
Queue Length 50th (ft)		166	456	0	84	229		0		119	80	20
Queue Length 95th (ft)		#344	#632	0	145	286		0		199	142	62
Internal Link Dist (ft)			1683			692					428	
Turn Bay Length (ft)		380		220	340			450		145		50
Base Capacity (vph)		202	1301	1656	235	1308		1422		246	285	514
Starvation Cap Reductn		0	0	0	0	0		0		0	0	0
Spillback Cap Reductn		0	0	0	0	0		0		0	0	0
Storage Cap Reductn		0	0	0	0	0		0		0	0	0
Reduced v/c Ratio		0.96	0.85	0.06	0.43	0.49		0.02		0.59	0.35	0.35

Intersection Summary

Cycle Length: 174.2

Actuated Cycle Length: 128.5

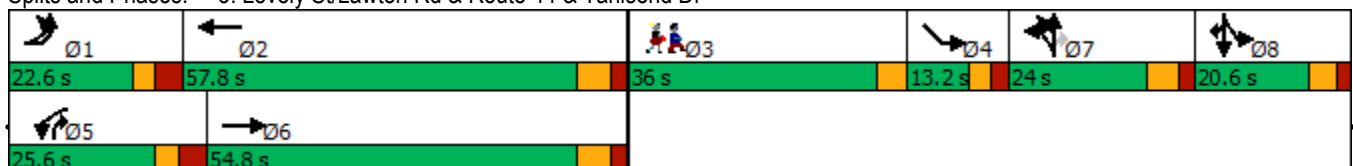
Natural Cycle: 145

Control Type: Actuated-Uncoordinated

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

Splits and Phases: 3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr



Queues
3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr

Existing AM
01/29/2024

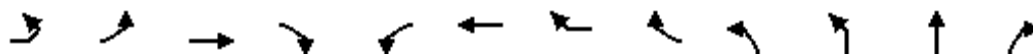


Lane Group	SBL	SBT	SBR	SBR2	SEL	SER	SER2	Ø3
Lane Configurations		↕	↗		↘			
Traffic Volume (vph)	50	60	150	10	10	10	20	
Future Volume (vph)	50	60	150	10	10	10	20	
Peak Hour Factor	0.83	0.83	0.83	0.83	0.69	0.69	0.69	
Heavy Vehicles (%)	1%	1%	1%	1%	7%	7%	7%	
Shared Lane Traffic (%)								
Lane Group Flow (vph)	0	132	193	0	57	0	0	
Turn Type	Split	NA	Prot		Prot			
Protected Phases	8	8	8		4			3
Permitted Phases								
Detector Phase	8	8	8		4			
Switch Phase								
Minimum Initial (s)	9.0	9.0	9.0		6.0			1.0
Minimum Split (s)	14.6	14.6	14.6		11.2			36.0
Total Split (s)	20.6	20.6	20.6		13.2			36.0
Total Split (%)	11.8%	11.8%	11.8%		7.6%			21%
Yellow Time (s)	3.6	3.6	3.6		3.0			4.0
All-Red Time (s)	2.0	2.0	2.0		2.2			0.0
Lost Time Adjust (s)		0.0	0.0		0.0			
Total Lost Time (s)		5.6	5.6		5.2			
Lead/Lag	Lag	Lag	Lag		Lag			Lead
Lead-Lag Optimize?	Yes	Yes	Yes		Yes			Yes
Recall Mode	None	None	None		None			None
v/c Ratio		0.70	0.67		0.62			
Control Delay		76.1	67.9		89.3			
Queue Delay		0.0	0.0		0.0			
Total Delay		76.1	67.9		89.3			
Queue Length 50th (ft)		109	89		48			
Queue Length 95th (ft)		#179	131		75			
Internal Link Dist (ft)		713			445			
Turn Bay Length (ft)			330					
Base Capacity (vph)		200	306		94			
Starvation Cap Reductn		0	0		0			
Spillback Cap Reductn		0	0		0			
Storage Cap Reductn		0	0		0			
Reduced v/c Ratio		0.66	0.63		0.61			
Intersection Summary								

HCM Signalized Intersection Capacity Analysis

3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr

Existing AM
01/29/2024



Movement	EBL2	EBL	EBT	EBR	WBL	WBT	WBR	WBR2	NBL2	NBL	NBT	NBR		
Lane Configurations		↔	↕	↗	↖	↕		↗		↖	↕	↗		
Traffic Volume (vph)	10	180	1090	90	90	550	10	20	130	0	90	160		
Future Volume (vph)	10	180	1090	90	90	550	10	20	130	0	90	160		
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900		
Lane Width	12	10	12	14	10	11	12	10	10	12	15	12		
Grade (%)			2%			0%					2%			
Total Lost time (s)		6.6	6.8	4.0	6.6	6.8		4.0		6.0	6.0	6.6		
Lane Util. Factor		1.00	0.95	1.00	1.00	0.95		1.00		1.00	1.00	1.00		
Frt		1.00	1.00	0.85	1.00	1.00		0.85		1.00	1.00	0.85		
Flt Protected		0.95	1.00	1.00	0.95	1.00		1.00		0.95	1.00	1.00		
Satd. Flow (prot)		1619	3470	1656	1589	3284		1422		1752	2029	1567		
Flt Permitted		0.95	1.00	1.00	0.95	1.00		1.00		0.95	1.00	1.00		
Satd. Flow (perm)		1619	3470	1656	1589	3284		1422		1752	2029	1567		
Peak-hour factor, PHF	0.98	0.98	0.98	0.98	0.88	0.88	0.88	0.88	0.89	0.89	0.89	0.89		
Adj. Flow (vph)	10	184	1112	92	102	625	11	23	146	0	101	180		
RTOR Reduction (vph)	0	0	0	0	0	0	0	0	0	0	0	110		
Lane Group Flow (vph)	0	194	1112	92	102	636	0	23	0	146	101	70		
Heavy Vehicles (%)	3%	3%	3%	3%	6%	6%	6%	6%	2%	2%	2%	2%		
Turn Type	Prot	Prot	NA	Free	Prot	NA		Free	Split	Split	NA	pm+ov		
Protected Phases	1	1	6		5	2			7	7	7	5		
Permitted Phases				Free				Free				7		
Actuated Green, G (s)		16.1	48.2	128.4	12.7	44.8		128.4		15.2	15.2	27.9		
Effective Green, g (s)		16.1	48.2	128.4	12.7	44.8		128.4		15.2	15.2	27.9		
Actuated g/C Ratio		0.13	0.38	1.00	0.10	0.35		1.00		0.12	0.12	0.22		
Clearance Time (s)		6.6	6.8		6.6	6.8				6.0	6.0	6.6		
Vehicle Extension (s)		2.0	3.0		2.0	3.0				2.0	2.0	2.0		
Lane Grp Cap (vph)		203	1302	1656	157	1145		1422		207	240	340		
v/s Ratio Prot		c0.12	c0.32		0.06	0.19				c0.08	0.05	0.02		
v/s Ratio Perm				0.06				0.02				0.02		
v/c Ratio		0.96	0.85	0.06	0.65	0.56		0.02		0.71	0.42	0.21		
Uniform Delay, d1		55.8	36.9	0.0	55.7	33.8		0.0		54.4	52.5	41.2		
Progression Factor		1.00	1.00	1.00	1.00	1.00		1.00		1.00	1.00	1.00		
Incremental Delay, d2		49.8	5.7	0.1	6.8	0.6		0.0		8.6	0.4	0.1		
Delay (s)		105.6	42.5	0.1	62.5	34.3		0.0		63.0	53.0	41.3		
Level of Service		F	D	A	E	C		A		E	D	D		
Approach Delay (s)			48.5			37.1					51.5			
Approach LOS			D			D					D			
Intersection Summary														
HCM 2000 Control Delay			47.7									HCM 2000 Level of Service	D	
HCM 2000 Volume to Capacity ratio			0.87											
Actuated Cycle Length (s)			128.4								34.2			
Intersection Capacity Utilization			74.5%										ICU Level of Service	D
Analysis Period (min)			15											
c Critical Lane Group														

HCM Signalized Intersection Capacity Analysis
 3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr

Existing AM
 01/29/2024



Movement	SBL	SBT	SBR	SBR2	SEL	SER	SER2
Lane Configurations		↕	↗		↘		
Traffic Volume (vph)	50	60	150	10	10	10	20
Future Volume (vph)	50	60	150	10	10	10	20
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900
Lane Width	12	11	11	12	12	16	12
Grade (%)		8%			8%		
Total Lost time (s)		5.6	5.6		5.2		
Lane Util. Factor		1.00	0.88		1.00		
Frt		1.00	0.85		0.90		
Flt Protected		0.98	1.00		0.99		
Satd. Flow (prot)		1707	2612		1512		
Flt Permitted		0.98	1.00		0.99		
Satd. Flow (perm)		1707	2612		1512		
Peak-hour factor, PHF	0.83	0.83	0.83	0.83	0.69	0.69	0.69
Adj. Flow (vph)	60	72	181	12	14	14	29
RTOR Reduction (vph)	0	0	0	0	0	0	0
Lane Group Flow (vph)	0	132	193	0	57	0	0
Heavy Vehicles (%)	1%	1%	1%	1%	7%	7%	7%
Turn Type	Split	NA	Prot		Prot		
Protected Phases	8	8	8		4		
Permitted Phases							
Actuated Green, G (s)		14.3	14.3		7.8		
Effective Green, g (s)		14.3	14.3		7.8		
Actuated g/C Ratio		0.11	0.11		0.06		
Clearance Time (s)		5.6	5.6		5.2		
Vehicle Extension (s)		2.0	2.0		2.0		
Lane Grp Cap (vph)		190	290		91		
v/s Ratio Prot		c0.08	0.07		c0.04		
v/s Ratio Perm							
v/c Ratio		0.69	0.67		0.63		
Uniform Delay, d1		54.9	54.8		58.9		
Progression Factor		1.00	1.00		1.00		
Incremental Delay, d2		8.6	4.4		9.3		
Delay (s)		63.5	59.2		68.2		
Level of Service		E	E		E		
Approach Delay (s)		60.9			68.2		
Approach LOS		E			E		
Intersection Summary							

Queues
4: Route 44 & CVS Drive

Existing AM
01/29/2024

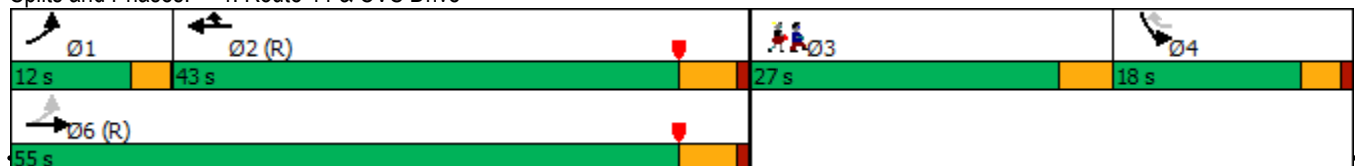


Lane Group	EBL	EBT	WBT	WBR	SBL	SBR	Ø3
Lane Configurations							
Traffic Volume (vph)	20	1230	660	30	20	10	
Future Volume (vph)	20	1230	660	30	20	10	
Peak Hour Factor	0.95	0.95	0.88	0.88	0.64	0.64	
Heavy Vehicles (%)	4%	4%	5%	5%	6%	6%	
Shared Lane Traffic (%)							
Lane Group Flow (vph)	21	1295	750	34	47	0	
Turn Type	pm+pt	NA	NA	custom	Prot		
Protected Phases	1	6	2	2	4		3
Permitted Phases	6			4			
Detector Phase	1	6	2	2	4		
Switch Phase							
Minimum Initial (s)	5.0	25.0	25.0	25.0	7.0		1.0
Minimum Split (s)	8.1	30.3	30.3	30.3	11.0		27.0
Total Split (s)	12.0	55.0	43.0	43.0	18.0		27.0
Total Split (%)	12.0%	55.0%	43.0%	43.0%	18.0%		27%
Yellow Time (s)	3.0	4.3	4.3	4.3	3.0		4.0
All-Red Time (s)	0.1	1.0	1.0	1.0	1.0		0.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0		
Total Lost Time (s)	3.1	5.3	5.3	5.3	4.0		
Lead/Lag	Lead		Lag	Lag	Lag		Lead
Lead-Lag Optimize?	Yes		Yes	Yes	Yes		Yes
Recall Mode	None	C-Max	C-Max	C-Max	None		None
v/c Ratio	0.04	0.45	0.27	0.03	0.20		
Control Delay	4.8	5.7	9.0	3.4	34.2		
Queue Delay	0.0	0.0	0.0	0.0	0.0		
Total Delay	4.8	5.7	9.0	3.4	34.2		
Queue Length 50th (ft)	1	79	30	0	9		
Queue Length 95th (ft)	15	363	319	12	18		
Internal Link Dist (ft)		692	628		200		
Turn Bay Length (ft)	155			140			
Base Capacity (vph)	568	2855	2744	1243	456		
Starvation Cap Reductn	0	0	0	0	0		
Spillback Cap Reductn	0	0	0	0	0		
Storage Cap Reductn	0	0	0	0	0		
Reduced v/c Ratio	0.04	0.45	0.27	0.03	0.10		

Intersection Summary

Cycle Length: 100
 Actuated Cycle Length: 100
 Offset: 40 (40%), Referenced to phase 2:WBT and 6:EBTL, Start of Yellow
 Natural Cycle: 80
 Control Type: Actuated-Coordinated

Splits and Phases: 4: Route 44 & CVS Drive



HCM Signalized Intersection Capacity Analysis

4: Route 44 & CVS Drive

Existing AM
01/29/2024



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↵	↑↑	↑↑	↵	↵↵	
Traffic Volume (vph)	20	1230	660	30	20	10
Future Volume (vph)	20	1230	660	30	20	10
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	10	12	12	10	12	12
Grade (%)		6%	4%		2%	
Total Lost time (s)	3.1	5.3	5.3	5.3	4.0	
Lane Util. Factor	1.00	0.95	0.95	1.00	0.97	
Frt	1.00	1.00	1.00	0.85	0.95	
Flt Protected	0.95	1.00	1.00	1.00	0.97	
Satd. Flow (prot)	1571	3367	3369	1407	3162	
Flt Permitted	0.34	1.00	1.00	1.00	0.97	
Satd. Flow (perm)	563	3367	3369	1407	3162	
Peak-hour factor, PHF	0.95	0.95	0.88	0.88	0.64	0.64
Adj. Flow (vph)	21	1295	750	34	31	16
RTOR Reduction (vph)	0	0	0	8	15	0
Lane Group Flow (vph)	21	1295	750	26	32	0
Heavy Vehicles (%)	4%	4%	5%	5%	6%	6%
Turn Type	pm+pt	NA	NA	custom	Prot	
Protected Phases	1	6	2	2	4	
Permitted Phases	6			4		
Actuated Green, G (s)	77.9	77.9	72.7	76.9	4.2	
Effective Green, g (s)	77.9	77.9	72.7	76.9	4.2	
Actuated g/C Ratio	0.78	0.78	0.73	0.77	0.04	
Clearance Time (s)	3.1	5.3	5.3	5.3	4.0	
Vehicle Extension (s)	1.5	0.2	0.2	0.2	2.0	
Lane Grp Cap (vph)	459	2622	2449	1081	132	
v/s Ratio Prot	0.00	c0.38	0.22	0.02	c0.01	
v/s Ratio Perm	0.03			0.00		
v/c Ratio	0.05	0.49	0.31	0.02	0.24	
Uniform Delay, d1	2.6	4.0	4.8	2.7	46.4	
Progression Factor	1.00	1.00	1.44	2.00	1.00	
Incremental Delay, d2	0.0	0.7	0.3	0.0	0.3	
Delay (s)	2.6	4.6	7.2	5.4	46.7	
Level of Service	A	A	A	A	D	
Approach Delay (s)		4.6	7.1		46.7	
Approach LOS		A	A		D	

Intersection Summary			
HCM 2000 Control Delay	6.4	HCM 2000 Level of Service	A
HCM 2000 Volume to Capacity ratio	0.47		
Actuated Cycle Length (s)	100.0	Sum of lost time (s)	16.4
Intersection Capacity Utilization	47.6%	ICU Level of Service	A
Analysis Period (min)	15		
c Critical Lane Group			

Queues

Existing AM

5: Shops at Farmington Valley/Private Driveway & Route 44

01/29/2024



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	10	1120	130	90	620	15	70	0	40	10	5	5
Future Volume (vph)	10	1120	130	90	620	15	70	0	40	10	5	5
Peak Hour Factor	0.97	0.97	0.97	0.87	0.87	0.87	0.64	0.64	0.64	0.63	0.63	0.63
Heavy Vehicles (%)	3%	3%	3%	6%	6%	6%	2%	2%	2%	0%	0%	0%
Shared Lane Traffic (%)							50%					
Lane Group Flow (vph)	10	1155	134	103	730	0	54	55	63	16	16	0
Turn Type	Prot	NA	Prot	Prot	NA		Split	NA	pt+ov	Split	NA	
Protected Phases	5	2	2	1	6		7	7	17	8	8	
Permitted Phases												
Detector Phase	5	2	2	1	6		7	7	17	8	8	
Switch Phase												
Minimum Initial (s)	5.0	15.0	15.0	5.0	15.0		5.0	5.0		7.0	7.0	
Minimum Split (s)	11.8	20.5	20.5	11.8	23.0		10.0	10.0		13.8	13.8	
Total Split (s)	13.0	53.0	53.0	13.0	53.0		20.0	20.0		14.0	14.0	
Total Split (%)	13.0%	53.0%	53.0%	13.0%	53.0%		20.0%	20.0%		14.0%	14.0%	
Yellow Time (s)	3.2	4.4	4.4	3.2	4.4		3.0	3.0		3.0	3.0	
All-Red Time (s)	3.6	1.1	1.1	3.6	1.1		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)	6.8	5.5	5.5	6.8	5.5		5.0	5.0		5.0	5.0	
Lead/Lag	Lead	Lag	Lag	Lead	Lag		Lead	Lead		Lag	Lag	
Lead-Lag Optimize?	Yes	Yes	Yes	Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	C-Min	C-Min	None	Min		None	None		None	None	
v/c Ratio	0.11	0.55	0.13	0.45	0.30		0.42	0.42	0.21	0.13	0.12	
Control Delay	45.9	20.4	6.3	54.7	4.8		52.7	53.1	5.0	46.1	33.4	
Queue Delay	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay	45.9	20.4	6.3	54.7	4.8		52.7	53.1	5.0	46.1	33.4	
Queue Length 50th (ft)	6	332	6	29	32		34	35	0	10	5	
Queue Length 95th (ft)	m12	490	85	62	83		51	52	3	22	17	
Internal Link Dist (ft)		628			773			306			149	
Turn Bay Length (ft)	50		220	370								
Base Capacity (vph)	104	2108	999	236	2464		250	250	375	162	165	
Starvation Cap Reductn	0	0	0	0	0		0	0	0	0	0	
Spillback Cap Reductn	0	0	0	0	0		0	0	0	0	0	
Storage Cap Reductn	0	0	0	0	0		0	0	0	0	0	
Reduced v/c Ratio	0.10	0.55	0.13	0.44	0.30		0.22	0.22	0.17	0.10	0.10	

Intersection Summary

Cycle Length: 100

Actuated Cycle Length: 100

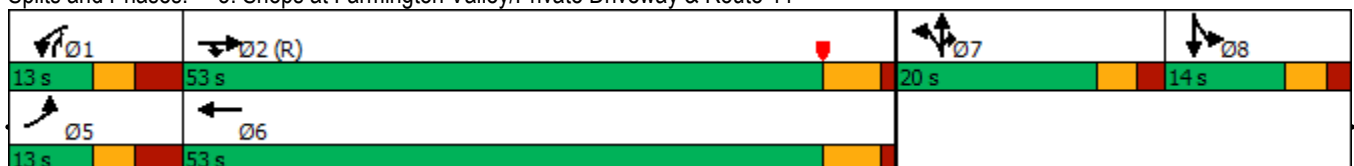
Offset: 85 (85%), Referenced to phase 2:EBT, Start of Yellow

Natural Cycle: 65

Control Type: Actuated-Coordinated

m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 5: Shops at Farmington Valley/Private Driveway & Route 44



HCM Signalized Intersection Capacity Analysis
 5: Shops at Farmington Valley/Private Driveway & Route 44

Existing AM
 01/29/2024



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	10	1120	130	90	620	15	70	0	40	10	5	5
Future Volume (vph)	10	1120	130	90	620	15	70	0	40	10	5	5
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	11	11	11	11	11	12	12	12	13	12	12	12
Grade (%)		1%			0%			1%			0%	
Total Lost time (s)	6.8	5.5	5.5	6.8	5.5		5.0	5.0	6.8	5.0	5.0	
Lane Util. Factor	1.00	0.95	1.00	0.97	0.95		0.95	0.95	1.00	1.00	1.00	
Frt	1.00	1.00	0.85	1.00	1.00		1.00	1.00	0.85	1.00	0.93	
Flt Protected	0.95	1.00	1.00	0.95	1.00		0.95	0.95	1.00	0.95	1.00	
Satd. Flow (prot)	1686	3371	1508	3193	3281		1673	1673	1628	1805	1758	
Flt Permitted	0.95	1.00	1.00	0.95	1.00		0.95	0.95	1.00	0.95	1.00	
Satd. Flow (perm)	1686	3371	1508	3193	3281		1673	1673	1628	1805	1758	
Peak-hour factor, PHF	0.97	0.97	0.97	0.87	0.87	0.87	0.64	0.64	0.64	0.63	0.63	0.63
Adj. Flow (vph)	10	1155	134	103	713	17	109	0	62	16	8	8
RTOR Reduction (vph)	0	0	54	0	1	0	0	0	54	0	8	0
Lane Group Flow (vph)	10	1155	80	103	729	0	54	55	9	16	8	0
Heavy Vehicles (%)	3%	3%	3%	6%	6%	6%	2%	2%	2%	0%	0%	0%
Turn Type	Prot	NA	Prot	Prot	NA		Split	NA	pt+ov	Split	NA	
Protected Phases	5	2	2	1	6		7	7	17	8	8	
Permitted Phases												
Actuated Green, G (s)	1.2	59.5	59.5	7.2	65.5		6.8	6.8	14.0	4.2	4.2	
Effective Green, g (s)	1.2	59.5	59.5	7.2	65.5		6.8	6.8	14.0	4.2	4.2	
Actuated g/C Ratio	0.01	0.60	0.60	0.07	0.66		0.07	0.07	0.14	0.04	0.04	
Clearance Time (s)	6.8	5.5	5.5	6.8	5.5		5.0	5.0		5.0	5.0	
Vehicle Extension (s)	1.5	2.0	2.0	1.5	2.0		2.0	2.0		2.0	2.0	
Lane Grp Cap (vph)	20	2005	897	229	2149		113	113	227	75	73	
v/s Ratio Prot	0.01	c0.34	0.05	c0.03	c0.22		0.03	c0.03	0.01	c0.01	0.00	
v/s Ratio Perm												
v/c Ratio	0.50	0.58	0.09	0.45	0.34		0.48	0.49	0.04	0.21	0.11	
Uniform Delay, d1	49.1	12.5	8.7	44.5	7.7		44.9	44.9	37.2	46.3	46.1	
Progression Factor	0.96	1.45	3.52	1.10	0.67		1.00	1.00	1.00	1.00	1.00	
Incremental Delay, d2	6.4	1.1	0.2	0.5	0.0		1.2	1.2	0.0	0.5	0.3	
Delay (s)	53.5	19.2	30.7	49.4	5.2		46.1	46.1	37.2	46.8	46.4	
Level of Service	D	B	C	D	A		D	D	D	D	D	
Approach Delay (s)		20.6			10.6			42.8			46.6	
Approach LOS		C			B			D			D	

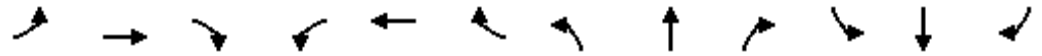
Intersection Summary		
HCM 2000 Control Delay	19.1	HCM 2000 Level of Service B
HCM 2000 Volume to Capacity ratio	0.54	
Actuated Cycle Length (s)	100.0	Sum of lost time (s) 22.3
Intersection Capacity Utilization	55.9%	ICU Level of Service B
Analysis Period (min)	15	
c Critical Lane Group		

Queues

Existing AM

12: Secret Lake Road/Acura of Avon & Route 44

01/29/2024



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	10	1140	10	20	680	20	20	0	20	10	0	10
Future Volume (vph)	10	1140	10	20	680	20	20	0	20	10	0	10
Peak Hour Factor	0.93	0.93	0.93	0.93	0.93	0.93	0.63	0.63	0.63	0.75	0.75	0.75
Heavy Vehicles (%)	4%	4%	4%	5%	5%	5%	2%	2%	2%	11%	11%	11%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	11	1237	0	22	753	0	0	64	0	13	0	13
Turn Type	pm+pt	NA		pm+pt	NA		Split	NA		Prot		Prot
Protected Phases	1	6		5	2		7	7		8		8
Permitted Phases	6			2								
Detector Phase	1	6		5	2		7	7		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0		9.0	9.0		7.0		7.0
Minimum Split (s)	9.0	25.0		9.5	25.0		15.2	15.2		13.2		13.2
Total Split (s)	10.0	58.0		10.0	58.0		17.0	17.0		15.0		15.0
Total Split (%)	10.0%	58.0%		10.0%	58.0%		17.0%	17.0%		15.0%		15.0%
Yellow Time (s)	3.0	4.4		3.0	4.4		3.0	3.0		3.0		3.0
All-Red Time (s)	1.0	2.6		1.0	2.6		3.2	3.2		3.2		3.2
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0		0.0
Total Lost Time (s)	4.0	7.0		4.0	7.0		6.2	6.2		6.2		6.2
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lead		Lag		Lag
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes		Yes
Recall Mode	None	C-Min		None	C-Min		None	None		None		None
v/c Ratio	0.02	0.51		0.07	0.31		0.20	0.20		0.12		0.05
Control Delay	1.5	4.1		5.0	6.9		1.5	1.5		46.2		0.4
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0		0.0		0.0
Total Delay	1.5	4.1		5.0	6.9		1.5	1.5		46.2		0.4
Queue Length 50th (ft)	1	49		2	57		0	0		8		0
Queue Length 95th (ft)	m1	51		12	179		0	0		23		0
Internal Link Dist (ft)		773			254			313				450
Turn Bay Length (ft)	50			105								
Base Capacity (vph)	544	2415		307	2464		345	345		141		268
Starvation Cap Reductn	0	0		0	0		0	0		0		0
Spillback Cap Reductn	0	0		0	0		0	0		0		0
Storage Cap Reductn	0	0		0	0		0	0		0		0
Reduced v/c Ratio	0.02	0.51		0.07	0.31		0.19	0.19		0.09		0.05

Intersection Summary

Cycle Length: 100

Actuated Cycle Length: 100

Offset: 89 (89%), Referenced to phase 2:WBTL and 6:EBTL, Start of Yellow

Natural Cycle: 75

Control Type: Actuated-Coordinated

m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 12: Secret Lake Road/Acura of Avon & Route 44



HCM Signalized Intersection Capacity Analysis
 12: Secret Lake Road/Acura of Avon & Route 44

Existing AM
 01/29/2024

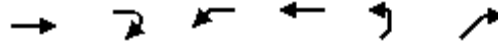


Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗			↕		↖		↗
Traffic Volume (vph)	10	1140	10	20	680	20	20	0	20	10	0	10
Future Volume (vph)	10	1140	10	20	680	20	20	0	20	10	0	10
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	11	11	12	10	11	12	12	16	12	12	16	12
Grade (%)		2%			2%			0%			2%	
Total Lost time (s)	4.0	7.0		4.0	7.0			6.2		6.2		6.2
Lane Util. Factor	1.00	0.95		1.00	0.95			1.00		1.00		1.00
Frt	1.00	1.00		1.00	1.00			0.93		1.00		0.85
Flt Protected	0.95	1.00		0.95	1.00			0.98		0.95		1.00
Satd. Flow (prot)	1661	3317		1588	3276			1921		1610		1440
Flt Permitted	0.36	1.00		0.17	1.00			0.98		0.95		1.00
Satd. Flow (perm)	629	3317		292	3276			1921		1610		1440
Peak-hour factor, PHF	0.93	0.93	0.93	0.93	0.93	0.93	0.63	0.63	0.63	0.75	0.75	0.75
Adj. Flow (vph)	11	1226	11	22	731	22	32	0	32	13	0	13
RTOR Reduction (vph)	0	0	0	0	1	0	0	59	0	0	0	13
Lane Group Flow (vph)	11	1237	0	22	752	0	0	5	0	13	0	0
Heavy Vehicles (%)	4%	4%	4%	5%	5%	5%	2%	2%	2%	11%	11%	11%
Turn Type	pm+pt	NA		pm+pt	NA		Split	NA		Prot		Prot
Protected Phases	1	6		5	2		7	7		8		8
Permitted Phases	6			2								
Actuated Green, G (s)	65.0	64.0		68.2	65.6			7.2		2.8		2.8
Effective Green, g (s)	65.0	64.0		68.2	65.6			7.2		2.8		2.8
Actuated g/C Ratio	0.65	0.64		0.68	0.66			0.07		0.03		0.03
Clearance Time (s)	4.0	7.0		4.0	7.0			6.2		6.2		6.2
Vehicle Extension (s)	1.5	2.5		3.0	2.5			1.5		1.5		1.5
Lane Grp Cap (vph)	419	2122		232	2149			138		45		40
v/s Ratio Prot	0.00	c0.37		c0.00	0.23			c0.00		c0.01		0.00
v/s Ratio Perm	0.02			0.06								
v/c Ratio	0.03	0.58		0.09	0.35			0.03		0.29		0.01
Uniform Delay, d1	6.2	10.3		6.3	7.7			43.2		47.6		47.3
Progression Factor	0.29	0.34		1.00	1.00			1.00		1.00		1.00
Incremental Delay, d2	0.0	1.0		0.2	0.4			0.0		1.3		0.0
Delay (s)	1.8	4.5		6.5	8.1			43.2		48.9		47.3
Level of Service	A	A		A	A			D		D		D
Approach Delay (s)		4.5			8.1			43.2			48.1	
Approach LOS		A			A			D			D	

Intersection Summary			
HCM 2000 Control Delay	7.5	HCM 2000 Level of Service	A
HCM 2000 Volume to Capacity ratio	0.50		
Actuated Cycle Length (s)	100.0	Sum of lost time (s)	23.4
Intersection Capacity Utilization	51.8%	ICU Level of Service	A
Analysis Period (min)	15		
c Critical Lane Group			

Queues
1: Canton Village Shopping Mall & Route 44

Existing Mid
01/29/2024

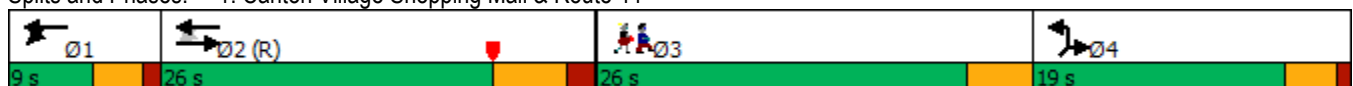


Lane Group	EBT	EBR	WBL	WBT	NEL	NER	Ø3
Lane Configurations	↑↑		↙	↑↑	↙	↗	
Traffic Volume (vph)	700	60	130	730	60	110	
Future Volume (vph)	700	60	130	730	60	110	
Peak Hour Factor	0.89	0.89	0.90	0.90	0.86	0.86	
Heavy Vehicles (%)	1%	1%	1%	1%	1%	1%	
Shared Lane Traffic (%)							
Lane Group Flow (vph)	854	0	144	811	70	128	
Turn Type	NA		D.P+P	NA	Prot	Prot	
Protected Phases	2		1	12	4	4	3
Permitted Phases			2				
Detector Phase	2		1	12	4	4	
Switch Phase							
Minimum Initial (s)	15.0		5.0		8.0	8.0	1.0
Minimum Split (s)	21.2		9.0		12.0	12.0	26.0
Total Split (s)	26.0		9.0		19.0	19.0	26.0
Total Split (%)	32.5%		11.3%		23.8%	23.8%	33%
Yellow Time (s)	4.4		3.0		3.0	3.0	4.0
All-Red Time (s)	1.8		1.0		1.0	1.0	0.0
Lost Time Adjust (s)	0.0		0.0		0.0	0.0	
Total Lost Time (s)	6.2		4.0		4.0	4.0	
Lead/Lag	Lag		Lead		Lag	Lag	Lead
Lead-Lag Optimize?	Yes		Yes		Yes	Yes	Yes
Recall Mode	C-Max		None		None	None	None
v/c Ratio	0.39		0.27	0.28	0.35	0.43	
Control Delay	9.0		3.0	2.2	37.9	11.5	
Queue Delay	0.0		0.0	0.0	0.0	0.0	
Total Delay	9.0		3.0	2.2	37.9	11.5	
Queue Length 50th (ft)	105		11	38	33	0	
Queue Length 95th (ft)	169		m21	64	66	42	
Internal Link Dist (ft)	340			1313	474		
Turn Bay Length (ft)			160				
Base Capacity (vph)	2165		542	2878	355	422	
Starvation Cap Reductn	0		0	0	0	0	
Spillback Cap Reductn	0		0	0	0	0	
Storage Cap Reductn	0		0	0	0	0	
Reduced v/c Ratio	0.39		0.27	0.28	0.20	0.30	

Intersection Summary

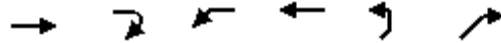
Cycle Length: 80
 Actuated Cycle Length: 80
 Offset: 74 (93%), Referenced to phase 2:EBWB, Start of Yellow
 Natural Cycle: 75
 Control Type: Actuated-Coordinated
 m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 1: Canton Village Shopping Mall & Route 44



HCM Signalized Intersection Capacity Analysis
1: Canton Village Shopping Mall & Route 44

Existing Mid
01/29/2024

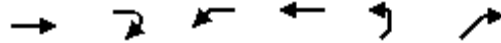


Movement	EBT	EBR	WBL	WBT	NEL	NER
Lane Configurations	↑↑		↵	↑↑	↵	↵
Traffic Volume (vph)	700	60	130	730	60	110
Future Volume (vph)	700	60	130	730	60	110
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	11	12	10	11	14	14
Grade (%)	0%			0%	1%	
Total Lost time (s)	6.2		4.0	4.0	4.0	4.0
Lane Util. Factor	0.95		1.00	0.95	1.00	1.00
Frt	0.99		1.00	1.00	1.00	0.85
Flt Protected	1.00		0.95	1.00	0.95	1.00
Satd. Flow (prot)	3414		1668	3455	1897	1697
Flt Permitted	1.00		0.31	1.00	0.95	1.00
Satd. Flow (perm)	3414		537	3455	1897	1697
Peak-hour factor, PHF	0.89	0.89	0.90	0.90	0.86	0.86
Adj. Flow (vph)	787	67	144	811	70	128
RTOR Reduction (vph)	4	0	0	0	0	117
Lane Group Flow (vph)	850	0	144	811	70	11
Heavy Vehicles (%)	1%	1%	1%	1%	1%	1%
Turn Type	NA		D.P+P	NA	Prot	Prot
Protected Phases	2		1	1 2	4	4
Permitted Phases			2			
Actuated Green, G (s)	49.8		58.8	62.8	7.0	7.0
Effective Green, g (s)	49.8		58.8	62.8	7.0	7.0
Actuated g/C Ratio	0.62		0.73	0.78	0.09	0.09
Clearance Time (s)	6.2		4.0		4.0	4.0
Vehicle Extension (s)	0.2		1.0		2.0	2.0
Lane Grp Cap (vph)	2125		521	2712	165	148
v/s Ratio Prot	c0.25		0.03	c0.23	c0.04	0.01
v/s Ratio Perm			0.17			
v/c Ratio	0.40		0.28	0.30	0.42	0.08
Uniform Delay, d1	7.6		3.2	2.4	34.6	33.5
Progression Factor	1.00		1.02	0.96	1.00	1.00
Incremental Delay, d2	0.6		0.1	0.0	0.6	0.1
Delay (s)	8.2		3.3	2.3	35.2	33.6
Level of Service	A		A	A	D	C
Approach Delay (s)	8.2			2.5	34.2	
Approach LOS	A			A	C	

Intersection Summary			
HCM 2000 Control Delay	8.0	HCM 2000 Level of Service	A
HCM 2000 Volume to Capacity ratio	0.42		
Actuated Cycle Length (s)	80.0	Sum of lost time (s)	18.2
Intersection Capacity Utilization	47.0%	ICU Level of Service	A
Analysis Period (min)	15		
c Critical Lane Group			

Queues
2: Dowd Ave & Route 44

Existing Mid
01/29/2024

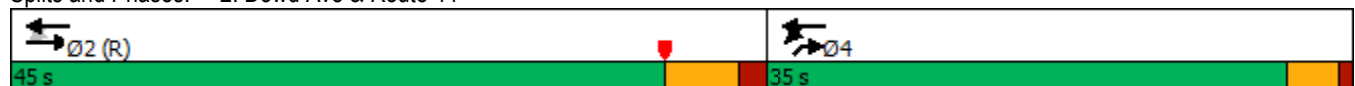


Lane Group	EBT	EBR	WBL	WBT	NEL	NER
Lane Configurations	↑↑			↑↑		↑
Traffic Volume (vph)	800	10	390	860	0	380
Future Volume (vph)	800	10	390	860	0	380
Peak Hour Factor	0.90	0.90	0.87	0.87	0.81	0.81
Heavy Vehicles (%)	1%	1%	1%	1%	1%	1%
Shared Lane Traffic (%)						
Lane Group Flow (vph)	900	0	0	1437	0	469
Turn Type	NA		D.P+P	NA		Over
Protected Phases	2		4	2 4		4
Permitted Phases			2			
Detector Phase	2		4	2 4		4
Switch Phase						
Minimum Initial (s)	15.0		7.0			7.0
Minimum Split (s)	21.1		11.0			11.0
Total Split (s)	45.0		35.0			35.0
Total Split (%)	56.3%		43.8%			43.8%
Yellow Time (s)	4.4		3.0			3.0
All-Red Time (s)	1.7		1.0			1.0
Lost Time Adjust (s)	0.0					0.0
Total Lost Time (s)	6.1					4.0
Lead/Lag						
Lead-Lag Optimize?						
Recall Mode	C-Max		None			None
v/c Ratio	0.53			0.67		0.77
Control Delay	14.5			3.6		31.6
Queue Delay	0.0			0.0		0.0
Total Delay	14.5			3.6		31.6
Queue Length 50th (ft)	194			1		198
Queue Length 95th (ft)	246			2		266
Internal Link Dist (ft)	1313			1683	869	
Turn Bay Length (ft)						
Base Capacity (vph)	1705			2166		624
Starvation Cap Reductn	0			0		0
Spillback Cap Reductn	0			0		0
Storage Cap Reductn	0			0		0
Reduced v/c Ratio	0.53			0.66		0.75

Intersection Summary

Cycle Length: 80
 Actuated Cycle Length: 80
 Offset: 36 (45%), Referenced to phase 2:EBWB, Start of Yellow
 Natural Cycle: 60
 Control Type: Actuated-Coordinated

Splits and Phases: 2: Dowd Ave & Route 44



HCM Signalized Intersection Capacity Analysis

2: Dowd Ave & Route 44

Existing Mid
01/29/2024



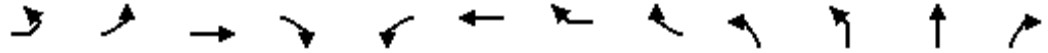
Movement	EBT	EBR	WBL	WBT	NEL	NER
Lane Configurations	↑↑			↑↑		↑
Traffic Volume (vph)	800	10	390	860	0	380
Future Volume (vph)	800	10	390	860	0	380
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	11	12	12	11	12	12
Grade (%)	0%			0%	2%	
Total Lost time (s)	6.1			6.1		4.0
Lane Util. Factor	0.95			0.95		1.00
Frt	1.00			1.00		0.86
Flt Protected	1.00			0.98		1.00
Satd. Flow (prot)	3449			3402		1611
Flt Permitted	1.00			0.55		1.00
Satd. Flow (perm)	3449			1896		1611
Peak-hour factor, PHF	0.90	0.90	0.87	0.87	0.81	0.81
Adj. Flow (vph)	889	11	448	989	0	469
RTOR Reduction (vph)	1	0	0	0	0	0
Lane Group Flow (vph)	899	0	0	1437	0	469
Heavy Vehicles (%)	1%	1%	1%	1%	1%	1%
Turn Type	NA		D.P+P	NA		Over
Protected Phases	2		4	2 4		4
Permitted Phases			2			
Actuated Green, G (s)	39.5			69.9		30.4
Effective Green, g (s)	39.5			69.9		30.4
Actuated g/C Ratio	0.49			0.87		0.38
Clearance Time (s)	6.1					4.0
Vehicle Extension (s)	0.2					3.0
Lane Grp Cap (vph)	1702			2228		612
v/s Ratio Prot	0.26			0.24		c0.29
v/s Ratio Perm				c0.32		
v/c Ratio	0.53			0.64		0.77
Uniform Delay, d1	13.9			1.5		21.7
Progression Factor	0.95			1.00		1.00
Incremental Delay, d2	1.1			0.6		5.7
Delay (s)	14.2			2.1		27.4
Level of Service	B			A		C
Approach Delay (s)	14.2			2.1	27.4	
Approach LOS	B			A	C	

Intersection Summary

HCM 2000 Control Delay	10.2	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.70		
Actuated Cycle Length (s)	80.0	Sum of lost time (s)	10.1
Intersection Capacity Utilization	67.7%	ICU Level of Service	C
Analysis Period (min)	15		
c Critical Lane Group			

Queues
3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr

Existing Mid
01/29/2024



Lane Group	EBL2	EBL	EBT	EBR	WBL	WBT	WBR	WBR2	NBL2	NBL	NBT	NBR
Lane Configurations												
Traffic Volume (vph)	10	130	970	70	180	1000	10	100	130	0	70	180
Future Volume (vph)	10	130	970	70	180	1000	10	100	130	0	70	180
Peak Hour Factor	0.94	0.94	0.94	0.94	0.96	0.96	0.96	0.96	0.94	0.94	0.94	0.94
Heavy Vehicles (%)	1%	1%	1%	1%	0%	0%	0%	0%	0%	0%	0%	0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	149	1032	74	188	1052	0	104	0	138	74	191
Turn Type	Prot	Prot	NA	Free	Prot	NA		Free	Split	Split	NA	pm+ov
Protected Phases	1	1	6		5	2			7	7	7	5
Permitted Phases				Free				Free				7
Detector Phase	1	1	6		5	2			7	7	7	5
Switch Phase												
Minimum Initial (s)	5.0	5.0	15.0		5.0	15.0			9.0	9.0	9.0	5.0
Minimum Split (s)	11.6	11.6	21.8		11.6	21.8			15.0	15.0	15.0	11.6
Total Split (s)	22.6	22.6	54.8		25.6	57.8			24.0	24.0	24.0	25.6
Total Split (%)	13.0%	13.0%	31.5%		14.7%	33.2%			13.8%	13.8%	13.8%	14.7%
Yellow Time (s)	3.0	3.0	4.5		3.0	4.5			4.0	4.0	4.0	3.0
All-Red Time (s)	3.6	3.6	2.3		3.6	2.3			2.0	2.0	2.0	3.6
Lost Time Adjust (s)		0.0	0.0		0.0	0.0			0.0	0.0	0.0	0.0
Total Lost Time (s)		6.6	6.8		6.6	6.8			6.0	6.0	6.0	6.6
Lead/Lag	Lead	Lead	Lag		Lead	Lag			Lead	Lead	Lead	Lead
Lead-Lag Optimize?	Yes	Yes	Yes		Yes	Yes			Yes	Yes	Yes	Yes
Recall Mode	None	None	Min		None	Min			None	None	None	None
v/c Ratio		0.83	0.89	0.04	0.86	0.87		0.07		0.78	0.36	0.38
Control Delay		99.5	58.9	0.0	97.5	55.3		0.1		96.5	71.6	6.9
Queue Delay		0.0	0.0	0.0	0.0	0.0		0.0		0.0	0.0	0.0
Total Delay		99.5	58.9	0.0	97.5	55.3		0.1		96.5	71.6	6.9
Queue Length 50th (ft)		129	434	0	162	432		0		119	61	0
Queue Length 95th (ft)		#332	#818	0	#402	#815		0		#262	138	56
Internal Link Dist (ft)			1683			692					428	
Turn Bay Length (ft)		380		220	340			450		145		50
Base Capacity (vph)		180	1160	1689	218	1214		1507		219	254	507
Starvation Cap Reductn		0	0	0	0	0		0		0	0	0
Spillback Cap Reductn		0	0	0	0	0		0		0	0	0
Storage Cap Reductn		0	0	0	0	0		0		0	0	0
Reduced v/c Ratio		0.83	0.89	0.04	0.86	0.87		0.07		0.63	0.29	0.38

Intersection Summary

Cycle Length: 174.2

Actuated Cycle Length: 149.1

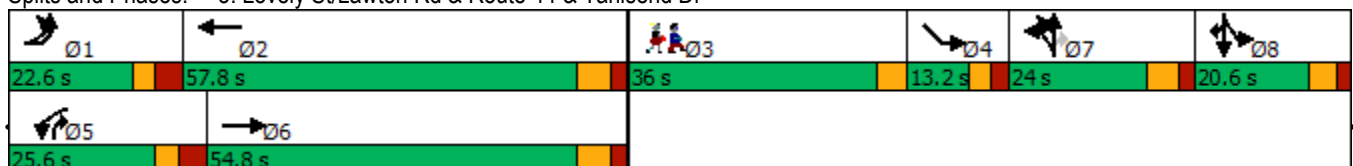
Natural Cycle: 145

Control Type: Actuated-Uncoordinated

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

Splits and Phases: 3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr



Queues
3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr

Existing Mid
01/29/2024

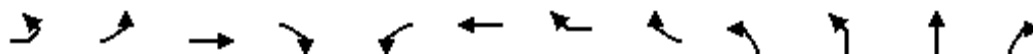


Lane Group	SBL	SBT	SBR	SBR2	SEL	SER	SER2	Ø3
Lane Configurations		↕	↗		↘			
Traffic Volume (vph)	100	70	190	10	10	10	10	
Future Volume (vph)	100	70	190	10	10	10	10	
Peak Hour Factor	0.93	0.93	0.93	0.93	0.56	0.56	0.56	
Heavy Vehicles (%)	1%	1%	1%	1%	0%	0%	0%	
Shared Lane Traffic (%)								
Lane Group Flow (vph)	0	183	215	0	54	0	0	
Turn Type	Split	NA	Prot		Prot			
Protected Phases	8	8	8		4			3
Permitted Phases								
Detector Phase	8	8	8		4			
Switch Phase								
Minimum Initial (s)	9.0	9.0	9.0		6.0			1.0
Minimum Split (s)	14.6	14.6	14.6		11.2			36.0
Total Split (s)	20.6	20.6	20.6		13.2			36.0
Total Split (%)	11.8%	11.8%	11.8%		7.6%			21%
Yellow Time (s)	3.6	3.6	3.6		3.0			4.0
All-Red Time (s)	2.0	2.0	2.0		2.2			0.0
Lost Time Adjust (s)		0.0	0.0		0.0			
Total Lost Time (s)		5.6	5.6		5.2			
Lead/Lag	Lag	Lag	Lag		Lag			Lead
Lead-Lag Optimize?	Yes	Yes	Yes		Yes			Yes
Recall Mode	None	None	None		None			None
v/c Ratio		1.06	0.81		0.64			
Control Delay		146.2	88.7		105.6			
Queue Delay		0.0	0.0		0.0			
Total Delay		146.2	88.7		105.6			
Queue Length 50th (ft)		163	105		47			
Queue Length 95th (ft)		#433	#242		71			
Internal Link Dist (ft)		713			445			
Turn Bay Length (ft)			330					
Base Capacity (vph)		173	267		89			
Starvation Cap Reductn		0	0		0			
Spillback Cap Reductn		0	0		0			
Storage Cap Reductn		0	0		0			
Reduced v/c Ratio		1.06	0.81		0.61			
Intersection Summary								

HCM Signalized Intersection Capacity Analysis

3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr

Existing Mid
01/29/2024



Movement	EBL2	EBL	EBT	EBR	WBL	WBT	WBR	WBR2	NBL2	NBL	NBT	NBR
Lane Configurations		↔	↕	↗	↖	↕		↗		↖	↕	↗
Traffic Volume (vph)	10	130	970	70	180	1000	10	100	130	0	70	180
Future Volume (vph)	10	130	970	70	180	1000	10	100	130	0	70	180
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	12	10	12	14	10	11	12	10	10	12	15	12
Grade (%)			2%			0%					2%	
Total Lost time (s)		6.6	6.8	4.0	6.6	6.8		4.0		6.0	6.0	6.6
Lane Util. Factor		1.00	0.95	1.00	1.00	0.95		1.00		1.00	1.00	1.00
Frt		1.00	1.00	0.85	1.00	1.00		0.85		1.00	1.00	0.85
Flt Protected		0.95	1.00	1.00	0.95	1.00		1.00		0.95	1.00	1.00
Satd. Flow (prot)		1651	3539	1689	1685	3485		1507		1787	2069	1599
Flt Permitted		0.95	1.00	1.00	0.95	1.00		1.00		0.95	1.00	1.00
Satd. Flow (perm)		1651	3539	1689	1685	3485		1507		1787	2069	1599
Peak-hour factor, PHF	0.94	0.94	0.94	0.94	0.96	0.96	0.96	0.96	0.94	0.94	0.94	0.94
Adj. Flow (vph)	11	138	1032	74	188	1042	10	104	138	0	74	191
RTOR Reduction (vph)	0	0	0	0	0	0	0	0	0	0	0	148
Lane Group Flow (vph)	0	149	1032	74	188	1052	0	104	0	138	74	43
Heavy Vehicles (%)	1%	1%	1%	1%	0%	0%	0%	0%	0%	0%	0%	0%
Turn Type	Prot	Prot	NA	Free	Prot	NA		Free	Split	Split	NA	pm+ov
Protected Phases	1	1	6		5	2			7	7	7	5
Permitted Phases				Free				Free				7
Actuated Green, G (s)		16.3	48.9	151.3	19.4	52.0		151.3		14.8	14.8	34.2
Effective Green, g (s)		16.3	48.9	151.3	19.4	52.0		151.3		14.8	14.8	34.2
Actuated g/C Ratio		0.11	0.32	1.00	0.13	0.34		1.00		0.10	0.10	0.23
Clearance Time (s)		6.6	6.8		6.6	6.8				6.0	6.0	6.6
Vehicle Extension (s)		2.0	3.0		2.0	3.0				2.0	2.0	2.0
Lane Grp Cap (vph)		177	1143	1689	216	1197		1507		174	202	361
v/s Ratio Prot		0.09	0.29		c0.11	c0.30				c0.08	0.04	0.02
v/s Ratio Perm				0.04				c0.07				0.01
v/c Ratio		0.84	0.90	0.04	0.87	0.88		0.07		0.79	0.37	0.12
Uniform Delay, d1		66.2	48.9	0.0	64.7	46.7		0.0		66.8	63.9	46.6
Progression Factor		1.00	1.00	1.00	1.00	1.00		1.00		1.00	1.00	1.00
Incremental Delay, d2		27.7	10.0	0.0	28.7	7.6		0.1		20.2	0.4	0.1
Delay (s)		94.0	58.9	0.0	93.4	54.3		0.1		87.0	64.3	46.6
Level of Service		F	E	A	F	D		A		F	E	D
Approach Delay (s)			59.6			55.6					63.7	
Approach LOS			E			E					E	
Intersection Summary												
HCM 2000 Control Delay			65.5			HCM 2000 Level of Service				E		
HCM 2000 Volume to Capacity ratio			0.82									
Actuated Cycle Length (s)			151.3			Sum of lost time (s)			34.2			
Intersection Capacity Utilization			77.8%			ICU Level of Service				D		
Analysis Period (min)			15									
c Critical Lane Group												

HCM Signalized Intersection Capacity Analysis
 3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr

Existing Mid
 01/29/2024



Movement	SBL	SBT	SBR	SBR2	SEL	SER	SER2
Lane Configurations		↕	↗		↘		
Traffic Volume (vph)	100	70	190	10	10	10	10
Future Volume (vph)	100	70	190	10	10	10	10
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900
Lane Width	12	11	11	12	12	16	12
Grade (%)		8%			8%		
Total Lost time (s)		5.6	5.6		5.2		
Lane Util. Factor		1.00	0.88		1.00		
Frt		1.00	0.85		0.91		
Flt Protected		0.97	1.00		0.98		
Satd. Flow (prot)		1696	2612		1633		
Flt Permitted		0.97	1.00		0.98		
Satd. Flow (perm)		1696	2612		1633		
Peak-hour factor, PHF	0.93	0.93	0.93	0.93	0.56	0.56	0.56
Adj. Flow (vph)	108	75	204	11	18	18	18
RTOR Reduction (vph)	0	0	0	0	0	0	0
Lane Group Flow (vph)	0	183	215	0	54	0	0
Heavy Vehicles (%)	1%	1%	1%	1%	0%	0%	0%
Turn Type	Split	NA	Prot		Prot		
Protected Phases	8	8	8		4		
Permitted Phases							
Actuated Green, G (s)		15.3	15.3		7.7		
Effective Green, g (s)		15.3	15.3		7.7		
Actuated g/C Ratio		0.10	0.10		0.05		
Clearance Time (s)		5.6	5.6		5.2		
Vehicle Extension (s)		2.0	2.0		2.0		
Lane Grp Cap (vph)		171	264		83		
v/s Ratio Prot		c0.11	0.08		c0.03		
v/s Ratio Perm							
v/c Ratio		1.07	0.81		0.65		
Uniform Delay, d1		68.0	66.6		70.5		
Progression Factor		1.00	1.00		1.00		
Incremental Delay, d2		88.7	16.4		13.1		
Delay (s)		156.7	83.1		83.5		
Level of Service		F	F		F		
Approach Delay (s)		116.9			83.5		
Approach LOS		F			F		
Intersection Summary							

Queues
4: Route 44 & CVS Drive

Existing Mid
01/29/2024

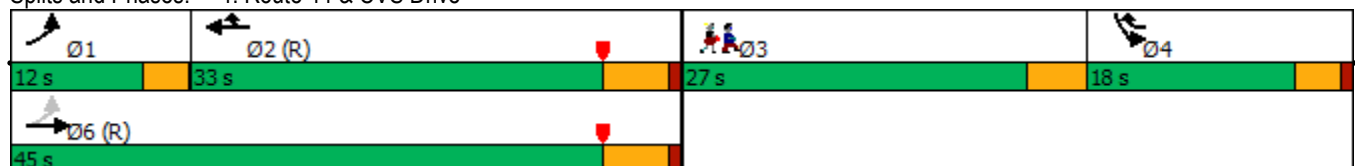


Lane Group	EBL	EBT	WBT	WBR	SBL	SBR	Ø3
Lane Configurations							
Traffic Volume (vph)	50	1200	1240	100	50	50	
Future Volume (vph)	50	1200	1240	100	50	50	
Peak Hour Factor	0.95	0.95	0.95	0.95	0.75	0.75	
Heavy Vehicles (%)	1%	1%	1%	1%	0%	0%	
Shared Lane Traffic (%)							
Lane Group Flow (vph)	53	1263	1305	105	134	0	
Turn Type	pm+pt	NA	NA	pt+ov	Prot		
Protected Phases	1	6	2	2 4	4		3
Permitted Phases	6						
Detector Phase	1	6	2	2 4	4		
Switch Phase							
Minimum Initial (s)	5.0	25.0	25.0		7.0		1.0
Minimum Split (s)	8.1	30.3	30.3		11.0		27.0
Total Split (s)	12.0	45.0	33.0		18.0		27.0
Total Split (%)	13.3%	50.0%	36.7%		20.0%		30%
Yellow Time (s)	3.0	4.3	4.3		3.0		4.0
All-Red Time (s)	0.1	1.0	1.0		1.0		0.0
Lost Time Adjust (s)	0.0	0.0	0.0		0.0		
Total Lost Time (s)	3.1	5.3	5.3		4.0		
Lead/Lag	Lead		Lag		Lag		Lead
Lead-Lag Optimize?	Yes		Yes		Yes		Yes
Recall Mode	None	C-Max	C-Max		None		None
v/c Ratio	0.19	0.48	0.54	0.09	0.40		
Control Delay	6.7	7.4	10.4	2.1	24.6		
Queue Delay	0.0	0.0	0.0	0.0	0.0		
Total Delay	6.7	7.4	10.4	2.1	24.6		
Queue Length 50th (ft)	3	75	121	0	19		
Queue Length 95th (ft)	32	365	#520	m16	34		
Internal Link Dist (ft)		692	628		200		
Turn Bay Length (ft)	155			140			
Base Capacity (vph)	334	2614	2435	1290	568		
Starvation Cap Reductn	0	0	0	0	0		
Spillback Cap Reductn	0	0	0	0	0		
Storage Cap Reductn	0	0	0	0	0		
Reduced v/c Ratio	0.16	0.48	0.54	0.08	0.24		

Intersection Summary

Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 10 (11%), Referenced to phase 2:WBT and 6:EBTL, Start of Yellow
 Natural Cycle: 80
 Control Type: Actuated-Coordinated
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.
 m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 4: Route 44 & CVS Drive



HCM Signalized Intersection Capacity Analysis

4: Route 44 & CVS Drive

Existing Mid
01/29/2024



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↖	↗↗	↗↗	↖	↖↖↖	
Traffic Volume (vph)	50	1200	1240	100	50	50
Future Volume (vph)	50	1200	1240	100	50	50
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	10	12	12	10	12	12
Grade (%)		6%	4%		2%	
Total Lost time (s)	3.1	5.3	5.3	5.3	4.0	
Lane Util. Factor	1.00	0.95	0.95	1.00	0.97	
Frt	1.00	1.00	1.00	0.85	0.93	
Flt Protected	0.95	1.00	1.00	1.00	0.98	
Satd. Flow (prot)	1618	3467	3503	1463	3293	
Flt Permitted	0.15	1.00	1.00	1.00	0.98	
Satd. Flow (perm)	257	3467	3503	1463	3293	
Peak-hour factor, PHF	0.95	0.95	0.95	0.95	0.75	0.75
Adj. Flow (vph)	53	1263	1305	105	67	67
RTOR Reduction (vph)	0	0	0	20	61	0
Lane Group Flow (vph)	53	1263	1305	85	73	0
Heavy Vehicles (%)	1%	1%	1%	1%	0%	0%
Turn Type	pm+pt	NA	NA	pt+ov	Prot	
Protected Phases	1	6	2	2 4	4	
Permitted Phases	6					
Actuated Green, G (s)	64.7	64.7	58.2	65.6	7.4	
Effective Green, g (s)	64.7	64.7	58.2	65.6	7.4	
Actuated g/C Ratio	0.72	0.72	0.65	0.73	0.08	
Clearance Time (s)	3.1	5.3	5.3		4.0	
Vehicle Extension (s)	1.5	0.2	0.2		2.0	
Lane Grp Cap (vph)	236	2492	2265	1066	270	
v/s Ratio Prot	0.01	c0.36	c0.37	0.06	c0.02	
v/s Ratio Perm	0.15					
v/c Ratio	0.22	0.51	0.58	0.08	0.27	
Uniform Delay, d1	5.7	5.6	9.0	3.5	38.8	
Progression Factor	1.00	1.00	0.84	1.03	1.00	
Incremental Delay, d2	0.2	0.7	0.9	0.0	0.2	
Delay (s)	5.8	6.3	8.5	3.6	39.0	
Level of Service	A	A	A	A	D	
Approach Delay (s)		6.3	8.1		39.0	
Approach LOS		A	A		D	

Intersection Summary			
HCM 2000 Control Delay	8.7	HCM 2000 Level of Service	A
HCM 2000 Volume to Capacity ratio	0.51		
Actuated Cycle Length (s)	90.0	Sum of lost time (s)	16.4
Intersection Capacity Utilization	55.1%	ICU Level of Service	B
Analysis Period (min)	15		
c Critical Lane Group			

Queues

Existing Mid

5: Shops at Farmington Valley/Private Driveway & Route 44

01/29/2024



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	5	850	400	320	980	10	360	0	360	5	5	10
Future Volume (vph)	5	850	400	320	980	10	360	0	360	5	5	10
Peak Hour Factor	0.89	0.89	0.89	0.91	0.91	0.91	0.87	0.87	0.87	0.50	0.50	0.50
Heavy Vehicles (%)	1%	1%	1%	1%	1%	1%	0%	0%	0%	0%	0%	0%
Shared Lane Traffic (%)							50%					
Lane Group Flow (vph)	6	955	449	352	1088	0	207	207	414	10	30	0
Turn Type	Prot	NA	Prot	Prot	NA		Split	NA	pt+ov	Split	NA	
Protected Phases	5	2	2	1	6		7	7	17	8	8	
Permitted Phases												
Detector Phase	5	2	2	1	6		7	7	17	8	8	
Switch Phase												
Minimum Initial (s)	5.0	15.0	15.0	5.0	15.0		5.0	5.0		7.0	7.0	
Minimum Split (s)	11.8	20.5	20.5	11.8	20.5		10.0	10.0		12.0	12.0	
Total Split (s)	20.0	36.0	36.0	20.0	36.0		20.0	20.0		14.0	14.0	
Total Split (%)	22.2%	40.0%	40.0%	22.2%	40.0%		22.2%	22.2%		15.6%	15.6%	
Yellow Time (s)	3.2	4.4	4.4	3.2	4.4		3.0	3.0		3.0	3.0	
All-Red Time (s)	3.6	1.1	1.1	3.6	1.1		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)	6.8	5.5	5.5	6.8	5.5		5.0	5.0		5.0	5.0	
Lead/Lag	Lead	Lag	Lag	Lead	Lag		Lead	Lead		Lag	Lag	
Lead-Lag Optimize?	Yes	Yes	Yes	Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	C-Min	C-Min	None	Min		None	None		None	None	
v/c Ratio	0.06	0.64	0.49	0.74	0.51		0.80	0.80	0.62	0.07	0.20	
Control Delay	40.4	25.6	6.8	56.9	10.9		59.3	59.3	11.2	39.6	24.8	
Queue Delay	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay	40.4	25.6	6.8	56.9	10.9		59.3	59.3	11.2	39.6	24.8	
Queue Length 50th (ft)	3	251	28	111	74		118	118	53	5	5	
Queue Length 95th (ft)	m9	263	139	156	225		#212	#212	103	12	13	
Internal Link Dist (ft)		628			773			306				149
Turn Bay Length (ft)	50		220	370								
Base Capacity (vph)	252	1485	919	517	2148		284	284	665	180	189	
Starvation Cap Reductn	0	0	0	0	0		0	0	0	0	0	
Spillback Cap Reductn	0	0	0	0	0		0	0	0	0	0	
Storage Cap Reductn	0	0	0	0	0		0	0	0	0	0	
Reduced v/c Ratio	0.02	0.64	0.49	0.68	0.51		0.73	0.73	0.62	0.06	0.16	

Intersection Summary

Cycle Length: 90

Actuated Cycle Length: 90

Offset: 84 (93%), Referenced to phase 2:EBT, Start of Yellow

Natural Cycle: 75

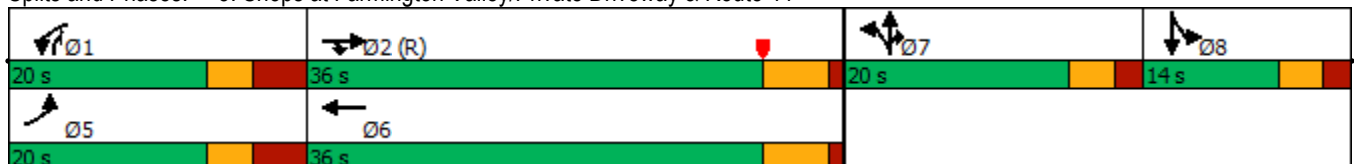
Control Type: Actuated-Coordinated

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 5: Shops at Farmington Valley/Private Driveway & Route 44



HCM Signalized Intersection Capacity Analysis
 5: Shops at Farmington Valley/Private Driveway & Route 44

Existing Mid
 01/29/2024



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	5	850	400	320	980	10	360	0	360	5	5	10
Future Volume (vph)	5	850	400	320	980	10	360	0	360	5	5	10
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	11	11	11	11	11	12	12	12	13	12	12	12
Grade (%)		1%			0%			1%			0%	
Total Lost time (s)	6.8	5.5	5.5	6.8	5.5		5.0	5.0	6.8	5.0	5.0	
Lane Util. Factor	1.00	0.95	1.00	0.97	0.95		0.95	0.95	1.00	1.00	1.00	
Frt	1.00	1.00	0.85	1.00	1.00		1.00	1.00	0.85	1.00	0.90	
Flt Protected	0.95	1.00	1.00	0.95	1.00		0.95	0.95	1.00	0.95	1.00	
Satd. Flow (prot)	1719	3438	1538	3351	3450		1706	1706	1660	1805	1710	
Flt Permitted	0.95	1.00	1.00	0.95	1.00		0.95	0.95	1.00	0.95	1.00	
Satd. Flow (perm)	1719	3438	1538	3351	3450		1706	1706	1660	1805	1710	
Peak-hour factor, PHF	0.89	0.89	0.89	0.91	0.91	0.91	0.87	0.87	0.87	0.50	0.50	0.50
Adj. Flow (vph)	6	955	449	352	1077	11	414	0	414	10	10	20
RTOR Reduction (vph)	0	0	265	0	0	0	0	0	167	0	19	0
Lane Group Flow (vph)	6	955	184	352	1088	0	207	207	247	10	11	0
Heavy Vehicles (%)	1%	1%	1%	1%	1%	1%	0%	0%	0%	0%	0%	0%
Turn Type	Prot	NA	Prot	Prot	NA		Split	NA	pt+ov	Split	NA	
Protected Phases	5	2	2	1	6		7	7	17	8	8	
Permitted Phases												
Actuated Green, G (s)	1.1	36.9	36.9	12.8	48.6		13.7	13.7	26.5	4.3	4.3	
Effective Green, g (s)	1.1	36.9	36.9	12.8	48.6		13.7	13.7	26.5	4.3	4.3	
Actuated g/C Ratio	0.01	0.41	0.41	0.14	0.54		0.15	0.15	0.29	0.05	0.05	
Clearance Time (s)	6.8	5.5	5.5	6.8	5.5		5.0	5.0		5.0	5.0	
Vehicle Extension (s)	1.5	2.0	2.0	1.5	2.0		2.0	2.0		2.0	2.0	
Lane Grp Cap (vph)	21	1409	630	476	1863		259	259	488	86	81	
v/s Ratio Prot	0.00	c0.28	0.12	c0.11	0.32		c0.12	0.12	0.15	0.01	c0.01	
v/s Ratio Perm												
v/c Ratio	0.29	0.68	0.29	0.74	0.58		0.80	0.80	0.51	0.12	0.14	
Uniform Delay, d1	44.1	21.7	17.8	37.0	13.9		36.8	36.8	26.3	41.0	41.1	
Progression Factor	0.97	1.03	2.09	1.31	0.85		1.00	1.00	1.00	1.00	1.00	
Incremental Delay, d2	2.5	2.4	1.1	4.6	0.3		14.7	14.7	0.3	0.2	0.3	
Delay (s)	45.2	24.8	38.3	53.0	12.1		51.6	51.6	26.6	41.3	41.3	
Level of Service	D	C	D	D	B		D	D	C	D	D	
Approach Delay (s)		29.2			22.1			39.1			41.3	
Approach LOS		C			C			D			D	
Intersection Summary												
HCM 2000 Control Delay			28.8			HCM 2000 Level of Service			C			
HCM 2000 Volume to Capacity ratio			0.68									
Actuated Cycle Length (s)			90.0			Sum of lost time (s)			22.3			
Intersection Capacity Utilization			66.0%			ICU Level of Service			C			
Analysis Period (min)			15									
c Critical Lane Group												

Queues
12: Secret Lake Road/Acura of Avon & Route 44

Existing Mid
01/29/2024



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↕		↖	↕			↕		↖		↖
Traffic Volume (vph)	10	1170	30	20	1230	10	50	0	20	10	0	20
Future Volume (vph)	10	1170	30	20	1230	10	50	0	20	10	0	20
Peak Hour Factor	0.95	0.95	0.95	0.97	0.97	0.97	0.69	0.69	0.69	0.70	0.70	0.70
Heavy Vehicles (%)	1%	1%	1%	1%	1%	1%	0%	0%	0%	0%	0%	0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	11	1264	0	21	1278	0	0	101	0	14	0	29
Turn Type	pm+pt	NA		pm+pt	NA		Split	NA		Prot		Prot
Protected Phases	1	6		5	2		7	7		8		8
Permitted Phases	6			2								
Detector Phase	1	6		5	2		7	7		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0		9.0	9.0		7.0		7.0
Minimum Split (s)	9.5	25.0		9.5	25.0		15.2	15.2		13.2		13.2
Total Split (s)	10.0	48.0		10.0	48.0		17.0	17.0		15.0		15.0
Total Split (%)	11.1%	53.3%		11.1%	53.3%		18.9%	18.9%		16.7%		16.7%
Yellow Time (s)	3.0	4.4		3.0	4.4		3.0	3.0		3.0		3.0
All-Red Time (s)	1.0	2.6		1.0	2.6		3.2	3.2		3.2		3.2
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0		0.0
Total Lost Time (s)	4.0	7.0		4.0	7.0		6.2	6.2		6.2		6.2
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lead		Lag		Lag
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes		Yes
Recall Mode	None	C-Min		None	C-Min		None	None		None		None
v/c Ratio	0.04	0.55		0.08	0.54		0.29	0.29		0.10		0.10
Control Delay	6.1	10.5		6.4	11.4		2.7	2.7		40.4		0.8
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0		0.0		0.0
Total Delay	6.1	10.5		6.4	11.4		2.7	2.7		40.4		0.8
Queue Length 50th (ft)	2	190		4	218		0	0		8		0
Queue Length 95th (ft)	m3	212		12	368		0	0		20		0
Internal Link Dist (ft)		773			254			313				450
Turn Bay Length (ft)	50			105								
Base Capacity (vph)	304	2299		289	2374		391	391		174		311
Starvation Cap Reductn	0	0		0	0		0	0		0		0
Spillback Cap Reductn	0	0		0	0		0	0		0		0
Storage Cap Reductn	0	0		0	0		0	0		0		0
Reduced v/c Ratio	0.04	0.55		0.07	0.54		0.26	0.26		0.08		0.09

Intersection Summary

Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 0 (0%), Referenced to phase 2:WBTL and 6:EBTL, Start of Yellow
 Natural Cycle: 75
 Control Type: Actuated-Coordinated
 m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 12: Secret Lake Road/Acura of Avon & Route 44



HCM Signalized Intersection Capacity Analysis
12: Secret Lake Road/Acura of Avon & Route 44

Existing Mid
01/29/2024

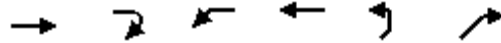


Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↖	↗		↖	↗			↕		↖		↗
Traffic Volume (vph)	10	1170	30	20	1230	10	50	0	20	10	0	20
Future Volume (vph)	10	1170	30	20	1230	10	50	0	20	10	0	20
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	11	11	12	10	11	12	12	16	12	12	16	12
Grade (%)		2%			2%			0%			2%	
Total Lost time (s)	4.0	7.0		4.0	7.0			6.2		6.2		6.2
Lane Util. Factor	1.00	0.95		1.00	0.95			1.00		1.00		1.00
Frt	1.00	1.00		1.00	1.00			0.96		1.00		0.85
Flt Protected	0.95	1.00		0.95	1.00			0.97		0.95		1.00
Satd. Flow (prot)	1710	3408		1651	3417			1999		1787		1599
Flt Permitted	0.16	1.00		0.16	1.00			0.97		0.95		1.00
Satd. Flow (perm)	293	3408		273	3417			1999		1787		1599
Peak-hour factor, PHF	0.95	0.95	0.95	0.97	0.97	0.97	0.69	0.69	0.69	0.70	0.70	0.70
Adj. Flow (vph)	11	1232	32	21	1268	10	72	0	29	14	0	29
RTOR Reduction (vph)	0	2	0	0	0	0	0	93	0	0	0	28
Lane Group Flow (vph)	11	1262	0	21	1278	0	0	8	0	14	0	1
Heavy Vehicles (%)	1%	1%	1%	1%	1%	1%	0%	0%	0%	0%	0%	0%
Turn Type	pm+pt	NA		pm+pt	NA		Split	NA		Prot		Prot
Protected Phases	1	6		5	2		7	7		8		8
Permitted Phases	6			2								
Actuated Green, G (s)	54.2	53.2		56.2	54.2			7.2		4.2		4.2
Effective Green, g (s)	54.2	53.2		56.2	54.2			7.2		4.2		4.2
Actuated g/C Ratio	0.60	0.59		0.62	0.60			0.08		0.05		0.05
Clearance Time (s)	4.0	7.0		4.0	7.0			6.2		6.2		6.2
Vehicle Extension (s)	1.5	2.5		1.5	2.5			1.5		1.5		1.5
Lane Grp Cap (vph)	192	2014		201	2057			159		83		74
v/s Ratio Prot	0.00	0.37		c0.00	c0.37			c0.00		c0.01		0.00
v/s Ratio Perm	0.03			0.06								
v/c Ratio	0.06	0.63		0.10	0.62			0.05		0.17		0.02
Uniform Delay, d1	8.2	12.0		7.9	11.4			38.2		41.2		40.9
Progression Factor	0.98	0.82		1.00	1.00			1.00		1.00		1.00
Incremental Delay, d2	0.0	1.2		0.1	1.4			0.0		0.4		0.0
Delay (s)	8.1	10.9		7.9	12.8			38.3		41.6		41.0
Level of Service	A	B		A	B			D		D		D
Approach Delay (s)		10.9			12.7			38.3			41.2	
Approach LOS		B			B			D			D	

Intersection Summary			
HCM 2000 Control Delay	13.3	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.52		
Actuated Cycle Length (s)	90.0	Sum of lost time (s)	23.4
Intersection Capacity Utilization	63.8%	ICU Level of Service	B
Analysis Period (min)	15		
c Critical Lane Group			

Queues
1: Canton Village Shopping Mall & Route 44

Existing PM
01/29/2024



Lane Group	EBT	EBR	WBL	WBT	NEL	NER	Ø3
Lane Configurations	↑↑		↙	↑↑	↙	↗	
Traffic Volume (vph)	640	60	100	800	70	80	
Future Volume (vph)	640	60	100	800	70	80	
Peak Hour Factor	0.91	0.91	0.94	0.94	0.80	0.80	
Heavy Vehicles (%)	2%	2%	1%	1%	1%	1%	
Shared Lane Traffic (%)							
Lane Group Flow (vph)	769	0	106	851	88	100	
Turn Type	NA		D.P+P	NA	Prot	Prot	
Protected Phases	2		1	12	4	4	3
Permitted Phases			2				
Detector Phase	2		1	12	4	4	
Switch Phase							
Minimum Initial (s)	15.0		5.0		8.0	8.0	1.0
Minimum Split (s)	21.2		9.0		12.0	12.0	26.0
Total Split (s)	27.0		13.0		24.0	24.0	26.0
Total Split (%)	30.0%		14.4%		26.7%	26.7%	29%
Yellow Time (s)	4.4		3.0		3.0	3.0	4.0
All-Red Time (s)	1.8		1.0		1.0	1.0	0.0
Lost Time Adjust (s)	0.0		0.0		0.0	0.0	
Total Lost Time (s)	6.2		4.0		4.0	4.0	
Lead/Lag	Lag		Lead		Lag	Lag	Lead
Lead-Lag Optimize?	Yes		Yes		Yes	Yes	Yes
Recall Mode	C-Max		None		None	None	None
v/c Ratio	0.39		0.21	0.33	0.45	0.38	
Control Delay	13.2		5.9	5.1	45.0	12.2	
Queue Delay	0.0		0.0	0.0	0.0	0.0	
Total Delay	13.2		5.9	5.1	45.0	12.2	
Queue Length 50th (ft)	92		8	44	48	0	
Queue Length 95th (ft)	265		m41	185	81	33	
Internal Link Dist (ft)	340			1313	474		
Turn Bay Length (ft)			160				
Base Capacity (vph)	1988		510	2552	421	454	
Starvation Cap Reductn	0		0	0	0	0	
Spillback Cap Reductn	0		0	0	0	0	
Storage Cap Reductn	0		0	0	0	0	
Reduced v/c Ratio	0.39		0.21	0.33	0.21	0.22	

Intersection Summary

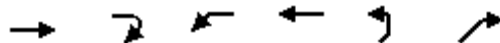
Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 75 (83%), Referenced to phase 2:EBWB, Start of Yellow
 Natural Cycle: 70
 Control Type: Actuated-Coordinated
 m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 1: Canton Village Shopping Mall & Route 44



HCM Signalized Intersection Capacity Analysis
1: Canton Village Shopping Mall & Route 44

Existing PM
01/29/2024



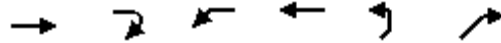
Movement	EBT	EBR	WBL	WBT	NEL	NER
Lane Configurations	↑↑		↵	↑↑	↵	↵
Traffic Volume (vph)	640	60	100	800	70	80
Future Volume (vph)	640	60	100	800	70	80
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	11	12	10	11	14	14
Grade (%)	0%			0%	1%	
Total Lost time (s)	6.2		4.0	4.0	4.0	4.0
Lane Util. Factor	0.95		1.00	0.95	1.00	1.00
Frt	0.99		1.00	1.00	1.00	0.85
Flt Protected	1.00		0.95	1.00	0.95	1.00
Satd. Flow (prot)	3377		1668	3455	1897	1697
Flt Permitted	1.00		0.32	1.00	0.95	1.00
Satd. Flow (perm)	3377		563	3455	1897	1697
Peak-hour factor, PHF	0.91	0.91	0.94	0.94	0.80	0.80
Adj. Flow (vph)	703	66	106	851	88	100
RTOR Reduction (vph)	4	0	0	0	0	90
Lane Group Flow (vph)	765	0	106	851	88	10
Heavy Vehicles (%)	2%	2%	1%	1%	1%	1%
Turn Type	NA		D.P+P	NA	Prot	Prot
Protected Phases	2		1	1 2	4	4
Permitted Phases			2			
Actuated Green, G (s)	49.7		58.1	62.1	9.3	9.3
Effective Green, g (s)	49.7		58.1	62.1	9.3	9.3
Actuated g/C Ratio	0.55		0.65	0.69	0.10	0.10
Clearance Time (s)	6.2		4.0		4.0	4.0
Vehicle Extension (s)	0.2		1.0		2.0	2.0
Lane Grp Cap (vph)	1864		466	2383	196	175
v/s Ratio Prot	c0.23		0.02	c0.25	c0.05	0.01
v/s Ratio Perm			0.13			
v/c Ratio	0.41		0.23	0.36	0.45	0.06
Uniform Delay, d1	11.7		6.2	5.7	37.9	36.4
Progression Factor	1.00		0.87	0.82	1.00	1.00
Incremental Delay, d2	0.7		0.1	0.0	0.6	0.1
Delay (s)	12.3		5.5	4.8	38.5	36.5
Level of Service	B		A	A	D	D
Approach Delay (s)	12.3			4.8	37.4	
Approach LOS	B			A	D	

Intersection Summary

HCM 2000 Control Delay	11.1	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.39		
Actuated Cycle Length (s)	90.0	Sum of lost time (s)	18.2
Intersection Capacity Utilization	43.6%	ICU Level of Service	A
Analysis Period (min)	15		
c Critical Lane Group			

Queues
2: Dowd Ave & Route 44

Existing PM
01/29/2024

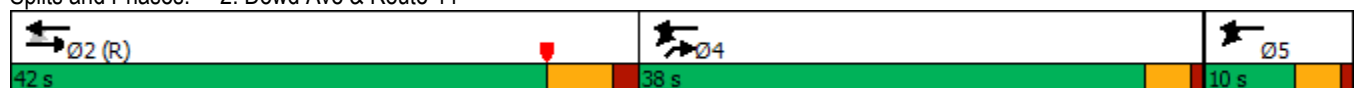


Lane Group	EBT	EBR	WBL	WBT	NEL	NER	Ø5
Lane Configurations	↑↑			↑↑		↑	
Traffic Volume (vph)	710	10	510	900	0	370	
Future Volume (vph)	710	10	510	900	0	370	
Peak Hour Factor	0.90	0.90	0.91	0.91	0.95	0.95	
Heavy Vehicles (%)	1%	1%	1%	1%	2%	2%	
Shared Lane Traffic (%)							
Lane Group Flow (vph)	800	0	0	1549	0	389	
Turn Type	NA		D.P+P	NA		Over	
Protected Phases	2		4 5	2 4 5		4	5
Permitted Phases			2				
Detector Phase	2		4 5	2 4 5		4	
Switch Phase							
Minimum Initial (s)	15.0					7.0	6.0
Minimum Split (s)	21.1					11.0	10.0
Total Split (s)	42.0					38.0	10.0
Total Split (%)	46.7%					42.2%	11%
Yellow Time (s)	4.4					3.0	3.0
All-Red Time (s)	1.7					1.0	1.0
Lost Time Adjust (s)	0.0					0.0	
Total Lost Time (s)	6.1					4.0	
Lead/Lag							
Lead-Lag Optimize?							
Recall Mode	C-Max					None	None
v/c Ratio	0.56			0.67		0.67	
Control Delay	15.0			3.8		30.5	
Queue Delay	0.0			0.0		0.0	
Total Delay	15.0			3.8		30.5	
Queue Length 50th (ft)	196			1		179	
Queue Length 95th (ft)	32			3		280	
Internal Link Dist (ft)	1313			1683	869		
Turn Bay Length (ft)							
Base Capacity (vph)	1426			2305		602	
Starvation Cap Reductn	0			0		0	
Spillback Cap Reductn	0			0		0	
Storage Cap Reductn	0			0		0	
Reduced v/c Ratio	0.56			0.67		0.65	

Intersection Summary

Cycle Length: 90
 Actuated Cycle Length: 90
 Offset: 21 (23%), Referenced to phase 2:EBWB, Start of Yellow
 Natural Cycle: 65
 Control Type: Actuated-Coordinated

Splits and Phases: 2: Dowd Ave & Route 44



HCM Signalized Intersection Capacity Analysis

2: Dowd Ave & Route 44

Existing PM
01/29/2024



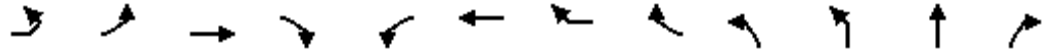
Movement	EBT	EBR	WBL	WBT	NEL	NER
Lane Configurations	↑↑			↑↑		↑
Traffic Volume (vph)	710	10	510	900	0	370
Future Volume (vph)	710	10	510	900	0	370
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	11	12	12	11	12	12
Grade (%)	0%			0%	2%	
Total Lost time (s)	6.1			6.1		4.0
Lane Util. Factor	0.95			0.95		1.00
Frt	1.00			1.00		0.86
Flt Protected	1.00			0.98		1.00
Satd. Flow (prot)	3448			3394		1595
Flt Permitted	1.00			0.54		1.00
Satd. Flow (perm)	3448			1877		1595
Peak-hour factor, PHF	0.90	0.90	0.91	0.91	0.95	0.95
Adj. Flow (vph)	789	11	560	989	0	389
RTOR Reduction (vph)	1	0	0	0	0	0
Lane Group Flow (vph)	799	0	0	1549	0	389
Heavy Vehicles (%)	1%	1%	1%	1%	2%	2%
Turn Type	NA		D.P+P	NA		Over
Protected Phases	2		4 5	2 4 5		4
Permitted Phases			2			
Actuated Green, G (s)	37.2			79.9		32.7
Effective Green, g (s)	37.2			75.9		32.7
Actuated g/C Ratio	0.41			0.84		0.36
Clearance Time (s)	6.1					4.0
Vehicle Extension (s)	0.2					3.0
Lane Grp Cap (vph)	1425			2235		579
v/s Ratio Prot	0.23			c0.30		0.24
v/s Ratio Perm				c0.29		
v/c Ratio	0.56			0.69		0.67
Uniform Delay, d1	20.2			2.7		24.1
Progression Factor	0.65			1.00		1.00
Incremental Delay, d2	1.5			0.9		3.1
Delay (s)	14.6			3.6		27.2
Level of Service	B			A		C
Approach Delay (s)	14.6			3.6	27.2	
Approach LOS	B			A	C	

Intersection Summary

HCM 2000 Control Delay	10.2	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.69		
Actuated Cycle Length (s)	90.0	Sum of lost time (s)	14.1
Intersection Capacity Utilization	69.8%	ICU Level of Service	C
Analysis Period (min)	15		
c Critical Lane Group			

Queues
3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr

Existing PM
01/29/2024



Lane Group	EBL2	EBL	EBT	EBR	WBL	WBT	WBR	WBR2	NBL2	NBL	NBT	NBR
Lane Configurations		↔	↕	↗	↖	↕		↗		↖	↕	↗
Traffic Volume (vph)	10	180	860	60	160	1190	10	100	170	0	90	200
Future Volume (vph)	10	180	860	60	160	1190	10	100	170	0	90	200
Peak Hour Factor	0.96	0.96	0.96	0.96	0.94	0.94	0.94	0.94	0.85	0.85	0.85	0.85
Heavy Vehicles (%)	2%	2%	2%	2%	1%	1%	1%	1%	0%	0%	0%	0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	0	198	896	63	170	1277	0	106	0	200	106	235
Turn Type	Prot	Prot	NA	Free	Prot	NA		Free	Split	Split	NA	pm+ov
Protected Phases	1	1	6		5	2			7	7	7	5
Permitted Phases				Free				Free				7
Detector Phase	1	1	6		5	2			7	7	7	5
Switch Phase												
Minimum Initial (s)	5.0	5.0	15.0		5.0	15.0			9.0	9.0	9.0	5.0
Minimum Split (s)	11.6	11.6	21.8		11.6	21.8			15.0	15.0	15.0	11.6
Total Split (s)	18.6	18.6	49.8		30.6	61.4			21.0	21.0	21.0	30.6
Total Split (%)	10.6%	10.6%	28.3%		17.4%	34.8%			11.9%	11.9%	11.9%	17.4%
Yellow Time (s)	3.0	3.0	4.5		3.0	4.5			4.0	4.0	4.0	3.0
All-Red Time (s)	3.6	3.6	2.3		3.6	2.3			2.0	2.0	2.0	3.6
Lost Time Adjust (s)		0.0	0.0		0.0	0.0				0.0	0.0	0.0
Total Lost Time (s)		6.6	6.8		6.6	6.8				6.0	6.0	6.6
Lead/Lag	Lead	Lead	Lag		Lead	Lag			Lead	Lead	Lead	Lead
Lead-Lag Optimize?	Yes	Yes	Yes		Yes	Yes			Yes	Yes	Yes	Yes
Recall Mode	None	None	Min		None	Min			None	None	None	None
v/c Ratio		1.38	0.72	0.04	0.77	0.93		0.07		1.03	0.47	0.46
Control Delay		256.5	44.1	0.1	79.5	52.1		0.1		130.3	65.8	10.0
Queue Delay		0.0	0.0	0.0	0.0	0.0		0.0		0.0	0.0	0.0
Total Delay		256.5	44.1	0.1	79.5	52.1		0.1		130.3	65.8	10.0
Queue Length 50th (ft)		~241	377	0	151	586		0		~196	92	30
Queue Length 95th (ft)		#405	486	0	226	#737		0		#330	147	65
Internal Link Dist (ft)			1683			692						428
Turn Bay Length (ft)		380		220	340			450		145		50
Base Capacity (vph)		143	1239	1672	292	1386		1492		195	226	573
Starvation Cap Reductn		0	0	0	0	0		0		0	0	0
Spillback Cap Reductn		0	0	0	0	0		0		0	0	0
Storage Cap Reductn		0	0	0	0	0		0		0	0	0
Reduced v/c Ratio		1.38	0.72	0.04	0.58	0.92		0.07		1.03	0.47	0.41

Intersection Summary

Cycle Length: 176.2

Actuated Cycle Length: 137.2

Natural Cycle: 145

Control Type: Actuated-Uncoordinated

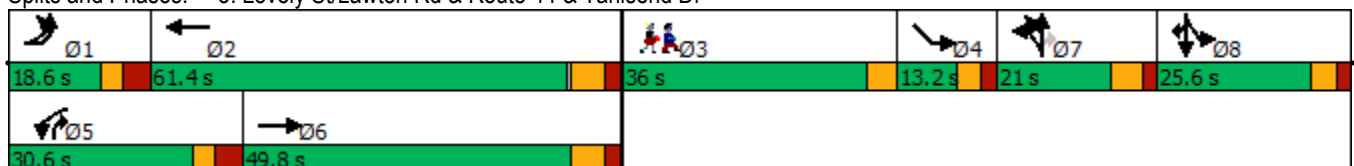
~ Volume exceeds capacity, queue is theoretically infinite.

Queue shown is maximum after two cycles.

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

Splits and Phases: 3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr



Queues
3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr

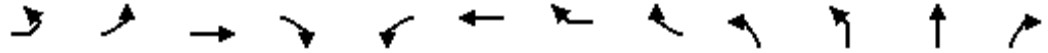
Existing PM
01/29/2024



Lane Group	SBL	SBT	SBR	SBR2	SEL	SER	SER2	Ø3
Lane Configurations		↕	↗		↘			
Traffic Volume (vph)	70	110	310	10	10	10	10	
Future Volume (vph)	70	110	310	10	10	10	10	
Peak Hour Factor	0.95	0.95	0.95	0.95	0.58	0.58	0.58	
Heavy Vehicles (%)	1%	1%	1%	1%	0%	0%	0%	
Shared Lane Traffic (%)								
Lane Group Flow (vph)	0	190	337	0	51	0	0	
Turn Type	Split	NA	Prot		Prot			
Protected Phases	8	8	8		4			3
Permitted Phases								
Detector Phase	8	8	8		4			
Switch Phase								
Minimum Initial (s)	9.0	9.0	9.0		6.0			1.0
Minimum Split (s)	14.6	14.6	14.6		11.2			36.0
Total Split (s)	25.6	25.6	25.6		13.2			36.0
Total Split (%)	14.5%	14.5%	14.5%		7.5%			20%
Yellow Time (s)	3.6	3.6	3.6		3.0			4.0
All-Red Time (s)	2.0	2.0	2.0		2.2			0.0
Lost Time Adjust (s)		0.0	0.0		0.0			
Total Lost Time (s)		5.6	5.6		5.2			
Lead/Lag	Lag	Lag	Lag		Lag			Lead
Lead-Lag Optimize?	Yes	Yes	Yes		Yes			Yes
Recall Mode	None	None	None		None			None
v/c Ratio		0.76	0.88		0.57			
Control Delay		76.9	82.6		88.7			
Queue Delay		0.0	0.0		0.0			
Total Delay		76.9	82.6		88.7			
Queue Length 50th (ft)		169	173		46			
Queue Length 95th (ft)		#289	#273		58			
Internal Link Dist (ft)		713			445			
Turn Bay Length (ft)			330					
Base Capacity (vph)		250	381		95			
Starvation Cap Reductn		0	0		0			
Spillback Cap Reductn		0	0		0			
Storage Cap Reductn		0	0		0			
Reduced v/c Ratio		0.76	0.88		0.54			
Intersection Summary								

HCM Signalized Intersection Capacity Analysis
 3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr

Existing PM
 01/29/2024



Movement	EBL2	EBL	EBT	EBR	WBL	WBT	WBR	WBR2	NBL2	NBL	NBT	NBR
Lane Configurations		↔	↕	↗	↖	↕		↗		↖	↕	↗
Traffic Volume (vph)	10	180	860	60	160	1190	10	100	170	0	90	200
Future Volume (vph)	10	180	860	60	160	1190	10	100	170	0	90	200
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	12	10	12	14	10	11	12	10	10	12	15	12
Grade (%)			2%			0%					2%	
Total Lost time (s)		6.6	6.8	4.0	6.6	6.8		4.0		6.0	6.0	6.6
Lane Util. Factor		1.00	0.95	1.00	1.00	0.95		1.00		1.00	1.00	1.00
Frt		1.00	1.00	0.85	1.00	1.00		0.85		1.00	1.00	0.85
Flt Protected		0.95	1.00	1.00	0.95	1.00		1.00		0.95	1.00	1.00
Satd. Flow (prot)		1635	3504	1672	1668	3451		1492		1787	2069	1599
Flt Permitted		0.95	1.00	1.00	0.95	1.00		1.00		0.95	1.00	1.00
Satd. Flow (perm)		1635	3504	1672	1668	3451		1492		1787	2069	1599
Peak-hour factor, PHF	0.96	0.96	0.96	0.96	0.94	0.94	0.94	0.94	0.85	0.85	0.85	0.85
Adj. Flow (vph)	10	188	896	62	170	1266	11	106	200	0	106	235
RTOR Reduction (vph)	0	0	0	0	0	0	0	0	0	0	0	132
Lane Group Flow (vph)	0	198	896	63	170	1277	0	106	0	200	106	103
Heavy Vehicles (%)	2%	2%	2%	2%	1%	1%	1%	1%	0%	0%	0%	0%
Turn Type	Prot	Prot	NA	Free	Prot	NA		Free	Split	Split	NA	pm+ov
Protected Phases	1	1	6		5	2			7	7	7	5
Permitted Phases				Free				Free				7
Actuated Green, G (s)		12.0	48.5	138.2	18.3	54.8		138.2		15.0	15.0	33.3
Effective Green, g (s)		12.0	48.5	138.2	18.3	54.8		138.2		15.0	15.0	33.3
Actuated g/C Ratio		0.09	0.35	1.00	0.13	0.40		1.00		0.11	0.11	0.24
Clearance Time (s)		6.6	6.8		6.6	6.8				6.0	6.0	6.6
Vehicle Extension (s)		2.0	3.0		2.0	3.0				2.0	2.0	2.0
Lane Grp Cap (vph)		141	1229	1672	220	1368		1492		193	224	385
v/s Ratio Prot		c0.12	0.26		0.10	c0.37				c0.11	0.05	0.04
v/s Ratio Perm				0.04				0.07				0.03
v/c Ratio		1.40	0.73	0.04	0.77	0.93		0.07		1.04	0.47	0.27
Uniform Delay, d1		63.1	39.1	0.0	57.9	40.0		0.0		61.6	57.9	42.6
Progression Factor		1.00	1.00	1.00	1.00	1.00		1.00		1.00	1.00	1.00
Incremental Delay, d2		218.8	2.2	0.0	14.2	11.8		0.1		74.6	0.6	0.1
Delay (s)		281.9	41.3	0.0	72.1	51.7		0.1		136.2	58.5	42.7
Level of Service		F	D	A	E	D		A		F	E	D
Approach Delay (s)			80.2			50.4					80.4	
Approach LOS			F			D					F	

Intersection Summary		
HCM 2000 Control Delay	67.6	HCM 2000 Level of Service E
HCM 2000 Volume to Capacity ratio	1.01	
Actuated Cycle Length (s)	138.2	Sum of lost time (s) 34.2
Intersection Capacity Utilization	85.3%	ICU Level of Service E
Analysis Period (min)	15	
c Critical Lane Group		

HCM Signalized Intersection Capacity Analysis
 3: Lovely St/Lawton Rd & Route 44 & Tarilsend Dr

Existing PM
 01/29/2024



Movement	SBL	SBT	SBR	SBR2	SEL	SER	SER2
Lane Configurations		↕	↗		↘		
Traffic Volume (vph)	70	110	310	10	10	10	10
Future Volume (vph)	70	110	310	10	10	10	10
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900
Lane Width	12	11	11	12	12	16	12
Grade (%)		8%			8%		
Total Lost time (s)		5.6	5.6		5.2		
Lane Util. Factor		1.00	0.88		1.00		
Frt		1.00	0.85		0.91		
Flt Protected		0.98	1.00		0.98		
Satd. Flow (prot)		1712	2612		1633		
Flt Permitted		0.98	1.00		0.98		
Satd. Flow (perm)		1712	2612		1633		
Peak-hour factor, PHF	0.95	0.95	0.95	0.95	0.58	0.58	0.58
Adj. Flow (vph)	74	116	326	11	17	17	17
RTOR Reduction (vph)	0	0	0	0	0	0	0
Lane Group Flow (vph)	0	190	337	0	51	0	0
Heavy Vehicles (%)	1%	1%	1%	1%	0%	0%	0%
Turn Type	Split	NA	Prot		Prot		
Protected Phases	8	8	8		4		
Permitted Phases							
Actuated Green, G (s)		20.0	20.0		6.2		
Effective Green, g (s)		20.0	20.0		6.2		
Actuated g/C Ratio		0.14	0.14		0.04		
Clearance Time (s)		5.6	5.6		5.2		
Vehicle Extension (s)		2.0	2.0		2.0		
Lane Grp Cap (vph)		247	378		73		
v/s Ratio Prot		0.11	c0.13		c0.03		
v/s Ratio Perm							
v/c Ratio		0.77	0.89		0.70		
Uniform Delay, d1		56.9	58.0		65.1		
Progression Factor		1.00	1.00		1.00		
Incremental Delay, d2		12.2	21.7		20.9		
Delay (s)		69.1	79.8		86.0		
Level of Service		E	E		F		
Approach Delay (s)		75.9			86.0		
Approach LOS		E			F		
Intersection Summary							

Queues
4: Route 44 & CVS Drive

Existing PM
01/29/2024

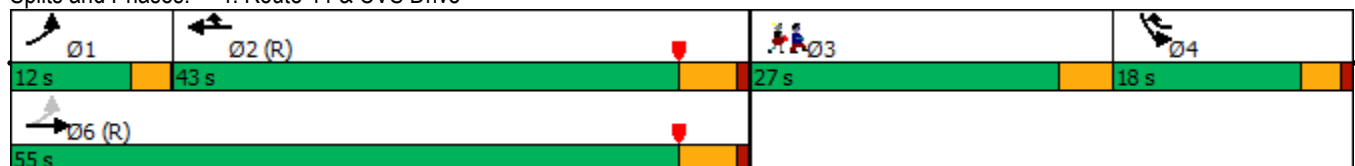


Lane Group	EBL	EBT	WBT	WBR	SBL	SBR	Ø3
Lane Configurations							
Traffic Volume (vph)	30	1100	1370	100	70	50	
Future Volume (vph)	30	1100	1370	100	70	50	
Peak Hour Factor	0.91	0.91	0.89	0.89	0.85	0.85	
Heavy Vehicles (%)	2%	2%	2%	2%	0%	0%	
Shared Lane Traffic (%)							
Lane Group Flow (vph)	33	1209	1539	112	141	0	
Turn Type	pm+pt	NA	NA	pt+ov	Prot		
Protected Phases	1	6	2	2 4	4		3
Permitted Phases	6						
Detector Phase	1	6	2	2 4	4		
Switch Phase							
Minimum Initial (s)	5.0	25.0	25.0		7.0		1.0
Minimum Split (s)	8.1	30.3	30.3		11.0		27.0
Total Split (s)	12.0	55.0	43.0		18.0		27.0
Total Split (%)	12.0%	55.0%	43.0%		18.0%		27%
Yellow Time (s)	3.0	4.3	4.3		3.0		4.0
All-Red Time (s)	0.1	1.0	1.0		1.0		0.0
Lost Time Adjust (s)	0.0	0.0	0.0		0.0		
Total Lost Time (s)	3.1	5.3	5.3		4.0		
Lead/Lag	Lead		Lag		Lag		Lead
Lead-Lag Optimize?	Yes		Yes		Yes		Yes
Recall Mode	None	C-Max	C-Max		None		None
v/c Ratio	0.15	0.45	0.61	0.09	0.45		
Control Delay	6.4	6.7	10.1	2.0	30.6		
Queue Delay	0.0	0.0	0.0	0.0	0.0		
Total Delay	6.4	6.7	10.1	2.0	30.6		
Queue Length 50th (ft)	2	70	172	1	26		
Queue Length 95th (ft)	22	344	#652	m27	50		
Internal Link Dist (ft)		692	628		200		
Turn Bay Length (ft)	155			140			
Base Capacity (vph)	275	2662	2512	1292	516		
Starvation Cap Reductn	0	0	0	0	0		
Spillback Cap Reductn	0	0	0	0	0		
Storage Cap Reductn	0	0	0	0	0		
Reduced v/c Ratio	0.12	0.45	0.61	0.09	0.27		

Intersection Summary

Cycle Length: 100
 Actuated Cycle Length: 100
 Offset: 20 (20%), Referenced to phase 2:WBT and 6:EBTL, Start of Yellow
 Natural Cycle: 90
 Control Type: Actuated-Coordinated
 # 95th percentile volume exceeds capacity, queue may be longer.
 Queue shown is maximum after two cycles.
 m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 4: Route 44 & CVS Drive



HCM Signalized Intersection Capacity Analysis

4: Route 44 & CVS Drive

Existing PM
01/29/2024



Movement	EBL	EBT	WBT	WBR	SBL	SBR
Lane Configurations	↖	↗↗	↗↗	↖	↘↘↘	
Traffic Volume (vph)	30	1100	1370	100	70	50
Future Volume (vph)	30	1100	1370	100	70	50
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900
Lane Width	10	12	12	10	12	12
Grade (%)		6%	4%		2%	
Total Lost time (s)	3.1	5.3	5.3	5.3	4.0	
Lane Util. Factor	1.00	0.95	0.95	1.00	0.97	
Frt	1.00	1.00	1.00	0.85	0.94	
Flt Protected	0.95	1.00	1.00	1.00	0.97	
Satd. Flow (prot)	1602	3433	3468	1448	3323	
Flt Permitted	0.11	1.00	1.00	1.00	0.97	
Satd. Flow (perm)	187	3433	3468	1448	3323	
Peak-hour factor, PHF	0.91	0.91	0.89	0.89	0.85	0.85
Adj. Flow (vph)	33	1209	1539	112	82	59
RTOR Reduction (vph)	0	0	0	16	54	0
Lane Group Flow (vph)	33	1209	1539	96	87	0
Heavy Vehicles (%)	2%	2%	2%	2%	0%	0%
Turn Type	pm+pt	NA	NA	pt+ov	Prot	
Protected Phases	1	6	2	2 4	4	
Permitted Phases	6					
Actuated Green, G (s)	74.4	74.4	68.1	75.8	7.7	
Effective Green, g (s)	74.4	74.4	68.1	75.8	7.7	
Actuated g/C Ratio	0.74	0.74	0.68	0.76	0.08	
Clearance Time (s)	3.1	5.3	5.3		4.0	
Vehicle Extension (s)	1.5	0.2	0.2		2.0	
Lane Grp Cap (vph)	184	2554	2361	1097	255	
v/s Ratio Prot	0.01	c0.35	c0.44	0.07	c0.03	
v/s Ratio Perm	0.13					
v/c Ratio	0.18	0.47	0.65	0.09	0.34	
Uniform Delay, d1	6.6	5.1	9.1	3.1	43.7	
Progression Factor	1.00	1.00	0.79	1.06	1.00	
Incremental Delay, d2	0.2	0.6	1.2	0.0	0.3	
Delay (s)	6.8	5.7	8.4	3.3	44.0	
Level of Service	A	A	A	A	D	
Approach Delay (s)		5.7	8.1		44.0	
Approach LOS		A	A		D	

Intersection Summary			
HCM 2000 Control Delay	8.8	HCM 2000 Level of Service	A
HCM 2000 Volume to Capacity ratio	0.58		
Actuated Cycle Length (s)	100.0	Sum of lost time (s)	16.4
Intersection Capacity Utilization	51.5%	ICU Level of Service	A
Analysis Period (min)	15		
c Critical Lane Group			

Queues

Existing PM

5: Shops at Farmington Valley/Private Driveway & Route 44

01/29/2024



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	5	900	270	230	1170	5	300	0	250	15	5	0
Future Volume (vph)	5	900	270	230	1170	5	300	0	250	15	5	0
Peak Hour Factor	0.94	0.94	0.94	0.94	0.94	0.94	0.90	0.90	0.90	0.59	0.59	0.59
Heavy Vehicles (%)	2%	2%	2%	1%	1%	1%	0%	0%	0%	0%	0%	0%
Shared Lane Traffic (%)							50%					
Lane Group Flow (vph)	5	957	287	245	1250	0	166	167	278	25	8	0
Turn Type	Prot	NA	Prot	Prot	NA		Split	NA	pt+ov	Split	NA	
Protected Phases	5	2	2	1	6		7	7	17	8	8	
Permitted Phases												
Detector Phase	5	2	2	1	6		7	7	17	8	8	
Switch Phase												
Minimum Initial (s)	5.0	15.0	15.0	5.0	15.0		5.0	5.0		7.0	7.0	
Minimum Split (s)	11.8	20.5	20.5	11.8	23.0		10.0	10.0		12.0	12.0	
Total Split (s)	30.0	36.0	36.0	30.0	36.0		20.0	20.0		14.0	14.0	
Total Split (%)	30.0%	36.0%	36.0%	30.0%	36.0%		20.0%	20.0%		14.0%	14.0%	
Yellow Time (s)	3.2	4.4	4.4	3.2	4.4		3.0	3.0		3.0	3.0	
All-Red Time (s)	3.6	1.1	1.1	3.6	1.1		2.0	2.0		2.0	2.0	
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0		0.0	0.0		0.0	0.0	
Total Lost Time (s)	6.8	5.5	5.5	6.8	5.5		5.0	5.0		5.0	5.0	
Lead/Lag	Lead	Lag	Lag	Lead	Lag		Lead	Lead		Lag	Lag	
Lead-Lag Optimize?	Yes	Yes	Yes	Yes	Yes		Yes	Yes		Yes	Yes	
Recall Mode	None	C-Min	C-Min	None	Min		None	None		None	None	
v/c Ratio	0.06	0.55	0.31	0.65	0.54		0.74	0.75	0.50	0.19	0.06	
Control Delay	53.8	15.5	2.4	61.2	7.4		62.0	62.4	10.1	47.3	44.0	
Queue Delay	0.0	0.0	0.0	0.0	0.0		0.0	0.0	0.0	0.0	0.0	
Total Delay	53.8	15.5	2.4	61.2	7.4		62.0	62.4	10.1	47.3	44.0	
Queue Length 50th (ft)	3	246	14	82	56		106	107	34	15	5	
Queue Length 95th (ft)	m7	105	14	127	179		#191	#193	80	27	13	
Internal Link Dist (ft)		628			773			306			149	
Turn Bay Length (ft)	50		220	370								
Base Capacity (vph)	394	1735	917	777	2300		255	255	722	162	171	
Starvation Cap Reductn	0	0	0	0	0		0	0	0	0	0	
Spillback Cap Reductn	0	0	0	0	0		0	0	0	0	0	
Storage Cap Reductn	0	0	0	0	0		0	0	0	0	0	
Reduced v/c Ratio	0.01	0.55	0.31	0.32	0.54		0.65	0.65	0.39	0.15	0.05	

Intersection Summary

Cycle Length: 100

Actuated Cycle Length: 100

Offset: 16 (16%), Referenced to phase 2:EBT, Start of Yellow

Natural Cycle: 70

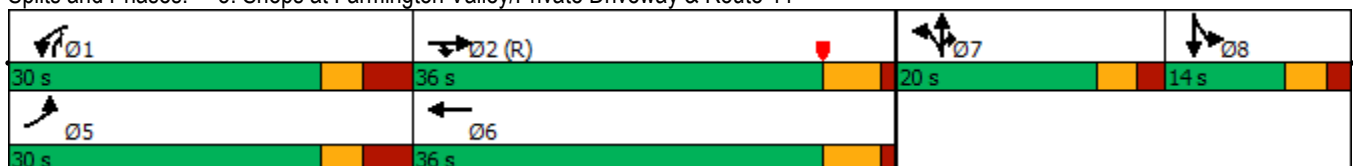
Control Type: Actuated-Coordinated

95th percentile volume exceeds capacity, queue may be longer.

Queue shown is maximum after two cycles.

m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 5: Shops at Farmington Valley/Private Driveway & Route 44



HCM Signalized Intersection Capacity Analysis

5: Shops at Farmington Valley/Private Driveway & Route 44

Existing PM
01/29/2024



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↘	↑↑	↗	↘↗	↑↑		↘	↗	↗	↘	↗	
Traffic Volume (vph)	5	900	270	230	1170	5	300	0	250	15	5	0
Future Volume (vph)	5	900	270	230	1170	5	300	0	250	15	5	0
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900
Lane Width	11	11	11	11	11	12	12	12	13	12	12	12
Grade (%)		1%			0%			1%			0%	
Total Lost time (s)	6.8	5.5	5.5	6.8	5.5		5.0	5.0	6.8	5.0	5.0	
Lane Util. Factor	1.00	0.95	1.00	0.97	0.95		0.95	0.95	1.00	1.00	1.00	
Frt	1.00	1.00	0.85	1.00	1.00		1.00	1.00	0.85	1.00	1.00	
Flt Protected	0.95	1.00	1.00	0.95	1.00		0.95	0.95	1.00	0.95	1.00	
Satd. Flow (prot)	1702	3404	1523	3351	3453		1706	1706	1660	1805	1900	
Flt Permitted	0.95	1.00	1.00	0.95	1.00		0.95	0.95	1.00	0.95	1.00	
Satd. Flow (perm)	1702	3404	1523	3351	3453		1706	1706	1660	1805	1900	
Peak-hour factor, PHF	0.94	0.94	0.94	0.94	0.94	0.94	0.90	0.90	0.90	0.59	0.59	0.59
Adj. Flow (vph)	5	957	287	245	1245	5	333	0	278	25	8	0
RTOR Reduction (vph)	0	0	146	0	0	0	0	0	141	0	0	0
Lane Group Flow (vph)	5	957	141	245	1250	0	166	167	137	25	8	0
Heavy Vehicles (%)	2%	2%	2%	1%	1%	1%	0%	0%	0%	0%	0%	0%
Turn Type	Prot	NA	Prot	Prot	NA		Split	NA	pt+ov	Split	NA	
Protected Phases	5	2	2	1	6		7	7	17	8	8	
Permitted Phases												
Actuated Green, G (s)	1.0	49.0	49.0	11.2	59.2		13.1	13.1	24.3	4.4	4.4	
Effective Green, g (s)	1.0	49.0	49.0	11.2	59.2		13.1	13.1	24.3	4.4	4.4	
Actuated g/C Ratio	0.01	0.49	0.49	0.11	0.59		0.13	0.13	0.24	0.04	0.04	
Clearance Time (s)	6.8	5.5	5.5	6.8	5.5		5.0	5.0		5.0	5.0	
Vehicle Extension (s)	1.5	2.0	2.0	1.5	2.0		2.0	2.0		2.0	2.0	
Lane Grp Cap (vph)	17	1667	746	375	2044		223	223	403	79	83	
v/s Ratio Prot	0.00	0.28	0.09	c0.07	c0.36		0.10	c0.10	0.08	c0.01	0.00	
v/s Ratio Perm												
v/c Ratio	0.29	0.57	0.19	0.65	0.61		0.74	0.75	0.34	0.32	0.10	
Uniform Delay, d1	49.1	18.1	14.3	42.5	13.0		41.8	41.9	31.2	46.3	45.9	
Progression Factor	1.16	0.74	0.65	1.29	0.58		1.00	1.00	1.00	1.00	1.00	
Incremental Delay, d2	3.2	1.3	0.5	2.5	0.3		11.1	11.4	0.2	0.8	0.2	
Delay (s)	60.2	14.7	9.8	57.3	7.9		53.0	53.2	31.4	47.2	46.1	
Level of Service	E	B	A	E	A		D	D	C	D	D	
Approach Delay (s)		13.8			16.0			43.2			46.9	
Approach LOS		B			B			D			D	

Intersection Summary

HCM 2000 Control Delay	20.4	HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio	0.65		
Actuated Cycle Length (s)	100.0	Sum of lost time (s)	22.3
Intersection Capacity Utilization	66.1%	ICU Level of Service	C
Analysis Period (min)	15		
c Critical Lane Group			

Queues

Existing PM

12: Secret Lake Road/Acura of Avon & Route 44

01/29/2024



Lane Group	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (vph)	10	1120	20	30	1350	10	30	0	30	20	0	20
Future Volume (vph)	10	1120	20	30	1350	10	30	0	30	20	0	20
Peak Hour Factor	0.94	0.94	0.94	0.90	0.90	0.90	0.63	0.63	0.63	0.73	0.73	0.73
Heavy Vehicles (%)	2%	2%	2%	1%	1%	1%	0%	0%	0%	0%	0%	0%
Shared Lane Traffic (%)												
Lane Group Flow (vph)	11	1212	0	33	1511	0	0	96	0	27	0	27
Turn Type	pm+pt	NA		pm+pt	NA		Split	NA		Prot		Prot
Protected Phases	1	6		5	2		7	7		8		8
Permitted Phases	6			2								
Detector Phase	1	6		5	2		7	7		8		8
Switch Phase												
Minimum Initial (s)	5.0	15.0		5.0	15.0		9.0	9.0		7.0		7.0
Minimum Split (s)	9.5	25.0		9.5	25.0		15.2	15.2		13.2		13.2
Total Split (s)	10.0	58.0		10.0	58.0		17.0	17.0		15.0		15.0
Total Split (%)	10.0%	58.0%		10.0%	58.0%		17.0%	17.0%		15.0%		15.0%
Yellow Time (s)	3.0	4.4		3.0	4.4		3.0	3.0		3.0		3.0
All-Red Time (s)	1.0	2.6		1.0	2.6		3.2	3.2		3.2		3.2
Lost Time Adjust (s)	0.0	0.0		0.0	0.0		0.0	0.0		0.0		0.0
Total Lost Time (s)	4.0	7.0		4.0	7.0		6.2	6.2		6.2		6.2
Lead/Lag	Lead	Lag		Lead	Lag		Lead	Lead		Lag		Lag
Lead-Lag Optimize?	Yes	Yes		Yes	Yes		Yes	Yes		Yes		Yes
Recall Mode	None	C-Min		None	C-Min		None	None		None		None
v/c Ratio	0.05	0.52		0.11	0.61		0.30	0.30		0.21		0.10
Control Delay	6.0	9.1		6.0	11.9		3.8	3.8		48.0		0.8
Queue Delay	0.0	0.0		0.0	0.0		0.0	0.0		0.0		0.0
Total Delay	6.0	9.1		6.0	11.9		3.8	3.8		48.0		0.8
Queue Length 50th (ft)	2	144		6	291		0	0		17		0
Queue Length 95th (ft)	m5	217		16	485		0	0		35		0
Internal Link Dist (ft)		773			254			313				450
Turn Bay Length (ft)	50			105								
Base Capacity (vph)	244	2320		308	2474		349	349		157		282
Starvation Cap Reductn	0	0		0	0		0	0		0		0
Spillback Cap Reductn	0	0		0	0		0	0		0		0
Storage Cap Reductn	0	0		0	0		0	0		0		0
Reduced v/c Ratio	0.05	0.52		0.11	0.61		0.28	0.28		0.17		0.10

Intersection Summary

Cycle Length: 100

Actuated Cycle Length: 100

Offset: 16 (16%), Referenced to phase 2:WBTL and 6:EBTL, Start of Yellow

Natural Cycle: 80

Control Type: Actuated-Coordinated




















m Volume for 95th percentile queue is metered by upstream signal.

Splits and Phases: 12: Secret Lake Road/Acura of Avon & Route 44



HCM Signalized Intersection Capacity Analysis
 12: Secret Lake Road/Acura of Avon & Route 44

Existing PM
 01/29/2024

													
Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR	
Lane Configurations													
Traffic Volume (vph)	10	1120	20	30	1350	10	30	0	30	20	0	20	
Future Volume (vph)	10	1120	20	30	1350	10	30	0	30	20	0	20	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	1900	
Lane Width	11	11	12	10	11	12	12	16	12	12	16	12	
Grade (%)		2%			2%			0%			2%		
Total Lost time (s)	4.0	7.0		4.0	7.0			6.2		6.2		6.2	
Lane Util. Factor	1.00	0.95		1.00	0.95			1.00		1.00		1.00	
Frt	1.00	1.00		1.00	1.00			0.93		1.00		0.85	
Flt Protected	0.95	1.00		0.95	1.00			0.98		0.95		1.00	
Satd. Flow (prot)	1694	3378		1651	3417			1959		1787		1599	
Flt Permitted	0.12	1.00		0.17	1.00			0.98		0.95		1.00	
Satd. Flow (perm)	210	3378		303	3417			1959		1787		1599	
Peak-hour factor, PHF	0.94	0.94	0.94	0.90	0.90	0.90	0.63	0.63	0.63	0.73	0.73	0.73	
Adj. Flow (vph)	11	1191	21	33	1500	11	48	0	48	27	0	27	
RTOR Reduction (vph)	0	1	0	0	0	0	0	89	0	0	0	26	
Lane Group Flow (vph)	11	1211	0	33	1511	0	0	7	0	27	0	1	
Heavy Vehicles (%)	2%	2%	2%	1%	1%	1%	0%	0%	0%	0%	0%	0%	
Turn Type	pm+pt	NA		pm+pt	NA		Split	NA		Prot		Prot	
Protected Phases	1	6		5	2		7	7		8		8	
Permitted Phases	6			2									
Actuated Green, G (s)	63.0	62.0		67.2	64.1			7.2		4.3		4.3	
Effective Green, g (s)	63.0	62.0		67.2	64.1			7.2		4.3		4.3	
Actuated g/C Ratio	0.63	0.62		0.67	0.64			0.07		0.04		0.04	
Clearance Time (s)	4.0	7.0		4.0	7.0			6.2		6.2		6.2	
Vehicle Extension (s)	1.5	2.5		1.5	2.5			1.5		1.5		1.5	
Lane Grp Cap (vph)	147	2094		245	2190			141		76		68	
v/s Ratio Prot	0.00	0.36		c0.00	c0.44			c0.00		c0.02		0.00	
v/s Ratio Perm	0.05			0.09									
v/c Ratio	0.07	0.58		0.13	0.69			0.05		0.36		0.02	
Uniform Delay, d1	8.8	11.3		6.9	11.6			43.2		46.5		45.8	
Progression Factor	1.03	0.71		1.00	1.00			1.00		1.00		1.00	
Incremental Delay, d2	0.1	1.0		0.1	1.8			0.1		1.0		0.0	
Delay (s)	9.1	9.1		7.0	13.4			43.3		47.5		45.9	
Level of Service	A	A		A	B			D		D		D	
Approach Delay (s)		9.1			13.2			43.3			46.7		
Approach LOS		A			B			D			D		
Intersection Summary													
HCM 2000 Control Delay			13.1									HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio			0.61										
Actuated Cycle Length (s)			100.0									Sum of lost time (s)	23.4
Intersection Capacity Utilization			67.1%									ICU Level of Service	C
Analysis Period (min)			15										
c Critical Lane Group													

F Meeting Notes

**Route 44 Corridor Study, Canton
Study Kick-off Meeting
June 12, 2023, 12:00pm
Draft Meeting Notes
Virtual Meeting via Microsoft Teams**

On June 12, 2023, the Route 44 Corridor Study (the Study) Kick-off Meeting was held virtually via Microsoft Teams with Canton representatives, CRCOG staff, CTDOT project team and the study Consultant, VHB and VN Engineers. This kickoff meeting was held to the Study, introduce the TAC committee members, the CTDOT representatives, and the project consultant team, review the project, review the scope of work, schedule, solicit comments and discuss next steps. A list of meeting attendees are included for reference.

Discussion Items

1. The meeting began with Pramod Pandey (CRCOG) introducing the study to the meeting attendees, with a brief overview of the study. Neil Pade, Town of Canton provided some background to the US-44 Corridor Study Project, followed by introduction of the other attendees, followed by a presentation by VHB and discussion.
 - a. Neil commented that US-44 serves as a major connector to Hartford. Recently Canton's form-based code has permitted the development of various higher-density parcels along the study's area. In addition, the Farmington River Trail Extension project to the south of Dowd Avenue is closely connected to this study. A consultant will be selected to design this shortly, with construction of the initial phase to be completed later this year or next year. It will extend the Farmington River Trail to the Shops at Farmington Valley with plans to continue it further east to Simsbury. There was a general comment about the need for signalization analysis along this corridor.
 - b. VHB then presented an overview of the project approach explaining corridor characteristics, traffic simulation, crash analysis, public involvement, and coordination with CTDOT, CRCOG and the Town of Canton.
2. An open discussion was then held to gain insight and feedback from the attendees about the issues facing the community. The following list details the discussion and a general summary of the discussion:
 - a. Neil Pade (Town of Canton):
 - i. Safety is paramount.
 - ii. Safe access must be ensured for all users, the needs of non-motorized users must be in balance with the need to move freight, motorized users, and transit users.
 - iii. Outreach needs to include CTTransit riders.
 - b. Jan Tanner (Benidorm Bikes, Bike Walk Canton):
 - i. There are major safety concerns along the Corridor.
 - ii. Not safe for cyclists.

- iii. The sidewalk along the corridor is deficient.
 - iv. Accessibility to shops must accommodate bikers and walkers.
 - v. Excessive Speeds all along corridor are dangerous.
 - vi. Requests the Project team analyze US-44 west to CT 179 intersection beyond the current parameters.
 - c. Fred Kulakowski (CTDOT):
 - i. Request for study parameters, using mile posts.
 - d. Tim Bowman (Canton Resident, Bike Walk Canton)
 - i. The signal at the intersection of Secret Lake Road and Mitchell Subaru are not synced, meaning that the side street approaches run separately. The suggestion was to review the potential realignment of approaches. The consultant should look at this during the study.
 - e. Bill Volovski (Board of Selectman):
 - i. Public outreach must be extended to the residents along Dowd Avenue at the four senior centers, two of which are within the project study area, and the other two extend beyond the area along Dowd Avenue.
 - ii. Speed is a major concern and must be incorporated into analysis.
 - iii. Affordable Housing units on Dowd Avenue should be consulted.
 - iv. Major non-profit employer along Dowd Avenue.
 - f. Pramod Pandey (CRCOG) explained to the meeting attendees about the tentative meeting schedule. The Technical Advisory Committee (TAC) meetings will be held during business hours, preferably between 12-2pm. Public information meetings will be scheduled during evenings and weekends starting in late September or October to ensure accessibility. Traffic counts were conducted last week to ensure school-generated traffic was included in the data set. There was a general suggestion that the consultant wait until Fall, 2023 to conduct outreach to include more people traditionally away during the summer months.
 - g. The group then discussed recent developments in the corridor.
 - i. Property at Southside of US-44 at 55 Albany Turnpike has a high-density residential proposal which is before the Planning and Zoning Commission for review.
 - ii. At 5 / 9 Albany Turnpike, another residential development is being reviewed by Planning and Zoning.
 - iii. A second access point was made to the Shops at Farmington Valley along Secret Lake Road
 - a. Sidenote that the Trail to rear of Shops site is component of Farmington River Trail Study.
 - iv. The Shoppe at Canton Village on Dowd Avenue
 - a. A 55-unit residential site is in the process of approval for development.
 - b. A 6-unit site on Dowd Avenue is also in the process of approval.
3. Pramod thanked the attendees for their support and involvement in the study and asked to contact [him](#) and/or [Joe Balskus](#) (VHB) if they have any questions and suggestions and mentioned that the meeting materials will be posted on the study webpage that will be hosted by CRCOG.

**Route 44 Corridor Study, Canton
Study Kick-off Meeting
June 12, 2023, 12:00pm
Virtual Meeting via Microsoft Teams
Meeting Attendees**

Name	Organization	Email
Cara Radzins	CRCOG	cradzins@crcog.org
Pramod Pandey	CRCOG	ppandey@crcog.org
Jacob Knowlton	CRCOG	jknowlton@crcog.org
Sotoria Montanari	CRCOG	smontanari@crcog.org
Neil Pade	Town of Canton	npade@townofcantonct.org
Robert Martin	Town of Canton	rmartin@townofcantonct.org
Melanie Carr	Shops at Farmington Valley	Melanie.Carr@wsdevelopment.com
Bill Volovski	Canton Board of Selectman	billvolovski@yahoo.com
Jan Tanner	Bike Walk Canton/Benidorm	jan@benidormbikes.com
Lisa Coggins	Bike Walk Canton	lc2w2kids@gmail.com
Fred Kulakowski	CTDOT	frederick.kulakowski@ct.gov
Caswell Smith	CTDOT	caswell.smith@ct.gov
Tim Bowman	Bike Walk Canton	tim.bowman@legrand.com
Grayson Wright	CTDOT	grayson.wright@ct.gov
Matthew Tyskinski	CTDOT	matthew.tyskinski@ct.gov
Will Britnell	VHB	wbritnell@vhb.com
Charlie Baker	VHB	charlesbaker@vhb.com
Dan Amstutz	VHB	damstutz@vhb.com
Eric Tang	VHB	etang@vhb.com
Joe Balskus	VHB	Jbalskus@VHB.com>
Mark Jewell	VHB	mjewell@vhb.com
Bridget Moriarty	VN	bmoriarty@vnengineers.com
David Evens		Canton Resident

**Route 44 Corridor Study, Canton
Technical Advisory Committee Meeting #2
October 16, 2023, 12:00pm
Draft Meeting Notes
Virtual Meeting via Microsoft Teams**

On October 13, 2023, the Route 44 Corridor Study (the Study) TAC Meeting #2 was held virtually via Microsoft Teams with Canton representatives, CRCOG staff, CTDOT project team and the study Consultant, VHB and VN Engineers. The purpose of the meeting was to go over existing conditions information collected by the Consultant Team to date, solicit comments and discuss next steps. A list of meeting attendees are included for reference. The TAC also discussed the first Public Information Meeting for the project which is scheduled for December 7 at the Canton Community Center.

Discussion Items

The meeting began with Pramod Pandey (CRCOG) welcoming the meeting attendees, with a brief overview of the study followed by attendees introducing themselves. Pandey shared the kick-off meeting notes and said that future meeting notes would be sent around for TAC review and approval at the TAC meetings.

1. Welcome and Introduction

Pramod thanked the attendees for their support and involvement in the study and asked to contact him and/or Joe Balskus (VHB) if they have any questions and suggestions and mentioned that the meeting materials will be posted on the study webpage that will be hosted by CRCOG.

2. Public Comments

Pandey asked if any attendees wanted to make public comments.

• Lisa Coggins:

- Near Route 44 and Dowd Ave is the FOCUS Center for Autism Fresh Start School. There is another building for the FOCUS Center for Autism on Route 44.

- There is missing sidewalk between the two buildings. They should be contacted for public feedback on the project.
- Coggins also asked if the meeting could be recorded. Pandey noted they will be recorded in the future.
 - Bill Volovski:
 - The public survey for the project needs to be more widely distributed.
 - Neil Pade noted that he circulated the public survey this morning to various Town contacts.
3. Existing Conditions Presentation
- VHB presented on existing conditions in the US-44/Dowd Ave corridor, including traffic counts, capacity analysis, safety data, bicycle and pedestrian observations and data, public transportation data, public outreach to date, and future public outreach including stakeholder interviews and the first public meeting. Additional information will be forthcoming with the development of the existing conditions technical memo.
4. Discussion
- i. Neil Pade (Town of Canton):
 - a. The Town is interested in determining the best location for a traffic signal on the east end of the corridor by the Simsbury Line. This would assist with proposed developments and also provide a crossing for the extension of the Farmington River Trail.
 - b. The Town has decided to hold the public meeting on December 7, to dovetail with a pedestrian safety committee that has recently been created. A time is to be determined.
 - ii. Bill Volovski (Board of Selectman)
 - a. More information needs to be shown about bicycle/pedestrian traffic on the Farmington River Trail. There should be a trail counter to collect data from.
 - b. Neil Pade noted he would forward this information to the VHB Team.
 - iii. Jan Tanner (Benidorm Bikes, Bike Walk Canton):
 - a. Explain the assessment 20 years into the future? Balskus explained that the VHB Team will look at things like traffic growth 20 years into the future, and future potential development. This is typical of corridor studies.

- b. What about development west of this corridor, and the Route 179/US-44 interchange? Can this be incorporated? Pandey said it could be a recommendation in the current study to follow-up with another study to look at this part of the town.
- c. Can the corridor be converted to have bike lanes? Is that possible? Caswell Smith from CTDOT explained that recent changes in CTDOT policy will require the department to include bicycle and pedestrian improvements in projects. Balskus noted that future concepts will need to include bicycle and pedestrian elements to them.
- d. Tanner requested to be interviewed as part of the stakeholder interviews.

5. Trail Project Status

Neil updated the group about the status of the trail project in the study area.

- a. The trail design project for the extension of the Farmington River Trail will lag behind the Corridor Study.
- b. Pade showed preliminary design concepts and noted constrained areas, such as on Route 44 near the intersection with Secret Lake Road, and near the rocky areas by the Simsbury border.
- c. The current plan is to keep the trail on the south side of US-44 until more information about a crossing is recommended.

6. Next Steps

- a. The VHB Team will send around the Existing Conditions Technical Memo in the next couple of weeks for review before the public meeting. Pandey said the goal was to finalize the memo by the time of the public meeting.
- b. The VHB Team will conduct stakeholder interviews over the next several weeks to gather feedback about the project and transportation issues in the area. Jan Tanner suggested adding the Martel Bus Company and the Canton Board of Education to the stakeholder list.
- c. Public Meeting #1: This meeting will be held on December 7 and will go over existing conditions and include a presentation, handouts, and a Q&A portion. The VHB Team will provide meeting flyer to The Town to send out to the public for this meeting. The meeting will likely be in the large meeting room in the Community Center. CRCOG and the VHB Team coordinate with the Town on the exact evening timing of the meeting.

Route 44 Corridor Study, Canton
Technical Advisory Committee Meeting #2
October 13, 2023, 12:00pm
Draft Meeting Notes
Virtual Meeting via Microsoft Teams

<u>NAME</u>	<u>ORGANIZATION</u>	<u>E-Mail</u>
Pramod Pandey	CRCOG	ppandey@crcog.org
Jacob Knowlton	CRCOG	jknowlton@crcog.org
Sotoria Montanari	CRCOG	smontanari@crcog.org
Neil Pade	Town of Canton	npade@townofcantonct.org
Robert Martin	Town of Canton	rmartin@townofcantonct.org
Melanie Carr	Shops at Farmington Valley	Melanie.Carr@wsdevelopment.com
Bill Volovski	Canton Board of Selectman	billvolovski@yahoo.com
Jan Tanner	Bike Walk Canton/Benidorm	jan@benidormbikes.com
Lisa Coggins	Bike Walk Canton	lc2w2kids@gmail.com
Fred Kulakowski	CTDOT	frederick.kulakowski@ct.gov
Caswell Smith	CTDOT	caswell.smith@ct.gov
Tim Bowman	Bike Walk Canton	tim.bowman@legrand.com
Grayson Wright	CTDOT	grayson.wright@ct.gov
Obesebea Aye-Addo	CTDOT	Obesebea.Aye-Addo@ct.gov
Matthew Tyksinski	CTDOT	Matthew.Tyksinski@ct.gov
Will Britnell	VHB	wbritnell@vhb.com
Charlie Baker	VHB	charlesbaker@vhb.com
Dan Amstutz	VHB	damstutz@vhb.com
Joe Balskus	VHB	Jbalskus@VHB.com
Mark Jewell	VHB	mjewell@vhb.com
Bridget Moriarty	VN Engineers	bmoriarty@vnengineers.com



Meeting Notes

Date: Thursday, December 7, 2023
6:00 pm – 7:30 pm

Place: Canton Community Center
Meeting Room F
40 Dyer Ave
Canton, CT 06019

Re: CRCOG Route 44 Corridor Study, Canton
Public Information Meeting #1

Project No.: 43281.00

- › This meeting was conducted as a hybrid meeting with an in-person component and a virtual component. The in-person meeting took place at the Community Center in Canton, CT, while the on-line meeting was conducted simultaneously on the Town of Canton's Zoom Meeting account. Approximately 55 people attended the in-person meeting, including staff from the Town of Canton, the Capitol Region Council of Governments (CRCOG), CTDOT, VHB and VN Engineers. Twenty people attended the online meeting. Audio and video of the meeting was recorded using Zoom. The recording of the meeting can be found at the project website: <https://crocog.org/rt44/>
- › Pramod Pandey, Principal Planner II at CRCOG, opened the meeting, introduced himself, and thanked all attendees for coming. He gave a brief overview of the Route 44 Corridor Study and the purpose of the meeting. He noted that staff from the Town of Canton and CTDOT were at the meeting as well.
- › Joe Balskus, Director of Transportation Systems with VHB, provided an introduction to the corridor study and introduced other staff from VHB and VN Engineers, the public outreach consultant. The meeting is on a tight timeframe to complete by 7:30, when another meeting will start. The public meeting will go over the existing conditions for the study area and have a Q&A session at the end of the meeting, then an opportunity to review maps and provide comments.
- › At 7:30, the Traffic and Pedestrian Safety Advisory Committee will have their meeting. The Committee advises the Board of Selectmen about Canton's traffic and transportation needs. The purpose of the committee is to make recommendations to improve the safety and effectiveness of all modes of travel within Canton, including roadways, bikeways and walkways.
- › Balskus briefly discussed the Route 44 Corridor history and noted that a previous Corridor Study was completed in 2000. The goal was to address safety and access issues particularly on the eastern side of Canton; a center median was proposed. The Corridor Study also covered other towns along Route 44. US 44 is part of the National Highway System, which can impact what changes can be made to the road and makes it more important as a through-route for traffic. Additionally, traffic volumes on the road peaked at 31,000 vehicles per day before the Great Recession in 2008 and have fallen to about 26,000 vehicles per day as of 2023.
- › The study corridor limits are US-44 from the Simsbury Town Line to approximately Old Canton Road (near the Canton Village Shops) and Dowd Avenue from US-44 to Canton Hollow. These limits have been set by the Town and CRCOG. Route 44 itself is a high-volume, high-speed arterial road with substantial through traffic that connects to Hartford in the east and points west in Connecticut. There has been significant new development on the roadway in recent years and it is dominated by automobile traffic.

- › The goals of the study are to review the corridor and develop recommendations to create a multimodal transportation corridor with transportation options for residents and visitors. Opportunities have been created by recent development to make the corridor more walkable and new development more compact to make walking easier. The Town has also created new Design Village Districts to support this and improve access to alternative modes. The Farmington River Trail runs through the corridor and is planned to be extended east by the Town. As part of the study, there may be potential changes identified for Route 44 for the next 20 years.
- › Baskus went over the Corridor Study process, which includes collecting data on existing conditions, assessing the existing conditions, looking at future conditions, identifying and analyzing alternatives, developing concepts, and creating a Transportation Improvement and Development Management Plan. Public outreach is included throughout the process. We are currently in the existing conditions for the project to understand what deficiencies exist.
- › Baskus discussed the traffic data collected.
 - Observations of traffic found that there are high speeds compared to posted speeds and weaving movements along Route 44; there are issues with Dowd Avenue from queuing to get onto Route 44 and with turning left onto Dowd Ave from Route 44; and the Route 177/Lawton Road intersection has recurring congestion and backups.
 - Traffic volumes for Route 44 area about 26,000 vehicles per day in both directions, which translates to about 2,000 vehicles during each peak hour (peak hours being about 8-10% of total daily traffic). Dowd Avenue carries about 8,600 vehicles per day, and for both facilities Tuesday has the highest volume with traffic declining through the remainder of the week.
 - Part of the traffic analysis includes looking at the various data points of traffic volumes and speed using computer models to understand the capacity of the intersections and roadways. These are standards set by CTDOT and CRCOG as well as national standards. Capacity is looked at in various ways, including volume to capacity, levels of service (graded at A to F), and queuing. From the data collected on Route 44, levels of service range from A to E, with Lawton Road/Lovely Street having the worst level of service at D and E grades. That is the worst intersection in the corridor. Some of the intersection movements are at level of service F and are over capacity.
- › Daniel Amstutz, Senior Transportation Planner at VHB, went over crash data collected for the study.
 - Crash data was collected through the Connecticut Crash Data Repository at UConn, which collects and tabulates crash reports from around the state. For the January 2018-July 2023 period there were 456 crashes in the study area, with 39% at key intersections, 21% angle crashes and 46% rear end crashes. 79 crashes were injury crashes, most of which (44%) were angle crashes. One fatality occurred in the study area on Route 44 west of Canton Valley Circle, which was a pedestrian crossing the road.
 - The highest crash density (where most crashes occur) is at Route 44/Lawton Road/Lovely Street with 70 crashes, and the second highest is at Route 44/Dowd Avenue with 50 crashes. However, the most injury crashes occurred at Route 44/Dowd Avenue and Dowd Avenue and Canton Springs Road.
 - There were 2 pedestrian crashes during the January 2018-July 2023 period. As noted earlier, one of these was a fatality. No bicycle crashes were reported in this period.
- › Amstutz discussed pedestrians, bicycles, and trails.

- The lowest pedestrian activity was seen on Route 44, where there are limited sidewalks in many places and issues of sidewalk maintenance, and the highest pedestrian activity was in the Shops at Farmington Valley. Flashing beacons help assist pedestrians to cross Dowd Avenue. In the Canton Village Shops, there is no defined pedestrian circulation.
 - Maps of pedestrian infrastructure including sidewalks and crosswalks were displayed.
 - For bicycling, some bike parking was found in the study area, and the Farmington River Trail is the primary bicycle accommodation. Part of the on-road portion of the trail north on Lawton Road is within the CTDOT On-Road Bicycle Planning Network that was developed in the recent CTDOT Active Transportation Plan. The Plan also developed a bicycle suitability map for understanding which state roads are more or less suitable for biking. In general, Route 44 has low suitability for bicycling, while Dowd Avenue is more suitable for biking.
 - As noted before, the Farmington River Trail passes through the study area and connects to the Farmington Canal Heritage Trail as a key spine of the bicycle and pedestrian access to the area. The Town is planning an extension of the trail from the intersection of Route 44/Lovely Street/Lawton Road through the Shops at Farmington Valley and to continue east. The first phase of this section is under design. For this Corridor Study, the Study Team will need to look at a potential crossing of Route 44 by the trail near the Simsbury Town Line.
- › Amstutz went over information about public transportation conditions.
- Three express bus routes travel through Canton: the 901, 926, and 927, as well as a combined 926/927 in the mid-day. The service runs only on weekdays and has 9 round trips per day. There are three bus stop pairs along Route 44 where the buses, when making local service runs, will stop. Generally, they all stop at the Canton Park and Ride farther west along Route 44, outside the project area. In addition, there are three school bus routes in the study area for the Cherrybrook Elementary School, Canton Intermediate School, and Canton High School.
 - Transit usage is generally low, with about 2,500 rides per month for the three express routes. Prior to the pandemic, Route 901 was seeing over 140 riders per day; this dipped significantly during the pandemic and has not gotten back to its previous high. Routes 926 and 927 also had decreased ridership during the pandemic, but have gotten slightly higher ridership than pre-pandemic, with about 37 daily riders in 2023 compared to 18 and 26 riders per day pre-pandemic, respectively.
- › Amstutz briefly went over information about land use.
- Route 44 primarily has commercial development in the study area. Some is new construction while others are former residential properties converted to commercial use. Residential development is confined to the side streets, except on Dowd Avenue. More development is expected and planned in the study area over the coming years, and the desire is to keep this development walkable and support more compact site designs.
 - Amstutz showed a land use map of the study area and a zoning map. Many properties in the study area are under different Design Village Districts to keep them walkable and compact, including the East Gateway Design Village District, Canton Village Design Village District, and the Applegate Design Village District.
- › Bridget Moriarty, from VN Engineers, went over public engagement for the project so far.
- Public engagement in many forms is being conducted for this study. A pop-up meeting at the Collinsville Farmer's Market was done on October 1, a public survey has been circulated, stakeholder meetings have been conducted, and there is the public meeting happening this evening.

- Stakeholder interviews with various key businesses and organizations were done to understand their concerns and ideas about the study area. Stakeholders included the Shops at Farmington Valley, the Avon/Canton Chamber of Commerce, Canton Village Shops, Mitchell Subaru, Bike Walk Canton, transit users, Favarh – the Arc of Farmington Valley, Farmington Valley Trails Council, Shoprite of Canton, Canton Senior and Social Services, and Benidorm Bikes. Additionally, CRCOG staff visited 41 businesses along the corridor.
 - Stakeholders noted that 97% or more of their workers and clients/customers arrive by private car. Others may use a shuttle, dial a ride, or bike and walk.
 - Many concerns were shared by the stakeholders about the corridor, including: speeding, safety, signal coordination, access management, illumination, sidewalk connectivity, wayfinding, traffic delays, connection to the Farmington River Rail Trail, insufficient places for left turns, congestion, safety for people biking and walking as well as crossings, and possible construction impacts on mobility and its effect on businesses.
 - The public survey has received 838 responses as of November 30. About 80% of responses have come from people in Canton, while the remaining 20% are from Avon, Simsbury, and other nearby towns. Almost half of respondents (43%) note that biking and walking safety and comfort is poor or very poor for both questions. Traffic concerns include high speeds, congestion, unsafe driver behavior, and long signal wait times. Improvements that people suggested most include better bicycle and pedestrian facilities, safer drivers, and traffic calming. Finally, improvements for non-motorized users people suggested include extending the trail, improving crossings, improving bike facilities, slowing traffic, and improving pedestrian facilities.
- › At the end of the presentation was a Q & A session for attendees. Questions and comments made during the in-person Q&A included the following:
- A suggestion was made to create a center left turn lane to make left turns safer in the corridor. An example from Bradenton, Florida was provided. In addition, stop bars for vehicles in the left turn lanes should be moved back to help trucks to make wide turns without cutting corners.
 - A commenter stated that the study does not pay enough attention to residences on Dowd Avenue or address issues that affect them, such as speeding, pollution, and road rage issues of drivers. They further stated that Dowd Avenue/State Route 565 is not suitable for heavy truck traffic and should not be considered a highway. Most of the corridor is not suitable for bicycling. The vision of the plan is outdated because it is based on an old Plan of Conservation and Development and there are concerns about the Town's form-based code. A roundabout should be considered Route 44 and Route 179/Lovely Street and Lawton Road.
 - Is there a way to see the goals and what was implemented in the Route 44 study from 2000, and what previous recommendations were not implemented? At what point will decisions get made on the Corridor Study? Also, it would be preferable for the study to include parts of Route 44 further to the west.
 - Pramod Pandey noted that conditions may have changed after the previous study from 2000 to make certain recommendations infeasible, which is why they were not implemented. Staff will go back and look at this study and more information about the older study will be included in the new study's report.
 - Does being a national highway mean that Route 44 has limits on construction/countermeasures? Is it possible to put bridges over Route 44 for crossing it? Will bus stops be protected and have sidewalk accessibility as part of the recommendations?

- Pandey noted that CRCOG visited many businesses as part of their investigation and had a hard time finding people who ride the bus. The next step is to talk with people at the bus stops to understand their commute patterns. The recommendations will look at bus and access improvements in the corridor.
 - Balskus explained that the Federal Highway Administration will need to be involved with changes to the corridor. USDOT is using a safe systems approach to transportation that has been embraced by CTDOT. He can't say if a bridge would be approved, but things are evolving at the state and federal level that is changing how they look at possible projects.
 - There is a need to protect the character of Canton. The Canton Planning & Zoning Commission is actively encouraging multi family development, and this will lead to more traffic. It's important to also keep trees along the road corridor to keep the small town character of Canton. In addition, it's difficult to cross Route 44 as a pedestrian.
 - The bus stops are not in places suitable for people who go to work in Hartford. They may have to cross the street after being let off in places where it's hard to cross.
 - There are many crashes caused by people turning left into commercial areas off Route 44, especially the Dunkin' Donuts west of Lovely Street. A center turn lane may help.
 - The traffic light cycles are too long, and do not seem synchronized. In the early morning hours, they are no longer blinking like they used to. It is confusing and seems dangerous, and people don't know what to do.
 - Planted medians would help with traffic calming. Please don't make the roads wider, as it will make people drive faster. The traffic signal at Route 44 and Lovely Street is OK for a pedestrian to cross but it's very slow to change for the pedestrian signal. Sidewalks on Dowd Avenue are in need of repair but are OK for most people.
 - Balskus noted that planted medians were put on Route 44 in front of the Shops at Farmington Valley, and that is from the 2000 plan. Whether medians will be recommended for this study remains to be seen.
 - The bike trail is great. Speed bumps are needed on side roads like Dowd Avenue to slow people down. Do tractor trailers really need to go down Route 44?
 - Balskus explained that speed humps could potentially be used on Dowd Avenue but not likely on Route 44 due to the high speeds. Prevailing speeds limit locations where speed humps can be placed.
 - Will there be some kind of gap analysis of what people are doing and what they want to do in terms of travel?
 - Moriarty noted that the public survey does ask questions about suggestions like this.
 - Traffic signal coordination is very important to help address traffic congestion at signals. There is a bridge over Route 6 near Tunxis Community College for the Farmington Canal Heritage Trail that seems to work well – bridges should be considered. Tunnels for trails could also be considered if they are not too long and dark. Snow plows may have issues getting around roundabouts.
 - Please do not let Route 44 look like Route 2 outside Boston, with jersey barriers down the middle and no place to turn.
- › Questions were also asked by online attendees during the Q&A session. They were:
- There was data collection on vehicle traffic, was there any data collection on bicycle and pedestrian traffic? Is there a breakdown of those modes available? Also, will the slides be available?

- Answer: yes, data was collected on all modes. A breakdown of modes will be included in the final report.
- The meeting slides will be made available.
- The lady speaking is making my point. We need center turn lanes. I grew up in Detroit and center turn lanes are very common and definitely help with traffic flow and safety.
 - Answer: the two way center left turn lanes can operate really efficiently in certain circumstances.
- Could you please share a link to the website?
 - Answer: Crcog.org/rt44
- Will the corridor study team make a point of walking and biking the study area to gain insight from both a pedestrian and bicyclist perspective?
 - Answer: we will discuss this with the Team. I will say that I have run alot in Collinsville and our team has several avid bicyclists, so we understand being vulnerable users. It's very scary at times. Thanks for suggestion and participating.
- Are there any successful communities similar to ours that we can visit and use as a model?
 - Answer: email Pramod Pandey at CRCOG for this question. His email is ppandey@crcog.org. Great idea!
- Can traffic lights be timed so that cars driving the speed limit don't have to stop every 50 feet?
 - Answer: the timing of traffic signals so that they move vehicles efficiently through intersections is something that will be reviewed as part of the study. Thank you!
- › Two written public comment forms were submitted at the end of the meeting. The comments were:
 - My concern is that Route 44 traffic will be diverted down Route 565 – my solution is for the traffic to travel Route 44 to Route 179. No through trucks on Route 565.
 - CT Transit needs to provide more trips west in the day and more trips east. They only have service for a 9-5, traditional work schedule.
- › After the Q&A session, in-person attendees were able to review maps of the project study area and ask additional questions or give comments.
- › After the Q&A, attendees were invited to talk to the Study Team and make comments on maps of the study area around the room.
- › The meeting was adjourned at 7:30 pm.

G Public Survey

Q1 What town or city do you live in?

Answered: 869 Skipped: 0

US-44 Corridor Study - Canton Public Survey

What town or city do you live in?

Answered **869**
Skipped **0**

Respondent ID	Response Date	Responses	Tags
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114452956256	Nov 02 2023 01:08 PM	Canton
114452967598	Nov 02 2023 01:05 PM	Canton
114452964101	Nov 02 2023 01:03 PM	Canton
114452961431	Nov 02 2023 01:02 PM	canton
114452962422	Nov 02 2023 01:00 PM	Canton
114452961539	Nov 02 2023 12:59 PM	Canton
114452942614	Nov 02 2023 12:57 PM	Canton
114452959280	Nov 02 2023 12:56 PM	Collinsville

114452957233	Nov 02 2023 12:56 PM	Canton
114452950981	Nov 02 2023 12:53 PM	Canton
114452952199	Nov 02 2023 12:52 PM	colinsville
114452942633	Nov 02 2023 12:43 PM	Canton
114452921121	Nov 02 2023 12:38 PM	canton
114452908663	Nov 02 2023 12:37 PM	Canton
114452931971	Nov 02 2023 12:35 PM	Canton
114452914982	Nov 02 2023 12:35 PM	Canton
114452933085	Nov 02 2023 12:34 PM	Canton
114452925103	Nov 02 2023 12:32 PM	Collinsville
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114452919906	Nov 02 2023 12:28 PM	Canton
114452920211	Nov 02 2023 12:25 PM	Collinsville
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114452908938	Nov 02 2023 12:22 PM	Canton
114452908395	Nov 02 2023 12:22 PM	Canton
114452909259	Nov 02 2023 12:20 PM	Canton
114452906923	Nov 02 2023 12:15 PM	Canton, ct
114452911726	Nov 02 2023 12:15 PM	CANTON
114452908567	Nov 02 2023 12:14 PM	Canton
114452901788	Nov 02 2023 12:09 PM	Canton
114452899139	Nov 02 2023 12:07 PM	Collinsville
114452892896	Nov 02 2023 12:05 PM	Simsbury
114452892509	Nov 02 2023 12:02 PM	Canton
114452889884	Nov 02 2023 11:59 AM	Canton
114452887747	Nov 02 2023 11:58 AM	collinsville
114452883439	Nov 02 2023 11:58 AM	Groton
114452817216	Nov 02 2023 11:57 AM	Canton
114452885556	Nov 02 2023 11:56 AM	Collinsville
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114452877244	Nov 02 2023 11:56 AM	Canton
114452837947	Nov 02 2023 11:55 AM	Canton
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114452866994	Nov 02 2023 11:51 AM	Canton
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114452856222	Nov 02 2023 11:39 AM	BURLINGTON
114452862581	Nov 02 2023 11:34 AM	Collinsville
114452860286	Nov 02 2023 11:33 AM	Canton
114452859739	Nov 02 2023 11:32 AM	canton
114452856833	Nov 02 2023 11:30 AM	Canton
114452858345	Nov 02 2023 11:30 AM	Canton
114452849184	Nov 02 2023 11:27 AM	Canton
114452848836	Nov 02 2023 11:24 AM	Collinsville

114452844707	Nov 02 2023 11:22 AM	Canton
114452844132	Nov 02 2023 11:18 AM	Canton
114452836379	Nov 02 2023 11:18 AM	Canton
114452812359	Nov 02 2023 11:16 AM	CantonCanton Hollow
114452842039	Nov 02 2023 11:16 AM	Canton
114452840156	Nov 02 2023 11:15 AM	Hartland
114452834313	Nov 02 2023 11:12 AM	canton
114452837068	Nov 02 2023 11:12 AM	Collinsville
114452815192	Nov 02 2023 11:12 AM	Collinsville
114452822369	Nov 02 2023 11:11 AM	Canton
114452831964	Nov 02 2023 11:09 AM	Collinsville
114452826617	Nov 02 2023 11:08 AM	Canton
114452828090	Nov 02 2023 11:06 AM	Collinsville
114452820310	Nov 02 2023 11:05 AM	Collinsville
114452823678	Nov 02 2023 11:04 AM	Collinsville
114452821466	Nov 02 2023 11:00 AM	Collinsville
114452825015	Nov 02 2023 11:00 AM	Canton
114452822004	Nov 02 2023 10:59 AM	Canton
114452818765	Nov 02 2023 10:58 AM	Winsted
114452817978	Nov 02 2023 10:58 AM	Canton
114452821335	Nov 02 2023 10:57 AM	Canton, CT
114452820531	Nov 02 2023 10:56 AM	Canton
114452819291	Nov 02 2023 10:56 AM	Canton
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114452799749	Nov 02 2023 10:40 AM	CANTON
114452797765	Nov 02 2023 10:39 AM	Collinsville
114452785733	Nov 02 2023 10:38 AM	Canton
114452799695	Nov 02 2023 10:37 AM	Canton , CT
114452793546	Nov 02 2023 10:36 AM	Canton
114452796285	Nov 02 2023 10:35 AM	Collinsville
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114452794458	Nov 02 2023 10:35 AM	Canton
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114452793897	Nov 02 2023 10:34 AM	New Hartford
114452792088	Nov 02 2023 10:34 AM	Collinsville
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114452796464	Nov 02 2023 10:34 AM	Canton
114452796827	Nov 02 2023 10:33 AM	Canton
114452792390	Nov 02 2023 10:31 AM	Canton
114452788666	Nov 02 2023 10:30 AM	Canton
114452788242	Nov 02 2023 10:30 AM	CANTON
114452793730	Nov 02 2023 10:29 AM	Canton
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114452785409	Nov 02 2023 10:26 AM	Canton
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114452784828	Nov 02 2023 10:24 AM	Collinsville
114452787267	Nov 02 2023 10:24 AM	Canton
114452787126	Nov 02 2023 10:24 AM	Canton
114452786723	Nov 02 2023 10:24 AM	Canton
114452785784	Nov 02 2023 10:24 AM	Canton
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114452783918	Nov 02 2023 10:23 AM	Canton
114452781628	Nov 02 2023 10:23 AM	Canton
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114452784964	Nov 02 2023 10:22 AM	Canton
114452783232	Nov 02 2023 10:22 AM	Canton
114452781019	Nov 02 2023 10:21 AM	Collinsville
114452780822	Nov 02 2023 10:21 AM	Collinsville
114452780846	Nov 02 2023 10:20 AM	canton
114452782842	Nov 02 2023 10:19 AM	Canton
114452780339	Nov 02 2023 10:19 AM	Canton
114452780231	Nov 02 2023 10:18 AM	Canton, CT
114452700078	Nov 02 2023 08:59 AM	Collinsville
114452665938	Nov 02 2023 08:20 AM	Canton
114452611389	Nov 02 2023 07:21 AM	Canton
114452600525	Nov 02 2023 06:59 AM	Canton
114452471775	Nov 02 2023 03:17 AM	Canton
114452434151	Nov 02 2023 01:11 AM	Canton
114452390924	Nov 01 2023 11:29 PM	Canton
114452385542	Nov 01 2023 11:20 PM	Canton
114452373909	Nov 01 2023 10:45 PM	Burlington (right next to collinsville)
114452357431	Nov 01 2023 10:21 PM	Burlington
114452346141	Nov 01 2023 09:55 PM	Canton

114452344763	Nov 01 2023 09:49 PM	Canton
114452310853	Nov 01 2023 08:52 PM	Unionville but grew up in Canton 40 plus years
114452252747	Nov 01 2023 07:24 PM	Collinsville
114452231108	Nov 01 2023 06:52 PM	Farmington
114452175232	Nov 01 2023 05:24 PM	Canton
114452149115	Nov 01 2023 04:56 PM	Out of town
114452092170	Nov 01 2023 03:46 PM	Simsbury
114452084449	Nov 01 2023 03:38 PM	Canton
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114451948907	Nov 01 2023 01:22 PM	Simsbury
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114451639512	Nov 01 2023 08:05 AM	Canton
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114451512548	Nov 01 2023 04:48 AM	West Hartford
114451288308	Oct 31 2023 09:42 PM	Collinsville
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114450746721	Oct 31 2023 11:22 AM	Canton
114450684323	Oct 31 2023 10:39 AM	Collinsville
114450648682	Oct 31 2023 09:45 AM	AVON
114450621478	Oct 31 2023 09:22 AM	Canton
114450355952	Oct 31 2023 03:54 AM	Canton
114447091547	Oct 30 2023 09:07 PM	West Hartford
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114449637714	Oct 30 2023 12:06 PM	Canton
114449492380	Oct 30 2023 09:28 AM	Avon
114449125679	Oct 29 2023 06:47 PM	Collinsville
114449082928	Oct 29 2023 04:32 PM	Canton
114449074884	Oct 29 2023 03:34 PM	Canton
114449065253	Oct 29 2023 03:14 PM	Canton
114449066910	Oct 29 2023 03:04 PM	Canton, CT
114449046714	Oct 29 2023 01:48 PM	Collinsville
114449030817	Oct 29 2023 12:53 PM	Avon
114448870207	Oct 29 2023 01:18 AM	Avon
114448850182	Oct 28 2023 11:33 PM	Canton

114448837940	Oct 28 2023 10:43 PM	Canton
114448837859	Oct 28 2023 10:37 PM	Canton
114448832296	Oct 28 2023 10:14 PM	Canton CT
114448761129	Oct 28 2023 10:06 PM	Farmington
114448827289	Oct 28 2023 09:52 PM	Canton
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114448779003	Oct 28 2023 06:30 PM	New Hartford
114448774056	Oct 28 2023 06:10 PM	Canton
114448755001	Oct 28 2023 04:57 PM	Canton
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114448714320	Oct 28 2023 02:36 PM	avon
114448706158	Oct 28 2023 02:15 PM	Canton
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114448672813	Oct 28 2023 12:35 PM	Canton
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114448422564	Oct 27 2023 10:19 PM	Canton
114448421610	Oct 27 2023 10:14 PM	SIMSBURY
114448391838	Oct 27 2023 08:50 PM	Avon
114448383385	Oct 27 2023 08:37 PM	Canton
114448278845	Oct 27 2023 04:57 PM	Canton
114448215309	Oct 27 2023 03:17 PM	Canton
114448178884	Oct 27 2023 02:33 PM	Collinsville
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114447806569	Oct 27 2023 07:18 AM	Canton
114447778959	Oct 27 2023 06:24 AM	Canton
114447773568	Oct 27 2023 06:07 AM	Canton
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114447391309	Oct 26 2023 05:33 PM	Canton
114447366418	Oct 26 2023 04:58 PM	Canton
114447255799	Oct 26 2023 02:59 PM	Canton, CT

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114447085011	Oct 26 2023 12:14 PM	Canton
114447045501	Oct 26 2023 11:53 AM	Canton
114447039163	Oct 26 2023 11:32 AM	Canton
114447036502	Oct 26 2023 11:27 AM	Canton, CT
114447031047	Oct 26 2023 11:19 AM	Collinsville
114447024344	Oct 26 2023 11:16 AM	Canton
114447023228	Oct 26 2023 11:11 AM	Avon
114447009757	Oct 26 2023 11:00 AM	Canton
114446960951	Oct 26 2023 10:13 AM	Unionville
114446801949	Oct 26 2023 06:46 AM	West Hartford
114446499240	Oct 25 2023 08:07 PM	canton
114446443025	Oct 25 2023 08:06 PM	Avon
114446472587	Oct 25 2023 07:23 PM	Avon
114446406905	Oct 25 2023 05:45 PM	Canton
114445988688	Oct 25 2023 10:22 AM	Canton
114445967939	Oct 25 2023 10:03 AM	Canton
114445825332	Oct 25 2023 07:09 AM	Avon
114445738658	Oct 25 2023 04:19 AM	Canton
114445593066	Oct 24 2023 11:14 PM	Collinsville
114445438539	Oct 24 2023 06:51 PM	Farmington
114445026846	Oct 24 2023 11:18 AM	Simsbury
114444993042	Oct 24 2023 10:53 AM	Canton
114444501078	Oct 23 2023 09:08 PM	Avon
114444493034	Oct 23 2023 08:45 PM	Burlington
114444458247	Oct 23 2023 08:00 PM	Canton
114444448057	Oct 23 2023 07:40 PM	Canton
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114444136986	Oct 23 2023 01:52 PM	Canton
114444113762	Oct 23 2023 01:23 PM	Canton, CT
114444000384	Oct 23 2023 11:35 AM	Collinsville
114443998003	Oct 23 2023 11:27 AM	Canton
114443987547	Oct 23 2023 11:18 AM	Avon
114443816111	Oct 23 2023 08:25 AM	Collinsville
114443720092	Oct 23 2023 05:18 AM	New Hartford
114443600828	Oct 23 2023 12:55 AM	Simsbury
114443587101	Oct 23 2023 12:16 AM	Canton
114443525588	Oct 22 2023 09:44 PM	Canton
114443450836	Oct 22 2023 08:25 PM	Collinsville, CT
114443474555	Oct 22 2023 07:09 PM	Avon

114443458196	Oct 22 2023 05:56 PM	Avon
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114443410528	Oct 22 2023 03:08 PM	Canton
114443401220	Oct 22 2023 02:43 PM	Simsbury
114443397180	Oct 22 2023 02:27 PM	Canton, CT
114443394017	Oct 22 2023 02:19 PM	Canton
114443361354	Oct 22 2023 12:39 PM	Collinsville
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114443326771	Oct 22 2023 11:08 AM	Canton
114443305614	Oct 22 2023 09:55 AM	Canton
114443277119	Oct 22 2023 08:29 AM	Avon
114443270404	Oct 22 2023 08:07 AM	Tariffville
114443255747	Oct 22 2023 07:29 AM	simsbury
114443168789	Oct 22 2023 12:59 AM	Canton
114443139532	Oct 21 2023 10:45 PM	Tariffville
114443105569	Oct 21 2023 08:34 PM	Weatogue
114443103427	Oct 21 2023 08:31 PM	Simsbury
114443092225	Oct 21 2023 08:04 PM	Canton
114443078441	Oct 21 2023 07:05 PM	New Hartford
114443076781	Oct 21 2023 06:54 PM	Bloomfield CT
114443074376	Oct 21 2023 06:41 PM	Simsbury
114443070170	Oct 21 2023 06:28 PM	Avon
114443064873	Oct 21 2023 06:10 PM	West Simsbury
114443034649	Oct 21 2023 04:24 PM	Canton
114442907929	Oct 21 2023 04:15 PM	Canton, CT
114443022192	Oct 21 2023 03:41 PM	Canton
114443011353	Oct 21 2023 03:11 PM	Farmington
114443007286	Oct 21 2023 03:05 PM	Simsbury
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114442942391	Oct 21 2023 01:52 PM	canton
114442885842	Oct 21 2023 01:31 PM	canton
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114442381635	Oct 20 2023 02:47 PM	Avon, CT
114442366957	Oct 20 2023 02:38 PM	Canton
114442336201	Oct 20 2023 02:09 PM	Canton
114442323310	Oct 20 2023 01:56 PM	New Hartford

114442253444	Oct 20 2023 12:36 PM	Collinsville
114442232303	Oct 20 2023 12:11 PM	Canton
114442217001	Oct 20 2023 12:01 PM	New Hartford
114442152204	Oct 20 2023 10:53 AM	Canton
114442076408	Oct 20 2023 09:34 AM	Canton
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114441970725	Oct 20 2023 07:17 AM	Canton
114441953292	Oct 20 2023 06:43 AM	Avon
114441766966	Oct 20 2023 12:55 AM	Simsbury
114441762957	Oct 20 2023 12:49 AM	Canton
114441698132	Oct 19 2023 10:29 PM	Canton
114441682517	Oct 19 2023 10:04 PM	Canton
114441640272	Oct 19 2023 08:45 PM	West simsbury
114441627298	Oct 19 2023 08:23 PM	Avon
114441604056	Oct 19 2023 07:55 PM	Canton
114441592079	Oct 19 2023 07:27 PM	Canton
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114441455284	Oct 19 2023 04:18 PM	Canton CT
114441441096	Oct 19 2023 04:02 PM	Canton
114441430660	Oct 19 2023 03:58 PM	Canton
114441434607	Oct 19 2023 03:51 PM	Can ton
114441422408	Oct 19 2023 03:38 PM	Canton
114441389752	Oct 19 2023 03:03 PM	Canton
114441360407	Oct 19 2023 02:33 PM	Canton
114441359309	Oct 19 2023 02:33 PM	Canton
114441357834	Oct 19 2023 02:30 PM	Canton
114441344923	Oct 19 2023 02:20 PM	Canton
114441340226	Oct 19 2023 02:14 PM	Collinsville

114441339184	Oct 19 2023 02:13 PM	Canton
114441303569	Oct 19 2023 01:40 PM	Canton
114441301671	Oct 19 2023 01:36 PM	Canton
114441284712	Oct 19 2023 01:20 PM	North Canton
114441275049	Oct 19 2023 01:13 PM	Canton
114441256383	Oct 19 2023 12:54 PM	Canton
114441236594	Oct 19 2023 12:33 PM	Canton
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114441120858	Oct 19 2023 10:50 AM	Canton
114440897105	Oct 19 2023 10:43 AM	CANTON
114441109937	Oct 19 2023 10:39 AM	Canton, CT
114441082825	Oct 19 2023 10:28 AM	Canton
114441098671	Oct 19 2023 10:28 AM	Barkhamsted
114441081974	Oct 19 2023 10:24 AM	Collinsville
114441083779	Oct 19 2023 10:17 AM	COLLINSVILLE
114441080018	Oct 19 2023 10:08 AM	CANTON
114441074704	Oct 19 2023 10:05 AM	Canton
114441069591	Oct 19 2023 09:58 AM	Canton CT
114441066245	Oct 19 2023 09:56 AM	Canton
114441061129	Oct 19 2023 09:51 AM	New Hartford
114441056773	Oct 19 2023 09:49 AM	avon
114441056756	Oct 19 2023 09:47 AM	Canton Center
114441041394	Oct 19 2023 09:33 AM	Canton
114441038642	Oct 19 2023 09:30 AM	canton
114441031567	Oct 19 2023 09:23 AM	Canton
114441031039	Oct 19 2023 09:22 AM	Canton
114441017751	Oct 19 2023 09:20 AM	Avon, CT
114441020719	Oct 19 2023 09:19 AM	Bristol
114441021986	Oct 19 2023 09:18 AM	Canton
114441012584	Oct 19 2023 09:16 AM	Harwinton
114441016529	Oct 19 2023 09:12 AM	Canton, CT
114441015118	Oct 19 2023 09:12 AM	Canton
114441018575	Oct 19 2023 09:11 AM	Canton
114441017523	Oct 19 2023 09:10 AM	Collinsville
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114441012147	Oct 19 2023 09:04 AM	Canton
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114441009465	Oct 19 2023 08:59 AM	Canton
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114441006346	Oct 19 2023 08:58 AM	Canton
114441003015	Oct 19 2023 08:56 AM	Canton
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114440979244	Oct 19 2023 08:25 AM	Hartland
114440975990	Oct 19 2023 08:24 AM	Canton
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114440965803	Oct 19 2023 08:12 AM	Canton
114440963403	Oct 19 2023 08:09 AM	Canton
114440957768	Oct 19 2023 08:02 AM	New Britain but work in Canton
114440946062	Oct 19 2023 07:52 AM	Canton
114440946505	Oct 19 2023 07:51 AM	Canton
114440930669	Oct 19 2023 07:40 AM	Canton
114440930628	Oct 19 2023 07:33 AM	Canton
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114440914402	Oct 19 2023 07:05 AM	Farmington
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114440632939	Oct 18 2023 10:00 PM	Prefer not to answer but live locally

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114440370072	Oct 18 2023 03:37 PM	West Hartford
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114438265335	Oct 16 2023 05:09 PM	Canton
114438257490	Oct 16 2023 05:08 PM	Canton
114438260415	Oct 16 2023 05:01 PM	Avon
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114438251520	Oct 16 2023 04:51 PM	Collinsville
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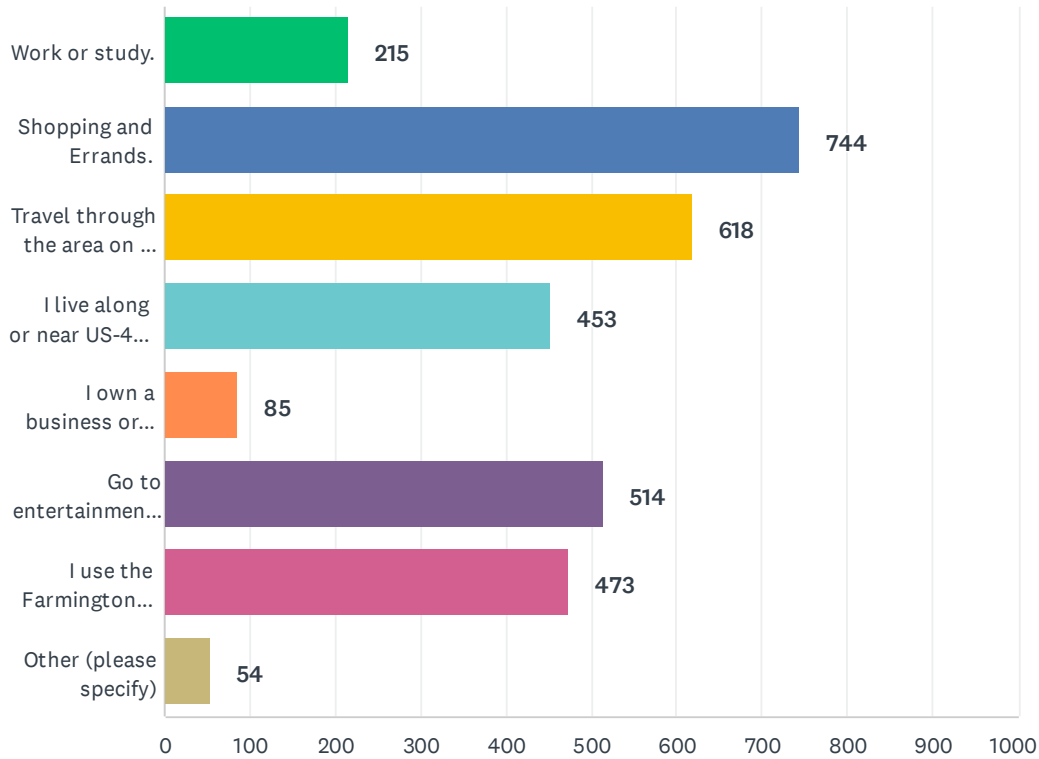
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114437966119	Oct 16 2023 12:07 PM	Canton
114437942157	Oct 16 2023 11:45 AM	Collinsville
114437923027	Oct 16 2023 11:35 AM	Property owner in Canton
114437922681	Oct 16 2023 11:27 AM	Collinsville, CT
114437917636	Oct 16 2023 11:22 AM	Collinsville
114437874995	Oct 16 2023 10:43 AM	Burlington
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114436027590	Oct 13 2023 08:26 AM	Simsbury
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114429740912	Oct 05 2023 02:35 PM	Bloomfield
114429490357	Oct 05 2023 10:33 AM	Collinsville
114429393040	Oct 05 2023 08:39 AM	Simsbury
114429073064	Oct 04 2023 09:55 PM	canton
114428993797	Oct 04 2023 07:27 PM	Harwinton
114428873488	Oct 04 2023 04:43 PM	Bristol
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114428628501	Oct 04 2023 12:44 PM	Canton
114428633986	Oct 04 2023 12:43 PM	Newington, CT
114428320616	Oct 04 2023 06:55 AM	Simsbury
114428220784	Oct 04 2023 03:39 AM	Burlington
114427784925	Oct 03 2023 03:04 PM	Windsor
114427687499	Oct 03 2023 01:31 PM	Canton
114427646592	Oct 03 2023 12:50 PM	Canton
114427535902	Oct 03 2023 11:08 AM	Canton
114427420563	Oct 03 2023 09:19 AM	Collinsville
114427396184	Oct 03 2023 08:54 AM	Collinsville
114427337728	Oct 03 2023 07:42 AM	Canton
114427331917	Oct 03 2023 07:34 AM	Woodbury, CT
114427211889	Oct 03 2023 03:50 AM	Granby
114427063996	Oct 02 2023 10:48 PM	Canton
114427043071	Oct 02 2023 09:21 PM	Farmington
114427038805	Oct 02 2023 09:12 PM	Avon
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114426979703	Oct 02 2023 07:29 PM	unionville
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114426927984	Oct 02 2023 06:19 PM	Canton
114426901594	Oct 02 2023 05:39 PM	Simsbury
114426882376	Oct 02 2023 05:14 PM	Simsbury
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114426852431	Oct 02 2023 04:37 PM	Burlington CT
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114426820362	Oct 02 2023 04:05 PM	Simsbury
114426818012	Oct 02 2023 04:04 PM	Granby CT
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114426336531	Oct 02 2023 08:15 AM	Canton
114426310921	Oct 02 2023 07:38 AM	Canton
114426296882	Oct 02 2023 07:15 AM	Canton/Collinsville
114426259537	Oct 02 2023 06:07 AM	Canton
114426241410	Oct 02 2023 05:38 AM	Canton
114426147913	Oct 02 2023 01:45 AM	Canton
114426125309	Oct 02 2023 12:34 AM	Canton
114426114851	Oct 02 2023 12:06 AM	Collinsville
114426104240	Oct 01 2023 11:36 PM	New Hartford
114426094828	Oct 01 2023 11:16 PM	Collinsville
114426083348	Oct 01 2023 10:36 PM	Canton
114426069732	Oct 01 2023 09:59 PM	Farmington
114426008497	Oct 01 2023 07:18 PM	Canton
114425920718	Oct 01 2023 02:47 PM	Burlington
114425896557	Oct 01 2023 01:13 PM	Unionville
114425844961	Oct 01 2023 10:45 AM	Canton
114425839053	Oct 01 2023 10:25 AM	Burlington

Q2 Why do you typically come to this section of US-44 / the study area in Canton? (chose all that apply)

Answered: 869 Skipped: 0



ANSWER CHOICES	RESPONSES	
Work or study.	25%	215
Shopping and Errands.	86%	744
Travel through the area on the way to other destinations.	71%	618
I live along or near US-44 / Dowd Avenue in Canton.	52%	453
I own a business or residence along US-44/Dowd Avenue in Canton.	10%	85
Go to entertainment venues, events, or restaurants.	59%	514
I use the Farmington River Rail Trail.	54%	473
Other (please specify)	6%	54
Total Respondents: 869		

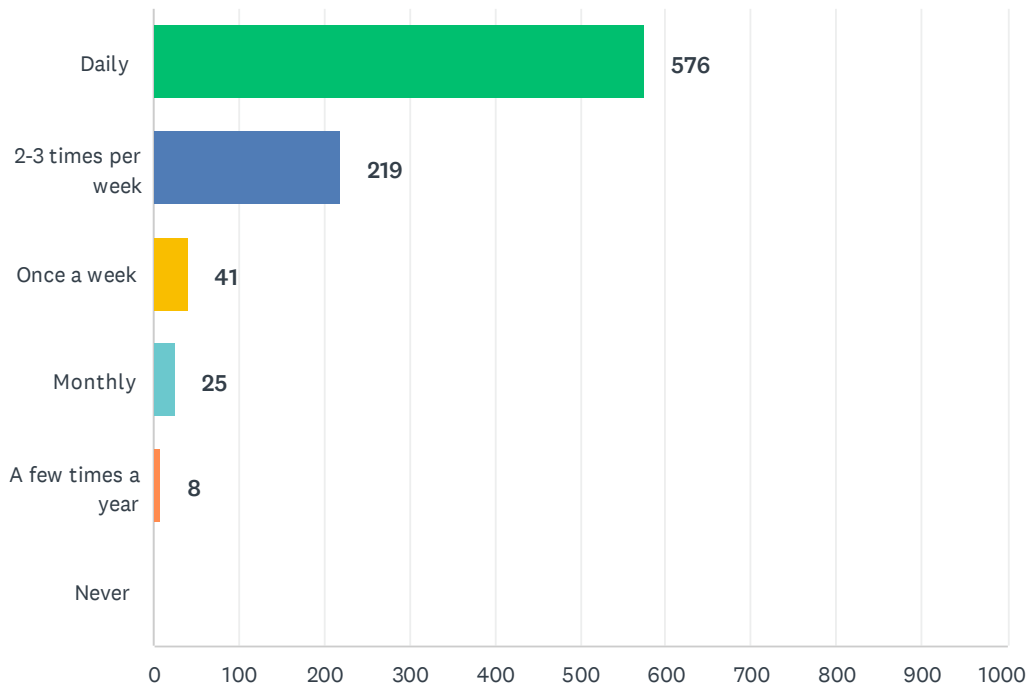
Respondent ID	Response Date	Other (please specify)
114489928080	Dec 15 2023 11	visit family in Canton
114484773816	Dec 09 2023 08	Traffic, based on current volume, is dangerous
114463468212	Nov 14 2023 12	I work in Collinsville, and live off of Route 202 near 197/202/44 interchange
114455913214	Nov 06 2023 07	To go to my home in Canton
114448747468	Nov 05 2023 04	Medical appointments
114455198803	Nov 05 2023 07	I live in Secret Lake
114454311668	Nov 03 2023 05	I live and own a residence not far from the study area
114453961135	Nov 03 2023 11	recreational riding on roads
114453759738	Nov 03 2023 08	Kids sports
114453572549	Nov 03 2023 02	Son lives on route 44
114453438260	Nov 02 2023 10	Drive kids to and from school
114453253035	Nov 02 2023 05	Live on Secret lake off of 44
114453172966	Nov 02 2023 04	I have family that lives in town.
114453108169	Nov 02 2023 03	Most of my medical appointments are in that direction.
114453084582	Nov 02 2023 02	I used to live on Canton Springs Road for 16 years
114453018261	Nov 02 2023 02	Can't think of any "other" but if there are I'm sure I would apply so I checked it.
114453017020	Nov 02 2023 01	union savings bank on the green is where I bank at
114452965846	Nov 02 2023 01	Family in. New Hartford
114452956256	Nov 02 2023 01	My child uses the 44 Corridor, the Farmington Greenway Trail and crosses Dowd Ave to bicycle to school
114452942614	Nov 02 2023 12	I live off of Canton Springs Rd

114452917176	Nov 02 2023 12:	Have property on Maple Ave (continuation of Dowd before it reaches ct 179)
114452883439	Nov 02 2023 11:	visit family
114452812359	Nov 02 2023 11:	Love Canton hollows small, treed residents
114452816737	Nov 02 2023 10:	Going to CIS &CHS
114452796285	Nov 02 2023 10:	Gallery on the Green, dog park
114452793897	Nov 02 2023 10:	Library use
114452787126	Nov 02 2023 10:	Kids sports etc...
114452783232	Nov 02 2023 10:	To get Home
114451880385	Nov 01 2023 12:	I travel these roads to get to/from my home
114450960456	Oct 31 2023 02:	Pokemon Go/Pikmin Bloom events
114448774056	Oct 28 2023 06:	I live in town
114448733064	Oct 28 2023 03:	I live at Secert Lake
114448714761	Oct 28 2023 02:	I live on the boarder
114447036502	Oct 26 2023 11:	Access to the Bike Shop
114444501078	Oct 23 2023 09:	I am nestled between Unionville and Collinsville, have to travel through either village to get to stores, gas, etc
114443987547	Oct 23 2023 11:	visit friends
114443103427	Oct 21 2023 08:	Bicycle on Rt 44 and Dowd as well as rail trail.
114441061129	Oct 19 2023 09:	Appointments
114441020719	Oct 19 2023 09:	Our daughter's family lives in Collinsville, and we regularly help with babysitting and transport of the children to appointments and activities in Canton/Avon. Also our primary physician is in Avon.
114441021986	Oct 19 2023 09:	Medical services
114441012584	Oct 19 2023 09:	St. Francis Hospital
114440968684	Oct 19 2023 08:	My family lives in Canton

114440946505	Oct 19 2023 07:	Gym, Post Office, gas station, grocery stores,
114440903371	Oct 19 2023 07:	Medical/Dental Appointments
114440879638	Oct 19 2023 06:	I live in Oxbow Ridge
114440485630	Oct 18 2023 05:	Friend's houses right off 44 in Canton.
114440433006	Oct 18 2023 04:	Dog park
114440373539	Oct 18 2023 03:	Through area for work commute I ride my bicycle in the area several times
114439810349	Oct 18 2023 06:	monthly
114438946430	Oct 17 2023 10:	main corridor my two teenagers use this route frequently
114438257490	Oct 16 2023 05:	for walking and biking Live on Maple Ave which is a section of
114438114925	Oct 16 2023 02:	Rte 565 RT 44 is the only direct route to the
114435334325	Oct 12 2023 12:	Farmington River from Hartford area.
114426069732	Oct 01 2023 09:	Hometown

Q3 How often do you travel along US-44/Dowd Avenue in Canton?

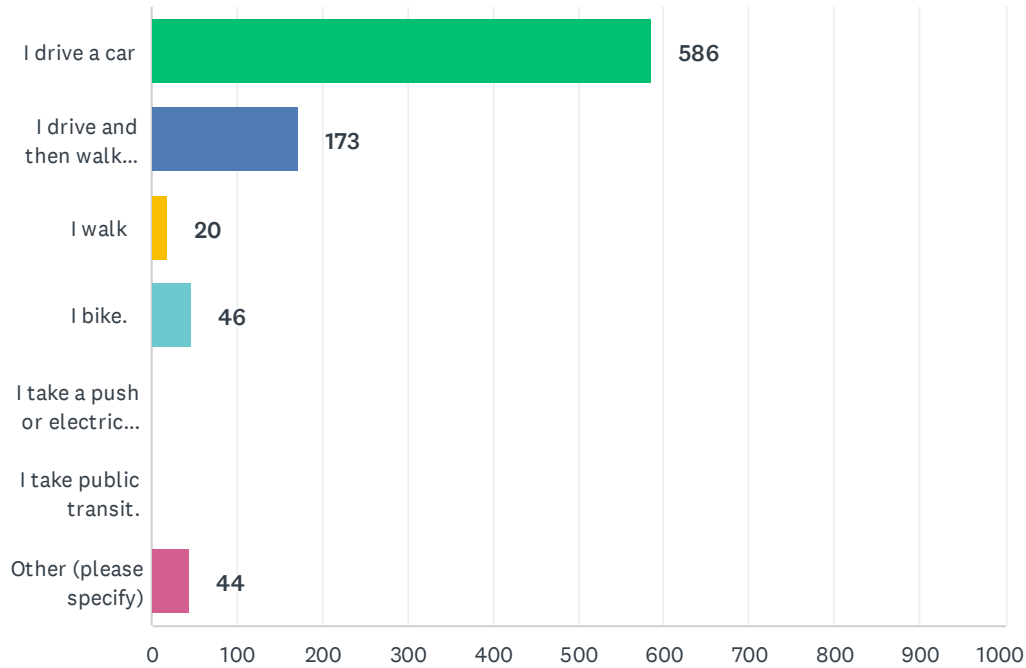
Answered: 869 Skipped: 0



ANSWER CHOICES	RESPONSES	
Daily	66%	576
2-3 times per week	25%	219
Once a week	5%	41
Monthly	3%	25
A few times a year	1%	8
Never	0%	0
TOTAL		869

Q4 How do you typically travel to and around US-44/Dowd Avenue in Canton (chose one)

Answered: 869 Skipped: 0



ANSWER CHOICES	RESPONSES	
I drive a car	67%	586
I drive and then walk around shops	20%	173
I walk	2%	20
I bike.	5%	46
I take a push or electric scooter.	0%	0
I take public transit.	0%	0
Other (please specify)	5%	44
TOTAL		869

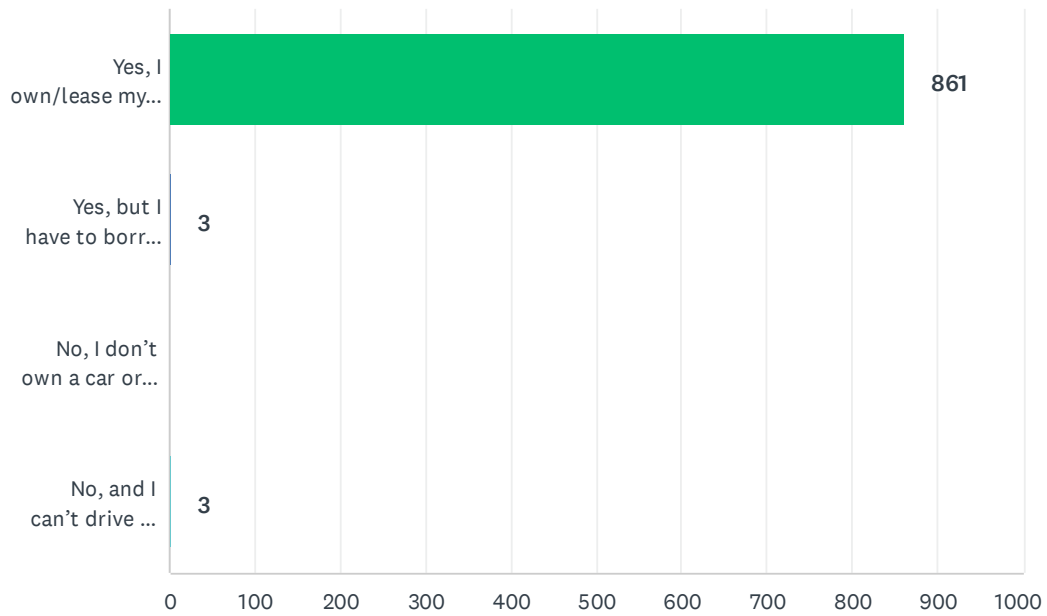
Respondent ID	Response Date	Other (please specify)
114488396612	Dec 13 2023 0	I drive, walk and bike this area
114486084664	Dec 11 2023 0	Drive, cycle & walk
114484773816	Dec 09 2023 0	Typically car since the area is not fully accessible by bike/walking
114476282928	Nov 30 2023 0	We do all of the above except use a scooter.
114454683084	Nov 04 2023 0	I drive to work in avon
114454311668	Nov 03 2023 0	I BOTH drive a car (to shops, work) AND walk the rail trail and around the shops
114453961135	Nov 03 2023 1	I drive and will ride as the type of errand permits.
114453801455	Nov 03 2023 0	this question deserves multiple answers...i drive to work on 44 and walk or ride from there. i will ride on the sidewalk or business parking lots which are less dangerous. I do not ride on 44
114453735212	Nov 03 2023 0	I walk and drive
114453321045	Nov 02 2023 0	I drive and walk on that area daily
114453317415	Nov 02 2023 0	I walk on the bike trail
114453266461	Nov 02 2023 0	I drive but would love to walk if could access the trail safely
114453051689	Nov 02 2023 0	I walk and bike a lot of the trail but don't like crossing at 44 and Lovely.
114453029417	Nov 02 2023 0	I drive my car and walk daily on the Greenway
114452965846	Nov 02 2023 0	My spouse drives most of time
114452961539	Nov 02 2023 1	Drive and walk

114452908938	Nov 02 2023 1 Walk as much as possible but also drive I both walk and drive to the Shops at FV
114452831654	Nov 02 2023 1 and Canton Village
114452815192	I drive thru daily and to destinations Nov 02 2023 1 within. I walk and bike the trail often. Usually travel by motorcycle except by car from about November through Mid-
114452806990	Nov 02 2023 1 March.
114452799749	Nov 02 2023 1 RIDE WITH ANOTHER DRIVER
114452385542	Nov 01 2023 1 Drive and walk. I live on Dowd Ave Drive for commuting but often walk to
114450746721	Oct 31 2023 1 Shoppes.
114450684323	Oct 31 2023 1 I drive, bike, and walk Drive to destinations and run or ride my
114449492380	Oct 30 2023 0 bike.
114447204175	Often drive for errands and out of town travel pass thru; bike for trail and access Oct 26 2023 0 to road and gravel riding beyond this area I drive to Collinsville and walk the dog in different areas daily. I drive to the shops
114444501078	Oct 23 2023 0 to shop
114443525588	I live on Dowd and drive and walk daily to Oct 22 2023 0 various nearby shops and trails
114443450836	We drive, walk and bike. This is our Oct 22 2023 0 neighborhood.
114443168789	I drive a car, and I walk on Dowd Ave and Oct 22 2023 1 the Rail Trail
114441151788	Oct 19 2023 1 I drive and use the walking path regularly.

114441082825	Oct 19 2023 10	Drive frequently, walk often and bike sometimes
114441012584	Oct 19 2023 09	I walk in other locations. I drive mainly here.
114440993254	Oct 19 2023 09	I drive since biking and/or walking is unsafe
114440648904	Oct 18 2023 10	I drive to destinations in Canton/Avon/Simsbury and beyond, but I do not walk to get to any of my destinations.
114440378270	Oct 18 2023 09	I would select both options 1 and 2.
114440356485	Oct 18 2023 09	School Bus Operator
114439308802	Oct 17 2023 09	I drive only
114438946430	Oct 17 2023 10	I drive and walk in the area
114438257490	Oct 16 2023 09	I drive along this route and also walk on the Farmington Valley trail daily.
114438224384	Oct 16 2023 09	I live on Maple Avenue where I walk and bike and drive
114438182736	Oct 16 2023 09	walk, bike and drive
114429073064	Oct 04 2023 09	I drive don't walk
114426691365	Oct 02 2023 09	Drive and Bike

Q5 Do you have access to an automobile?

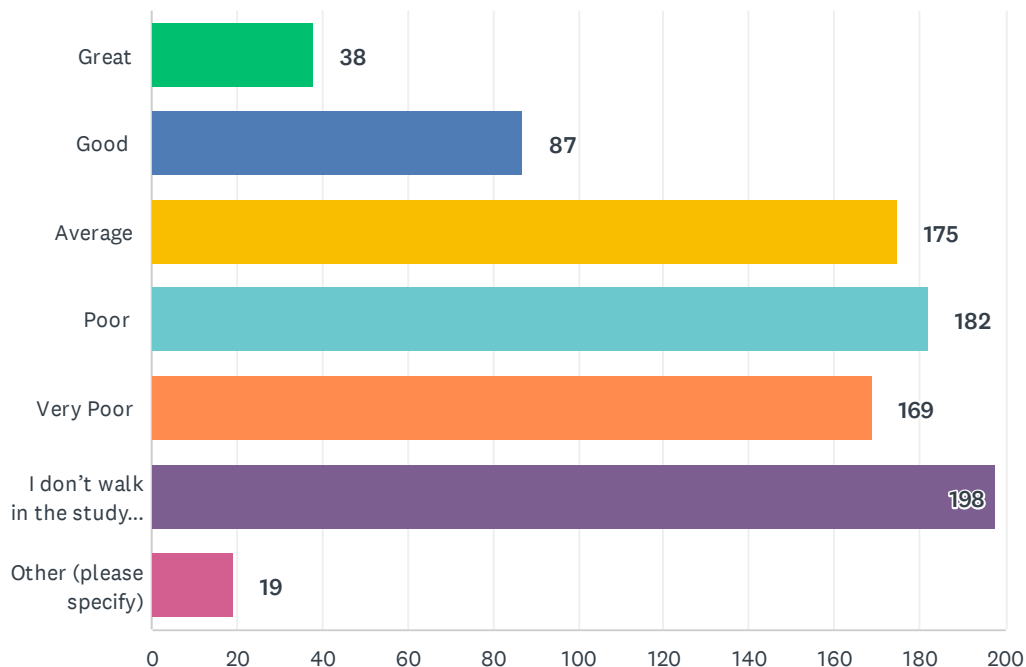
Answered: 867 Skipped: 2



ANSWER CHOICES	RESPONSES	
Yes, I own/lease my own car.	99%	861
Yes, but I have to borrow someone else's car.	0%	3
No, I don't own a car or have access to one.	0%	0
No, and I can't drive for lack of license or other reason.	0%	3
TOTAL		867

Q6 In terms of traffic safety, what is your level of safety and comfort walking along US-44/Dowd Avenue in Canton? (choose one)

Answered: 868 Skipped: 1



ANSWER CHOICES	RESPONSES	
Great	4%	38
Good	10%	87
Average	20%	175
Poor	21%	182
Very Poor	19%	169
I don't walk in the study area	23%	198
Other (please specify)	2%	19
TOTAL		868

Respondent ID	Response Date	Other (please specify)
114486084664	Dec 11 2023 0	Too much traffic and noise, cars move much too fast with disregard to safety
114484773816	Dec 09 2023 0	Safe on biking trail but extremely dangerous to bike on route 44
114476282928	Nov 30 2023 0	Rt. 44 should have sidewalks extended to housing on the north side..
114452908395	Nov 02 2023 1	I feel safe on sidewalks and trail. Never ride bike on street
114452856222	Nov 02 2023 1	I DON'T WALK ALONG 44 AS I DON'T HAVE TO, BUT I WOULDN'T BE COMFORTABLE DUE TO THE SPEED AT WHICH CARS AND TRUCKS TRAVEL.
114452794221	Nov 02 2023 1	Walking or riding a bike on RT 44 is just dangerous. I have done it, and will not do it again. The area has become far too built up. Too much traffic. On top of that, drivers have become too distracted, too ignorant of other vehicles and courteous driving rules.
114452390924	Nov 01 2023 1	44 is a 4-lane highway. It should not be made pedestrian friendly.
114452084449	Nov 01 2023 0	I live on Dowd and nearly all vehicles travel a minimum of 10 mph above the speed limit. If there were not sidewalks, the road would be treacherous.

114450684323	Oct 31 2023 10	Walking along 44 is not something I'd do. 44 is for driving, and there are other ways to get pretty much anywhere by walking. The only way that 44 would be better for walking would be the installation of sidewalks, and the cost/benefit is not worth it.
114444501078	Oct 23 2023 09	Crossing Rt.44 I feel very unsafe and unsafe walking in areas as cars go fast and there is no wide safe lane for us.
114442907929	Oct 21 2023 09	While I rarely walk along Rt 44, I have done so and it does not feel safe in many places. Driving, it strikes me as a terrible place if one needs or wants to walk to and between businesses
114443007286	Oct 21 2023 09	I do not walk or bike this area as it is not safe.
114440908836	Oct 19 2023 09	It is highly dangerous to walk along 44. I would never and forbid my teenage children
114440648904	Oct 18 2023 10	I don't walk in this area, however, I feel with the increased building and traffic it is unsafe to bike or walk along this route. You could add sidewalks, but I feel they would need to be separated from traffic by a buffer zone with some type of guardrail for safety.
114440485630	Oct 18 2023 09	I fear for pedestrians every time I see them walking along 44.
114438946430	Oct 17 2023 10	horrid for pedestrians and cyclists
114438026009	Oct 16 2023 09	Along that route, I am usually on the greenway ...

114429073064

Oct 04 2023 0: I don't walk it

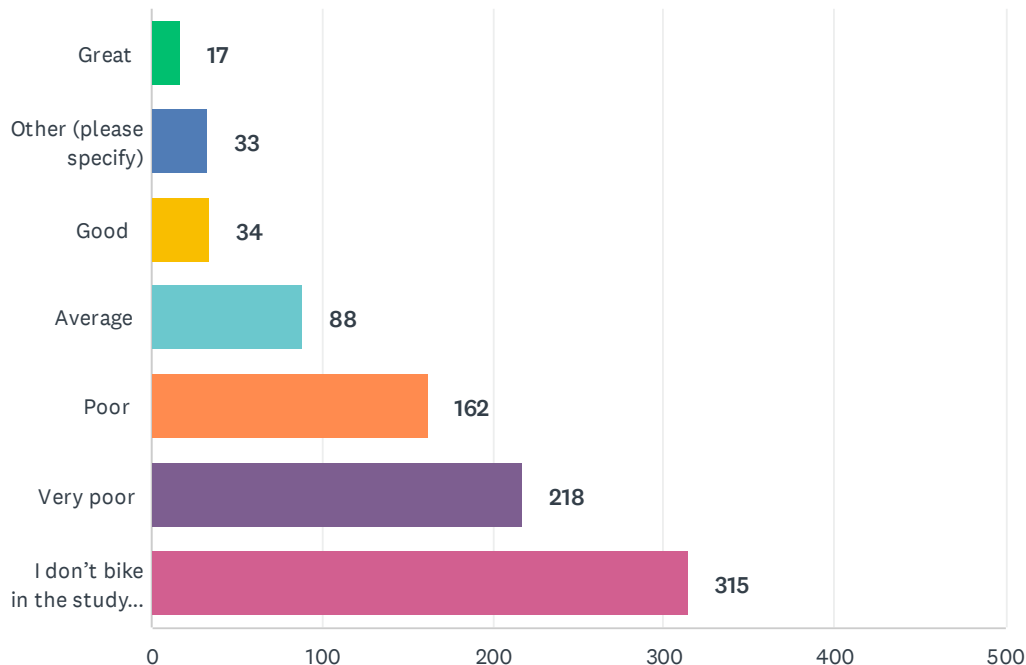
I don't feel safe or comfortable based solely on the

114426344342

Oct 02 2023 0: aggressive driving.

Q7 In terms of safety, what is your level of safety and comfort biking along US-44/Dowd Avenue in Canton? (Choose one.)

Answered: 867 Skipped: 2



ANSWER CHOICES	RESPONSES	
Great	2%	17
Other (please specify)	4%	33
Good	4%	34
Average	10%	88
Poor	19%	162
Very poor	25%	218
I don't bike in the study area.	36%	315
TOTAL		867

Respondent ID	Response Date	Other (please specify)
114486084664	Dec 11 2023 0	Average on paths but extremely poor on Route 44. Route 44 is NOT designed for bicycling. Emphasis is on moving car capacity, not safety and the well-being of people. There is NO safe non-car movement going East on Route 44 from Lovely Street.
114484773816	Dec 09 2023 0	I'm a cyclist, but route 44 clearly favors cars.
114476282928	Nov 30 2023 0	We avoid biking on any road--too unsafe. We bike on trails for safety.
114455913214	Nov 06 2023 0	Bikes do not belong on this stretch of road. And this is a terrible map, very hard to read.
114453725352	Nov 03 2023 0	Bikes should not be on RT 44
114453438260	Nov 02 2023 1	I wouldn't bike in this area. It's very difficult because the road is always jammed with people turning to Dunkin' Donuts and the blue bagel cafe or there is a huge backup at the left signal by canton green
114453157527	Nov 02 2023 0	Speed on both Dowd and route 44 is a huge issue
114453156794	Nov 02 2023 0	44 unsafe for walking or bikes
114453104033	Nov 02 2023 0	I only Nike on the trail not along the road
114453018261	Nov 02 2023 0	I don't bike anymore, if I did I would stick to the railtrail.
114453051689	Nov 02 2023 0	I bike only on the actual trail to Collinsville. Not safe going to the shoppes.
114452925103	Nov 02 2023 1	I only bike on the rail trail & feel safe there - definitely NOT on the roads!
114452908395	Nov 02 2023 1	safe on sidewalk or river trail only

114452884241	Great on the path - Average on Dowd - Very poor on 44 Nov 02 2023 1
114452866994	I am an active user of the bike path but am not comfortable biking or walking on US44. Nov 02 2023 1
114452856222	I WOULD NOT BIKE ON 44 OR DOWD, Nov 02 2023 1
114452812359	SIDEWALKS MAYBE. Nov 02 2023 1
114452815192	I use the beautiful path to Collinsville From the trail east I feel very safe. The new construction and sidewalk installation is nice. From the trail west there are very few connected sidewalks and I do not feel safe Nov 02 2023 1
114452806990	walking or bicycling. I don't bicycle here often but it's apparent that improvements need to be made. Nov 02 2023 1
114452794221	It is dangerous. I have done it. Will not do it again. Nov 02 2023 1
114452390924	44 is a 4-lane highway. It should not be made bike friendly Nov 01 2023 1
114450684323	I feel ok biking on 44, but no matter where I am, the number of cars around increases the chance of a problem happening. People pay no attention. THIS HAS NOTHING AT ALL TO DO WITH SPEED. I'd much rather someone competently pass me going 65 than someone nearly hit me going 30 because he doesn't know where his car is. Oct 31 2023 1
114449492380	I just take the rails to trails Oct 30 2023 0

114442907929	Oct 21 2023 0	Biking in general is out of the question.
114441056773	Oct 19 2023 0	Again, I don't bike any more but I see bike readers out in traffic in the study area and elsewhere and it strikes me as dangerous.
114440901353	Oct 19 2023 0	you should not bike on a highway
114440908836	Oct 19 2023 0	Due to speeders on Dowd, I do not feel safe riding a bike.
114439429789	Oct 19 2023 0	Highly unsafe would never I ride a bike but I would never feel safe riding on Rt 44
114439308802	Oct 17 2023 0	I do not bike in the study area and do not support efforts to make the area a high density area
114439036704	Oct 17 2023 1	I avoid 44 as much as possible. If I go I use the sidewalks which are not in good condition. Dowd is o.k. Unless making left hand turns.
114438026009	Oct 16 2023 0	I have ridden along Rt 44... I do Not feel safe. That is why the Greenway that parallels that route is my preferred roadway"
114435334325	Oct 12 2023 1	I bike a lot. I fear for people's lives when I see them actually riding a bike on RT 44.
114429073064	Oct 04 2023 0	I do t bike should be outlawed on rt 44

Q8 Are there any specific areas, such as intersections or crossings, that you feel are unsafe or unappealing to walk or bike in the US-44 / Dowd Avenue area? Please explain.

Answered: 651 Skipped: 218

US-44 Corridor Study - Canton Public Survey

Are there any specific areas, such as intersections or crossings, that you feel are unsafe or unappealing to walk or bike in the US-44 / Dowd Avenue area? Please explain.

Answered 651
Skipped 218

Respondent ID	Response Date	Responses
114489928080	Dec 15 2023	1 all intersections and areas of US-44 The trail crossing on Dowd. People drive too fast and have had too many near misses of my son and I while biking. Prefer to drive into collinsville to avoid this whereas if it were safe, we could
114488435062	Dec 13 2023	0 bike straight from our home. This area is very congested with motor traffic that when I do bike this area, I feel at risk. Often I take the bike path to access business in the Canton Village, Canton shops and 44. I tend to bike
114488396612	Dec 13 2023	0 on the sidewalk to stay safe once I'm off the bike path.
114488351022	Dec 13 2023	0 Lawton and rte 44 intersection
114488306631	Dec 13 2023	0 Any attempts to cross 44 are fraught with peril.
114488254000	Dec 13 2023	0 RT 44 at Colonial Road
114488024943	Dec 13 2023	1 Trail crossings are to often ignored by cars. Would be great if pedestrian movement (not in vehicles) is the key focus of our 40 year plan to improve the roads. I foresee the Route 44 corridor becoming a new Berlin Turnpike without
114486084664	Dec 11 2023	0 strong leadership at the State and Local level. For the study, prefer to understand if the conclusion states that Route 44 and the area is safe for pedestrians. I feel the current condisions are unsafe and the future, without more regards to
114484773816	Dec 09 2023	0 bikes/walking, will be more concentrated on car movement.
114484336461	Dec 08 2023	0 Lovely Street/Lawton Road
114483775510	Dec 08 2023	0 All crosswalks as no motorist pay attention to walkers/bikers
114483618406	Dec 07 2023	0 Getting in and out of the Dunkin/canton pizza area
114482345890	Dec 06 2023	0 US-44 near the intersection of Dowd. The W bound traffic should stop at intersection, also.
114482195050	Dec 06 2023	0 N/a
114482107118	Dec 06 2023	0 N/A

			East Hill Rd/Rt 44: there's a crosswalk and light, but I frequently see drivers speeding up to get through the light and often running through the end of the yellow and into the red. It's a dangerous intersection to cross on foot or bike. Drivers typically travel at 10 mph over posted
114478952968	Dec 03 2023	0	speed limit (40)
114478411524	Dec 02 2023	0	The large intersection before the shoppes at canton
			There should be sidewalks and a crosswalk in the area of the mini golg/new housing for those
114476282928	Nov 30 2023	0	who live on the north side of Rt. 44.
			The entire route 44 is a pedestrian hazard. Motorists are rude, in a hurry, distracted. This is true
114475514216	Nov 29 2023	0	from the intersection at Saybrook Fishhouse all the way through to the Shops.
114467180963	Nov 18 2023	0	Canton village
			Really at any of the intersections with traffic lights. People regularly speed and run the red lights by a few seconds. I live near the intersection with East Hill Rd and before I turn onto 44 with a green light I wait 2-3 seconds because often someone runs the red light. If I were biking or
114466854468	Nov 17 2023	0	walking it would be fatal.
			Crossing is OK by bike / turning left from Lawton onto US-44 unsafe because of no "NO TURN ON RIGHT" sign from Lovely St. Lawton has a (green) left turn arrow / leading green and cars turning right from Lovely St. rarely stop before making "right on red", nearly colliding with cars
114466376288	Nov 17 2023	1	turning left from Lawton.
			Dunham Road should be one way only. There are no sidewalks yet there is a school and a
114465807328	Nov 16 2023	0	fitness studio that use that road daily for walking.
114464853017	Nov 15 2023	0	The Canton Village exits are dangerous for pedestrians.
			At the crossing going onto Lawton, cars that turning right onto Lawton seldom yield for cyclists
114463788268	Nov 14 2023	0	when using the crosswalk.
			Biking along route 44 is narrow with no bike lane, and drivers regularly exceed the speed limit
114463468212	Nov 14 2023	1	due to the road design.
114460727990	Nov 10 2023	0	Unappealing throughout due to lack of significant bike lanes/walking area
114460538555	Nov 10 2023	1	Almost all
			I have run up/down the hill between the shoppes and the Simsbury town line on 44. It is very unsafe but to avoid it you have to take a 4 mile detour on Lawton road to west Simsbury then
114460361447	Nov 10 2023	0	back down west mountain road to the Simsbury commons.

All of them, every crosswalk and every available sidewalk. Being on any sidewalk on US-44 is similar to walking on highway with cars whizzing by you. There is not one safe place to cross US-44. □

Dowd Avenue is incredibly dangerous to walk along or try to cross. It needs sidewalks on both sides.

I feel that the entire stretch in the study area is VERY unsafe for bicycling. There is NOT enough shoulder space to safely pass thru.

All of them, use the paths and not the roads...

44 / lawton road I would not cross to walk or bike
RT 179 and RT 44 □

RT 44 and RT 177

The entire stretch of road in question.

Yes. Rr 44 and Lawton Rd.

All of it

any of the intersections that allow Right on red or have now provisions for walk signals

Rt 44 and East hill. People speed and go through the red light almost daily

All of 44 due to proximity to the road and lack of shoulder in most places. 44 @ Dowd ave is not a friendly intersection to motor or foot traffic

There is no way for pedestrian crossing of 44 at Dowd Avenue. Due to volume of vehicle traffic, pedestrian cross walk at Commerce Drive is best option, but not recognized by drivers very well.

There should also be a separate turning lane west bound at 44 and Dowd for vehicles.

from downtown Collinsville to route 44

Areas with no sidewalk

Dowd ave

The section of 565 between Thayer and Atwater has no safe pedestrian or bike access

114457646138

Nov 08 2023 0

114456430389

Nov 06 2023 0

114455913214

Nov 06 2023 0

114455892829

Nov 06 2023 0

114448747468

Nov 05 2023 0

114455424445

Nov 05 2023 0

114455234431

Nov 05 2023 0

114455198803

Nov 05 2023 0

114454958131

Nov 04 2023 0

114454783657

Nov 04 2023 1

114454776818

Nov 04 2023 1

114454708203

Nov 04 2023 0

114454701751

Nov 04 2023 0

114454683084

Nov 04 2023 0

114454673162

Nov 04 2023 0

114454608354

Nov 04 2023 0

			US44 is not safe to walk or bike on. It's designed for more motor vehicle traffic. It is a heavily traveled thoroughfare. Why would you ever put pedestrians and cyclists on it? Canton, like most of the USA has been zoned for the motor vehicle. If canton has a real vision for making the town pedestrian friendly there would be sidewalks in Case St and a mom and pop store on the corner for when I need a load of bread or the kids wanted to spend their allowance on a candy. Forget it! Those days are long gone and they ain't coming back. So why not stop raising our taxes on crazy projects for a handful of loud mouths that want to cycle on a busy thoroughfare like rte 44.
114454504172	Nov 03 2023	1	Honestly, I don't get it.
114454462371	Nov 03 2023	0	I don't consider this a safe area. I'd stick to the paths if I was waking. It's somewhat tough to walk down that area because of how noisy and tight it can get near the cars. The traffic lights and sounds make for a car-centric experience. Would be awesome to
114454460026	Nov 03 2023	0	make it more walking-friendly! The light at Lovely and Lawton Rd can be tricky for bicyclists. And those of us driving alongside
114454450494	Nov 03 2023	0	them.
114454428742	Nov 03 2023	0	The 44/Dowd intersection and am from the Shops to East Hill along 44.
			While there are crossing lights at 177/44/Lawton Road intersection, it is a dangerous intersection and hard for pedestrians and riders to cross. The crossing by the Shoppes entrance
114454311668	Nov 03 2023	0	is even worse - people ignore walkers. Sidewalks are needed the length of the study area.
114454312233	Nov 03 2023	0	Commerce and Dowd Avenue
114454299033	Nov 03 2023	0	The gazebo
114454262661	Nov 03 2023	0	None of them feel especially safe because it's such a busy main road.
114454191773	Nov 03 2023	0	the entire area
114454155029	Nov 03 2023	0	Better sidewalks, speed bump on Dowd as it turns into Maple. □ route 44 in general especially from the Shoppes East to Simsbury□
114454139460	Nov 03 2023	0	
114454023704	Nov 03 2023	1	Intersection of Dowd and Canton Springs Rd
			Intersection at the Canton Green and Dowd (RT565) is a very dangerous one especially with the light travelling West near the Nursery always staying green which allows for vehicles to pick up
114453971169	Nov 03 2023	1	speed and travel at excessive speeds up to 50-60 miles per hour

			All crossing areas can be improved which include rt 44 intersection with Lovely Street, Dowd Ave., The Canton Plaza (not to mention that there is not a way to safely get from business to business on the north side of route 44, Old Canton road and several intersections to the west of
114453961135	Nov 03 2023	1	this study
114453938138	Nov 03 2023	1	Most
114453932863	Nov 03 2023	1	I dont know any.
114453801455	Nov 03 2023	0	crossing 44 is lethal due to the speed cars travel unless you are at an intersection. drivers cannot be trusted at any crosswalk either
114453821804	Nov 03 2023	0	From the shops on, particularly on the north side of the road. Sidewalk is nonexistent and the traffic can be really heavy at the light with the intersection with lovely street.
114453812032	Nov 03 2023	0	N/a
114453806903	Nov 03 2023	0	Crossing the street near the Farmington Valley Shops
114453806472	Nov 03 2023	0	All of it. Yes, there needs to be more signs posted on the trail asking bicyclists to announce when passing pedestrian's on the trail. I only see one sign posted on the trail near the firehouse. I feel unsafe due to the speed in which some bicyclists pass me in such close proximity and don't realize there coming up on me until they're right next to me, I may have started to veer off trail and been struck. Many pedestrians feel this way. It would also to see a police officer on bicycle
114453790792	Nov 03 2023	0	patrolling the area
114453747160	Nov 03 2023	0	44/Lawton/Lovely
114453735212	Nov 03 2023	0	Crossing Dowd Ave to get to the sidewalk
114453732080	Nov 03 2023	0	The area is just too busy for walking or riding bikes safely.
114453725352	Nov 03 2023	0	nope
			intersection of Dowd and Simonds/East Hill; it's awkwardly shaped and is not always optimal as a driver for visibility of pedestrians. Intersection of 44 and lovely street; people take a right onto 44 from lovely street and there's no visibility or the crosswalk signal is flashing so I believe there
114453725032	Nov 03 2023	0	should be a "no turn on red" sign there, like there is from Lawton onto 44.
114453635527	Nov 03 2023	0	No

			<p>The area in question is heavily traveled most of the day and is made more hazardous with steadily increasing pass-through commuter traffic. Although that situation is beyond town control, decisions for adding the recently approved developments were not, and will exacerbate the existing over congestion. Regardless whether this is a serious impact study, it certainly did not take account of the foregoing. □</p> <p>Sadly, it's difficult to anticipate positive outcomes from town plans given the long standing state of current conditions. Have we forgotten that Rte. 44, and increasingly Dowd, were not always used as race tracks, apparently with impunity. □</p>
114453558589	Nov 03 2023	0	Difficult to get into shops around Tower Farms, Luke's donuts. Getting across 5 way
114453572549	Nov 03 2023	0	intersections at Lovely Street/Rt 44 No bike lane on Dowd or 44, but the Farmington River Trail works well to get from the center of Collinsville to the corner of Lovely Street. □ □
114453572004	Nov 03 2023	0	There is no crosswalk at Canton Springs Road across Dowd Ave, so it's difficult to walk from the trail to businesses on the other side of Route 44. The US-44/Dowd Ave area has many existing crosswalks and sidewalks, but the difficulty is getting to these sidewalks from residential areas that do not have sidewalks. If my subdivision provided sidewalks, then at least I could safely get to the existing sidewalks/crosswalks in the
114453503720	Nov 03 2023	1	US-44/Dowd Ave area.
114453488043	Nov 02 2023	1	Area around giv coffee, people walk and cross 44 often. Very dangerous All the places that don't have a sidewalk. No one stops and everyone drives 10-15 over the
114453467184	Nov 02 2023	1	speed limit. There is no way to walk from □ The canton river trail by Lawton and shop at the stores across from the ski shop. There are □ No sidewalks. It's very uncomfortable to walk from Luke's donuts to the bagel cafe. There is a huge amount of congestion because of the bagel shop and the bagel shop has too many people and no turn around in □
114453438260	Nov 02 2023	1	The parking lot. No safe sidewalks. crossing intersection at 44 and Dowd avenue, walking on 44 from Dowd to Lovely St. Crossing
114453440531	Nov 02 2023	1	44 at any pont

			I live on Dowd on the opposite side of the road from Canton Village. Other than walking in the dirt on people's lawn or by the side of the road and crossing multiple times there is no way for me to safely get to either the Village or the Shoppes or any other business. The blinking lights were a nice idea. But, I don't think they are that effective based on the speed and volume of traffic on my road. If I were to try to cross at Canton Green having crossed over to the sidewalk opposite my house I have to contend with that mess of three way traffic at the light at the end of the street. I have no faith whatsoever that I would not be run over from any one of the directions. People can hardly keep from running into each other in their cars. A person has little
114453428783	Nov 02 2023	1	chance on foot! Dowd has very little sidewalk. I wouldn't feel safe walking on mist of it. 44 has no sidewalk until it
114453433904	Nov 02 2023	0	reaches the Mobil. The sidewalk areas I would feel safe.
114453419298	Nov 02 2023	0	Too much traffic
114453417595	Nov 02 2023	0	Near the green
114453398895	Nov 02 2023	0	N/A
114453385266	Nov 02 2023	0	Bike trail crossing at 44 is not safe
114453407042	Nov 02 2023	0	N/A
114453398513	Nov 02 2023	0	the entire route The turn on to Dowd from 44 is awful. The intersection on Dowd with a left on to Canton Springs
114453379481	Nov 02 2023	0	Road is extremely dangerous both ways.
			There are no crosswalks on 44 other than at the Lovely street intersection. There is no sidewalk
114453362884	Nov 02 2023	0	for much of the north side of Rte 44. The sidewalk on Dowd is narrow and in terrible shape. Us 44 is just too busy to walk or ride a bike. Right now I feel very safe walking or riding a bike.
114453326920	Nov 02 2023	0	However if traffic increases I would feel the same as I do about us 44.
114453324756	Nov 02 2023	0	Dowd Avenue, East Hill crossings From Atwater to Mills is treacherous. Otherwise I mostly stick to sidewalks and trails. I don't like
114453321045	Nov 02 2023	0	biking on Dowd or 44 at all.
114453317415	Nov 02 2023	0	The corner of Canton Springs Rd. and Dowd Ave. needs a traffic light.
114453316182	Nov 02 2023	0	No 44&179□
114453304325	Nov 02 2023	0	202 &179

114453305682	Nov 02 2023 0	44/Dowd intersection, 44/177 intersection
114453294339	Nov 02 2023 0	Rt.44 and Lovely St.
114453274112	Nov 02 2023 0	no Route 44 intersection with route 177 is extremely unsafe. Lawton and Washburn are narrow roads with a lot of traffic. □ Bicycling and walking is extremely unsafe. With the future development plans it will become
114453266461	Nov 02 2023 0	more dangerous.
114452840128	Nov 02 2023 0	Unsafe: Canton Springs Rd - Dowd Ave - Canton Green Rd intersection. Unappealing right onto 44 from Dowd, left onto Dowd from 44 □ □
114453253035	Nov 02 2023 0	Left onto 44 from Secret Lake Getting from Old Albany Ave to ShopRite is really hard as there are no sidewalks. I often walk
114453250017	Nov 02 2023 0	down Old Albany and then walk through private property instead. The crossing near CVS/Vincent Funeral Home, the area around the green/Union Savings Bank
114453187191	Nov 02 2023 0	& the area where East Hill Rd. meets Dowd ate all particularly awful.
114453237657	Nov 02 2023 0	Left turn from 44 west onto Dowd Ave. could benefit from a dedicated turn lane
114453221684	Nov 02 2023 0	NA The entire area is a pedestrian nightmare. □
114453208089	Nov 02 2023 0	
114453212532	Nov 02 2023 0	All of it. It is not set up for people at all.
114453190964	Nov 02 2023 0	Intersection Dowd/Commerce Intersection of Canton Spring Road onto Dowd is treacherous. Traffic jams up and emergency
114453157527	Nov 02 2023 0	vehicles cannot get thru The Lawton Rd portion of the Farmington River Trail is not safe at all. I ride along that part to
114453175292	Nov 02 2023 0	get to the trail head on the other side of 44. I avoid 44 and 565 completely on my bike. I feel that the area down by Dunkin Donuts on Rt 44 is very unsafe - both for cars turning left out
114453172966	Nov 02 2023 0	of DD and for anyone wanting to walk or bike in that area. Certain times of day in the area of rt 44 and Dowd ave get congested making it difficult to visit
114453162432	Nov 02 2023 0	the shops and restaurants.
114453159706	Nov 02 2023 0	Any and all crossings on rt. 44 and Dowd
114453156794	Nov 02 2023 0	Crossing 44 anywhere

		Honestly most of it. The intersection with lovely is so large I will not walk on it with my family.
114453156359	Nov 02 2023 0	Anywhere with no sidewalks is an immediate no-go for me.
114453150267	Nov 02 2023 0	no
		Lawton road intersection. It's hard to come up with ANT area that is safe fir walking or biking. I
114453146442	Nov 02 2023 0	would not do either in Route 44.
114453143027	Nov 02 2023 0	US-44 and 179
114453104033	Nov 02 2023 0	They have added a crossover on Diwd which is good.
114453128279	Nov 02 2023 0	No
114453108169	Nov 02 2023 0	The two crossings of the Bike Trail are very unsafe. No one stops for the blinking lights.
		Intersection of Dowd and Route 44 traveling East and intersection of Dowd and Canton Springs
114426498018	Nov 02 2023 0	Road. Motor vehicles traveling East along Dowd are traveling much too fast for safety.
114453101558	Nov 02 2023 0	44-Dowd is dangerous, 44-canton springs rd is dangerous
114453094473	Nov 02 2023 0	None
		Just the crosswalks - nobody stops when you are at a crosswalk and even when there are lights
114453084661	Nov 02 2023 0	flashing!
		The roads near the green, Canton Springs road. Cars are driven by very fast, there are more
		than 1 way for cars to cut through to dowd ave causing congestion, accidents. I saw many
114453084582	Nov 02 2023 0	accidents in those intersections over the years. Narrow entry/exit
		The pedestrian cross walk at the i;ntersedtion of Dowe Ave. and Commerce Drive is the worst
		design I have ever seen. It doesn't serve the pedestrian traffic at all and makes them cross both
		roads in a very unsafe manner. Redesign to move the crossing of Dowd westerly to avoid
		pedestrian having to cross Commerce Street from a position of NO sidewalk to the existing
		Sidewalk on the west side of Commerce. (no brainer) Please address this existing problem, I've
		witnessed numerous close call during my personal walks from Dowd to Commerce Drive. It's
		important to note that the majority of pedestrian traffic is heading eastbound to turn right onto
114453018261	Nov 02 2023 0	Commerce Drive.
114453082237	Nov 02 2023 0	No
114453051689	Nov 02 2023 0	44 and Lovely- Lawton Rd. Way too many accidents and speeders.
		the rt44/rt177/trails intersection☐
		the Dowd/rt 44 intersection☐
		the village is absolutely a nightmare!☐
114453050714	Nov 02 2023 0	

114453041230	Nov 02 2023 0	All of them
114453041044	Nov 02 2023 0	Intersection of Rt44 and Doud
114453027409	Nov 02 2023 0	there s/b a better way to cross at 44/177. could we go Over or Under that intersection instead? The intersection of Canton Springs Rd and Dowd Ave is dangerous. Depending on the time of day it is difficult to take a right or left hand turn onto Dowd Ave. I have witnessed a number of
114453029417	Nov 02 2023 0	accidents .
114453029043	Nov 02 2023 0	No
114453022950	Nov 02 2023 0	Route 44 Near the gazebo, also impossible to visit businesses, such as blue house bagel and Luke's
114453019483	Nov 02 2023 0	donuts. Traffic won't let you in or out
114453017020	Nov 02 2023 0	no
114453011737	Nov 02 2023 0	No, I can't think of any specifics
114453007390	Nov 02 2023 0	44 is ok for biking if I use the sidewalk- however after the shoppes at Farmington valley there is really not a safe wall to walk or bike- essentially you can't bike past there to go east. As a motorist, I get apprehensive especially at the US-44/Dowd Ave and US-44/Lawton Rd intersections when it comes to pedestrians and bicyclists. Those spots are congested at times, and I worry that: 1) I won't see non-car traffic, 2) the amount of space available for side-by-side non-car and car traffic is tight, 3) car traffic speeds are not conducive to a safe roadway for all in
114452998765	Nov 02 2023 0	those areas.
114452993128	Nov 02 2023 0	Dowd and Rt 44 intersection
114452977712	Nov 02 2023 0	There is no sidewalk or bike lane along US-44
114452978165	Nov 02 2023 0	The entire 44 corridor is treated by drivers as if it was a highway like I84 or I91
114452969831	Nov 02 2023 0	Canton Springs Road

		Walking or biking anywhere on Route 44 through the study area that isn't sidewalked is unsafe and unappealing. I see many pedestrians navigating that stretch along the narrow shoulders, occasionally a biker- it always looks hairy. □
		□
		The transition from the Shoppes to Route 44. When biking, We use the Shoppes fire access road to reach the sidewalk on 44 to avoid the main entrance to the shoppes. □
		□
		The US 44/Dowd Ave intersection is complicated for some drivers that may be unfamiliar with it, and too hectic for us when we're biking. We use the Greenway trail to access points West of the
114452956256	Nov 02 2023 0	shoppes
114452967598	Nov 02 2023 0	Areas without sidewalks
114452964101	Nov 02 2023 0	There is barely a shoulder on rt 44 to bike safely on.
		The insane crosswalk at Commerce which requires crossing □
114452961431	Nov 02 2023 0	three streets instead of one!
		There is not a single section of 44 I would feel comfortable walking or biking - the road is not
114452962422	Nov 02 2023 0	wide enough and travel is too fast to allow for safe bike travel.
		Light near Gazebo, Dowd Avenue and 44. I was first at the red light. It's only a right hand turn. I waited. Looked up at the light and it turned green. I was about to turn right when a white car on 44 zoomed pass me heading toward the Shops. There could have easily had a very bad
114452942614	Nov 02 2023 1	accident.
114452959280	Nov 02 2023 1	no
		Walking along the trail is terrific, and I do that frequently. Walking anywhere along 44 feels risky. There are few sidewalks. While I don't bike, friends and family who do generally take the trail as far as 44 and stop there because crossing is risky and there's no real safe place to ride
114452950981	Nov 02 2023 1	north of 44.
114452952199	Nov 02 2023 1	any crossing across Rt44
		Intersection of Dowd ave and Canton Springs road is always very busy and hard to cross. I feel they are all unappealing to me. I would not take any crosswalks in that area. It is a race track. I feel there should be a center lane to turn R/L just like in Massachusetts. Lots of
114452921121	Nov 02 2023 1	businesses are cut off due to fast pace traffic.
114452914982	Nov 02 2023 1	East Hill and 44, East Hill and Dowd Ave, Spring St and Dowd Ave, Dyer Ave and 44.
114452931971	Nov 02 2023 1	Anywhere on Rt 44.

			East Hill and 44 ☐
			Dowd and 44 ☐
114452933085	Nov 02 2023	1	The shops and 44
114452917176	Nov 02 2023	1	Maple Ave between High/Dyer and its intersection with Dowd I would like to walk between shops on 44 but the sidewalk is only on one side of the street and there are not enough crossing opportunities. Navigating the intersection of 44/Lovely, where the
114452919906	Nov 02 2023	1	trail ends, can be tricky on a bike or on foot.
114452920211	Nov 02 2023	1	The junction of the Rail to Trail and 44 is dangerous. I bike across often on the trail The entire area is unsafe for biking and walking w/o proper bike lanes or wider sidewalks. The Dowd/Canton Springs intersection is also very unsafe given the congestion and how narrow the
114452917862	Nov 02 2023	1	roads are there.
114452908938	Nov 02 2023	1	Crossing Rt 44 anywhere but especially at Lovely St, All crosswalks are unsafe. Always at least one car runs the red light. Many ignore blinking
114452908395	Nov 02 2023	1	yellow light.
114452909259	Nov 02 2023	1	Intersection of East Hill Rd. and Route 44.
114452911726	Nov 02 2023	1	No; as long as one pays attention, one shouldn't have any problems.
114452906923	Nov 02 2023	1	Can't think of any
114452908567	Nov 02 2023	1	All All intersections and driveways are dangerous due to the level of speed and short distances to
114452892896	Nov 02 2023	1	stop or slow for cars.
114452892509	Nov 02 2023	1	Any crossing 44
114452889884	Nov 02 2023	1	The 44/Dowd connection. Dowd ave,east hill,simonds ave hard to cross ☐ canton springs road access in and out ☐
114452887747	Nov 02 2023	1	
114452817216	Nov 02 2023	1	From 177 to canton shopping plaza (Apple gate store)
114452884241	Nov 02 2023	1	Rt 44 is terrible from Lovely Street to RT 179 but the path takes care of that. I feel the area is safe, but with increasing traffic, there are many more unsafe or distracted drivers. Too much over-development of the area. The town needs to slow down and strive to
114452877244	Nov 02 2023	1	keep Canton a small and charming village it has always been.
114452881122	Nov 02 2023	1	No
114452881986	Nov 02 2023	1	Washburn and lovely street right near cvs, best cleaners, and UConn health

114452866994	Nov 02 2023	1	Anywhere on US44 is not good for walking. Dowd Ave has some path which is acceptable.
			Crossing from Canton Spring road to Dowd can be difficult. The walk crossing Dowd from Commerce should be straight accross to match the sidewalks. The walk signal at Lovely street seems like it could have it's own signal during the cycle for when cars on 44 are going east and
114452831654	Nov 02 2023	1	west there are no turns allowed to Lovely from the area between the two walk signals.
114452856222	Nov 02 2023	1	NEITHER 44 NOR MOST OF DOWD ARE WALK OR BIKE FRIENDLY. Atwater/Dowd/Maple ave needs a cross walk with lights.□
114452862581	Nov 02 2023	1	speed limits need to be enforced, especially during school bus hours.
114452860286	Nov 02 2023	1	Anywhere along Dowd Avenue
114452859739	Nov 02 2023	1	Cherry Brook Road Intersection at 44 and Lovely Street- cars typically run red lights. Cars often switch lanes suddenly in anticipation of green turn arrow back up at light at 44 and Dowd avenue - lots of
114452856833	Nov 02 2023	1	close calls and accidents All traffic travels WAY too fast. Little to no patrols don't help. Route 44 from East Hill Road West
114452858345	Nov 02 2023	1	is literally a speedway. The whole road is busy, but it's very unsafe at the left turn from 44 on to Dyer. I am concerned about additional issues when the new apartment building is occupied. There should be no
114452848836	Nov 02 2023	1	turning allowed there.
114452844707	Nov 02 2023	1	There needs to be a side walk along 44 by Cape cod fence, and those brown stone buildings The area The Shoppes at Farmington is scary crossing the street if you want to walk over to Aldi or the CVS. Also the area Canton Green is pretty scary with very small sidewalks very close to
114452836379	Nov 02 2023	1	the road where people tend to go fast.
114452844132	Nov 02 2023	1	Not really a walkable area The old grocery store sat far too many years decaying. Take it down (entertainment or Trader Joe's will liven it up). Make the entrances more inviting andsafer. The tenants satisfy my walk to
114452812359	Nov 02 2023	1	needs. Renovations needed but PLEASE keep the small town charms of our area.
114452840156	Nov 02 2023	1	Rt 44 is a racetrack from rt 179 to the Simsbury line. Very unsafe stretch of road
114452837068	Nov 02 2023	1	River road section
114452815192	Nov 02 2023	1	From the trail west towards Canton Village. Hard to get to places like Luke's safely

114452822369	Nov 02 2023	1	<p>I own all parcels from 163 Albany turnpike to 175 Albany Turnpike. This is the area of the Dowd Ave intersection and is all parcels on the north side of the intersection . I have owned them for 40 years. The neglect that the state has shown for sidewalks in that area has resulted in numerous fatalities- I have witnessed first hand. To make things safer I have unilaterally closed four of our seven driveways and removed the associated driveways. Twenty years later the state hasn't even repaired the curb cuts. From time to time a car entering rt 44 from Dowd Ave will enter one of the curb cuts and hit our apartment building or fence that we have constructed. There is no sidewalk other than remnants of asphalt at those curb cuts. It is highly dangerous. I witnessed one fatality of a pedestrian. I did what I can bur the problem is so obvious even with a high speed driveby that it makes me doubt the state's commitment to safety. Steven Stang owner 860-966-3172</p>
114452826617	Nov 02 2023	1	<p>From Lovely Street west, it seems undesirable to walk or bike as even in the places where there is a sidewalk, it is very close to the road and speeding cars and cars don't seem to yield to pedestrians when turning into businesses</p>
114452820310	Nov 02 2023	1	<p>Crossing 44 at Lawton Rd intersection of Allen Place and Dowd, crossing at Dowd Ave by the Bridge or any other area</p>
114452823678	Nov 02 2023	1	<p>people attempt to cross the street.</p>
114452825015	Nov 02 2023	1	<p>No</p>
114452821466	Nov 02 2023	1	<p>All. These areas are not currently pedestrian/bike friendly. They prioritize cars. There needs to be protected sidewalks. We should be following Vision Zero and prioritize pedestrians with Complete Streets.</p>
114452822004	Nov 02 2023	1	<p>Commerce dr pedestrian light approach is too short to safely walk. Road speed too fast</p>
114452817978	Nov 02 2023	1	<p>The area near the gazebo..</p>
114452818765	Nov 02 2023	1	<p>Rte 44 is a traffic nightmare! It is the most congested and unsafe road I drive. Stop building and causing this traffic.</p>

			Us-44 is not appealing for biking. the fast moving cars and many entrances into businesses along with intersections makes it challenging. ☐ ☐
114452799301	Nov 02 2023	1	As far as Dowd, there is little shoulder for biking, specifically in the section of maple ave to Dowd ave. right before reaching the Farmington trail, there are narrow streets, little to no biking space and due to trees, brush which also causes broken branches on the road, biking is dangerous there.
114452811951	Nov 02 2023	1	It makes no sense that DOT removed the Simonds Ave part of the 901 bus route. It is DANGEROUS to walk or bike to the Cherry Brook Park and Ride or Shoppes bus stops to pick up the bus. An IMMENSE disservice to those who live and work along the Simonds and Dyer Ave corridor.
114452777956	Nov 02 2023	1	Would love the intersection connecting the bike trail to be a little safer for bikes and pedestrians. I would utilize my bike for travel more often if this were the case. I have a five year old and live up the "mountain"
114452806990	Nov 02 2023	1	From the Dowd/44 intersection to the Lawton Road/44 intersection traffic is far too congested and turning across traffic into shops is extremely difficult.
114452807761	Nov 02 2023	1	Dowd ave needs work done. Speeding is out of control. People drive into oncoming. Speed bumps should be placed on the street!
114452803735	Nov 02 2023	1	I wouldn't feel safe walking or biking in the area unless there was a dedicated bike/pedestrian path. My experience in trying to walk along Rt 44 in Avon shapes my opinion intersection of Lawton Road ☐
114452800530	Nov 02 2023	1	intersection around Canton Green
114452799749	Nov 02 2023	1	VERY DANGERIOUS INTERSECTION DOWD AVENUE AND RT 44
114452796370	Nov 02 2023	1	Need sidewalks from Lawton Rd. to Dowd Avenue on the NORTH side of 44. Need sidewalks on BOTH sides from Secret Lake Road to the Simsbury line (and beyond in Simsbury).
114452797765	Nov 02 2023	1	Dowd and 44 all the way to lovely street and 44 are extremely dangerous for pedestrians and drivers. The amount of collisions there is insane to say the least. The entrances to businesses on that section of the road are extremely dangerous, and something needs to be done. These accidents take up too much time for our emergency services, and I genuinely dont feel safe in this area in a car, or as a pedestrian or biker on the sidewalk.

114452785733	Nov 02 2023	1	The traffic light at 44 and Dyer is not very respected by motorists
114452799695	Nov 02 2023	1	Intersection of Lovely Street & Rt. 44 There are limited places to safely cross 44 on a bike so I drive. I would bike if there was a safe
114452793546	Nov 02 2023	1	way to bike along 44. US-44 is now more like a thoroughfare. Not enough room to walk / bike, safely, and the
114452794221	Nov 02 2023	1	carelessness of drivers makes it worse.
114452796285	Nov 02 2023	1	The Lawton Road intersection seems dangerous to me. Dowd intersection is tough for visibility with traffic coming up 44 (driving towards Avon). Plus
114452798446	Nov 02 2023	1	with the new "apartment" monstrosity there will be even more traffic
114452794458	Nov 02 2023	1	All. Not enough dedicated safe space for people walking, jogging, bicycling, etc. There are far too many access driveways along this stretch... too many options to enter/exit
114452792088	Nov 02 2023	1	stores. Vehicles try and "beat" others to get to the road or their destination.
114452793897	Nov 02 2023	1	No
114452796827	Nov 02 2023	1	No
114452792390	Nov 02 2023	1	Trail crossing
114452788242	Nov 02 2023	1	Intersection of 44 and East Hill. Often run thru from the west Canton Valley Circle which is where I live, is getting harder and harder to take a left hand turn - traffic has probably doubled in the 17 years we have lived here. My real concern is the new apartment developments on RT 44 down toward 202 interchange - and the new housing development that will be built on Lawson Road by the CVS/UCONN health care buildings - I
114452788666	Nov 02 2023	1	believe all of this new housing will only increase the traffic on RT 44. the bike trail crossing is a disaster, speeding cars and too many intersected roads at one point
114452789772	Nov 02 2023	1	to be safe
114452793730	Nov 02 2023	1	No Yes, the intersection of 44 and Dowd, and the intersection of 44 and Lovely Street are horrific
114452789296	Nov 02 2023	1	for pedestrians. It's also pretty bad trying to cross 44 at Canton Village.
114452788854	Nov 02 2023	1	Corner of US-44 and Dowd Ave The cross walk and light system you have at the entrance of Maple Glen is a HUGE PLUS!!!
114452782545	Nov 02 2023	1	Thank you!
114452787267	Nov 02 2023	1	No
114452787126	Nov 02 2023	1	Most all areas

114452784828	Nov 02 2023 1	Excessive speeding, automobiles not obeying traffic laws, cyclists not obeying traffic laws, little to no police presence.
114452783918	Nov 02 2023 1	To the west of the Dowd/44 intersection - there is a lack of sidewalks
114452781628	Nov 02 2023 1	No, plenty of sidewalks to bike or walk safely
114452784734	Nov 02 2023 1	Crosswalk @ Post Office Square
114452782105	Nov 02 2023 1	Improvements that include pedestrian safety and access by foot and bike is needed in this corridor.
114452781019	Nov 02 2023 1	It is unsafe taking a left hand turn from the businesses in that area, especially green papaya and Davidson Chevrolet.
114452780822	Nov 02 2023 1	Intersection of Dowd and US-44 is not safe for walking, biking, or driving a car. ☐
114452780846	Nov 02 2023 1	Intersection of US-44 and 179 is unsafe for bikes and walking.
114452780231	Nov 02 2023 1	Rt 177 and Rt 44 intersection. Often people take a right on red from either Trailsend Drive or Lawton Road, even though there are signs stated No Right on Red.
114452700078	Nov 02 2023 0	All
114452611389	Nov 02 2023 0	Intersection of canton springs and Dowd is very unsafe for drivers, poor viability, especially in the AM when traffic builds and blocks intersection. It is also one of the trickiest parts to walk through.
114452600525	Nov 02 2023 0	The area on Dowd between Canton Springs Rd and Rte 44 could use some improvement. Also the stretch of sidewalk from Lovely Street intersection and down towards Canton Village and beyond, feels very close to traffic and needs sidewalk repair in some spots
		Walking in trail can be dangerous with bikers ignoring to announce 'on your left' and using the path as a raceway. ☐
		☐
114452471775	Nov 02 2023 0	Several intersections on route 44 are dangerous due to motorists ignoring traffic lights turning red—44 and East Hill; 44 and Lovely Street.
114452434151	Nov 02 2023 0	the corner of US-44 and Dyer Ave feels very dangerous... and even hard to turn onto Dyer by car. Stop Lights there? Sidewalks on 44... and along Cherry Brook Rd. would be a huge plus.
114452390924	Nov 01 2023 1	Bicyclists and walkers down Cherry Brook Rd. with no sidewalks is always an issue.
		I can't believe this is being discussed. Canton is not a city.

			Crossing Dowd at all is unsafe. People drive so fast and no one pays attention. No good places
114452385542	Nov 01 2023	1	to cross 44 that ever real safe
114452373909	Nov 01 2023	1	walking / biking on 44, crossing dowd
			All areas. Vehicles drive fast, shoulders are narrow, sidewalks are too close to curbs/roads,
114452357431	Nov 01 2023	1	there are too many driveways where cars and walkers/bikers can conflict.
			Anything on rte 44 is horrible to bike on. As a cyclist for almost 30 years, 20+ of it in canton, I
			can say I have only road on 44 to go from a cut thru road from Dowd to 44 to Benidorm bike
114452346141	Nov 01 2023	0	shop, traveling approx 100 yards on 44. 44 is too dangerous as it is for pedestrians
			The entire area is dangerous due to sloppy and speedy driving. I avoid the area as much as I
114452344763	Nov 01 2023	0	can
114452310853	Nov 01 2023	0	Just the drivers.
			All of them. Cars don't stop at lights or stop signs. They pay little attention to crosswalks lighted
			or not. Speeding is out of hand , including tailgating, cell phone use, texting and there is almost
114452252747	Nov 01 2023	0	no police presence in town.
114452231108	Nov 01 2023	0	Any of them if I am staying on 44. If crossing, then it can be OK
114452175232	Nov 01 2023	0	NA
114452149115	Nov 01 2023	0	Nope
			Between the intersection and the beginning of Dowd Ave. lots of traffic, too many businesses
114452092170	Nov 01 2023	0	with no stop lights.
114452084449	Nov 01 2023	0	Canton Village area
			Rt 44 177 - bad sight lines from 177 taking a right on rt 44. Too many curb cuts off rt 44 to
114452039393	Nov 01 2023	0	businesses.
			Near the Dunkin on 44. I wish there were side walks on 44 near Indian hill so we could bike
114451995011	Nov 01 2023	0	places instead of drive
			The end of Dowd going onto Route 44 and taking any left turn to any business off 44 due the
114451966667	Nov 01 2023	0	speed of vehicles.

				I work at a nonprofit (FOCUS Center for Autism) and have to walk autistic students between 44, Dunham, and Dowd Avenue - lots of speeding and reckless drivers. We've witnessed a fatal accident at our special education school during operating hours at our Dowd ave location between a city bus and a sedan. ☐
				Canton Springs often has accidents and reckless drivers. ☐
				We also have an office on the corner of Albany TPKE and East Hill Road. At the office we have witnessed many serious accidents - also one fatal to a dog - and see people blowing through the intersection almost daily. Overall, all three FOCUS locations are in highly unsafe driving areas. The Canton Shops are also a nightmare to navigate to and from. There is no really desirable way for our students to get walking exercise or transport each other without being on high alert. It is nearly impossible to make a left turn out of Dunham to get to our secondary
114451948907	Nov 01 2023	0		school location or office on Albany Turnpike.
114451880385	Nov 01 2023	1		Most of them
				The bike trail crosses 44 at Lovely Street and I see confusion and tentativeness among bicyclists when Im at the red light. ideal would be a bicycle pedestrian cross over bridge,
114451687049	Nov 01 2023	0		admittedly at great expense, but a huge enhancement to use of the trails.
114451639512	Nov 01 2023	0		Intersection of 44 and East Hill
				The intersection of Lawton Rd. w/ Rte 44 is especially congested and unsafe for biking or walking. Many accidents happen here. Also, the intersection of 44 and East Hill Rd. is
114451556481	Nov 01 2023	0		treacherous because of the number of cars running the red light there.
114451512548	Nov 01 2023	0		NA
				The entire roadway has no pedestrian amenities. It is a state highway not a "village." There is
114451288308	Oct 31 2023	0		tremendous vehicular traffic, too many traffic lights.
114451165853	Oct 31 2023	0		Lawton road, all along 44
				Never attempt to cross Route 44☐
				Crossing Dowd Avenue is very risky. Majority of vehicles traveler over the speed limit and cell
114451080969	Oct 31 2023	0		phone distraction is high.
				Commerce & Dowd. Heading northbound on Commerce the street is steep and allows cars to speed through the bike/pedestrian crosswalk. I was hit by an automobile while riding my bike at
114451012505	Oct 31 2023	0		that intersection and suffered serious injuries
114450960456	Oct 31 2023	0		Crossing rte 44 especially is a nightmare

114450866918	Oct 31 2023 0	I would to see more lightning along 44 and the intersection. With all the businesses there are a lot of people walking and can be hard to see them
114450848880	Oct 31 2023 1	No
114450746721	Oct 31 2023 1	Though crossing lights are in place, the junction of 44/Lawton/Trailsend and 167 is PLAGUED by people turning right on red despite signage to the contrary. I live on Trailsend Drive and turning into my street or using the crosswalk is treacherous because drivers from Lawton Road pay no attention to the No Turn on Red Sign. I have reported this to Canton PD. 44 is a large, busy road, and gets worse with every new development. It's just not a place friendly to...ANYthing- cars, pedestrians, bikes, whatever. With the town of Canton trying to develop the area into the nightmare of Rte 1 in Milford, it's never going to be a particularly appealing place to recreate, so there's not much point in trying to modify a certain place or two
114450684323	Oct 31 2023 1	(in a lame attempt to help.
114450621478	Oct 31 2023 0	That entire stretch is very unappealing and unsafe. There is no where to walk.
114450355952	Oct 31 2023 0	Dowd/44 and 177/44 intersections are not pedestrian or bike friendly. Crossing 177/44 as part of the Farmington river trail is confusing, chaotic, and dangerous. The corridor is in desperate need of traffic calming measures, beautification in the spirit of a quaint New England town, easier/safer business access, and a connected network of sidewalks.
114447091547	Oct 30 2023 0	No
114449637714	Oct 30 2023 1	Route 44 and Route 179 intersection
114449125679	Oct 29 2023 0	Most
114449082928	Oct 29 2023 0	Both intersection of Dowd Avenue with Canton Springs Road and Dowd Avenue with Route 44 are dangerous. No sight line when trying to make left (west) turn from Canton Springs Road onto Dowd in AM and PM rush when eastbound traffic on Dowd is backed up across Canton Springs Road intersection. Extremely dangerous. Speeding a real concern on Dowd as well. Intersection of Dowd with Route 44 is very awkward as currently exists. Short term, make right turn on red OK from Dowd to east Route 44 or make the intersection come in at a right angle to make it safer to make a right turn. Better yet , moving the light at Dowd & 44 a short distance west to make left turn on Canton Green Road would make things a whole lot safer.

They are all dangerous with accidents waiting to happen. Would not bike or walk along this road for any reason. Building going unchecked without any safety consideration. □

- 114449065253 Oct 29 2023 0:
- 114449066910 Oct 29 2023 0: From Simsbury town line to New Hartford town line.
- 114449046714 Oct 29 2023 0: Just way too much traffic,going to be a lot of accidents
- 114449030817 Oct 29 2023 1: Does Ave to 44 is a dangerous intersection. No one obeys the lights
- 114448870207 Oct 29 2023 0: Aldi's and the shops at Farmington valley.
- 114448850182 Oct 28 2023 1: Crossing Rt 177 is unappealing
 - Crossing Dowd Ave to Junk Store. □
 - Canton Spring Road to Route 44. □
 - Canton Village .. onto Dowd Ave. □
- 114448837940 Oct 28 2023 1:
- 114448837859 Oct 28 2023 1: US-44 starting at the Lawton Rd intersection and west from that point
- 114448832296 Oct 28 2023 1: Any area that does not have sidewalks
- 114448761129 Oct 28 2023 1: All seem very unsafe
 - Better sidewalks are needed and crosswalk lights along Dowd Ave similar to the one where the
- 114448790094 Oct 28 2023 0: bike trail crosses Maple Avenue.
- 114448788997 Oct 28 2023 0: I've not walked this stretch often but when I need to walk in the area by Midas it's intimidating.
 - Length of time to get from McDonald's light on 44 onward thru to left up to Torrington/New
- 114448772683 Oct 28 2023 0: Hartford split. ((to be fair. Traffic before McDonalds is very heavy as well mostly
- 114448779003 Oct 28 2023 0: No
- 114448755001 Oct 28 2023 0: N/a
 - My experience has been that there is lack of continuity adjacent to the shoppes and connecting
- 114448751309 Oct 28 2023 0: to the bike path
- 114448743648 Oct 28 2023 0: East Hill Intersection with the hill and sunset it's hard to see the light
- 114448718097 Oct 28 2023 0: None
- 114448714761 Oct 28 2023 0: No intersections or crossings are safe in these areas
 - Crossing at any crosswalk on Dowd Ave. I will not let my daughter cross the street to get on the
- 114448706158 Oct 28 2023 0: bus because of cars not stopping.

Very dangerous to cross Dowd near Lifetime Family Dentistry. Also, I do not like the crosswalk on Dowd Ave. where the trail crosses. Not enough warning to cars to stop. Rt. 44 is not safe or appealing to walk on. Very unsafe to cross this road. Also cars jump the traffic light on 44 right near Glenwood Apartments. We have frequently seen people going through a red light.

114448682340 Oct 28 2023 0: near Glenwood Apartments. We have frequently seen people going through a red light.

114448685267 Oct 28 2023 0: Dowd Ave from Rt 44; Rt 44 and Rt 177 intersection

114448676613 Oct 28 2023 1: 44 and Lawton is EXTREMELY unsafe even for motors vehicles.
The junction at the emergency vet

114448672813 Oct 28 2023 1: Pulling out of canton center onto dowd
Crossings at Lovely street and the Shoppes seem unfriendly to pedestrians. Slow traffic down.

114448604275 Oct 28 2023 0: Make sure there is always a sidewalk for pedestrians.

114448391838 Oct 27 2023 0: Too much traffic

114448383385 Oct 27 2023 0: The Lovely St, Trailsend Dr, Lawton Rd, Rt 44 intersection

The entirety of 44 throughout Canton terrifies me, and I avoid walking it as much as possible. There are sections of the road that don't even have sidewalks starting from East Hill Road westward. Too many people are driving way too fast and dangerously for me to feel comfortable walking along the road without some kind of barrier to protect me in case a car ends up driving off the road. I would very much prefer to be able to walk/bike to destinations in town such as the Farmington Shoppes, but I don't feel safe enough to do so.

114448278845 Oct 27 2023 0: Farmington Shoppes, but I don't feel safe enough to do so.
intersection of simonds & east hill & old canton road. the traffic never stops! no one knows abt

114448178884 Oct 27 2023 0: the right of way

114447812636 Oct 27 2023 0: Anywhere in the corridor.
Anywhere there is no sidewalk. I would like to see sidewalks along the entire course of 44 in

114447806569 Oct 27 2023 0: Canton (East of 179), preferably on both sides, but at a minimum along one side.

I have children at CMS and CIS and would love them to be able to walk/bike to school but don't feel its safe enough for them to travel so I don't allow it. They would also like to go down to the plaza to get a milkshake/lunch and I don't feel quite comfortable allowing them on bike/scooter without following in my car to make sure traffic is ok. I don't trust the crosswalks and speeders

114447778959 Oct 27 2023 0: or being in the parking lot without a clear walking/bike path.

114447773568 Oct 27 2023 0: Sidewalks and its location to traffic, crossing in front of the firehouse on dowd (trail entrance)

114447764310	Oct 27 2023 0	I feel like with the way people drive you just need to be on high alert. People don't pay attention to crosswalks and barely slow down everyone's in a rush to go no where the anxiety isn't just with walking or biking it's driving as well. People are not safe.
114447759482	Oct 27 2023 0	Rte44 - Dowd intersection, crossing Dowd near Little Oak, Dowd-E. Hill
114447575215	Oct 26 2023 1	Crossing Dowd to commerce is dangerous even when the lights are flashing cars do not stop
114447480678	Oct 26 2023 0	By the cleaners trying to cross RT 44 is in need of safety improvements. There are virtually no safe areas to cross rte 44. The intersection of 44/Lawton/Lovely is the only intersection that is safe to cross. Everywhere else is unsafe.
114447391309	Oct 26 2023 0	I feel much safer walking on dowd versus anywhere on 44.
114447366418	Oct 26 2023 0	Canton Springs Rd and Dowd Ave intersection is very unsafe and had many accidents. Canton Green Rd and Rt 44 intersection is unsafe as is Dowd Ave & Rt 44 interection
114447255799	Oct 26 2023 0	Rt44 itself is not safe. no shoulder and vehicles drive in excess of speed limits. Dowd is ok. Dowd and 44 and East hill, old canton, Symonds, & Dowd intersection. Conducting this survey is ill timed with all the utility work being done along the route. We have plenty of bike and walking trails that I'm sure are underutilized.
114447204175	Oct 26 2023 1	Biking along 44 is not an option the road is extremely dangerous. Sidewalks are not conducive for pedestrian and bike traffic. There are no reasonable crossings of 44 to access shops on north side (especially the bike shop). □ Dowd is more pedestrian friendly with that sidewalk being in better shape. Bike traffic on the road - there is no shoulder to speak of and typically lots of debris in the roadway. Traffic travels well in excess of the speed limit and makes riding this section of Dowd nerve wracking. I do this as a means to an end to access the rail trail to BEST cleaners and the 44 -Lawton rd intersection.
114447085011	Oct 26 2023 1	Down avenue through to 44. I typically use the bike path to make the crossing to Lawton
114447039163	Oct 26 2023 1	I don't walk rt 44 or Dowd ave because of the poor driving of others. Not because of how busy the roads are but I don't trust the driving and attentiveness of others driving to look out for those walking in these areas
114447036502	Oct 26 2023 1	Speeding and distracted driving is my main concern
114447031047	Oct 26 2023 1	crossing US-44
114447024344	Oct 26 2023 0	The whole of the corridor feels unsafe for walking/biking
114447009757		
114446960951		
114446801949		

114446499240	Oct 25 2023 0:44 by Dunkin
114446443025	Oct 25 2023 0: All the crossings on the bike path are unsafe. Many cars do not slow down or stop while pedestrians are in the crossings.
114446472587	Oct 25 2023 0: There are no sidewalks. There is also a lot of construction and roadwork being done, so visibility is poor. Traffic builds up.
114445988688	Oct 25 2023 1: No
114445825332	Oct 25 2023 0: I'm not sure what could be done - it's just really busy and on a bike it feels very risky. I only ever ride from dowd to lovely and race as fast as I can to get it over with.
114445593066	Oct 24 2023 1: The intersection with pedal and paws is especially dangerous
114445026846	Oct 24 2023 1: Lawnton, Washburn, Notch Roads
114444993042	Oct 24 2023 1: Traffic is generally much too fast to feel safe walking or biking on US-44.
114444501078	Oct 23 2023 0: All along Rt. 44, there needs to be all red lights for pedestrians to cross, people run the lights also.
114444458247	Oct 23 2023 0: No
114444448057	Oct 23 2023 0: Died Ave in particular is very congested & unsafe driving, walking, biking need some improvement in that section.
114444409485	Oct 23 2023 0: Canton Springs Rd and Dowd Ave
114444163464	Oct 23 2023 0: Very dangerous intersection. Many accidents.
114444136986	Oct 23 2023 0: From the plaza on dowd to rt 44. the intersection of dowd and rt 44
114444113762	Oct 23 2023 0: Unsafe biking on Dowd and walking/biking on Lawton due to increases traffic over the years as well as cars speeding
114444000384	Oct 23 2023 1: Anywhere along US-44, especially where there are no sidewalks
114443816111	Oct 23 2023 0: I feel walking on US-44/Dowd is something I would never do. There are too many drivers recklessly driving/speeding in that area.
114443720092	Oct 23 2023 0: Intersection of Rt. 44 and Dowd Avenue. Heavy traffic. Aggressive drivers. Don't feel safe at crosswalks or along sidewalks. Sidewalk along other side of 44 is uneven and not a complete sidewalk.
	Oct 23 2023 0: Every intersection is unsafe for walking or biking

114443525588

Oct 22 2023 08:01

The entire road is unsafe for any pedestrian or bike. Getting out of my driveway can be a nightmare and now they want to add more housing units! More cars and more traffic! Sometimes traffic is backed up to the "Village" and getting out of Spring Street is just waiting for a tragedy to occur. With backed up traffic you cannot safely take a left because you can't see the opposing lane to pull out into. Something needs to be done!

through the corridor and also make it safer for pedestrians and bicycles. These goals are diametrically opposed. There needs to be a compromise. CALM TRAFFIC, MORE STOP LIGHTS, BETTER SIDEWALKS, SAFER CROSSWALKS WITH RED LIGHTS, NOT RFFBs, SPECIAL LANE FOR BICYCLES PAINTED GREEN. Look at road design in NYC with buffer for bikes and pedestrians and clear street markings, timed lights, safe diagonal crossings. □

□
All crosswalks in Canton/Collinsville should have orange barrels, bollards or safety cones in the center to help visibility for pedestrians. Flashing lights (RFFBs) are not sufficient for visibility or safety. Drivers frequently fail to stop for pedestrians in crosswalks or allow 3 ft. minimum when passing bicycles. Crosswalks should have protective "STOP LINE" at least 10 feet from actual crosswalk and sign with arrow indicating, "Stop Here for Pedestrians." □

□
There are no proper connections between side streets (with access to bike trail) and Dowd/Maple/Rt. 44. For example, if you ride the bike trail and exit on Canton Springs Road toward Dowd, when you reach Dowd, there is NO CROSSWALK to safely cross. There is no sidewalk. There is only a sidewalk on one side of SR565, so you are always forced to cross a dangerous road to access a "safe" pedestrian route. At Commerce Dr./ Dowd, the crosswalk is on the WRONG SIDE of the road (pedestrians walk against traffic), so it is rarely utilized. If you exit the bike trail on Atwater to get to Canton Public Schools or Mills Pond Park via Mills Lane (to Gildersleeve to Simonds), there is NO SAFE CROSSWALK across Maple Ave. Even if you travel to E. Hill from Maple on foot or bike, there is no crosswalk to get to schools and this is an extremely heavy traffic area with dangerous turns and confusing intersection. □

□
The entire study area is plagued by speeding reckless vehicles. There is news footage of a young student waiting for a school bus on Dowd Ave. who almost got hit by a speeding car, but his/her mother saved the child in time. The entirety of Maple Avenue and Dowd Avenue is EXTREMELY DANGEROUS for pedestrians and bicyclists. Route 44 is a literal highway not fit for peds or bikes. SR 565 is treated like a highway by cars and trucks improperly using this

114443450836

Oct 22 2023 08:01

collector as an arterial to save a few minutes. The only vehicles that deserve a "fast pass" are

	The entire length of Rt. 44 through Canton & Avon (and beyond each of those towns, both to the
114443474555	Oct 22 2023 0 east and to the west).
114443458196	Oct 22 2023 0 No
114443444954	Oct 22 2023 0 Rt 44 and Lawton Road. The crossing is very busy and not a straight shot.
114443410528	Oct 22 2023 0 All intersections/crossings are dangerous nowadays... drivers are not paying attention
114443401220	Oct 22 2023 0 Left turn onto Dowd from 44W
114443397180	Oct 22 2023 0 The 44/177 crossing. And where Dowd intersects with 44.
	The Corredor does not have easily accessible, biking or walking paths. It also does not have
114443394017	Oct 22 2023 0 effective, turning options to access resources via vehicle.
114443361354	Oct 22 2023 1 No
	I walked from Ethan Allen to Benidorm in the summer to pick up my bike and there were not
	always sidewalks to walk on. And some parts that did have sidewalks were in poor shape. A lot
	of traffic pulling into stores. Required very careful crossing of driveways as most drivers were
	more focused on making their turn than looking out for pedestrians, which is understandable
114443326771	Oct 22 2023 1 given the low level of foot traffic.
114443277119	Oct 22 2023 0 I don't walk there.
114443270404	Oct 22 2023 0 All of 44 is bad from Saybrook Fish House to Scarborough in Hartford
	I don't see ANY safe areas on RT 44. Only sidewalks on Dowd Ave. seem to have some level
114443255747	Oct 22 2023 0 of safety.
114443168789	Oct 22 2023 1 Canton Springs / Canton Green area can be difficult to navigate.
	Bicycling along Rt 44 due to minimal shoulder. Intersections are also large/multi lane; bicycle
114443103427	Oct 21 2023 0 travel lanes need to be made clearer for both cyclists and cars.
	The intersection of East aHill Road and Rt 44. Cars blow through the stop lights at a high rate of
114443092225	Oct 21 2023 0 speed.
114443078441	Oct 21 2023 0 Traffic travels too fast
	44 is very unsafe for bikes. To get off the rail trail and interact with the 44 area businesses by
114443076781	Oct 21 2023 0 bike is prohibitive.
114443074376	Oct 21 2023 0 Crossing 44
114443070170	Oct 21 2023 0 No
114443064873	Oct 21 2023 0 US-44 and Lovely St

		My Niece was hit by a car several years ago crossing □ From trail on Dowd also cars never stop on cross between canyon Village Condo and a canyon
114443034649	Oct 21 2023 0:	Hollow Not well acquainted. But trouble spots are where there is no appreciable shoulder on the side of
114442907929	Oct 21 2023 0:	the road and no sidewalk.
114443022192	Oct 21 2023 0:	East Hill Rd and Rt44 intersection is extremely dangerous
114443011353	Oct 21 2023 0:	All of them US-44/Lovely Street is a very dangerous 4 way intersection. Better crosswalks and lights are
114443007286	Oct 21 2023 0:	very much needed at this intersection.
114442982278	Oct 21 2023 0:	Just crossing Rt44 is unappealing.
114442885842	Oct 21 2023 0:	44/179 intersection is terrible
114442906354	Oct 21 2023 1:	No
		The current US-44 crossing is unsafe since it crosses both 44 and the intersecting roads at the
114442851165	Oct 21 2023 0:	same time. I'm not always sure if all of the traffic will remain stopped while I cross.
114442582442	Oct 20 2023 0:	Dowd/US-44 intersection, Dowd/Canton Green Rd, all along US-44
114442548894	Oct 20 2023 0:	Crossing over Dowd Avenue at the intersection with Rt. 44
114442545551	Oct 20 2023 0:	The scariest intersection is in front of the junk shop
		Almost All of rt 44 through Canton feels unsafe for walking, the limited areas with sidewalks are
		safe but don't adequately connect to other areas to make walking between locations feel safe.
		Driving often feels unsafe in this area as well, turning left anywhere off the road is difficult due to
		traffic speeding or frequent lane switching by drivers. Speeding is a serious problem in this area
114442381635	Oct 20 2023 0:	(as it is in other areas/towns along Route 44 and adjacent side roads) no one should bike on a highway they should use the sidewalk though some places have no
114442323310	Oct 20 2023 0:	walk on either side The light in front of the junk store when you have to turn left from 44 onto Dowd. People being
114442253444	Oct 20 2023 1:	you are going too fast and then switch lanes at the last minute
114442232303	Oct 20 2023 1:	All of the roads Cherry brook should have a light on dowd ave or blinking light to get out onto 44. Traffic travels
114442217001	Oct 20 2023 1:	very fast

114442076408	Oct 20 2023 0	I would consider the entire 44 study area unsafe to walk or bike because vehicles are recklessly traveling at a high rate of speed.
114442062149	Oct 20 2023 0	Rt 44 and east hill rd, people driving on 44 always run the red light. Many accidents have happened there
114442008195	Oct 20 2023 0	No
114441971839	Oct 20 2023 0	The light at the nursery near the gazebo never turns red for traffic traveling from avon to canton. This makes it extremely difficult to get in or out of canton Valley circle safely due to higher traffic volumes and the speeds that aren't controlled by cops anymore.
114441970725	Oct 20 2023 0	East Hill intersection, the area near the green, coming up over the hill by Cape Cod Fence - people go too darn fast! Dangerous lane changes, tailgating. Been here since 1999. They've paved paradise and put up a concrete jungle of stores.
114441953292	Oct 20 2023 0	Na
114441766966	Oct 20 2023 1	Dowd Ave to Canton Shops on 44 East Hill Rd
114441698132	Oct 19 2023 1	Not sure. I only stick to the bike trail or sideswalks at the shoppes because the rest is unappealing to walk/bike. I wouldn't feel safe crossing 44 anywhere in Canton.
114441682517	Oct 19 2023 1	Secret lake area
114441640272	Oct 19 2023 0	Rt 44
114441627298	Oct 19 2023 0	When I do walk, the section of 44 between the light at Dowd and light at Lovely St./UConn-CVS is noisy, and cars are driving very fast, even with those two traffic lights. I used to walk that stretch but now I take the Trail instead.
114441604056	Oct 19 2023 0	Crossing over 44 to continue on the Farmington trail is not good. The five way intersection and heavy car volume makes it dangerous.
114441592079	Oct 19 2023 0	No
114441591819	Oct 19 2023 0	Trying to cross dowd ave near Canton springs road
114441571869	Oct 19 2023 0	US-44 and Lovely Street is a dangerous place to walk, bike, and drive. It also leads to very long traffic delays due to the intersection's configuration.
114441561715	Oct 19 2023 0	All of them
114441556749	Oct 19 2023 0	Yes, Secret Lake Rd access, too narrow to take left from 44, too many accidents
114441542391	Oct 19 2023 0	crossing either way into dowd from 44 or from 44 into dowd
114441540583	Oct 19 2023 0	

114441532447 Oct 19 2023 0: While the infrastructure could be better, it's the fast speed of the drivers that make it so unsafe. Sometimes it is difficult to pull onto Dowd from the Canton Village because of the volume of

114441474123 Oct 19 2023 0: traffic.
crosswalk in front of Commerce and old IGA - so much debris and dirt; lack safe of pedestrian

114441472476 Oct 19 2023 0: crossing at Dowd/44 intersection
Intersection of Canton Springs Road at Dowd Ave and US 44. Bad sight lines, lots of car traffic.

114441441096 Oct 19 2023 0: Difficult to enter Dowd or US 44.

114441430660 Oct 19 2023 0: Yes'trying get on rt44. is very hard coming out Canton Valley Circle @noon and evening.

114441434607 Oct 19 2023 0: Exiting/entering Bristol Drive is treacherous!!
The intersections of east hill road with 44 and dowd are dangerous, especially for kids who ride

114441422408 Oct 19 2023 0: bikes to/from school
The stretch of Rt. 44 from Lovely Street (aka Rt. 177) to Dowd Avenue is a nightmare for both

114441389752 Oct 19 2023 0: pedestrians and vehicles.

114441360407 Oct 19 2023 0: No

114441359309 Oct 19 2023 0: Dowd and Rte 44

114441357834 Oct 19 2023 0: no

114441344923 Oct 19 2023 0: Small shoulders, curves, speeding cars

114441340226 Oct 19 2023 0: Yes. People fly in this area.

114441339184 Oct 19 2023 0: no
Intersection of Dowd Ave and 44 is not safe. People run/push the light all the time.□

114441303569 Oct 19 2023 0:

114441284712 Oct 19 2023 0: Too many people running red lights. The intersections all along 44 are bad.

114441275049 Oct 19 2023 0: Mills pond intersection
Traffic speed is out of control and vehicles dangerously running red lights is the norm. East hill

114441256383 Oct 19 2023 1: and 44 intersection is particularly bad

114441236594 Oct 19 2023 1: Along Rt 44 from McDonald's in Simsbury to intersection of Lovely St and Rt 44

114441228638 Oct 19 2023 1: All of 44. People drive way to fast.

114441219931 Oct 19 2023 1: Dowd and Canton Springs intersection.

114441198318 Oct 19 2023 1: Anywhere along 44 doesn't feel safe for walking or biking.

		There are constantly accidents at the intersection of Cheryl Drive and Rt 44. It is very difficult to safely make a left out of Cheryl drive onto 44. Also the intersection of Dowd and 44, where it is right turn only and oncoming traffic does not stop is confusing to some and has frequent accidents. It is difficult to get in and out of the businesses there as well (Blue House, Whole Donut). Traffic on 44 moves at a ridiculous rate of speed through here as well. I often see
114441180563	Oct 19 2023 1	people driving 55-60 mph.
114441151788	Oct 19 2023 1	No
		Rte 44 at Rte 179, people sometimes do not obey the stop lights. Especially when there is a left
114441153281	Oct 19 2023 1	turn arrow.
114441120858	Oct 19 2023 1	No.
114440897105	Oct 19 2023 1	Crossing at Lawton Rd
114441109937	Oct 19 2023 1	Almost everywhere, particularly the traffic light intersection at Dowd/44
114441098671	Oct 19 2023 1	The entire stretch of 44.
114441082825	Oct 19 2023 1	Crossing Rt 44 anywhere is risky at the few existing crosswalks and very unsafe anywhere else. If you walk on the south sidewalk west of Rte177 to East Hill Road there are portions that leave you dangerously exposed to the east bound traffic. Some with no barrier at all and no curb with
114441081974	Oct 19 2023 1	fast moving cars and trucks.
114441083779	Oct 19 2023 1	44 at Lovely St and Lawton Rd.
114441080018	Oct 19 2023 1	none
114441069591	Oct 19 2023 0	Most crossings
114441061129	Oct 19 2023 0	People moving from lane to lane when people are turning onto Dowd Avenue from Route 44
114441056773	Oct 19 2023 0	no
114441056756	Oct 19 2023 0	N/A
114441041394	Oct 19 2023 0	In front of Dunkin Donuts. Poor visibility, congested
114441038642	Oct 19 2023 0	no
		Simsbury town line to shops at canton, uncomfortable to drive let alone walk or bike. 4 lanes
114441017751	Oct 19 2023 0	undivided with high vehicle speeds
114441020719	Oct 19 2023 0	In general, too many drivers are speeding all along US-44.
114441021986	Oct 19 2023 0	Rt 179 & Rt 44 intersection. No sidewalks.
114441012584	Oct 19 2023 0	N/A

114441015118	Oct 19 2023 0	the intersection at 44 and 177. the bike trail ends there and dumps cyclists at the bus corner there. bike trail needs a tunnel or overpass there.
114441016529	Oct 19 2023 0	The entire intersection of Route 44 and Lawton Road. Traffic is often very heavy and if you are looking to turn left on Route 44 the traffic that is turning onto Lawton Road from Route 44 comes very close to the cars that are stopped at the intersection looking to turn onto Route 44. Rte 44/Lawton road is very dangerous. Also, I think the Rte 44/Dowd is very bad.. I'm afraid driving in that area, can't imagine walking or biking there!!
114441017412	Oct 19 2023 0	Making a left turn off Canton Springs Road onto Dowd Avenue is very dangerous.
114441012147	Oct 19 2023 0	pedestrian crossing. very few people push the button.
114441002346	Oct 19 2023 0	All of the above
114441009465	Oct 19 2023 0	Lovely st and rt44 intersection
114441006346	Oct 19 2023 0	All of US-44 is a nightmare to drive, nevermind walk or bike. it's very dangerous to walk and bike along the road.
114441007630	Oct 19 2023 0	All of them. People CANNOT drive in CT. Where are the cops?
114441003015	Oct 19 2023 0	The Rt.44, Lawton Rd. area is very congested as well
114440992237	Oct 19 2023 0	Speeding, texting, distracted driving
114440975990	Oct 19 2023 0	Don't walk or bike
114440963945	Oct 19 2023 0	Trail crossing on Dowd!
114440965803	Oct 19 2023 0	All of 44/Dow's because people don't pay attention and drive like idiots
114440963403	Oct 19 2023 0	No
114440946062	Oct 19 2023 0	Too dangerous to walk or bike on 44
114440946505	Oct 19 2023 0	202 / 44/ 179 intersection
114440930669	Oct 19 2023 0	Intersection of dowd & 44.
114440930628	Oct 19 2023 0	Don't walk or bike in area.
114440916815	Oct 19 2023 0	all
114440922479	Oct 19 2023 0	The whole thing is unsafe
114440920225	Oct 19 2023 0	The left turn from 44 onto Dowd and the intersection of 44, Lawton Road, Lovely Street, and the end of the bike trail, etc. The exit from The Shops onto 44 with the new car dealership on 44
114440903371	Oct 19 2023 0	No
114440914402	Oct 19 2023 0	No

114440901353	Oct 19 2023 0	Yes, all of the above. Primary concern is people behind the wheel of their cars speeding on these heavily traveled roads. Need to slow down.
114440908836	Oct 19 2023 0	The intersection of 44 and East Hill Road and 44 and Dowd are exceptionally dangerous. Drivers are highly unsafe, attempt to beat lights or go straight through red lights and stop sign at Dowd all day every day.
114440892449	Oct 19 2023 0	Merge lane from US-44 east onto Lovely St. makes it difficult to navigate on bicycle. Cars should be forced to stop at red lights. Cars traveling north on Lovely St. making right on red do not stop even with pedestrians in crosswalk.
114440890993	Oct 19 2023 0	The entire area is not very safe to walk or bike All over dowd Avenue
114440889494	Oct 19 2023 0	Dangerous speeders on the road Trying to enter Dowd from Canton Springs Road is very dangerous due to visibility of vehicles
114440879690	Oct 19 2023 0	headed toward Collinsville. Many accidents have occurred at that location. Intersection of RT44 and Dowd Ave is dangerous to cross, meaning going across RT44 to Dowd Ave side
114440885754	Oct 19 2023 0	
114440883356	Oct 19 2023 0	Crossing Dowd Ave at Allen Place to continue on the bike path All! People drive too fast and traffic is very heavy in this area. Dowd ave is very high traffic for
114440880951	Oct 19 2023 0	what's supposed to be a residential road!
114440879638	Oct 19 2023 0	The Dowd ave Rte 44 intersection.
114440770734	Oct 19 2023 0	US44 to Avon
114440764519	Oct 19 2023 0	Intersection of 44 and dowd All of 44. People drive way to fast and do not pay attention. All major intersections are dangerous. Crossing the street to go to all businesses are dangerous because of the speed of
114440701479	Oct 19 2023 1	traffic
114440648904	Oct 18 2023 1	Where Dowd Avenue meets Route 44. Sight line for crosswalk there is often blocked by traffic turning from Route 44 to Dowd Avenue. Traffic also moves very fast on both roads.
114440632939	Oct 18 2023 1	Someone unfamiliar with the area can get very confused about the flow of traffic, what lanes to turn, & what places not to turn or when not to try to change lanes particularly going west
114440609759	Oct 18 2023 0	No
114440610879	Oct 18 2023 0	all of it

114440606990	Oct 18 2023 0	Speeding on route 44 excessive up to light near petals and paws and mini golf. Never police stationed there.
114440602451	Oct 18 2023 0	Crossing anywhere on 44 Anyplace on US-44 from the Route 177 intersection east past the Farmington Valley shops and
114440574401	Oct 18 2023 0	through Avon is very unsafe for riding.
114440575137	Oct 18 2023 0	Commerce road canton village condos area All of it. Poor speed controls, insufficient sidewalks and bike lanes, not enough traffic lights for
114440561630	Oct 18 2023 0	pedestrian crossings The intersection of US-44 and Dowd Avenue is accident prone when drivers going west onto Dowd run the turn arrow. Also the intersection of Rt 177 and US-44 it is difficult to make a right turn on Red (going eastbound) with the vegetation blocking the view. □
114440530148	Oct 18 2023 0	
114440528426	Oct 18 2023 0	44 and East Hill Rd intersection. Too many drivers run the red light.
114440496474	Oct 18 2023 0	NO All of them? An entire mall/shopping center was built and you expect people to waltz across 44
114440485630	Oct 18 2023 0	anywhere around there? Dumb.
114440477421	Oct 18 2023 0	Every area of 44 From the intersection of staples/Berkshire bank all the way up through the main section of canton on route 44 up into New Hartford. There isn't enough space on the side of the road to
114440456991	Oct 18 2023 0	feel comfortable and safe biking.
114440437037	Oct 18 2023 0	Taking a left onto Dowd from 44W is dangerous
114440433006	Oct 18 2023 0	Improvement around the "triangle" Lights at intersection of route 44 and 177 lovely street□
114440424880	Oct 18 2023 0	Many cars are seen going through red lights ALL of 44 is unsafe to walk along. Sidewalks stop after shopping plaza four lanes still not
114440373539	Oct 18 2023 0	enough for traffic flow during rush hour. The crosswalk from the Subaru and VW dealership over to the Shoppes is terrible - you have to walk into unmowed grass to hit the signal. And overall the crosswalks do not feel safe along
114440370072	Oct 18 2023 0	Route 44.
114440356485	Oct 18 2023 0	That entire area is quite busy and too small

114440330934	Oct 18 2023 0:	Exactly here! Witnessed several accidents as a former Davidson Chevy employee and life long local resident. I've seen people in the line to go right on 44 go straight across to access the nursery while there is a left turn arrow for those turning onto Dowd. Also a HUGE issue walking from 44 onto East Hill to access the town park/pool. There's no side walk, tons of kids and pedestrians walking down the road to get there, and serious speeding, it's dangerous. We need a sidewalk to finish connecting the town park on East Hill to 44.
114440251125	Oct 18 2023 0:	The entire area - too busy of a road to be walking and biking. Those activities should Be done elsewhere
114440067316	Oct 18 2023 1:	Yes, between Commerce and Canton Springs
114439953925	Oct 18 2023 0:	There are too many entrances and exits to single or small businesses. Posted road speeds are too high. Sidewalks are not everywhere along the road. Bike paths are narrow or non-existent. Landscaping is sporadic and/or UGLY. Places need identifying markers (like the rocking chair further on 44) as one cannot read signs easily if traveling at the posted speeds.
114439832220	Oct 18 2023 0:	US-44 in general
114439810349	Oct 18 2023 0:	None of the intersections are bicycle friendly unless you are on the rail trail
114439450785	Oct 17 2023 0:	ALL INTERSECTIONS ON DOWD AVENUE/US-44 FEEL TREACHEROUS. TRAFFIC MOVES QUICKLY, VISIBILITY IS POOR, IT IS COMMON TO WAIT FOR A SIGNIFICANT AMOUNT OF TIME FOR CARS TO STOP WHEN WALKING A CROSSWALK. OF PARTICULAR CONCERN IS THE INTERSECTION AT THE BOTTOM OF HIGH ST/DOWD
114439428073	Oct 17 2023 0:	I have not been walking or biking in the study area, and have no educated opinion
114439425822	Oct 17 2023 0:	Dowd and Atwater Trail Crossing - cars on Atwater do not stop or slow down at this trail crossing
114439308802	Oct 17 2023 0:	The area should not be made a high-density area. This is not a City
114439279623	Oct 17 2023 0:	I consider it virtually suicidal to bike anywhere on 44 east of route 179.
114439244617	Oct 17 2023 0:	By the Dunkin' Donuts shop
114439231288	Oct 17 2023 0:	Near Simsbury border traffic is too fast moving.
114439236291	Oct 17 2023 0:	It is not a great area in which to bike. The only area in which I feel comfortable is the park.
114439182064	Oct 17 2023 0:	All of 44 from new Hartford to avon

114439117862	Oct 17 2023 0	<p>Route 44 from Canton Village to Canton Green. I like to get my car services at Fast Lane quick Lube and enjoy coffee at Giv coffee across the street. I usually have a friend pick me up and drive me across the road because crossing on foot terrifies me during the day. I go to Giv coffee often and sometimes use the river trail to Dowd. There are no sidewalks on Dunham and people use it as a cut through. The road is narrow. I question if it could be a pedestrian only zone? There is no curb nor has buffer for the sidewalks in front of Giv coffee on that stretch of 44, and when the large 18 wheelers come whipping by it feels like they can suck you onto the highway. I should be able to take my coffee to the Canton Green, but it's just too scary.</p>
114439058173	Oct 17 2023 1	<p>E Hill Road (intersects with US-44) is very narrow and winding which makes it unsafe for walkers and drivers alike. The utility poles along the road are extremely close to the roadway which forces vehicles and trailers to cross the yellow line. This is exacerbated when there are delivery or service vehicles in the area. I appreciate the consideration for widening the roadway where possible given the elevation and terrain.</p>
114439036704	Oct 17 2023 1	<p>Dowd, East Hill, old Canton Rd and Simond Rd intersection Cars come speeding off of Dowd plus fail to signal turn getting off and again failing to signal they are turning left on Simond. Many fail to stop for pedestrian crossing there. Plus teen drivers coming from High School.</p>
114439034585	Oct 17 2023 1	<p>The entire section is not conducive for pedestrian traffic. I've witnessed accidents from moving traffic on 44 hitting stationary vehicles coming out establishments along 44</p>
114438946430	Oct 17 2023 1	<p>there is not enough room and BOTH drivers and cyclists do not share the roadway. additionally neither understands the laws regarding moving vehicles</p>
114438923543	Oct 17 2023 1	<p>The area between where the Bike path crosses 44 down to the shops could be made more bikable because the narrow sidewalk, narrow shoulder, and busy road/intersections make sharing that space with any other pedestrian nearly impossible without stopping to pass.</p>
114438914954	Oct 17 2023 1	<p>crossing Rt44, even in hashed markings, can be like crossing a NASCAR racetrack. Similar concerns on Dowd when people cross between the marked zones.</p>
114438787029	Oct 17 2023 0	<p>Bike crossings on bike trail are dangerous, especially the Dowd/Maple Ave. crossing. Cars DRIVE TOO FAST.</p>
114438738135	Oct 17 2023 0	<p>At the top of the hill above saybrook even way on the grass cars are crazy there</p>
114438724277	Oct 17 2023 0	<p>None</p>
114438690989	Oct 17 2023 0	<p>The corner of High and Dowd is particularly dangerous for crossing by car or on foot.</p>

114438556178	Oct 17 2023 0	N/A
114438471319	Oct 16 2023 10	I am more concerned with Maple ave / River rd
114438465941	Oct 16 2023 10	Dowd, Simmons, east hill 4 way intersection Crossing Dowd on a bicycle makes me nervous but I'm grateful for the signals and wait for cars to stop. The part of the trail after the bridge where it's a thin sidewalk On Dowd Ave makes me nervous too. We come off the bridge and make
114438451249	Oct 16 2023 0	A tight corner in an area with dense traffic and no guardrails. The red lights are run nearly constantly by at least one motorist per red light. It's insanely unsafe and we have to give significant pause before proceeding on our green lights at each intersection. I couldn't imagine walking or biking in the area. More cops? Cameras? Something
114438424363	Oct 16 2023 0	needs to deter the speeding and running reds.
114438388986	Oct 16 2023 0	East hill, old canton, dowd intersection can be treacherous
114438383588	Oct 16 2023 0	The intersection where lovely street meets 44, near Best cleaners and Uconn health
114438382188	Oct 16 2023 0	Route 44 near cherry brook health center I live on Bristol Drive off 44 and many of my street's residence have to cross 44 for work or to walk to other locations and there is no crossword nor is there a sidewalk to a crosswalk so we
114438373950	Oct 16 2023 0	have to cross unsafely and it is very scary.
114438351379	Oct 16 2023 0	Crossing 44
114438333223	Oct 16 2023 0	Rt 179 by police station. Cars fly on that road. They also fly on Dowd. The corner at the Canton Green is particularly problematic. The left turn onto Dowd from 44 can
114438305390	Oct 16 2023 0	be dangerous. I typically bypass this turn and cut through Canton Village or turn after Citco. High st intersection dip - loose sight of oncoming traffic
114438299085	Oct 16 2023 0	Traffic speed - distracted drivers not watching / stopping at pedestrian crossings
114438293141	Oct 16 2023 0	The new sidewalks in the center of Collinsville are pointless and a waste of taxpayer's money. Dangerous to try to cross road. Some cars ignore speed limits and cross walks.
114438282208	Oct 16 2023 0	
114438265335	Oct 16 2023 0	No
114438257490	Oct 16 2023 0	The crossing at the intersection of Route 44 and Lovely Street seems especially dangerous, but it is a necessary crossing for cyclists traveling from Collinsville to the Shoppes.

114438260415 Oct 16 2023 0: The whole US-44 in Canton & Avon seems inhospitable to biking
I would never cross 44 on foot. For example, if I am at giv coffee and I go across to the junk

114438258299 Oct 16 2023 0: shop, I drive.

114438251520 Oct 16 2023 0: All of 44

114438224384 Oct 16 2023 0: 565 and 44, 565 and 179, 179 and 202
All areas along roadside are un safe...between walk areas and crosswalk areas...bad, cars do

114438197436 Oct 16 2023 0: not travel the speed limit, they do not watch out for pedestrians

114438182736 Oct 16 2023 0: All intersections on US-44 and Dowd Avenue are not safe for pedestrians.

114438168972 Oct 16 2023 0: Unsafe to bike on 44, period. Unappealing to bike across 44 at intersection with East Hill, where
(unless cars are present to trigger the light) you must get off your bike and walk to the button
Intersection of Rte 44 and Dowd, intersection of Rte 44 and East Hill Rd., Rte 44 at Canton
Village, and Rte 44 at Lovely Street are all hazardous. □

114438163905 Oct 16 2023 0: Terrifying to try to cross the road in these locations. Would love to feel safe walking there.

114438114925 Oct 16 2023 0: All
I live on Bristol Drive right off of route 44 and feel I would be risking my life to cross rte 44.
Sidewalks are needed on both sides of the road and more pedestrian crossings are needed
both on rte 44 and dowd ave. Its a shame to live so close to so much but not be able to walk

114438099221 Oct 16 2023 0: safely
US-44 at Lovely and Lawton is unsafe because of the incredible volume of cars and high
speeds. All crossings of 44 are unsafe to some degree for the same reason. However, using

114438106921 Oct 16 2023 0: proper techniques and locations, I have always been safe.
Crossing 44 at East Hill as a driver is treacherous due to people running that light. I am shocked
when I'm at that light and someone *doesn't* run it. My daughter is blind and would love to cross

114438064846 Oct 16 2023 0: there on foot to access the town park and libraries as a non-driver. It is impossible.

114438062149 Oct 16 2023 0: Unsafe crossing at all crosswalks in town, traffic does not stop or slow for pedestrians

		US-44 at Dowd, both directions. Turning left from 44 east bound/west bound. From Harts corner/Cherry Brook to the Simsbury line. Any left turn on RT 44 is dangerous. I live off of Rt-44 and from my road it is nearly impossible to make a left turn onto Rt44 during typical driving hours. 7am-5pm. Many times I make a right, and then go down a side road. We hear cars tires screeching during typical rush hour times constantly. Lawton-Cherry Brook/202 merger RT west bound. Needs to be labeled a village. And the speed needs to be dropped to 30-35 mph. It is very concerning when the town planner is labeling this area walkable, and trying to make this more of a village, park and walk around. However, road safety audits are labeling this dangerous to walk around. I suggest a sign at Lawton/Rt44 that flashes speed limit drop to 30 mph, pedestrian area. There NEEDS to be traffic calming measures put in place for this area. DOT and Canton officials needs to work together to take action. We have the data, time and time again. That this area is dangerous. Every which way you look at it. Time to take a drastic
114438050909	Oct 16 2023 0	measure before more undue risk is placed upon residents of this town.
114438055947	Oct 16 2023 0	US 44/ does intersection is very dangerous whether trying to cross or to pull out from businesses in the immediate area. Traffic flow is 55+ mph
114438055220	Oct 16 2023 0	This area needs to be changed. A dedicated left turn lane on 44 west to turn onto Dowd should be designed
114438053159	Oct 16 2023 0	All of 44 feel unsafe to walk and ride a bike.
114438045350	Oct 16 2023 0	Walking or biking anywhere on Rt 44 is dangerous and I avoid it at all costs.
		I order to make the study area work for cycling... there needs to be some level of protection from vehicles Right now - there is nothing - I bet most motorists in the area have No Clue,
114438026009	Oct 16 2023 0	there is a 3' law ... perhaps some Sharrows and/or signage would help...
114438036788	Oct 16 2023 0	None
		All of the crosswalks on Dowd Avenue should have RRFBs. Although the crossing of the Farmington Rail Trail at Dowd Avenue already has an RRFB, it is a high-speed area. Better
114438029737	Oct 16 2023 0	signage and proper night-time lighting would be beneficial to trail users and motorists.
114438027126	Oct 16 2023 0	All of them honestly.
		Rt. 44 and Lovely St intersection, Rt. 44 & Dowd, Shops at Farmington entrance area, Rt. 44 &
114438023376	Oct 16 2023 0	Secret Lake along with Old Albany Tpke. Intersections.
114438017562	Oct 16 2023 0	The rest of Dowd Ave onto Maple Ave

114438001503	Oct 16 2023 1;	No. Dollars and focus should go to other areas in the town. We shouldn't just appease a few people that live on Dowd/Maple that complain no matter how fast anyone drives. That's where enforcement can help. No way 44 can become a walkable pedestrian area given the need to move traffic through town and it's the only east-west. Would only make commuting through the area worse having more crosswalks.
114437977752	Oct 16 2023 1;	Crossing 44 to access any store/property on the far side (east) of 44...ie Benidorm, Blue House Bagel is difficult and often very unsafe. Better, safer connections (sidewalk) and crossings.
114437987320	Oct 16 2023 1;	Anytime crossing 44
114437981887	Oct 16 2023 1;	Pedestrian crossings across route 44 ie from old Mitchell Subaru to shops are very stressful due to high speeds
114437969477	Oct 16 2023 1;	All of the crosswalks, attempting to turn left onto Dowd from Commerce Drive or Canton Springs Rd
114437923027	Oct 16 2023 1;	None feel safe or appealing
114437874995	Oct 16 2023 1;	no
114437869236	Oct 16 2023 1;	All unsignalized intersections are unappealing and often unsafe depending on the traffic on Rt. 44.
114437832482	Oct 16 2023 1;	Crossing by Alan street. Nobody stops or regards the yellow flashing light. I run in trail 5x week and I'm petrified I will be hit at one point. They need a stop light. I feel as though the road is made for passing thru traffic and not the residents of Canton that frequent the trail.
114437822708	Oct 16 2023 0;	All of them! But Hart's Corner, East Hill Rd., and the whole Lovely St/177/Dunkin Donuts area are all terrible
114437810480	Oct 16 2023 0;	Lawton intersection doesn't feel safe to traverse on foot or bike and Farmington river trail passes right through it. I also see people making turns on red when they should not. Dunkin' to Blue house bagel parking lot is very dangerous stretch.
114437809907	Oct 16 2023 0;	No, do not waste my tax dollars on pointless "improvements "
114437767730	Oct 16 2023 0;	Route 44 area by the businesses around Old Albany Turnpike and Colonial Drive
114437406043	Oct 15 2023 0;	I bike on the rails to trails in Collinsville and along the Farmington river and behind Dowd, but once that trail ends at US44, I don't go any further. I turn back. Neither 44 nor the streets (Gracey, etc) are safe for biking.
114437367598	Oct 15 2023 0;	Where there is no sidewalk and the road bends towards Collinsville

114436027590	Oct 13 2023 0	Crossing 44 at Lovely St (Best Cleaners) where the trail ends. Sidewalk by Davidson Chevrolet/Giv Coffee/Luke's Donuts. I would never ride a bike on route 44 in the area of Dowd Ave ever. The crossing at lovely Street isn't so bad because you have the light. But the only way I would ever think of going is straight
114435334325	Oct 12 2023 1	across from the dry cleaner to Lawton road.
114435335315	Oct 12 2023 1	The whole section of this study.
114434339865	Oct 11 2023 1	The intersection off 177 and 44 is tricky
114432864924	Oct 10 2023 0	Anywhere on 44 where there isn't a sidewalk, especially east of the Shops of FV. 44 from 179 to Canton Village as there are no sidewalks. It's also a dumping ground for
114430314230	Oct 06 2023 0	garbage and nip bottles.
114429977999	Oct 05 2023 0	I don't like seeing all the runners who are working out running in the street. I feel that there are too many speeding cars that whip through the intersections or are quick to
114429876615	Oct 05 2023 0	cut off other drivers. Too many serious accidents on 44.
114429740912	Oct 05 2023 0	aside from the bike crossing on the trail, all other crossings are unsafe Anywhere on the way from the Shoppes to the Simsbury town line. The shoulder is narrow and cars do not give bicyclists the lane. Multiple times I was nearly taken out by CT Transit buses
114429393040	Oct 05 2023 0	and their wide mirrors as the bus sought to pass me.
114429073064	Oct 04 2023 0	I don't walk or bike in town
114428993797	Oct 04 2023 0	Best Buy McDonald's sections
114428873488	Oct 04 2023 0	44 when it intersects with the river trail Dowd and 44 are awful, cars go and are not paying attention. There are always accidents and
114428826101	Oct 04 2023 0	poor sight lines
114428628501	Oct 04 2023 1	Entire length off 44 from 179 intersection east us extremely dangerous for cyclists
114428320616	Oct 04 2023 0	N/A Signage and safety considerations could be better for crossing Rt. 44 on the River Trail link
114427784925	Oct 03 2023 0	going ENE from Unionville to Simsbury.
114427687499	Oct 03 2023 0	Rt 44 itself has no shoulder for biking in the corridor. Bike trail absorbs most of the problem but more pedestrian friendly Rt 44 would improve business access and slow traffic. Approaching downtown Collinsville is the worst for walkers even with crosswalks. Cars fly on this
114427396184	Oct 03 2023 0	road.
114427337728	Oct 03 2023 0	All

114427331917	Oct 03 2023 0	Would like to see more trail, less on-road cycling when riding the loop. Dowd Ave near Rattlesnake Brook to US. 44 is super narrow and condition of pavement makes
114427038805	Oct 02 2023 0	biking through this zone a white-knuckle experience.
114427003902	Oct 02 2023 0	There are not enough crosswalks and traffic travels through town at excessive speed.
114426934020	Oct 02 2023 0	5 way stop Trailsend/Lawton etc. Blight property and also way to much traffic and noise!!!! Intersection near the green. It is horrible. Speeding, near misses frequently if you're sitting to turn. Both directions. Sometimes I pass where I need to go and turn around. Too dangerous to
114426927984	Oct 02 2023 0	sit with a blinker on.
114426901594	Oct 02 2023 0	the crossing and connection via bike from 44 to Simsbury
114426882376	Oct 02 2023 0	Canton Springs/Dowd Ave intersection
114426871870	Oct 02 2023 0	none
114426833480	Oct 02 2023 0	Crossing 44 is sometimes scary, drivers aren't always paying attention to bikers
114426820362	Oct 02 2023 0	No
114426818012	Oct 02 2023 0	None
114426663190	Oct 02 2023 0	River rd / maple ave intersection Biking along 44 feels unsafe, only on the trail feels safe. I prefer to ride on the road, usually
114426813413	Oct 02 2023 0	taking other roads to avoid being out on 44 as long as possible.
114426798620	Oct 02 2023 0	Rt 44 and rt 179 intersection
114426773800	Oct 02 2023 0	Honestly, it's all a little scary, based on the speeding cars going by. Very hard to access the businesses between The Junk Shop and Fatty's by bike. I sometimes
114426691365	Oct 02 2023 0	ride on the sidewalk just to avoid cars but this is frowned upon by walkers.
114426614489	Oct 02 2023 1	No I think most of 44 is unsafe for walking. The motorists speed and tailgate, and police presence is
114426582721	Oct 02 2023 1	underwhelming. Width of road is an issue when cycling on RT 44 in this area- The usable roadway is narrow due to debris and shoulder width. Also catch basins along this route generally stick out in the road and should not be ridden across. Traffic is what it is, and speed limits are generally exceeded by
114426570078	Oct 02 2023 1	traffic in the area.
114426537848	Oct 02 2023 1	There is generally not enough space to allow drivers to provide required safe distance. Dowd/44, lovely street/44 are terrible. No one stops at redlights, the speeding and wreckless
114426499342	Oct 02 2023 1	driveing is out of control

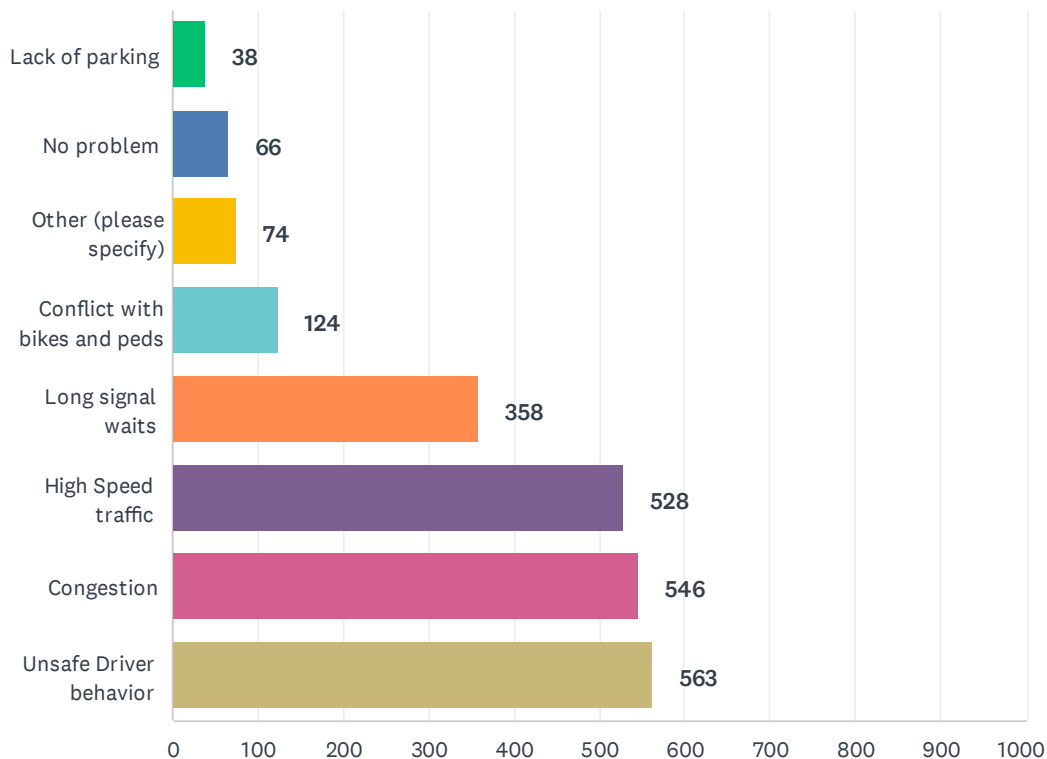
	No sidewalks, or even shoulders, between East Hill Rd. and the 44/202/179 intersection. Not safe to walk.
114426486446	Oct 02 2023 1
114426441359	Oct 02 2023 1 RT 44 and Dowd Ave intersection
114426389615	Oct 02 2023 0 Anything along either road. Traffic speed is outrageous.
114426350107	Oct 02 2023 0 Na
114426336531	Oct 02 2023 0 Traffic is traveling fast, too many cars turning.
	I sometimes walk from Mitchell Jeep over to The Shoppes at Farmington Valley when I get my car serviced. There is a cross walk by the CVS but it's still a little daunting. I lived in DC for
114426259537	Oct 02 2023 0 years and felt better about crossing roads there.
	The intersection of Dowd and Canton springs road should have a crosswalk <input type="checkbox"/>
	<input type="checkbox"/>
	The sidewalks along 44 from lovely to the Canton village are very poorly maintained <input type="checkbox"/>
	<input type="checkbox"/>
	It's just outside the area asked about but the people on Bristol dr have no way to get to a sidewalk on 44 safely. They either have to walk through tall poorly maintained grass or run across 44 at Canton savings bank. It's extremely dangerous and frustrating <input type="checkbox"/>
	<input type="checkbox"/>
	The stretch of 44 from the shoppes to Simsbury town line is completely unsafe for pedestrians. I spend a lot of time using the sidewalks in the area covered by the survey (multiple times a
114426241410	Oct 02 2023 0 week) but I would never consider walking past secret lake road
114426125309	Oct 02 2023 1 Throughout the corridor.
	Absolutely nobody should be walking or biking on 44. It's like Talladega. Dowd, at least has
114426104240	Oct 01 2023 1 sidewalks.
	The area where the trail ends. I wish the bike trail didn't end in a slim sidewalk, but instead
114426094828	Oct 01 2023 1 continued as a trail.
114426083348	Oct 01 2023 1 From the Shops to the Simsbury town line
114426008497	Oct 01 2023 0 Shoulders very narrow, no protection from vehicles
	Intersection of Dowd & 44. Many accidents wouldn't feel safe walking. Intersection of 44 & 177.
	Even crossing street to Shoppes from car dealerships although haven't done it since new
114425920718	Oct 01 2023 0 dealerships have been completed
114425896557	Oct 01 2023 0 Na
	44@Lawton, all of the ingress & egress from canton valley shops <input type="checkbox"/>
114425844961	Oct 01 2023 1 44 @ Dunkin'

114425839053

Oct 01 2023 1(Farmington River Trail north of 44

Q9 If you often drive along US-44/Dowd Avenue, do you experience any problems when driving through the study area? (check all that apply)

Answered: 867 Skipped: 2



ANSWER CHOICES	RESPONSES	
Lack of parking	4%	38
No problem	8%	66
Other (please specify)	9%	74
Conflict with bikes and peds	14%	124
Long signal waits	41%	358
High Speed traffic	61%	528
Congestion	63%	546
Unsafe Driver behavior	65%	563
Total Respondents: 867		

Respondent ID	Response Date	Other (please specify)
114476282928	Nov 30 2023 0	Slow drivers who need to move to the left lane. Traffic stuck turning right going west onto Dowd with drivers in the right lane speeding and drivers stuck in left lane
114463468212	Nov 14 2023 1	making dangerous lane change.
114457391887	Nov 07 2023 0	visibility issues accessing Dowd Ave from Canton Springs Rd
114455424445	Nov 05 2023 0	Mostly unsafe driver behavior. I drive to work very early in the morning -- usually before 6 am. The lights USED to be blinking in Canton as they still are in Simsbury and Avon. They are long lights with few cars on Route 44, so folks run the reds regularly. Blinking lights seem safer at that time of day. In heavy traffic during rush hour, turns across Route 44, such as the westbound lane attempting to turn into Dunkin Donuts, is dangerous. That left
114454311668	Nov 03 2023 0	should be prevented. Many drivers run the lights.
114453801455	Nov 03 2023 0	lack of police presence and lack of enforcement.
114453777074	Nov 03 2023 0	Tough to take left turns especially towards the Dunkins
114453536348	Nov 03 2023 0	Safe Driving requires navigating these things all the time. Drivers abruptly making left turns from Route 44 Eastbound between Secret Lake Road and Route 167, blocking the left travel lane and causing accidents when drivers attempt to get around the left-turning car. It is ridiculous that no left-hand turn bays are provided on this stretch of Route 44, and this
114453503720	Nov 03 2023 1	condition is very UNSAFE

114453473999	Nov 02 2023 1	Canton Springs & Dowd Ave intersection can be very dangerous. Cars block the intersection. Should be a light there.
114453398895	Nov 02 2023 0	CANTON Springs Road at Dowd Avenue cannot see traffic very well and it's a very dangerous intersection as it's always blocked by cars or high speeding cars coming off of 44
114453294339	Nov 02 2023 0	not commuting any more, use at less demanding times
114453250017	Nov 02 2023 0	People making the left turn at Old Albany or into Mizu 21 can block up traffic . A stoplight would have a big impact.
114453159706	Nov 02 2023 0	Drivers do not stop for pedestrian crossings
114453156359	Nov 02 2023 0	High speed traffic with no central median makes it dangerous.
114452942614	Nov 02 2023 1	The only problem I could see is at the Shops when I stop at a red light ready to make a right hand turn. For cars coming out of the Shops they get a green light to turn left onto 44. But there is no green arrow to allow me to turn right into the Shops. I'm in the lane to right so I do. There should be a green arrow there to allow us to go into the shops.
114452908395	Nov 02 2023 1	running red light
114452911726	Nov 02 2023 1	People drive too slowly
114452831654	Nov 02 2023 1	The poorly timed lights are generally farther down 44 closer to Bushy hill rd intersection all the way to base of Avon mountain. Making left hand turns can be difficult and cause of traffic congestion between Lovely street and points west of that.and the
114452812359	Nov 02 2023 1	I read More settlements coming to Dowd. Traffic should go thru rt 44, not dowd as much.

114452840156	Nov 02 2023	1	Distracted driving
			Congestion is higher at rush hours and backs up if someone has pushed the cross walk button but is expected. What is painful and problematic is the speed and acceleration thru
114452815192	Nov 02 2023	1	yellow / red lights by impatient drivers. concern for walkers etc crossing the road as some simply do not look. at other times some drivers do not stop for the
114452823678	Nov 02 2023	1	flashing light
114452800909	Nov 02 2023	1	Difficult pulling out of some businesses onto 44.
114452796285	Nov 02 2023	1	Many traffic accidents near the Dunkin Donuts motorcycles TEAR down Dowd Ave. at times! (mostly at
114452782545	Nov 02 2023	1	night) Turning left off old Canton rd onto dowd is very unsafe and
114452385542	Nov 01 2023	1	extremely dangerous
114452310853	Nov 01 2023	0	Idiots causing gridlock squeaking thru lights
114452231108	Nov 01 2023	0	Some business driveways are difficult to exit due to fast traffic
114450866918	Oct 31 2023	0	Lighting
			Traffic lights are the main problem. They seem to be added with no regard for the point of 44, which should be unimpeded travel. Similarly, why would "high speed traffic" be considered a problem on 44? If anything, the speed limits are too low (made obvious by the fact that virtually no one adheres to them, and combined with the traffic lights and increasing development, it makes traffic intolerable. That then pushes toward worse and more dangerous driving habits (weaving/cutting off; looking at phones more often because there's nothing else to do sitting in the traffic; running lights because stopping for 10 just to pick up a pizza is ridiculous;
114450684323	Oct 31 2023	1	(etc).

114450648682	Oct 31 2023 0: left turns holding up traffic So many people run red lights. Many are commercial
114450621478	Oct 31 2023 0: vehicles. Unsafe access to/from businesses that aren't supported by a
114450355952	Oct 31 2023 0: traffic light
114447091547	Oct 30 2023 0: Lacking lighting around area near fence place to McDonalds left hand turn at the Green backs up forever, also left hand
114448714320	Oct 28 2023 0: turns prior to cross to Dunkin Donuts backs up
114448676613	Oct 28 2023 1: Out of town assholes everywhere I drive, don't walk or bike but foot traffic should be
114448604275	Oct 28 2023 0: encouraged.
114447366418	Oct 26 2023 0: Left turns out of any business along 44 are very dangerous
114447039163	Oct 26 2023 1: Again timing of the study is ill timed. Coming out of Canton Springs Rd very difficult to make a left
114444409485	Oct 23 2023 0: turn onto Dowd. Erratic lane changing, weaving, distracted driving, racing,
114443450836	Oct 22 2023 0: crossing double line, excessive speeding
114443277119	Oct 22 2023 0: I don't drive there often enough
114443255747	Oct 22 2023 0: No problem on Dowd but RT 44 has become exceedingly congested. Speeders, unsafe drivers abound.
114442885842	Oct 21 2023 0: 44/cherrybrook intersection needs light timing fixed Drivers on 44 stopping to let someone take a left turn out of a business or driveway. Dangerous because traffic in the opposing direction can't see what is going on and may not
114442587348	Oct 20 2023 0: have time to stop.

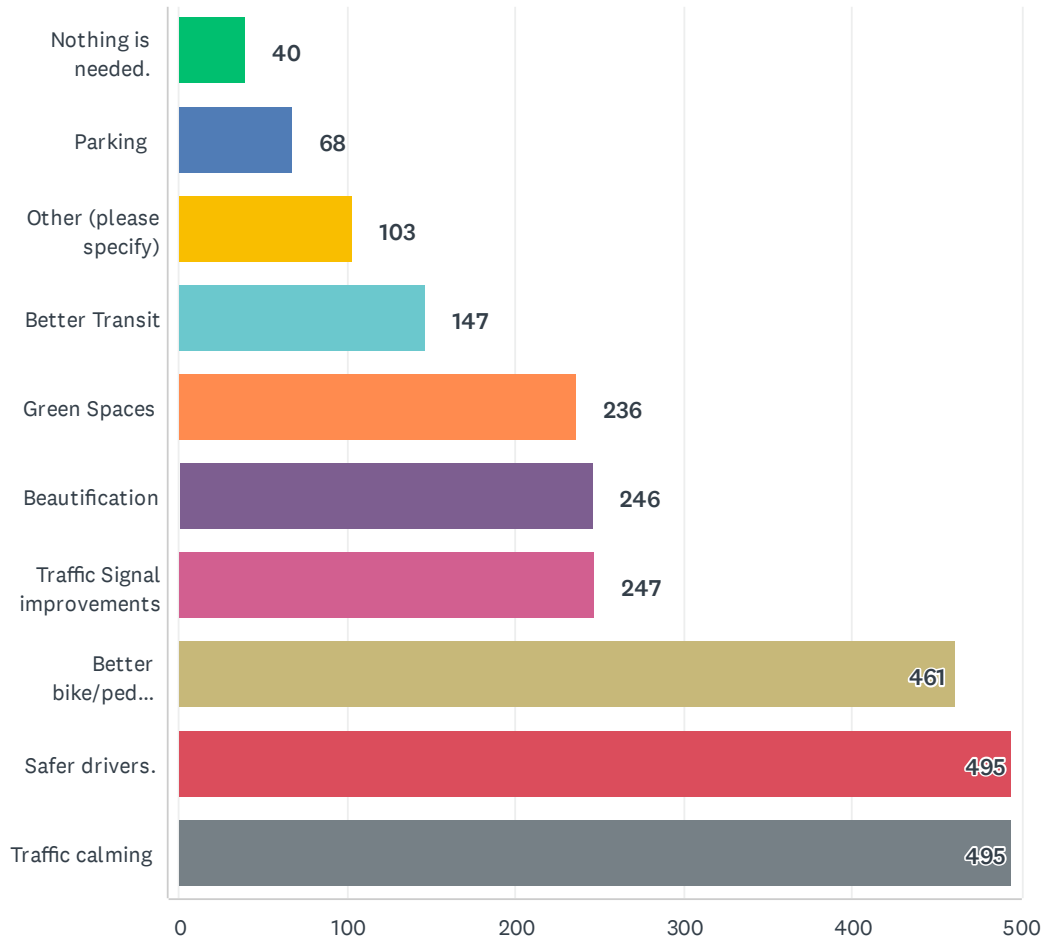
114442548894	Oct 20 2023 0	The light at Dowd to turn right (only) is rather long and creates tension when my husband is trying to get to work on time. When we lived in Illinois all of the traffic red lights were 20-30 seconds long and meant no backed up traffic lines and people getting irritable and impatient as much. Am rush hour light congestion at light by gazebo to get on rt44. Pm rush hour lights at shops and cvs are always backed
114441971839	Oct 20 2023 0	up.
114441698132	Oct 19 2023 1	People going through red lights on a DAILY basis. Difficulty pulling onto Dowd from side streets or Canton
114441474123	Oct 19 2023 0	Village
114441301671	Oct 19 2023 0	Constant construction delays in many spots
114441180563	Oct 19 2023 1	I just want to put unsafe driver behavior down twice because it is OUT OF CONTROL. Speeding, lane changing, running red lights is rampant.
114440897105	Oct 19 2023 1	Issues with west bound traffic taking a left on Dowd Rd Lack of turning lanes and/or turning prohibitions at popular businesses in a congested area. (eg: Dunkin, Blue House
114441082825	Oct 19 2023 1	Bagel) I think straightening the Dowd corner at the gazebo and making it a regular intersection instead of the right turn only
114441081974	Oct 19 2023 1	would improve things.

114441012584	<p>This is always a congested area, but when utilities need to block sections of the road to work on buildings that are feet from the street, it makes for a confusing and dangerous commute. It will continue to happen because there is no buffer zone for parking. I Don t know how far from the road these buildings are, but I know I can't put a portable shed within 50' of the road. Just some context.</p>
114440930669	<p>taking a left going west only allows one lane of moving traffic at the Dowd intersection</p>
114440892449	<p>Traveling south on Lawton, making left at light with green turn arrow, cars traveling north on Lovely St. make right on red w/o stopping, nearly hitting cars making left turn in intersection. Should be no turn on red sign on Lovely St. since drivers are unaware drivers on Lawton may have green turn arrow.</p>
114440879690	<p>Drivers stopped on 44 waiting to cross traffic into businesses. For example at Dunkin.</p>
114440885754	<p>When traveling West on RT44, the area before gets congested in left lane with people stopping waiting on traffic to turn into Dunkin Donuts and other small businesses there. People tend to weave in and out of those lanes at fast rates of speed.</p>
114440373539	<p>traffic lights are congested, the left to collinsville/burlington should be its own lane which would leave two open for traffic. Lights are horribly times. I drive 18 miles to work and it takes me 1 hour. rt 44 is a mess. Canton is just as bad as rt 44 in Hartford.. minus the pedestrians.</p>
114440067316	<p>Long time to enter roadway from parking lot</p>

114439058173	Oct 17 2023 1: See E Hill Road comment
114438257490	Oct 16 2023 0: It feels unsafe when drivers turn left onto Route 44 at places that lack traffic lights (ie from Dunkin Donuts)
114438099221	Oct 16 2023 0: Police used to enforce speed limitss by driving the speed limit during rush hour and that helped
114438029737	Oct 16 2023 0: Consideration should be given to a three way signal at Dowd/44. Currently only right turn only when heading east from Dowd, forcing those who wish to turn left to cut through plazas or turning left across two lanes of eastbound traffic from local roads.
114437832482	Oct 16 2023 1: Extremely high speeds. A pedestrian or bicyclist will be hit.
114437809907	Oct 16 2023 0: Too much construction
114436027590	Oct 13 2023 0: The speeding on Route 44 through the "old" narrow part between Dowd and Lovely is chronic.
114435334325	Oct 12 2023 1: Driving west on 44 past dowd Ave requires you to be in the right lane. And you must pay attention to the traffic in the left lane because those people will realize that the last possible minute that traffic is turning on dowd Ave and they'll either have to stop or get in the right lane
114428628501	Oct 04 2023 1: Speeding!! Unsafe lane changes. Dangerous left turn urn egresses
114427646592	Oct 03 2023 1: Left turning cars
114426486446	Oct 02 2023 1: Delays due to people waiting to turn left into businesses.
114426083348	Oct 01 2023 1: Turning left onto 44 from Old Albany Turnpike

Q10 Are there any transportation or public space improvements needed along US-44 / Dowd Avenue in Canton? (check all that apply).

Answered: 863 Skipped: 6



US-44 Corridor Study - Canton Public Survey

ANSWER CHOICES	RESPONSES	
Nothing is needed.	5%	40
Parking	8%	68
Other (please specify)	12%	103
Better Transit	17%	147
Green Spaces	27%	236
Beautification	29%	246
Traffic Signal improvements	29%	247
Better bike/ped facilities	53%	461
Safer drivers.	57%	495
Traffic calming	57%	495
Total Respondents: 863		

Respondent ID	Response Date	Other (please specify)
114488293378	Dec 13 2023 0	Stop building apartments!
114484773816	Dec 09 2023 0	Less traffic volume to increase safety
114476282928	Nov 30 2023 0	Higher speed limits for today's vehicles (not 50+ year old speed limits). Would cutting 44 down to 1 lane each way with a turn lane and bike lanes, sidewalks through canton help calm the excess speed and make better walkability.
114463468212	Nov 14 2023 1	There is no public transit.
114448747468	Nov 05 2023 0	NO TRAFFIC ISLANDS OR SPEED BUMPS!
114454776818	Nov 04 2023 1	Beautification of the parking area where Ace is would be great. That area would also be great for condos or a small apartment complex.
114454428742	Nov 03 2023 0	traffic circles to slow/calm traffic and reduce wait time at lights
114454311668	Nov 03 2023 0	The bus stop that is currently available on rt 44 traveling East should include a stop in to the back parking lot of The Shoppes of Farmington Valley to promote assist with people using public transit to shop there and also for people seeking employment within companies located in The Shoppes. There is a stop in the commuter lot of Walmart parking lot in Avon. Canton should have the same option- there is plenty of space in the back parking lot area near the red bard for a Public Bus to be able to
114453971169	Nov 03 2023 1	have a bus top
114453961135	Nov 03 2023 1	IMPROVED SIDEWALKS AND BETTER BIKING AND PEDESTRIAN ACCESS TO BUSINESSES ALONG ROUTE 44

114453801455	Nov 03 2023 0	us-44 cannot be used safely by a cyclist due to the lack of space and the traffic congestion. sidewalks are not maintained, and are in disrepair. site lines are also impaired due to foliage. Turn lanes into some of the plazas/shopping areas or a central turn lane so people making left turns don't block traffic for long periods of time.
114453821804	Nov 03 2023 0	sidewalks
114453716151	Nov 03 2023 0	Sidewalks on both sides of Dowd & Rt 44
114453572549	Nov 03 2023 0	Left-hand turn bays on Route 44 between Secret Lake Rd and route 167. Also the overhead power lines on Dowd are very ugly and unsightly. These lines should be run underground. It would vastly improve the aesthetics of the area.
114453503720	Nov 03 2023 1	Canton Springs Rd & Dowd Ave. Cars block the intersection.
114453473999	Nov 02 2023 1	Should have a light.
114453438260	Nov 02 2023 1	There is too much congestion. There is a conflict of people speeding through canton and people who shop and live here. It's very dangerous trying to stop for a turn to go into shops because of the people speeding behind you and on the sides zigzagging through traffic. It doesn't make sense that the speed limit is 35 on 44 and 40'on cherry brook btw because cherry brook is residential and people walk and it's too fast Need to be considerate of kids and people on the roads infront of their houses and the noise level. Should be 35.
114453428783	Nov 02 2023 1	Traffic signals are not well coordinated. I can sometimes be stopped at Dowd, Lovely, CVS, and the Shoppes. CANTON Springs Road should go straight out to Route 44 with a traffic light block off the Dow Avenue side coming from 44 and put a stop sign for cars coming down Dow Avenue to CANTON
114453398895	Nov 02 2023 0	Springs Road

114453316396	Nov 02 2023 0	Bikers and pedestrians need more space on Dowd for safety. Pedestrians should be using the sidewalk not the road.
114452840128	Nov 02 2023 0	Canton village/ ACE/Post Office plaza and access to Dowd Ave. Sidewalks....we could really use a sidewalk from secret lake road
114453250017	Nov 02 2023 0	to bushy hill.
114453156359	Nov 02 2023 0	Put a roundabout at 44/dowd insection to simultaneously slow traffic but keep it flowing.
114453084582	Nov 02 2023 0	Close off the road entry way from the green to dowd.
114453018261	Nov 02 2023 0	see #8 regarding pedestrian crossing at intersection of Dowd and Commerce Drive.
114453051689	Nov 02 2023 0	I think it's rally and unsafe area to walk or bike - mostly at the aforementioned corner. Also lots of traffic with cvs and Mitchell and mall
114452978165	Nov 02 2023 0	Winter/snow makes it unsafe to walk along 44-very little sidewalks. No shelters while waiting for bus.
114452942614	Nov 02 2023 1	Stop speeding.
114452914982	Nov 02 2023 1	Sidewalks connecting shopping areas from Canton Village to Best Buy.
114452917176	Nov 02 2023 1	Discourage through traffic use of ct565 as shortcut between rt 44 and collinsville. Is there some way to make the intersection of Dowd/Maple/Symonds/Old Canton Rd/East Hill Rd impassible to through traffic (except emergency vehicles)?
114452877244	Nov 02 2023 1	More shade trees along Rt. 44 and Dowd Ave., which would help to hide some of the hideous condos and car dealerships. All the beautiful trees that used to line the streets were taken down and never replaced.
114452856222	Nov 02 2023 1	PLEASE - NO MURALS OR PLANTERS - MANY PEOPLE ARE TOO DISTRACTED TO BEGIN WITH.
114452848836	Nov 02 2023 1	Please do not add more public transit

		Don't add more un charming developments/ apartments (transients rather than owners) on or around Dowd. Keep them on rt 44. Keep trees and vegetation = charm of Canton and
114452812359	Nov 02 2023 1	Collinsville
114452822369	Nov 02 2023 1	Sidewalks
		Stop adding to the problems with increased development and
114452794221	Nov 02 2023 1	high density housing.
114452783918	Nov 02 2023 1	More EV charging stations
114452611389	Nov 02 2023 0	Dowd Av and canton springs intersection is unsafe
		A traffic light where Canton Springs Road meets Dowd would be
114452600525	Nov 02 2023 0	good
114452373909	Nov 01 2023 1	better sidewalks and crosswalks
114452310853	Nov 01 2023 0	Traffic cameras for ticketing like up north
		A bypass. This is a state highway with alot of vehicular traffic. It
114451288308	Oct 31 2023 0	is not a "village shopping" pedestrian area. One or the other.
		I'd add "better bike and pedestrian facilities" and "safer drivers" but I know that that will just be translated to "slow down," and that is absolutely, positively, not the solution. That makes everything less efficient and WORSE. I would much prefer to walk/ride on roads that aren't 44 than to try to make 44 even worse for cars. And people who can't handle driving at reasonable speeds shouldn't be driving. The world doesn't need
114450684323	Oct 31 2023 1	to go 20 so five people in Priuses can be comfortable.
		Better lighting along corridor to enhance New England feel and
114450355952	Oct 31 2023 0	promote safety for pedestrians
114447091547	Oct 30 2023 0	More sidewalks
114449637714	Oct 30 2023 1	lighting in high traffic areas

	The fabric of Canton is being altered. It seems it's being converted to another road like Berlin Turnpike. There is no urban planning. The new apartments on 44 on north side near Dyer Cemetary will just add traffic volume without any change to the
114449065253	Oct 29 2023 0: road. It's quite unbelievable.
114448761129	Oct 28 2023 1: B
114448751309	Oct 28 2023 0: Consider rotary instead of stop lights
114448604275	Oct 28 2023 0: Emphasis on public transportation. Regular busses or jitneys. Left turns off 44 can be dangerous with cars stopping abruptly or cars stuck behind someone turning left deciding to switch into
114447366418	Oct 26 2023 0: the right lane
	Increased visibility (East & West) at Canton Springs Rd and Dowd Ave intersection. Better traffic control of cars entering Dowd Ave from Rt 44 westbound. Better traffic control at Canton Springs Rd & Dowd Ave. Drivers on Dowd coming east & approaching Canton Springs Rd "block the box" even though there is a sign saying NOT to do so. The "box" needs to actually
114447255799	Oct 26 2023 0: be lined and defined on the blacktop to bring attention to it.
114447045501	Oct 26 2023 1: Less congestion
114447039163	Oct 26 2023 1: A center turn lane is sorely needed all along the route. Too late. Congestion already here and with the building going on
114446406905	Oct 25 2023 0: it's just adding to it The road is too heavily used and more and more congested as well as way too fast traffic. Sometimes I feel like I need a traffic
114443525588	Oct 22 2023 0: light at my driveway just to get out.
114443270404	Oct 22 2023 0: Shoulders
	Safety should be the main focus. More of a police presence. No
114443255747	Oct 22 2023 0: one should be walking or biking on RT 44, EVER.
114443007286	Oct 21 2023 0: Better crosswalks and lights
114442587348	Oct 20 2023 0: Signage regarding safe/unsafe left turns onto 44.

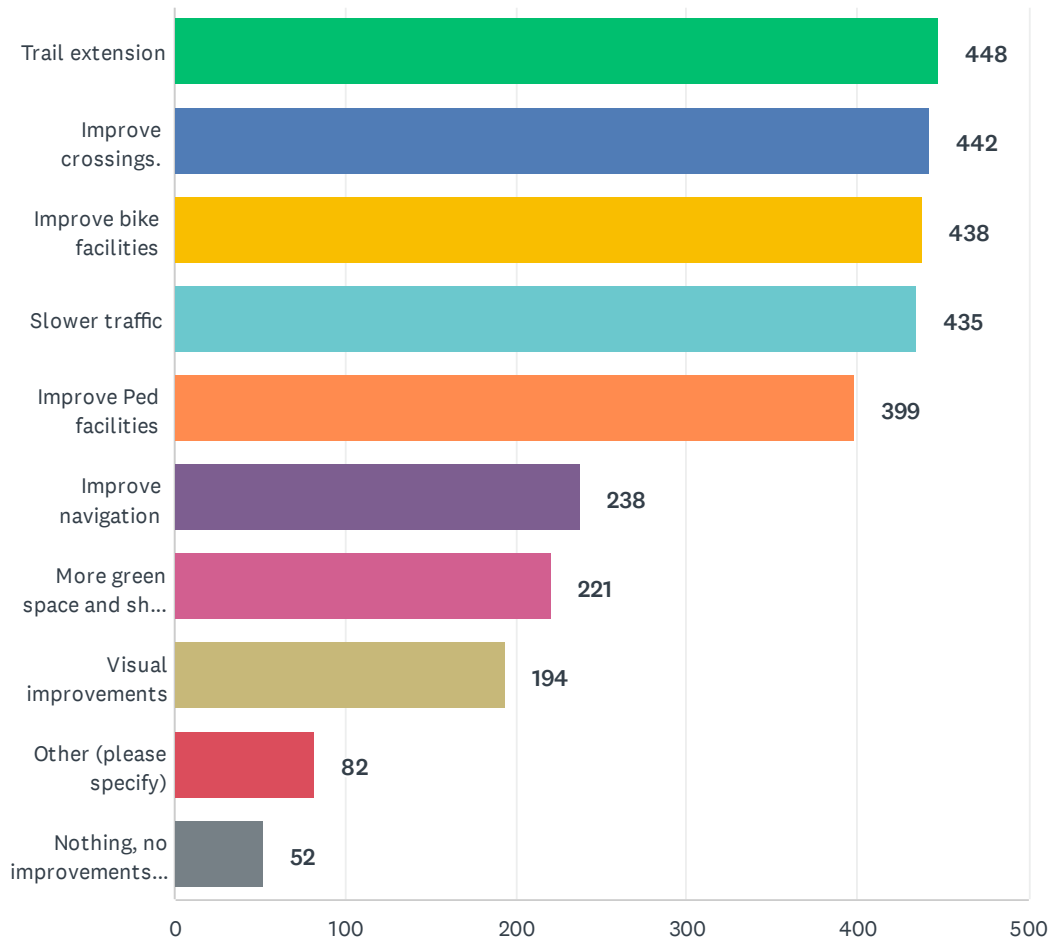
114442323310	Oct 20 2023 0	Just the missing sidewalk The lanes become very narrow in that area compared to the rest
114442253444	Oct 20 2023 1	of 44 The sidewalk along the professional building by Canton Valley Circle near the old white church looking building and antique
114441971839	Oct 20 2023 0	shop is non existent and ends..
114441970725	Oct 20 2023 0	Stop building!! We don't need so many darn apartments Cut down on the developments contesting the small town of
114441360407	Oct 19 2023 0	canton
114441284712	Oct 19 2023 0	Traffic cams to dissuade people from running red lights. More enforcement of traffic violations. Almost nihl enforcement
114441098671	Oct 19 2023 1	right now.
114441081974	Oct 19 2023 1	I have always thought that the CT Transit bus stops should have a dedicated shelter for passengers to wait safely and out of the weather. Right now the Dowd Ave. stop is dangerous. Left turn lanes. Traffic piles up and there are sometimes sudden stops because of left turning drivers. It causes people to swerve
114441031567	Oct 19 2023 0	around them
114441012584	Oct 19 2023 0	N/A Better traffic signal coordination. If they were all synced
114440957768	Oct 19 2023 0	together, it would be better.
114440892449	Oct 19 2023 0	Dedicated center turn lane onto Dowd Ave. A town gathering space needs to be establish, the green is too
114440890993	Oct 19 2023 0	small and too congested Barrier preventing cars from crossing highway between Lovely
114440879690	Oct 19 2023 0	street and Down
114440880951	Oct 19 2023 0	People cut over on Dowd to avoid all the traffic lights on 44
114440883356	Oct 19 2023 0	Automatic bike signal at the Allen Place/Dowd Ave crosswalk Regarding traffic calming, I am not in favor of reducing the width
114440648904	Oct 18 2023 1	of traffic lanes.

114440609759	Oct 18 2023 0:	Sidewalks on both sides of Rt44 and Dowd
114440561630	Oct 18 2023 0:	Decent light controlled crosswalks traffic should be flowing not dead stop every light. Rt 44 is a shamble of lights very similar to Albany Ave in Hartford and Main
114440373539	Oct 18 2023 0:	street in Winsted.
114440067316	Oct 18 2023 1:	shifting some bus stops and provide shelter
114439429789	Oct 17 2023 0:	Isn't there an unlit crosswalk near canton Village? Scary unsafe
114439308802	Oct 17 2023 0:	Who is suppose to pay for these improvements
114439231288	Oct 17 2023 0:	Middle lane on 44
114439058173	Oct 17 2023 1:	See E Hill Road comment Public transportation is REALLY low very few times buses come
114438738135	Oct 17 2023 0:	by. Makes it harder to plan things with my teens
114438373950	Oct 16 2023 0:	traffic light at Blue House Bagel Co. or Dunkin' Donuts I would like this area to be integrated into the community better, including walkability and features intended for walkers or bikers
114438257490	Oct 16 2023 0:	(safe paths, benches, and especially, safe street crossings. Route 565 road designation as a collector road and not an arterial should be respected. Traffic coming from a southern direction (Farmington and Burlington should be directed onto 179 and then to 44. Traffic coming from an easterly / northeast direction should be directed to remain on 44 and then travel in a southerly direction on route 179. There should be a roundabout
114438224384	Oct 16 2023 0:	placed at 179 and 202 as well as 565 and 179 Faster access in and out of shops - only one entrance causes
114437981887	Oct 16 2023 1:	inconvenient delays Shared parking among the businesses and better pedestrian
114437869236	Oct 16 2023 1:	connectivity.
114435334325	Oct 12 2023 1:	Signs that say no bikes on 44!
114430314230	Oct 06 2023 0:	Need left turn lanes along the entire route

114429977999	Oct 05 2023 0:	Crazy loud drivers ☐
114429073064	Oct 04 2023 0:	get the bikes of the Road Dedicated center turn lanes. Multiple accidents have occurred at the Dunkin on 44 due to no dedicated turn lanes into businesses. Poor sight lines at 44 and Old Albany Tpk and poor sight lines at
114428826101	Oct 04 2023 0:	44 from Dunkin to Dowd Not sure how to improve this but drivers often do not stop to allow trail users to cross. Maybe a full blown red light when you hit the button. Signage that includes bikers too not just pedestrians. I've had people yell at me that I am not a
114427043071	Oct 02 2023 0:	pedestrian as I am trying to tobcross I have the people at the car dealership keep their automobiles and trucks off of the public sidewalks. There are times that they
114426871870	Oct 02 2023 0:	force people into the road to walk/cycle by.
114426773800	Oct 02 2023 0:	Bus pull off areas Shoulder width of the road should be improved if road work is done. This would require road redesign to do this. I am also not in favor of "traffic calming" as it generally creates choke points which create unsafe conditions for cars AND CYCLISTS. Good
114426570078	Oct 02 2023 1:	predictable traffic flow works well for all parties.
114426537848	Oct 02 2023 1:	Separated bike/walk lanes. Less driveway cuts. A turn lane to keep both lanes flowing. A rear service road to funnel turning vehicles to common exit/entry points. Bike/pedestrian crossings should be elevated or tunneled
114426350107	Oct 02 2023 0:	under roadway
114426114851	Oct 02 2023 1:	Not let cyclists use MAIN ROADS

Q11 How do you think US-44/Dowd Avenue could be improved for non-motorized users (bicyclists, pedestrians, scooters, etc.)? (Check all that apply)

Answered: 859 Skipped: 10



US-44 Corridor Study - Canton Public Survey

ANSWER CHOICES	RESPONSES	
Trail extension	52%	448
Improve crossings.	51%	442
Improve bike facilities	51%	438
Slower traffic	51%	435
Improve Ped facilities	46%	399
Improve navigation	28%	238
More green space and shade trees.	26%	221
Visual improvements	23%	194
Other (please specify)	10%	82
Nothing, no improvements are needed.	6%	52
Total Respondents: 859		

Respondent ID	Response Date	Other (please specify)
114488293378	Dec 13 2023 0	Stop building apartments.
114488254000	Dec 13 2023 0	Sidewalks Roads are not safe "as-is". Need less cars in the
114486084664	Dec 11 2023 0	area, not more.
114484773816	Dec 09 2023 0	Less car volume in the future We have a rail trail. The crossings should be enhanced like elsewhere on the trail. Bikes and cars
114484336461	Dec 08 2023 0	don't mix well
114456437933	Nov 06 2023 0	Put a traffic light there Restickkes bikes from Albany TPK. the road is not
114455913214	Nov 06 2023 0	safe or wide enough for bi more sidewalks, maybe on both sides. Green space
114454311668	Nov 03 2023 0	and trees along Rt 44 are essential
114454155029	Nov 03 2023 0	Wider sidewalks on both sides, with protective curbs. License all bike riders / make it mandatory to wear
114453725352	Nov 03 2023 0	reflective clothing for pedestrians and bike riders
114453725032	Nov 03 2023 0	A "no turn on red" sign from Lovely street onto 44 Intersection of Simonds onto Dowd is dangerous (4
114453572549	Nov 03 2023 0	way there then feet before turning) Dowd is not an aesthetically pleasing street due to ugly overhead power lines. These lines are an
114453503720	Nov 03 2023 1	eyesore and should be run underground.

114453438260	<p>Eliminate advertising signs. It's an embarrassment that as you cross into canton on rt 44 there is an adverstismwnt bigger than the twin sign. And new businesses put too many flags that block the views to rt 44 as you pull in and out of the parking. There should be no signs in the apron front of these</p> <p>Nov 02 2023 1 premises.</p>
114453398895	<p>And all the years I've lived in CANTON. I've only seen one person bike down 44. I think it's a waste of money and time bicycles shouldn't be on that road anyway it's unsafe and I don't think it's appropriate. I certainly wouldn't wanna ride my bike down that road.</p> <p>Nov 02 2023 0</p> <p>I am happy with the crossing at Lawton/44 and all stops east. Crossings to the west of Lawton are irregular. The side walks on 44 are missing or</p>
114453175292	<p>Nov 02 2023 0 narrow.</p> <p>I would not treat Rte. 44 the same as Dowd Ave. Rte. 44 is definitely a major commuter highway and to encourage pedestrian/bicycle use only creates further incompatibility safety issues. Longterm study may indicate direcdting commuter traffic from Dowd to River Road Rte. 179 to avoid residential uses</p>
114453018261	<p>Nov 02 2023 0 along the highway?</p> <p>The pedestrians and bikers really need sidewalks, area away from the main road. It's too laden with car</p>
114452978165	<p>Nov 02 2023 0 traffic and those cars drive way too fast!</p> <p>Consider a blinking red light at crosswalk so cars have to stop. Not all of them but near Riverside</p>
114452908395	<p>Nov 02 2023 1 Nursery and also on Maple/Dyer</p>
114452887747	<p>Nov 02 2023 1 maybe a bridge to cross rt 44 for the trail use, safer for bikers and walkers</p>

114452866994	Nov 02 2023 1	A bikeway similar to the Farmington trail on the north side of US44 would sandwich the businesses in this area with pedestrian friendly traffic and create a walkable downtown atmosphere. More access to businesses from the existing bike path.
114452856222	Nov 02 2023 1	THIS AREA WAS NOT CREATED FOR bicyclists, pedestrians OR scooters AND COULD REALLY BE TOUGH TO RE-ENGINEER, AND FOR WHAT - WALK TO SHOPRITE AND BACK WITH 2-3 BAGS
114452844707	Nov 02 2023 1	OR GROCERIES? Put in a sidewalk
114452812359	Nov 02 2023 1	Don't destroy the town's charm. The dowd entrance welcomes a traveler. No more gaudy signage or flashing lights. Speed bumps work!!!!lights
114452815192	Nov 02 2023 1	Sidewalks on both sides of the street
114452823678	Nov 02 2023 1	no idea
114452785733	Nov 02 2023 1	A turn lane for Dunkin Donuts or traffic light would help for that complex
114452794221	Nov 02 2023 1	Extend a trail along 44? Truth is, very few people walk it.
114452782545	Nov 02 2023 1	I see bicycles alot in summer, I worry about them getting hit! People just CAN'T SLOW DOWN!!! :(pedestrians and cyclists should abide by traffic laws
114452784828	Nov 02 2023 1	and should be cited for infractions.
114452783918	Nov 02 2023 1	More sidewalks
114452780339	Nov 02 2023 1	Specifically bad/dangerous drivers
114452175232	Nov 01 2023 0	Not allow bike on 44
114452084449	Nov 01 2023 0	Widen sidewalks

114451687049	Nov 01 2023 0	bicycle-pedestrian cross over bridge t 44 and Lovely St.
114451556481	Nov 01 2023 0	A safer way to cross 44 at East Hill Rd and also Lawton Rd.
114451288308	Oct 31 2023 0	Remove the through traffic I'll say AGAIN since it's being pushed in so many questions - slowing down car traffic does NOTHING to help pedestrians or bikers, and I say that as an avid outdoors-person. All it does is piss off decent drivers.
114450684323	Oct 31 2023 1	lighting in some hihg traffic areas
114449637714	Oct 30 2023 0	Rails to trails is right there. And there are sidewalks. Provide better and more available public information about Farmington River Trail and options for accessing it.
114449492380	Oct 27 2023 0	Drain the excess water that is killing all the trees on the bike trail between Canton Springs road and Commerce Avenue.
114447806569	Oct 25 2023 0	I don't bike or use scooters or walk there much so I have no view
114446443025	Oct 23 2023 1	Cleanup of unkempt properties on Dowd and stop the building of more units which only add to more traffic congestion than the road can comfortably hold. Perhaps closing off the intersection at 44 and making it a dead end. So many accidents occur at that intersection. All thru traffic should probably use River Road if the object of the drive is US 44. So many large trucks now use Dowd.
114443987547	Oct 22 2023 0	Shoulders
114443525588		
114443270404		

114443255747	Oct 22 2023 0	SAFETY FIRST, not beautification. Get crazy drivers off the road. MORE POLICE PRESENCE!!!
114442323310	Oct 20 2023 0	non motorized do not belong on rt 44 anywhere
114441970725	Oct 20 2023 0	Route 44 is not meant for walking or biking unless you have a death wish. Non motorized activities do not belong there.
114441698132	Oct 19 2023 1	I don't trust the motorists on Rt 44 in a vehicle. I would NEVER trust walking or biking in the area. I'm afraid to drive in this area over the past 5 yrs. I do drive on Rt 44 every day.
114441492253	Oct 19 2023 0	I don't feel the area is meant for pedestrian traffic. too busy
114441472476	Oct 19 2023 0	Better access to Donut and Bagel places on north side of 44 for bikers and walkers
114441430660	Oct 19 2023 0	Fix dowd ave. entrance from rt 44.
114441284712	Oct 19 2023 0	Bicyclists should not ride on 44. its the country. Budgets are limited and funds should be used to improve bike safety in the city
114441056773	Oct 19 2023 0	where people actually ride bikes build off road trails no one feels safe on the road
114441038642	Oct 19 2023 0	even in bike lanes
114441012584	Oct 19 2023 0	N/A
114441015118	Oct 19 2023 0	continuous sidewalks along 4 in canton and avon
114440890993	Oct 19 2023 0	Create an alternate route to lessen traffic Farmington trail is not maintained in winter and is not always a safe place to walk due to lack of
114440879690	Oct 19 2023 0	surveillance and bears crossing the trail.
114440770734	Oct 19 2023 0	bike lane to Avon While I know many people enjoy biking to work and to get to their destinations, I don't think that
114440648904	Oct 18 2023 1	44 is safe for bike traffic.

114440067316	Oct 18 2023 1	Thank you Do not add apartments or high density housing to
114439308802	Oct 17 2023 0	the area
114439279623	Oct 17 2023 0	Making decent shoulders on 44. Sidewalks are in bad shape. Uneven, cracks. Very dangerous on incline in front of the shops at
114439036704	Oct 17 2023 1	Farmington Valley clear bike paths that branch off the FRRT so that it could be used for transportation as well as recreation. Because in my opinion the FRRT is pretty useless to a lot of canton as anything but a place to
114438923543	Oct 17 2023 1	ride your bike for fun. Enforce pedestrian crossing rules. Sometimes I swear drivers sometimes speed up when I go to cross at a cross walk. The blinkers seem to have helped to a point but I fear some drivers see them as
114438305390	Oct 16 2023 0	a challenge. Correctly utilize routes in and around study area to coincide with their respective designs. Arterial traffic should stay on arteries and collector traffic on collector roads. Route 44 and 179 are arterial roads.
114438224384	Oct 16 2023 0	SR 565 is a collector road.
114438099221	Oct 16 2023 0	More sidewalks and crosswalks Perhaps a pedestrian/bicycle flyover of 44 for the
114438106921	Oct 16 2023 0	trail at the two largest crossings. . A spot to cross 44 that goes over or under so as not
114437987320	Oct 16 2023 1	to disrupt traffic Add a stop light by Alan and also by riverside nursery. Somebody is going to be killed or seriously
114437832482	Oct 16 2023 1	hurt.
114436027590	Oct 13 2023 0	Dedicated bike/walking path physically separated from Route 44

		If you value your life stay off of route 44 and out of unless there's a light and a crossing of the farm into
114435334325	Oct 12 2023 1	River trail there
114429977999	Oct 05 2023 0	Add sidewalks where there are none
114429490357	Oct 05 2023 1	Keep bicyclist and cars separated
114428873488	Oct 04 2023 0	More signage
114427337728	Oct 03 2023 0	Right on red should be allowed at Dowd/44 Again, shoulder width should be improved. Narrowing of car lanes a bit on RT 4 in my town greatly improved the ability to cycle on RT 4. It may
114426570078	Oct 02 2023 1	apply to Rt 44 also.
114426537848	Oct 02 2023 1	Sperate bike/walk lanes.
114426350107	Oct 02 2023 0	Crossings should be elevated or tunnelled
114426083348	Oct 01 2023 1	Bike trail through the Shops

Q12 Please feel free to leave additional thoughts about how to improve US-44/Dowd Avenue in Canton. Please limit comments to 100 words.

Answered: 378 Skipped: 491

US-44 Corridor Study - Canton Public Survey

Please feel free to leave additional thoughts about how to improve US-44/Dowd Avenue in Canton. Please limit comments to 100 words.

Answered 378
Skipped 491

Respondent ID	Response Date	Responses
114488435062	Dec 13 2023	0 US-44 is very dangerous. I was hit in front of Stepping Stones two weeks ago at a high speed while I was stopped waiting to turn into the parking lot. It's not safe for parents or kids to go to and from our daycares in town.
114488396612	Dec 13 2023	0 Extending the bike path to the Canton Shops would be greatly appreciated! More of a buffer and safer walking/running conditions along 44 would greatly improve quality of life in the
114488306631	Dec 13 2023	0 area.
114488293378	Dec 13 2023	0 Stop building apartments!
114488254000	Dec 13 2023	0 Traffic Light at Rt 44 and Colonial Road
114488129084	Dec 13 2023	0 No more traffic lights. Stop developing in a way that adds more street lights. Use Route 44 corridor from W Avon Rd in Avon to
114488123524	Dec 13 2023	0 Avon Mountain as prime example of what canton is to become.
114486084664	Dec 11 2023	0 Focus on people and how we to provide leadership to facilitate and encourage walking, bicycling, ebiking, wheelchairs, electric wheelchairs and future travel modes, instead of building better roads to enable more car traffic.
114484773816	Dec 09 2023	0 If the study is supposed to set guidance for 30-40 years in the future, please consider that bike/pedasstrian movement should be a focal area. We, as a town, must think about our footprint and impact to our surroundings. I applaud improvements to our roads, but the emphasis should be on non-vehicle movement.
114484336461	Dec 08 2023	0 Traffic patterns have changed with the loss of over 1,000,000 sf of occupied office space in Simsbury and Avon over the past 10 years. This has changed traffic patterns dramatically on Route 44. In particular the Lawton Road intersection was used as a cutoff the to Hartford Insurance Group and Chubb.

114483775510 Dec 08 2023 0 Some how make it so less traffic will be on residential side roads like Lawton and Washburn roads. The traffic and speed of drivers on these SMALL RESIDENTIAL roads is going to take a life if nothing is done.
114483618400 Dec 07 2023 0 Nothing to add at this time

114482195050 Dec 06 2023 0 Any minor traffic issue (fender bender, construction/utility work, etc) can snarl traffic on US-44 in Canton even during off peak travel hours. The frequency that the road (US-44) gets closed for periods due to accidents that are more serious but non-fatal or non-extensive within Canton is also alarming. Bicyclists and pedestrians create a traffic hazard when they don't follow the laws of the road and/or make use of the provided lights at crossings (for the crosswalks that have them - not all do).
114482107110 Dec 06 2023 0 N/A

114481899050 Dec 06 2023 0 The traffic congestion in the study area has significantly worsened in the last year. I have noticed that once I am out of Canton Rte. 44 opens up and traffic flows more quickly.
114478411524 Dec 02 2023 0 A physical barrier between the bike lane and the traffic

114476282920 Nov 30 2023 0 Design is the problem here--not speeding. The speed limits were set many years ago when vehicles did not have all the safety features they have now. The actually physical signs are older then almost every car on the road. Instead of complaining that drivers go faster than the speed limits, improve the design by increasing the limits by 5-15 mph depending on the road.

114475514210 Nov 29 2023 0 This area is just a nightmare. And to think of adding more people and cars to this! I can't move out of Canton fast enough.

114466376280 Nov 17 2023 1 Traffic lights at Secret Lake Road and car dealerships are offset by 25 ft and not sync'd allowing independent operation / timing. Alignment should be adjusted so a single light can be used by both. Also, exit out of shops should have an option to connect directly to Lovely St.

114465807320 Nov 16 2023 0 Pedestrian Bridge in multiple locations to cross route 44, central turning lane on 44, designated, safe cycling & Pedestrian lanes with curbing as shown in diagrams found in Canton POCD, protected bus stops with connectivity to businesses.

114463468210 Nov 14 2023 1 Would a round about work better or a different configuration at that area? Traffic calming is needed. People go way over the speed limit. After Down going west cut to 1 lane each direction with middle turn lane and bike lanes, sidewalks to calm traffic.

114457646130 Nov 08 2023 0 Speeders need to be held accountable by giving them tickets not verbal/written warnings. It will never change otherwise.

11444874746 Nov 05 2023 0 There are now 2 bedroom apartments north of RT 44 and no safe way for tenants (children) to cross to the south side of RT 44.

11445542444 Nov 05 2023 0 Better driver behavior.

114455234431 Nov 05 2023 0 Extending the safety of the bike and walk trail further on Lawton or Notch road.

114454783657 Nov 04 2023 1 More police presence

114454708203 Nov 04 2023 0 The green is attractive and a benefit to the town but accessed very little due to safety concerns at the intersection.

114454504172 Nov 03 2023 1 Route 44 is a business/strip mall/shopping/get to where you need to go type of road. If you slow it down you are really messing with something that isn't broken and just going to cause delays and much frustration for everyone. I don't use Dowd as often so I won't comment. Collinsville is where you can relax as a pedestrian because it was designed Century ago. The rest of Canton was not. Leave it alone.

114454460026 Nov 03 2023 0 The more green space, and maybe some twinkly lights on the trees in the area can really liven up the walkability and the attractiveness. Little details like that make a huge difference. Especially the little twinkly lights that many towns put in their centers. It attracts people to take a stroll as opposed to drive right through.

114454311668 Nov 03 2023 0 Traffic humps on Dowd Avenue, traffic circles, reducing turns across traffic (such as at Dunkin Donuts), and MORE TREES and green spaces. Extending the Farmington River Trail is important too

114454299033 Nov 03 2023 0 I feel that it's fine the way it is. I moved to a more rural area and Canton seems to be nonstop building, making more suburban. We have to much now!

114454262661 Nov 03 2023 0 Even if there were major improvements to sidewalks, etc., it still wouldn't be a pleasant place to walk because of all the traffic and noise. That's just the way it is. If it was safer, that would be a bonus.

114454191773 Nov 03 2023 0 All of traffic needs to be calmed; all are in a rush & drive way too fast.

114454139460 Nov 03 2023 0 probably the best way to improve Route 44 is to reduce traffic which is not going happen. Another is to make left turns easier, and reduce the the drive through traffic at Dunkin Donuts

114454036810 Nov 03 2023 1 There doesn't seem to be any police presence to slow down erratic, fast drivers. It's dangerous to even drive on Rt.44! Lots more traffic in past few years.

114454023704 Nov 03 2023 1 Install a mirror so that traffic coming from Canton Springs Rd could see cars coming into Dowd from US 144.

114453971169 Nov 03 2023 1 This is well needed. Thank you for your investment into this study. Canton has a high traffic flow which affects the town EMS which can be strained by constant accidents in this section. The area to turn Left into Dunkin Donuts traveling West is also a high problematic spot with vehicles traveling way too fast along the 44 corridor through Canton. Please attempt to bring a Public Bus Stop in to The Shoppes of Farmington Valley-it can be done!

114453801455 Nov 03 2023 0 We have a lots of cops in this town...how about enforcing the laws. They, the Canton P.D., needs be more present. You don't need a grant or state or federal money to enforce distracted driving. They are already hired to serve and protect.

114453812032 Nov 03 2023 0 Lots of road rage and speeding unfortunately. Not sure how that's fixable though.

114453790792 Nov 03 2023 0 Police bicyclists on patrol especially on the trail. Signs asking bicyclists to announce before they pass pedestrians whether announcing " on your left" or ringing a bike bell, please, this is so important for safety Highly support walkability, extending the bike trail and beautification of the area. We live a few miles away and use this stretch often, really love the idea of walking/biking access to the small businesses in the area and possibly slowing down traffic as pedestrians grow.

114453777074 Nov 03 2023 0 More police presence at those areas where "NO Turn on Red" signs are. Rarely are they obeyed. Or better light timing is needed.

114453762944 Nov 03 2023 0 I think a push to get more people outside in our town would be great. More bike and walking paths are great. The proposed new pump track is also a wonderful idea. We live in such a beautiful area it would be great to enjoy it as much as possible!

114453732080 Nov 03 2023 0 people are the problem License all bicycles that want to be on the streets / make it mandatory to wear reflective clothing for pedestrians and bike riders so we all can see you ,

114453716151 Nov 03 2023 0 Collinsville is a fantastic village. Rail trails are super, too!□
Keep the historic and charming character of Collinsville. Canton Canoe and Kayak, LaSalle's Market and the Crown and Hammer are all great, too. Wish something could be done to save the Axe Factory. Sidewalks on both sides of Simonds, Dowd, Route 44 with additional crossings. Biggest mistake made was allowing 2-way traffic both sides in the Ace/Post Office plaza. Speeding, accidents. Need to avoid this for this new area of study

114453572549 Nov 03 2023 0 (1) Add left hand turn bays to Route 44 between Secret Lake and Route 167 to improve safety. (2) Dowd is not an aesthetically pleasing street due to ugly overhead power lines. These lines are an eyesore, ruin the look of the town, and should be run underground.

114453503720 Nov 03 2023 1

114453467184 Nov 02 2023 1 Stop pandering to people that drive through this area. Make it safe and nice for the people that actually live here.

114453438260 Nov 02 2023 1 It's not safe for canton residents to drive slowly and do shopping with distracted and speed drivers. There are accidents so often. My kids are transported to west Hartford daily and I've seen so many accidents. Especially during the holiday season shopping. The traffic is unbearable with all the tree cutting and pole fixing and holiday traffic. I have opted to drive north through simsbury instead

114453438260 Nov 02 2023 1 Of driving on rt 44

114453428783 Nov 02 2023 1 I am concerned that the upcoming developments on Rte 44, Dowd and on Lawton Road will push us over the limits we currently are able to handle. I often remark that I feel as though I have moved to New Jersey based on the volume, speed and driving habits of the folks in this area. The Dunkin Donuts as well as the Rt. 44 - Dowd intersections are a nightmare. Best of luck in your efforts to revamp the area.

114453428783 Nov 02 2023 1 I hope you are successful.

The Farmington River trail is wonderful, I'm a frequent walker. It's unfortunate that once snow hits it is unusable. It would be amazing if parts of it could be kept clear for continued winter use.

114453433904 Nov 02 2023 0 Getting in and out of many parking lots along 44 is difficult & unsafe. Traffic should be slowed to allow safer entrance & exiting.

114453398895 Nov 02 2023 0 I think the short road from Route 44 to Dowd Avenue to CANTON Springs Road should be eliminated. I think there's about two houses on that short road that should still be able to go out to 44 and put a stop sign instead of the existing light with only right turn only. Move that traffic light up to the CANTON Green Road at 44 and make all the cars turn into that road to get to Dowd ave. Or straight to Canton Springs Road. This should slow traffic down on Dow Avenue. It seems to be flying down that road because of the angle at 44 and would allow us coming from CANTON Springs Road to get to 44 without getting

114453398895 Nov 02 2023 0 clobbered.

114453385260 Nov 02 2023 0 Safer acces to exiting from smaller independent businesses along 44

114453407042 Nov 02 2023 0 N/A

114453321045 Nov 02 2023 0 Light at Dunkin Donuts

114453316182 Nov 02 2023 0 NA

114453294339 Nov 02 2023 0 I'd love to see the rail trail and road crossings to be easier to use. I don't know how to do it.

114453274112 Nov 02 2023 0 i really like living off dowd avenue

114453266461 Nov 02 2023 0 Route 44 is very congested. The traffic at certain times of day is extreme. The amount of traffic will only increase with the planned developments. Driving is difficult- bicycling and walking very dangerous.

114453250017 Nov 02 2023 0 Route 44 sidewalk ... All the way along. Both sides. Even the crosswalk at secret lake is bad as the walk light isn't at fully stopped traffic.

114453221684 Nov 02 2023 0 Think about seniors in town. Everything moves too fast.

114453208089 Nov 02 2023 0 Build safe bike/pedestrian areas along route 44 that connects to New Hartford on Route 44. From the Satan's kingdom bridge one can safely bike along the east side of the river into New hartford, Pleasant Valley and Riverton. It would be a legit bike commutting valley if we cold connect to New Hartford!

114453157527 Nov 02 2023 0 Installing speed bumps would be horrific, but perhaps permanent signage with correct speed and flashing lights would be a helpful reminder, along with demarcation of streets that enter/exit to ensure emergency vehicles and residents can access residences.

114453172966 Nov 02 2023 0 I appreciate that you are looking into this. In the U.S. as a whole we have very poor safety for pedestrians compared to other parts of the world. I am happy to slow down to make it safer for those walking or biking.

114453159706 Nov 02 2023 0 Police need more presence and start handing out tickets to speeders and motorists breaking the laws at pedestrian crossings! Also install cameras to record and ticket offenders. Nothing changes that doesn't change.

114453156794 Nov 02 2023 0 Don't ruin it with unnecessary so called improvements as was done in Collinsville.

114453156359 Nov 02 2023 0 Make it more pleasant and meaningful for people to walk and more of us will.

114453108169 Nov 02 2023 0 Limit the building so there are less entrances onto Rt 44.

114426498018 Nov 02 2023 0 I hope that this survey is not an attempt to placate residents of the area (as previous corridor studies have been) and that all feedback is thoughtfully considered. These are serious concerns and need to be addressed in a timely manner.

114453101558 Nov 02 2023 0 An enforced speed limit or etc 44 of 35 mph between Lovely street and East Hill Rd. Also cycle a red light on the westbound lane of 44 at the intersection of Dowd Ave (westbound is ALWAYS green unless the crossing button is pressed)

114453094843 Nov 02 2023 0 Thank you for this work

114453084582 Nov 02 2023 0 Thanks for looking into this

114453018261 Nov 02 2023 0 Thru commuter traffic going to Hartford morning rush and afternoon reverse is always going to be a speeding issue. Canton PD does the best they can to slow traffic. Good Luck!

114453082237 Nov 02 2023 0 N/a

11445305168 Nov 02 2023 0 I think if there were a safer place to walk at the end of the Farmington Trail I would walk to the mall for light shopping. Additional thoughts- the Dunkin Donuts traffic is very dangerous. Lots of accidents and close calls. Also Dowd and 44 and Dowd and Canton spring are getting worse. Way more traffic and speeders. Never see a speed trap at all.

114453050714 Nov 02 2023 0 I live off Canton Valley Circle and it's very difficult to get out of our street due to the Dowd/rt44 intersection. There needs to be a dedicated lane for Dowd ave because cars go around the ones in that lane at a high speed so by the time they come to CVC it's dangerous!!!!

11445302740 Nov 02 2023 0 lots of drivers ignore the No Right on Red sign from Dowd onto 44

114453011737 Nov 02 2023 0 I think that the above survey has addressed the problems.

11445299312 Nov 02 2023 0 Dowd and Rt 44 intersection needs to be redone..Dowd and East Hill intersection needs redone..

114452977712 Nov 02 2023 0 Build a sidewalk and decrease traffic lights
More speed enforcement (never see CPD out there pulling speeders over) and a continuous sidewalk system are necessary to even try to begin to make this area safe.

11445297816 Nov 02 2023 0 I don't bike but worry about them on rte 44 very narrow bike lane particularly Sunday AM

11445296584 Nov 02 2023 0 The intersection of Canton Springs Road and Dowd is terrible. Cannot see traffic coming from the east when making a left turn onto Dowd. Very dangerous high speed area.

114452969831 Nov 02 2023 0 We appreciate the opportunity to provide this feedback. Please engage the community and neighbors in a meaningful way to help design any improvements

11445295625 Nov 02 2023 0 There is no reason for the "No Right on Red" onto 44 from Dowd.
No additional thoughts. □

114452942614 Nov 02 2023 1 Thank you.
The tall grass plantings at the intersection of 177 and 44 are lovely, but they block the view of oncoming traffic when turning right (east) from 177 onto 44.

114452950981 Nov 02 2023 1 My observation for past more than 25 years living here is that the drivers coming down the hill of RT202 are wound up going fast on that stretch of highway from Torrington then when they merge at the RT44 junction on, feel like they need to continue that speed on.

114452921121 Nov 02 2023 1 Speed control is needed on the west end of 44 as well as traffic control at the intersection of 44 and East Hill to prevent cars from running red light. Sidewalks are needed from Farmington Valley Shoppes to Best Buy.

114452914982 Nov 02 2023 1

11445291717 Nov 02 2023 1 Discourage through traffic use of ct565 as shortcut between rt 44 and collinsville. (Encourage use of 179-44 instead, it's only a mile or so longer.) Is there some way to make the intersection of Dowd/Maple/Symonds/Old Canton Rd/East Hill Rd useable for local traffic but impassible to through traffic (except emergency vehicles)?

11445291990 Nov 02 2023 1 I would like to see the area be more walkable with sidewalks on both sides and more crossings At pedestrian crossings many accidents happen when drivers decide to go around vehicles that have stopped for pedestrian/bike crossing. Drivers don't stop for flashing pedestrian lights or they don't notice them.

11445292021 Nov 02 2023 1 Thnx for looking into this!

11445291786 Nov 02 2023 1 The running of red lights is my biggest concern. When I used to walk my dog on leash and we got the green light to cross and she stepped off curb I had to pull her back to avoid getting hit. This happened at least once a week. Very sad.

11445290839 Nov 02 2023 1 One of the biggest issues is the increasing volume of traffic. Driving speeds on Rt 44 are dangerous. The other main issue is the bike/ped crossing Rt 44 at the busiest intersection. There should be a tunnel underground as in Avon & Unionville. How did Canton get left out?

11445290893 Nov 02 2023 1 Right now there are too many posted signs they take away from the country atmosphere

11445290692 Nov 02 2023 1 It's not broken...don't try to FIX it.

11445289289 Nov 02 2023 1 I grew up in a neighborhood right off 44 and east hill rd. The amount of red light runners was very scary. I now own a home in Simsbury not far from the old mall on bushy hill. The congestion of 44 keeps growing, the speed on 44 keeps growing despite the speed limit never changing. It's a huge thruway but the infrastructure has never (or has been very little) improved. I remember the. The upper part of 44 was redone in the 80s/early 90s by the saybrook fish house, that is one of the least congested areas with the most improvement, the middle and lower parts of 44 should be updated accordingly.

11445281721 Nov 02 2023 1 Love you are focusing on this. Beautification and use ability are a value add for canton property I don't feel drastic changes should be made to this area. Enough has been done already, which is why we are in this predicament now. Rather than change, we should try to "enhance" the historic beauty of Canton.

11445287724 Nov 02 2023 1 N/A

11445288112 Nov 02 2023 1 N/A

114452866994 Nov 02 2023 1 The current bike path does not facilitate access to the businesses on Dowd and US44. We need to treat the whole area as one space designed for walking and riding. Connections to businesses from walkways and paths would create more of a town feel.

114452881986 Nov 02 2023 1 Please fix speeding and traffic

114452831654 Nov 02 2023 1 Good Luck

114452856222 Nov 02 2023 1 CONTROL VEHICLE SPEED WITH MARKED AND UNMARKED POLICE VEHICLE AND REMOTE CAMERAS. THIS AREA REALLY IS NOT A SIDEWAY, PUBLIC TRANSIT TYPE OF AREA. HOW MANY PEOPLE LIVING IN THE FARMINGTON VALLEY ARE WITHIN EASY WALKING DISTANCE OF 44 OR DOWD?

114452862581 Nov 02 2023 1 Connect the sidewalks between Thayer Ave and Dyer along Maple ave

114452860286 Nov 02 2023 1 Mostly around the Crossing walks, the lights haven't been doing any good people seem to speed up recently because the lights are not on continuously now they are only on when you get there so you have to be sure you push the button or the come around the corner and hit you

114452856833 Nov 02 2023 1 Traffic needs to SLOW DOWN!!!! I think it is human behavior rather than lights/speed limits. Everyone runs the reds now and I always have close calls when it is my green.

114452848836 Nov 02 2023 1 Please do not increase public transit. With all the apartments coming in, the whole feel of the town is changing for the worse and public transit will add to that

114452844707 Nov 02 2023 1 I would like to see a sidewalk that runs from Canton Green gazebo all the way into Avon along 44

114452836379 Nov 02 2023 1 I would love for each of the shopping areas on 44 to be more walkable and less car-centric.

114452812359 Nov 02 2023 1 Grass and trees in post office, ace hardware, eaterys plaza (is there a name?) No rental apartments on Dowd. Owners only w minimal rentals like Canton Hollow's rules. It's that plaza that's ugly, Not Dowd Avenue itself. Inexpensive speed bumps!!!! Use rt 44 more. Avon mountain added lanes = higher traffic and speed...what?Speeders will slow down with SPEED BUMPS!!!!!!

114452837068 Nov 02 2023 1 Slow down the drivers in maple ave and Dowd. Hire more officers to enforce speeds

114452815192 Nov 02 2023 1 All in all I think the added sidewalks around the new construction are a great improvement . Adding sidewalks past existing businesses up RT44 would be awesome. AND FIGURE OUT HOW TO SLOW DOWN TRAFFIC!!

114452822369 Nov 02 2023 1 Please see my earlier extensive comments

114452826617 Nov 02 2023 1 The Canton Village could be a lovely shopping and walking area if it were revised to have clear driving lanes and additional landscaping and shade. Right now it's just an asphalt mess

114452820310 Nov 02 2023 1 Constant Speeding of cars. If I drive the speed limit, it's highly likely that I will have someone right on my bumper. Occasionally there are some bicyclists that are reckless as well. If there could be a bike lane with some sort of median, not just a painted line in certain places. Despite the law being that a driver must give the lane to bicyclists, many people don't. It creates very unsafe situations.

114452821466 Nov 02 2023 1 We should be focusing on walkable, bike-able, pedestrian friendly areas first. Then cars should be considered. Again, Complete/Mixed-use streets have been shown to be highly effective (e.g., Europe) through numerous research studies. You have to design the streets in a way to prioritize people over cars.

114452822004 Nov 02 2023 1 Speed too fast. No turn lanes. Congested area

114452817978 Nov 02 2023 1 More flowers, plants. Near the gazebo area.

114452818765 Nov 02 2023 1 It's a nightmare to commute to Hartford everyday. This issue has gotten worse with all the buildings and apartments along the Rte 44 corridor in Canton.

114452816737 Nov 02 2023 1 It is the worst part of 44 to travel on but unfortunately I have to use it daily. I appreciate efforts to try to make it better.

114452811951 Nov 02 2023 1 Go to the Netherlands and learn how to build bike and pedestrian roads!! I would walk or bike to bus stops if they were safer to access like the one that used to be at the intersection of Simonds and Dyer.

114452796370 Nov 02 2023 1 The bike trail should be made continuous to Simsbury.

114452797765 Nov 02 2023 1 Fix it, please.

114452794455 Nov 02 2023 1 Not that a rotary circle will fix this. But more are needed in the Farmington Valley.

114452794221 Nov 02 2023 1 Building up these outer Towns is not beneficial to the towns, or the people. □

114452792088 Nov 02 2023 1 Not only does it increase congestion, it is changing the character of the towns.

114452796827 Nov 02 2023 1 Traffic needs to SLOW DOWN and be less aggressive. Passing on the right in the shoulder lane needs to stop. Right on red from the shoulder lane needs to stop.

114452792390 Nov 02 2023 1 Leave it alone. No improvements needed and it will waste taxpayer funds.

114452792390 Nov 02 2023 1 Speed is the major problem I have seen vehicles going 60 to 70 mph on Dowd

it needs rotaries at the very least, and some way to calm what are high speeding drivers, there arent any turn lanes for the amount of businesses so its grossly over used for amount of actual roadway/lanes, and it needs either over road bike trail or tunnel but the way drivers behave, the bike path is a tragedy waiting to happen on its crossings of this highway speed roadway

114452789772 Nov 02 2023 1
114452793730 Nov 02 2023 1 N/A

114452782545 Nov 02 2023 1 Very heavy traffic at times, more people should use the sidewalks on their bikes instead of the roads.

I'm more concerned over the US-44 / Trailsend Drive / Lawton Road/ Lovely Street intersection. Very dangerous with people running red lights, turning right on red when prohibited, etc.

114452785409 Nov 02 2023 1

44 and the Dowd area are a great potential asset to Canton, but seem like an afterthought - it would be great to help make the space more walkable from business to business and more welcoming

114452785784 Nov 02 2023 1
114452783918 Nov 02 2023 1 Thanks for the opportunity
114452784734 Nov 02 2023 1 Sidewalks badly need repair

Nothing needs to be changed, should not say improved as the area is fine as is. I walk and drive these areas daily and it is my responsibility to walk and drive safely.

114452781628 Nov 02 2023 1

There is too much traffic along 44, cars are excessively loud, when we first moved here you couldn't hear 44. Now with all of the trees cut down where the Shoppes and Mitchell's is, there is nothing but noise and the sky lit up. And we are a solid mile to mile and a half away.....

114452783232 Nov 02 2023 1
I'm currently in the Netherlands. Would be great to see a more pedestrian centric future for our town of Canton.

114452782105 Nov 02 2023 1

Traffic along 44 from Saybrook fishhouse area is simply a commuter raceway from West to East to Avon. Major changes to the road to slow traffic down would be the first step—more traffic lights. As you enter our town, know that you'll be stopping at just about every intersection—sidewalks on both sides with little areas of open space that encourage folks walking — maybe a beautified median is needed all along 44 defining the beginning and end of Canton-sidewalks, plantings, murals in little open areas and traffic lights. □

□

Bike lanes would also help and crossing 44 —not sure where—make the area like the little city it's become—cities slow traffic flow all the time with increased traffic lights, bike lanes, pedestrian traffic.

114452471775

Nov 02 2023

0

114452434151

Nov 02 2023

0

My big problem area is Cherry Brook Rd... Sidewalks would be a huge benefit.

114452390924

Nov 01 2023

1

Terrible, terrible idea

Turning left on Dowd from old Canton rd is dangerous. Can not see traffic flying off 44. Turning Left off of 44 onto side streets is very scary. Cars weave in and out and do not slow down enough. Middle Turn

114452385542

Nov 01 2023

1

lane may help? Pedestrians and bikes are not safe on 44..

Bus only lane to incentivize use of Public transit, offer bike/scooter parking, separate oncoming travel

114452357431

Nov 01 2023

1

lanes with a buffer.

114452310853

Nov 01 2023

0

Cameras at all lights tagging speeders and light blowers. Automatic ticketing.

No one cares about beautification. We care about the town doing something about traffic. We need

114452252747

Nov 01 2023

0

police pulling over cars and doing traffic stops.

I bike across 44 but avoid biking on 44 due to safety concerns. Would be great to stop during a ride for

114452231108

Nov 01 2023

0

food.

114452084449

Nov 01 2023

0

Slow drivers down, they travel unreasonably fast.

Get rid of the “no turn on red” sign at the light from dowd to 44! It's silly, you can see all the way down your left on 44 to citgo, and it would relieve traffic there if drivers could go when it's safe.

114452020556

Nov 01 2023

0

Would love bike paths or side walks along 44 for walking and biking. Need to control the speeding!

114451995011

Nov 01 2023

0

People are driving so fast or too close which can cause accidents.

114451556481

Nov 01 2023

0

I love the idea of traffic circles and pedestrian bridges

Is this a state highway that connects Hartford to Albany & Providence? Or is it a village hub? There is too much through traffic for it to be a pedestrian area. There are too many traffic lights for it to function as a state highway. There could be a bypass that is routed North of rte 44, so the current highway becomes a village. There could be a train line down the middle of the right of way so through traffic is removed (the trains would be free, and through traffic would pay a heavy toll.) How about TIMING the traffic lights so you don't have to drive 60 to make the next light? How about computer controls in the lights so you don't have to wait 2 minutes at 7 in the morning to cross rte 44 at 179? or even merge at Dowd? I suppose it could be worse, but can't think of how . . .

114451288308 Oct 31 2023 0

114451165853 Oct 31 2023 0

At bike/pedestrian trail-roadway crossings the shrubbery/plant growth on either side of the trail should be pruned back to provide better line-of-sight for bike/pedestrians and cars approaching the intersection.

114451012508 Oct 31 2023 0

114450866918 Oct 31 2023 0

More lightening along 44 would be great.
The Canton PD need to be more present on Rte 44 now that Case Street seems to finally be under control. Turning left off 44 to any of the daycares (especially Stepping Stones as the hill crests just slightly behind you) is dangerous. □

Perhaps a State Trooper could be an asset or a dedicated officer for 44 traffic control. The reality is a stronger police presence is needed and we need to fund our PD accordingly so they can have more officers. □

114450746721 Oct 31 2023 1

Fewer impediments to traffic will help get traffic through. The number of traffic lights is ridiculous, especially when they aren't timed, though if they're timed to a fantasy low speed that nobody obeys (for good reason), it wouldn't help anyway. Stop trying to make 44 something it is NOT, focus on the greenways and green spaces for outdoor recreation and make 44 more efficient for vehicles to get through it. □

□

Related, the more hideous development there is, the less likely people will want to do anything but get through it at 60mph. Canton already decided that. As the development has gotten more like Jersey, the driving has too. Big surprise. Don't bother trying to fight it now.

114450684323 Oct 31 2023 1

114450621478 Oct 31 2023 0

Cyclists and motorists do not follow the traffic signals / stop signs. This is a huge problem.

114450355952 Oct 31 2023 0: The Rt 44 corridor should be modeled after Hopmeadow Street in the heart of Simsbury. Hopmeadow has an optimal mix of New England charm, beautification, calm traffic, and pedestrian friendliness.

114449082928 Oct 29 2023 0: Emphasis on making the intersection of Dowd with Route 44 safer and traffic calming of some sort on Dowd to slow speeding vehicles. Keep eastbound traffic on Dowd from blocking Canton Springs Road. At very least, install mirror on Dowd on Canton Green Road to allow some sight line when turning left onto Dowd from Canton Springs Road when vehicles are backed up from the light at 44 to (and beyond or across) Canton Springs road. Have nearly gotten hit many times trying to get out of Canton Springs. Comments above. Do better to make area safe and take people's comments seriously. Too often comments are sought but ignored by officials. With new poles being installed along 44, how can you make it better? Seems too late. Chance completely missed! Would this survey been better if asked 2

114449065253 Oct 29 2023 0: years ago?

114449066910 Oct 29 2023 0: It's very, very dangerous

114449046714 Oct 29 2023 0: It's terrible, I lived in Collinsville for 62 years,now it's like hartford

114449030817 Oct 29 2023 1: Cameras at intersections and ticket the abusers

114448870207 Oct 29 2023 0: The exit from the shops at Farmington onto secret lake road is a complete disaster. There are going to be accidents guaranteed...coming right onto 44 with cars turning left off 44 in scary.

114448832296 Oct 28 2023 1: coordinate traffic lights to allow smooth flow if traffic

114448827289 Oct 28 2023 0: Don't build anymore apartment complexes or developments

114448788997 Oct 28 2023 0: The lack of reliable public transit is a real barrier to access in all of this area not just the study area

114448772683 Oct 28 2023 0: .

114448779003 Oct 28 2023 0: None

114448774056 Oct 28 2023 0: People drive on this road like it's a highway. Slow folks down

114448743648 Oct 28 2023 0: Widen the road and add additional turning lane in the middle

114448718097 Oct 28 2023 0: None

114448714320 Oct 28 2023 0: Longer green left hand turn arrow signal to Dowd from 44

I live in Canton Village. The amount of traffic in the morning is heavy. I will not let my daughter cross the street to get on the bus because of how fast people drive and they go through the bus stop light. Walking to school is out of the question because of Dowd and east street people are not stopping at those signs.

114448706158 Oct 28 2023 0: I had some mad at me for stopping and not speeding down the road the high school is on.

114448676613 Oct 28 2023 1: Stop all the new housing which is impounding the problem

114448604275 Oct 28 2023 0: Pedestrians and Bikers should be valued and encouraged more.
Dedicated left turn lane, besides 2 travel lanes, on to Dowd from west bound 44. Improve Dunkin entrance and exit, no left turn out, dedicated left turn lane on 44 west bound, move entrance further west

114448383385 Oct 27 2023 0: to reduce cars waiting in road to enter

I love Canton and think it's a great community, but 44 is a complete hazard and I hate driving it. I also live next to the road so I unfortunately have to experience how loud and dangerous it is every day. People drive way too fast down it, making me too afraid to try and turn left on the road to try and pull into a shop. I fully support the idea of a beautification project to improve the entire area and make it much more walkable, as I would much prefer to walk/bike along the road and avoid driving it at all costs. Please also consider the barrier idea I mentioned before, even just planting some trees would be a world

114448278845 Oct 27 2023 0: of improvement along the road.

114448178884 Oct 27 2023 0: i sometimes have to wait to pull onto dowd from simonds for 20-25 cars to go by its crazy
Sidewalks, sidewalks, sidewalks. Redevelopment of Canton Village would also have huge benefits and

114447806569 Oct 27 2023 0: could spur a virtuous cycle of positive development.
I would love my kids to be able to take their bikes to school etc or run down for lunch somewhere but don't feel safe sending them on their own due to road traffic. □

114447778959 Oct 27 2023 0:

Be mindful with development so we don't lose the aesthetic appeal of Canton and quality sidewalks as

114447759482 Oct 27 2023 0: well as bike lanes would not only create transportation options along with safety.

114447575215 Oct 26 2023 1: Drivers seem to think that road is a race track, extremely dangerous for walkers and bike riders

114447085011 Oct 26 2023 1: Thanks for asking.

114447039163 Oct 26 2023 1: More police presence. More ticketing.
In the future, don't allow buildings to be built so close to the road. With a lack of parking, side roads

114446472587 Oct 25 2023 0: become clogged and it makes it hard to navigate.

114446406905 Oct 25 2023 0: The boards of the town have decided to overdevelop for the size of the town. Bringing in more apartments creates even a bigger problem of congestion.

114445967935 Oct 25 2023 1: Stop building so many new structures. There is already so much traffic on 44 and the Dowd intersection, and it keeps getting worse. There is going to be no green space left in that area and it's changing our small town community. Rt 44 is becoming like the Silas Deane highway or the Post rd through Milford.

114444993042 Oct 24 2023 1: Biggest problem is high speed of traffic and volume of traffic has increased over the past 15 years. The addition of the Shoppes, and general increase of retail in the area CVS, Aldi's etc. has changed the character of the area, and increased traffic to an unpleasant degree for pedestrians and motorists.

114444409485 Oct 23 2023 0: Something needs to be done about the intersection of Canton Springs Rd and DowdAve. Small improvement last few months to widen Canton Springs Rd. But still difficult making left hand turn onto DowdAve. Especially when traffic is backed up at the light.

114444000384 Oct 23 2023 1: There needs to be better traffic control in order to cut down on the reckless and speeding drivers in the area. I know the CPD is a team of few so, cannot always be on traffic duty but, innovative solutions need to be implemented.

114443816111 Oct 23 2023 0: Don't put bike lanes on Rt44. Bikers can use Farmington River trail. Turning lanes would reduce backups and accidents.

114443720092 Oct 23 2023 0: Coordinate the lights, similar to rt 202 in Torrington, where if a driver is going the speed limit, in theory, they should be able to drive without stopping.

114443525588 Oct 22 2023 0: Have homeowners be responsible for the condition of their properties and the blight problem of some homes as well as closing off the major intersection and using US 44 for thru traffic.

114443450836 Oct 22 2023 0: REDUCE SPEED LIMIT ON ROUTE 44, consider one lane for vehicles and wide bike/ped lane with clear painted delineation and buffer. Crosswalks with RFFBs are not safe. Add center lane rumble to reduce head-on collisions. Be inspired by NYC Safe Streets. Read this article:

<https://www.curbed.com/2021/11/perfecting-nyc-street.html>

The biggest obstacle to be overcome on Rt. 44 is the SPEED of motorists, and the UNSAFE driving of many (most!) motorists in this area. It has become a scary ordeal to drive in Canton because of the erratic, distracted, weaving in & out of traffic, running every yellow (and frequently red!) light by impatient disrespectful motorists. Enforcement of traffic laws, lights, speed limits, driving behavior would need to be the first step before any beautification, and before adding any pedestrian/bicycle lanes & crossings, which given the current unsafe driver behavior would only put people in danger. I sincerely hope that this study will lead to safer streets for all of us. Thank you!

114443474555 Oct 22 2023 0: Pull over the speeders and ticket them!

114443397180 Oct 22 2023 0: Improve bike trail from where it connects to 177 near 44 as it moves north.
I would walk to the hardware, store, bank, post office, etc. if there were better sidewalks and I felt safer traveling that area. Definitely car-focused.

114443326771 Oct 22 2023 1: I have lived in the area for 50 years. Never have I seen such traffic on RT 44. What's more, cars blast through stop lights, speed, weave in and out of lanes. Forget trees and landscape so things will be "pretty". Hire more police to get a handle on these people and ticket them. Or, VOTE for town representatives that will support the effort to do so.

114443255747 Oct 22 2023 0: Wider shoulders to improve safety for bicyclists. Either wide enough for improved safety but not to encourage more cycling on the road. Or a dedicated bike lane. Or extended rail trail.

114443092225 Oct 21 2023 0: Better enforcement of speed limits and deterrents are needed such as cameras at traffic lights, speed radars and police presence.

114443078441 Oct 21 2023 0: Encourage safe driving
A pedestrian bike path adjunct to the river trail with spurs terminating in major shopping areas similar to how they did it in Manchester and Northampton so that one can come right up to main areas without having to disturb traffic on 44 too much.

114443076781 Oct 21 2023 0: N/A

114443070170 Oct 21 2023 0: N/A

114443064873 Oct 21 2023 0: Need a tunnel under US44 for the cyclists!
Cars just don't stop for pedestrians at crossing several people have been it at crossing from trail one I know was on bike my Niece was walking

114443034649 Oct 21 2023 0: To the extent possible, something like mass transit. Of course, there is no mass of population in this area, and the economics are difficult. We have only 1 car and I imagine we face difficulty in event that is out of commission even temporarily. Zip cars, a nonprofit van service, maybe. □

114442907929 Oct 21 2023 0:

The River Trail need to connect to somewhere. Wayfinding signage is poor/nonexistant. RT 10 north at O'Neils Chevrolet could be a connection but the crossing at grade would be almost impossible. Using the tunnel, and trying for another east west corridor like Country Club is another option. 44 is going to be difficult to get done without going in back of many properties - there are too many curb cuts for any real safety.

114443011353 Oct 21 2023 0: safety.

114443007286 Oct 21 2023 0: This is one of the most dangerous and unsafe areas to cross on the Farmington River Trail.

My main problem is with speeding drivers, on rt 44 I never go over the limit of 40 but am constantly passed on either lane and sometimes drivers using the extra left or right turning lane to pass. People are angry with me for going the limit, same nasty hand signals in my direction. Some people are going way over the speed limit and they never seem to be pulled over or stopped. Also school bus signals are ignored. Rt 44 is used like a speedway not a local congested highway.

114442942391 Oct 21 2023 0

114442885842 Oct 21 2023 0 The large intersection at 44 and cherrybrook road is hazardous to drivers

Connecting and extending the Farmington River Trail is important. Also, need to allow left-turn lane from Dowd Avd. onto US-44

114442582442 Oct 20 2023 0

Place brighter streetlights a little more often on Rt. 44 and minimize the brightness of all signage lighting that is close to the road and low enough that they can be confused with oncoming traffic headlights.

114442548894 Oct 20 2023 0

There are plenty of places to walk in Canton, let's not turn it into Long Island with signs everywhere... We are really on the edge of turning into Southington with all of the apartments and stores. It's time to dial it back

114442545551 Oct 20 2023 0

The lanes are very narrow specifically between Dunkin' Donuts and Dowd Avenue. Many people turn into the Dunkin' Donuts parking lot, causing congestion and lane changing. Then that happens again for the people turning left onto Dowd Avenue in front of the junk store. It doesn't seem like there is enough room for a turning lane, or to widen the road but that might help

114442253444 Oct 20 2023 1

114442062149 Oct 20 2023 0 Maybe put a lane in for just biking , like they have in west hartford

114441970725 Oct 20 2023 0 Stop building apartments. Revitalize the closed buildings instead of building more.

114441953292 Oct 20 2023 0 Na

Drivers ignore "no turn in red" at Dowd and 44. I would not try to walk across 44 to the plant nursery there. Unsafe. Use speed checks, put a cop at Bahre's or Manny's? Add a light?

114441604056 Oct 19 2023 0

The left turn onto Down is not safe. I know multiple people who have been rear ended turning on to Dowd from 44 or hit while going straight and someone turning left into them

114441592079 Oct 19 2023 0

114441561715 Oct 19 2023 0: A traffic circle at US-44/Lovely may help with the back-ups and traffic light accidents.

114441540583 Oct 19 2023 0: Need to extend the Farmington Trail for walkers and bikers.

114441430660 Oct 19 2023 0: redesign the entrance to Dowd ave.from 44.

114441422408 Oct 19 2023 0: Please find a way to slow down speeders

114441389752 Oct 19 2023 0: There's some large-scale (i.e. apt. complexes) on the table for Dowd Avenue and Old Canton Road (with access to Dowd Avenue). We're looking at about 150 apartment units. The current state of Dowd Avenue, and it's intersection with Rt. 44 cannot sustain that much additional traffic.

Separation of bikes and pedestrians would be great. ☐

114441344923 Oct 19 2023 0:

114441340226 Oct 19 2023 0: Bikes do not belong on any roads!

114441275049 Oct 19 2023 0: Very congested Traffic with people cutting across Canton Green Road or Dunham to avoid light at Dowd to 44. Traffic backs up from Dowd to Canton springs road often blocking intersection with poor line of sight. Light from Canton village to 44 way too short, leads to most people running light.

Put a light at the Canton Springs and Dowd intersection. It's impossible to cross as a pedestrian or bike. Driver's fill the intersection waiting for the light and it creates no visibility for other drivers trying to turn.

114441219931 Oct 19 2023 1: Not safe to try and cross as a pedestrian.

It is a major thoroughfare connecting western and central Connecticut. I don't really think its feasible to make the actual roadway really appealing to bikers/walkers. Extending the rail trail or providing walking/biking paths that are set off the roadway is the only thing that would be appealing to me. I really think improving safety on the route is important (through traffic enforcement and traffic management with lights, etc) but that wouldn't make it more appealing for me to walk there. I use the rail trail frequently

114441180563 Oct 19 2023 1: and it is a great local resource.

114441151788 Oct 19 2023 1: The town needs to stop adding unappealing apartment buildings that add to TRAFFIC. I've spoken to business owners and no one sees foot traffic in their business due to apartments.

114441120858 Oct 19 2023 1: The traffic in this area is getting out of hand. STOP adding more apartment buildings and adding more congestion in the process.

114441098671 Oct 19 2023 1: Ticketing, ticketing, ticketing. More Law Enforcement. Check real residences of out-of-state drivers.

Consider adding a light rail or busway system to serve the growing population in Farmington Valley

114441082825 Oct 19 2023 1: towns and Western CT along with a campaign to end our addiction to our cars.

Slowing speeding traffic is a lack of police issue. In my opinion we do not have the staffing for a strong enough presence. There are strategies that can be applied to slow traffic. I said that the Dowd Ave to 44 intersection should be changed into a straight regular intersection. Another and maybe cheaper alternative would be a traffic circle. There could be shapes and hard stop signs that could be applied to Dowd to slow traffic and there could be dedicated bike lanes added as well.

114441081974 Oct 19 2023 10: Although most residents of the area realize that you cannot turn left from Dowd onto 44, I have seen people try to make that turn and was almost hit by someone trying to do that.

114441083779 Oct 19 2023 10: Step up law enforcement until actual improvements can be implemented. Minimize stop controlled left turns with center median.

114441017751 Oct 19 2023 09: At the intersection of the two roads it would improve traffic movement if the lane where traffic turning left onto Dowd Avenue was a "left turn only" lane. Too often vehicles that intend to go straight on west on 44 suddenly dart into the right lane when the driver realizes that those in front of them are turning left.

114441020719 Oct 19 2023 09: Promote business opportunities to increase tax base.

114441021986 Oct 19 2023 09: See above

114441012584 Oct 19 2023 09: keep building th bike trail to get across 44 on to Lawton and then extend the trail separate from the road. The traffic coming from Simsbury onto Lawton Rd and ultimately Route 44 is often very heavy and people coming down the hill in this direction Drive very fast.

114441016529 Oct 19 2023 09: not sure if anything can be done.

114441002346 Oct 19 2023 09: Speed limit too high. More enforcement of traffic laws

114441006346 Oct 19 2023 09: Deny any plans for additional apartments/condominiums anywhere near the area. Its already a nightmare to travel.

114441007630 Oct 19 2023 09: Fix the intersection!!!!The Green is Useless

114441003015 Oct 19 2023 09: There need to be more traffic signals or turn lanes. Turning into or out of businesses along 44 is extremely dangerous. Particular areas include Dunkin Donuts and then turning out of Cheryl drive

114440984528 Oct 19 2023 09: The businesses on US -44 in Canton are difficult to get in and out of safely. Cyclist cannot safely get to any of the businesses.

114440993254 Oct 19 2023 09: Good luck.

114440975990 Oct 19 2023 09: Somehow slow the many impatient drivers. I go the speed limit and no one stays behind me!

114440963403 Oct 19 2023 0 Don't build more apartments to bring in more traffic

114440946062 Oct 19 2023 0 Adding roundabouts or rotaries to slow traffic and improve the visual site line
widening 44 for a left-hand turn would help. Also the possibility of a pedestrian bridge / tunnel would
114440930669 Oct 19 2023 0 make it safer to cross
I try to avoid the area as much as possible. Unfortunately the trail goes in front on my house, so
114440930628 Oct 19 2023 0 unavoidable.

114440916815 Oct 19 2023 0 Less construction and more "rural" feel would cut down on the amount of traffic!

114440922479 Oct 19 2023 0 Thanks for your work on this situation! :)

Much of the congestion problem is a lack of a unified building and zoning code. It seems that
businesses can add access to their buildings without regard for the general public. When I drive on 44
114440903371 Oct 19 2023 0 and see another business opening, I think here we go again. Commerce vs general public.

114440901353 Oct 19 2023 0 Having more local police awareness of the speeders in these areas and acting on it.
Add additional exit out of Shops directly onto Lovely St. so drivers do not have to enter US-44 to go
114440892449 Oct 19 2023 0 south.
The area is a pass through, and solits the town, create alternate routes, the area is also very ugly, it
114440890993 Oct 19 2023 0 needs better zoning tegulations
For yrs dowd avenue has been a drag strip for everyone ☐
Lit up pedestrian signs r a joke☐
I have seen emergency vehicles take that route☐
Any drives in front of them just speed up☐

114440889494 Oct 19 2023 0 Getting off comers Ave on dowd Ave is very dangerous
Enforce lower speed through this area. A signal to know when it is safe to cross Dowd at the Canton
114440879690 Oct 19 2023 0 Springs Rd. intersection.
An "extra" turn lane onto Dowd Ave from RT44 westbound. Different from what is there, while left lane is
designated for turning, only one lane can then move forward going westbound on RT44, again causing
people to do quick and sometimes very dangerous, lane changes if they want to continue westward on
114440885754 Oct 19 2023 0 RT44
People cut through my condo complex at high speed to avoid the long wait at the light at 44/East hill.

114440880951 Oct 19 2023 0 Way too much traffic on Dowd Ave

114440883356 Oct 19 2023 0 Continue the bike trail past Canton Shoppes

114440770734 Oct 19 2023 0 none

114440701475 Oct 19 2023 1: One travel lane in both directions with a turning lane down the center.

Route 44 carries an enormous amount of traffic not only for shopping but as a way for people who live in surrounding towns to get to work and home. The new apartment building past the mini golf range is an

114440648904 Oct 18 2023 1: accident waiting to happen with poor sight lines for cars entering and exiting from the apartments. I am quite familiar with the area, but those who are not, can get very confused. The no left turn on to 44 from Dowd makes sense, but those not familiar with the area need to go right & then figure out how to turn around if their intention was really to go West. Also making a left on to Dowd can be tricky as the left

114440632935 Oct 18 2023 1: lane can also be used to go straight ahead.

114440591615 Oct 18 2023 0: More police are needed to slow down traffic and to pull over drivers who are texting and speeding.

It would be great to have the Farmington Rail Trail extended along US-44 in some way so biking would be a viable means of transportation. The current on-road Farmington River Trail on Lawton and Dry

114440574401 Oct 18 2023 0: Bridge is hilly and inaccessible to novice cyclists, and also doesn't access any services.

114440575137 Oct 18 2023 0: Radar on Dowd Ave by police

114440561630 Oct 18 2023 0: Ticket speeders

With the continued build-up of communities and commerce all along 44, it will be necessary to enact means to deal with the larger influx of traffic, not just at Dowd Ave. Y'all need to look at 44 from the

114440485630 Oct 18 2023 0: base of the mountain all the way down to 202/44.

114440433006 Oct 18 2023 0: Could be a great introduction to Canton. Just a bit more beautification

More police presence as so many cars running the red light at intersection of 177 and 44

114440424880 Oct 18 2023 0: Drivers are tailgating and going above speed limit daily

Add a left turn lane for collinsville/ burlington keep two lanes empty for commuters north.

□

114440373535 Oct 18 2023 0: Calibrate your lights to work with each other for a flow of traffic.

Better signage, road widening, enforce speed limit, longer green turn arrow, better indication for those on

114440330934 Oct 18 2023 0: down trying to go straight to nursery across the street. Side walks with a better buffer from cars.

114440251125 Oct 18 2023 0: Not every part of town need to Be pedestrian friendly

114440067316 Oct 18 2023 1: More pedestrian walkways. Clearer information on how to use busses

114439953925	Oct 18 2023 0	This area is NOT just a transit route. People live, work, and visit small businesses here. Treat the area like a town center and not just a pass-through. People should be able to say: I love driving along Rt. 44/Dowd Ave. in Canton (It's so quaint, edgy, colorful, tempting . . .). The roads shouldn't be an irritation on the way to somewhere else.
114439810349	Oct 18 2023 0	Cycling lanes on RT 44 and extension of rail trail to Canton Shoppes are important as well as improves of area with more trees, etc
114439683741	Oct 18 2023 0	Speed bumps more police on route 44 and extra fines for speeding and extra fines for going through red lights
114439486519	Oct 17 2023 0	Please do more to stop speeders on river road
114439450785	Oct 17 2023 0	I APPRECIATE THIS EFFORT AS PEDESTRIAN AND VEHICLE VOLUME HAVE INCREASED SIGNIFICANTLY OVER THE 23 YEARS THAT I'VE LIVED IN COLLINSVILLE AND IT IS THE RESPONSIBLE THING TO DO TO ADDRESS AND INVEST IN INCREASED SAFETY MEASURES.
114439429789	Oct 17 2023 0	I do my best to avoid the area. Traffic is fast and reckless, some obstructed sight lines. Most of the problems I see are the direct result of unsafe drivers. Improving walkability and alternative transportation is always a good idea.
114439336346	Oct 17 2023 0	Completely redesign and rebuild Canton village. Too much asphalt
114439308802	Oct 17 2023 0	Canton is not Hartford. Please leave the town alone.
114439231288	Oct 17 2023 0	Add middle lane to 44 for left turns and cross merging traffic from Shopps to Simsbury.
114439236291	Oct 17 2023 0	Thanks for initiating this study. It is long overdue. The final word is traffic control. Police need to give out tickets for speeding, going through red lights, tailgating, unsafe passing.
114439182064	Oct 17 2023 0	There needs to be a buffer between the sidewalks and the main road. Create a center turning lane with one lane of flow on either side, then buffer zone, then bike lane, buffer zone, then sidewalks then businesses. Also please add benches and shelter at public bus stops, and highlight bus stops with paint and lighting.
114439117862	Oct 17 2023 0	
114439036704	Oct 17 2023 1	I bike to the shops at Farmington Valley. I use the sidewalk. Going west to cross at lovely is very dangerous. Pedestrian light is short. Drivers race up lovely and if they have red light look left to see if clear never look right to see pedestrian or biker is ready to cross. Drivers do not even seem to be aware that pedestrian have a light showing them to cross. Occasionally happens when I am heading east too as I always wait for the pedestrian light and do not cross just when the drivers have a red.

11443894643 Oct 17 2023 1 this study is long overdue. i have lived/worked in the area for decades and as the traffic increased the dangers did as well. anyone who uses this corridor is equally culpable in contributing to the dangers of this roadway

11443892354 Oct 17 2023 1 canton/ farminton valley region public transportation system.
I live on Dyer. My husband works at Davidson Chevrolet & my son gets on/off the school bus there (Davidson). It is unsafe. Cars do not stop for the bus. Cars speed past him. When I pick him up from Davidson, I try to make a left turn to head home. It's dangerous. Adding a high-density area in this portion of town is irresponsible. Please contact me if you'd like to further discuss:
shollenbeck923@gmail.com. Thank you

11443878702 Oct 17 2023 0 N/A

11443855617 Oct 17 2023 0 N/A

11443847131 Oct 16 2023 1 Again, I am more concerned with River rd needing a sidewalk all the way down, at least to Trinity nursery school
Build fewer apartments as the density of the people in the area seems to increase as more apartments are built

11443845124 Oct 16 2023 0 are built

11443842436 Oct 16 2023 0 Already stated comments previously. Thanks for looking into this!

11443838898 Oct 16 2023 0 More sidewalks around gildersleeve or down down past dyer would be amazing
My biggest fear is that with the three new complex being built in Canton, the traffic will increase tremendously and it will become even more unsafe for bikers and people walking.

11443838358 Oct 16 2023 0

11443838218 Oct 16 2023 0 Cars need to drive safer and SLOW DOWN. On a daily basis I see drivers going 60+ Mph, tailgating etc.

11443833196 Oct 16 2023 0 Rt 44 is very dangerous! Also the bike/walk shoulders are way too narrow
One other intersection that can be very challenging to Pedestrians/ Bicycles is at the Maple Ave/River Rd. Intersection. Drivers appear to be to too distracted by other cars and avoiding accidents to pay attention to Pedestrians or Bikes.
Please consider cameras it will automatically ticket drivers who are speeding, making the route less desirable

11443829908 Oct 16 2023 0

11443826533 Oct 16 2023 0 Drivers drive fast, can often be seen glancing or texting on their phone, and often cross the double yellow lines between the intersection of 44/Dowd and 44/Bushy Hill
Dowd/ Maple Avenue connect downtown Collinsville with other commercial areas of town and should be much safer for our families who travel there by foot and bike.

11443825749 Oct 16 2023 0

11443825829 Oct 16 2023 0 I'd love to figure out how to manage cars making left turns that stop traffic, lead others to drive around them etc.

114438224384 Oct 16 2023 0: Hold a public hearing for residents of Canton, especially for those who are being marginalized on SR-565

114438197436 Oct 16 2023 0: Raised cross walks on Dowd and Maple. A designated traffic officer, the plan should include the rest of Dowd, all of Maple Ave and on Rt.44 to 179. There should NOT be any more housing projects built.

114438197436 Oct 16 2023 0: There is NO room for all these people coming in town to live!!!!

Stop building apartment complexes bringing more people who speed through our town to get to their destination.

More police presence to enforce speed limits posted.

Better cross walks that encourage walking and biking.

114438182736 Oct 16 2023 0: More stop lights to discourage high rates of speed on the straight aways.

114438168972 Oct 16 2023 0: Narrow the lanes. Add substantial trees and shrubs in new median isles and along roadsides. Greenery on the road will slow traffic and signify that this area is considered worthwhile.

114438114925 Oct 16 2023 0: Through truck traffic needs to be eliminated completely and directed ahead on Rte 44 to South on Rte 179 (River Rd) as at least 95% of Rte 575 is not only residential, but Maple Ave is in a Historic District. Also, as there are virtually no businesses on Rte 575 (Dowd/Maple) which cannot be accessed via Rte 44, by improving accessibility to Dowd, you are negating the opportunity of businesses on Rte 44 from the ability to grow from increased visibility. The intersection of Rte 44 and Dowd, which needs improvement, should not make it more accessible as a cut through.

114438099221 Oct 16 2023 0: Very unsafe for pedestrians and bikes. Need more sidewalks on both sides of the street and more crosswalks.

114438106921 Oct 16 2023 0: A flyover (crossing bridge) would be helpful (and costly).

114438062149 Oct 16 2023 0: Something needs to be done about speeding in town

114438050909 Oct 16 2023 0: Please take action. Lower the speed limit. Avon is 35 mph and the businesses are further removed from the roadway. We have the canton green area that people would like to walk to but it is dangerous when we have studies saying the average speed is 48mph. Lower the speed to 30.

114438055947 Oct 16 2023 0: Timing of lights is a big issue. Speed is top issue

11443802600	Oct 16 2023 0	<p>The greenway needs to be continued to connect with Avon & Simsbury. Fragmented greenways Need to be finished. □</p> <p>Lawton Rd. to Simsbury is not really practical. The trail needs to connect to the shoppes & continue east...</p>
11443803678	Oct 16 2023 0	<p>None. Please do not approve any new apartments, condos or new homes in the area. It's too congested. People are absolutely out of control behind the wheel lately. Speeding down 44, weaving in and out of lanes</p>
11443802712	Oct 16 2023 0	
11443802337	Oct 16 2023 0	<p>N/a</p> <p>Raised cross walks on dowd. More policemen. In both areas. Writing tickets. Town needs more police officers.</p>
11443801756	Oct 16 2023 0	
11443801578	Oct 16 2023 1	<p>Speeding has always been an issue on 44 in Canton. Not really sure how to fix that other than police presence. Some of the local businesses are hard to leave onto 44 such as 50 Albany Turnpike area. Please see my previous comment. This is a waste of money to appease a few people complaining. Its a state highway with a bunch of barely visited antiques stores. Dowd to 44 should have a smart light or sensor. Takes way too long to turn green and eastbound traffic rarely backs up in that area during rush hour.</p>
11443800150	Oct 16 2023 1	<p>The thruway streets in the area are already hard enough to navigate via car and this is one of the only major ways to travel through. Please DO NOT put slowdown or traffic limiting measures in place on one of the only ways to drive though. It's ridiculous that the town keeps limiting car traffic in so many areas. It takes way too long to drive anywhere already!!</p>
11443798843	Oct 16 2023 1	
11443797775	Oct 16 2023 1	<p>Extending the existing Farmington rail trail past CVS/Applegate area would be fantastic.</p>
11443798732	Oct 16 2023 1	<p>Over/under crossing of 44 for bikes and pedestrians</p> <p>Improve flow of traffic in and out of Shops at Farmington Valley. Shoppers need convenience or they will shop elsewhere - any modifications need to take into consideration throughput to Shops to protect businesses there</p>
11443798188	Oct 16 2023 1	
11443783248	Oct 16 2023 1	<p>I feel that this traffic that is thru traffic is constantly driving way over the speed limit. This traffic offers no value for the town of canton. They don't frequent Collinsville or our local shops. They are looking to drive thru and they don't have any regard for the people (ped or bicyclists) or the beauty.</p>
11443781048	Oct 16 2023 0	<p>Drivers in general need to slow down, and it should be right turn only out of dunkin or blue house bagel.</p>

114437809907 Oct 16 2023 0: Stop wasting tax dollars

Fewer stores. Too many stores for a small town. Keep through trucks off Dowd and other non-main roads. More natural/native plantings. Don't mow so much or so often. Limit sign lighting...The LEDs that illuminate store and business signs are blinding and hence dangerous.

114437406043 Oct 15 2023 0: Thank you.

114437367595 Oct 15 2023 0: Again, the speeding on Route 44 is endemic. The river trail is a major asset that needs to be safely connected to 44. Benidorm Bikes can be a great ally here.

114436027590 Oct 13 2023 0: Create a bike trail that goes from the dry cleaners up to where you pick up the trail again in simsbury

Traffic is very congested and is usually unsafe. Traffic calming or more law enforcement needs to be looked into.

114435334325 Oct 12 2023 1: Route 44 in Canton is a major thoroughfare. I drive it often, and while at times congested, I don't believe major changes are needed. I noted phrases like road diet in the traffic study. I believe this to be a weasel term, that seeks to keep the true purpose of the proposal from the public. If Canton is seeking to narrow Route 44, and thereby create a traffic nightmare, the town should be honest with residents and not hide behind vague phrases. I am not in favor of narrowing the roadway, but do believe there is room for safety improvements such as bump outs and roundabouts.

114435106844 Oct 12 2023 0: FRT should connect to the Shops of FV and continue east. Improve signs for FRT at Lawton-44 intersection.

114432864924 Oct 10 2023 0: Stop additional approvals of apartment complexes along and near the area. Traffic congestion has gotten out of hand. We are fast becoming the Silas Deane Hwy or Berlin Tpke. Our residential community is being lost.

114430314230 Oct 06 2023 0: I see a lot of folks walking with no problem. But there are spots with no sidewalks.

114429977995 Oct 05 2023 0: Need to deal with the number of unsafe drivers on the road. Too many are speeding or distracted by phones.

114429876615 Oct 05 2023 0: Intersection of Center Street / River and Dowd is a mess. And when I drive the posted speed limit, there are often drivers tailgating.

114429490357 Oct 05 2023 1: In general riding/walking east/west along 44 is scary at best and extremely dangerous at worse. Dedicated bike/walkways would infinitely improve it and provide a greater and greener/less car dependent connection between Canton, Collinsville and the surrounding towns.

114429393040 Oct 05 2023 0:

114429073064 Oct 04 2023 0: make your bike riders and walkers more accountable to actually push the button for a crossing

114428873485 Oct 04 2023 0: Speed limits could be reduced

114428826101 Oct 04 2023 0: The road needs to have better sight lines and a dedicated center turn lane for local businesses

114427687495 Oct 03 2023 0: Create a town square traffic pattern between Dowd and 44 to allow more and safer access between the two streets.

114427646592 Oct 03 2023 1: Reduce and aesthetically improve signage. Add native trees and shrubs. Combine parking areas of businesses with fewer entrances.

114427396184 Oct 03 2023 0: Police presence is extremely rare on this route.

114427331917 Oct 03 2023 0: Would love to see a pedestrian bridge over Route 44 as part of a trail extension.

An increased presence of law enforcement along Rt. 44 would significantly reduce overall traffic speeds that are excessive. Years ago there was little reckless driving and speeding. Back then it was well known in the valley, that between saybrook fish house and Macdonalds that you could expect to see a parked,

114427063996 Oct 02 2023 1: off the road Canton PD vehicle ready to be of assistance when needed.

Rte 44 in canton is a race track. More appropriate cross walks are needed for sure to keep pedestrians safe and encourage people to walk. The intersection of rte 44 and 565 is also very dangerous. There should be a turning lane from 44 west bound to 565. The area where rte 44, 202, and 179 all join could be much nicer looking than having those ugly islands and pits. Please clean it up! Plant some nice trees!

114427003902 Oct 02 2023 0: Plant wild flowers for pollinators! Mowing those areas is a waste of tax dollars.

As a driver, many parking lots in the area can be improved. The large lot at Canton Valley Shopping Center is horrendous and has lots of pot holes, and unclear directions (stop signs, one way / two way roads not followed, unmarked parking spots). Smaller businesses on route 44 have small and congested parking lots and if not located at a traffic light are oftentimes locations of accidents and wreck less

114426953341 Oct 02 2023 0: driving.

114426934020 Oct 02 2023 0: Stop building and creating more traffic and noise

We understand staffing issues, however there has to be more police patrol activity, and actually ticket speeders. Stop with the warnings. Money hits home. Solar powered speed signs.

114426927984 Oct 02 2023 0: Connect the bike trail so people can do the Avon canton to simsbury loop

114426820362 Oct 02 2023 0: —

114426663190 Oct 02 2023 0: Possible center island with trees
I'd love to see more police cars during rush hour. Particularly at the death intersections (44 and Lawler, 44 and Dowd, and 44 and □

114426773800 Oct 02 2023 0: 179/202)

Please avoid making choke points where cars/cyclists will conflict with each other. descent shoulders will greatly improve the ability of bicycles to use the road. Also, in areas where cyclists will go straight, and possibly conflict with cars, (Such as "the shops at farmington valley" signs should be placed telling drivers they must yield to cyclists proceeding straight. I have ridden RT 192 in Kissimmee FL and found

114426570078 Oct 02 2023 1: this helps greatly (and that's saying a lot with Florida drivers....)

114426537848 Oct 02 2023 1: There is generally not enough room for bikes/walkers on the road where there are no sidewalks. Having a separate walkway/trail like Iron Horse in Simsbury would be the best.

114426441359 Oct 02 2023 1: Realign Canton Spring/Dowd Ave/RT 44 intersection. Install traffic calming media on Dowd Ave. and. RT 44.

114426389615 Oct 02 2023 0: A bike lane on Dow's and 44 would be great, but speed limits would have to be enforced. The reckless driving is shameful. I don't even feel safe in a car with what's tolerated by the other car drivers. More traffic policing is desperately needed to make it safe for everyone.

114426350107 Oct 02 2023 0: Baseline other states best practices. Turning lanes, common exit/entry points. Less driveway cuts. Limit left turns from driveways which cut across multiple lanes. Dedicated turn lane from rte 44 onto dowd. Flashing yellow arrows where yield is required. Increase police enforcement

114426344342 Oct 02 2023 0: The erratic and aggressive driving along RT. 44 is at an all time high. Not only is it congested, you have motorists driving 20-30+miles above the speed limit, passing on the right, running red lights, cutting off buses, and there doesn't seem to be an officer in sight. I'm uncertain that any aesthetic modification will make a difference without traffic laws being enforced.

114426241410 Oct 02 2023 0: More sidewalks and maintenance of them. There are stretches that are too narrow for 2 people to pass each other (outside giv and the vet's office). During the winter it's always blocked by impassable snow banks. No sidewalk on the side of Canton savings bank so anyone who lives from the village to east hill road on the north side of the road has no access to walking

114426147913 Oct 02 2023 0: More attention to speeders and red light runners it's getting horrendous, but also stop building things and build parks or something useful not apartments

114426114851 Oct 02 2023 1: Just build an underground tunnel for cyclists. Thanks .
The reckless drivers need to be stopped. It's getting completely out of hand. The police try so hard to do
114426104240 Oct 01 2023 1 their jobs safely, but it's impossible.
It would be amazing if the area was developed to allow for green corridors that had separate very wide
trails for pedestrians and bikes surrounded by beautiful landscaping away from the road which winds
through and connects all of the shopping and restaurant destinations (including bridges to get from one
114426094828 Oct 01 2023 1 side of the road to the other).
114425896557 Oct 01 2023 0 Na
114425844961 Oct 01 2023 1 Eliminate left turns from west bound 44 into retail establishments.